

5.2. MATERIAL CHANGE OF USE FOR RESORT COMPLEX AT 71-85 PORT DOUGLAS RD PORT DOUGLAS

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DEPARTMENT	Environment and Planning
APPLICATION NO	MCUI 2020_3711/1
PROPOSAL	Material Change of Use (Resort Complex)
APPLICANT	Chiodo Corporation C/- GHD PO Box 930 TOWNSVILLE QLD 4810
LOCATION	71-85 Port Douglas Road PORT DOUGLAS
PROPERTY	LOT: 1 SP: 150468
PLANNING SCHEME	2018 Douglas Shire Council Planning Scheme Version 1.0
ZONING	Tourist Accommodation Zone
LEVEL OF ASSESSMENT	Impact
PROPERLY MADE DATE	5 January 2021
STATUTORY ASSESSMENT DEADLINE	10 September 2021
REFERRAL AGENCIES	State Assessment & Referral Agency
LOCALITY	



Figure 1 – Locality Plan

RECOMMENDATION

That Council refuses the development application for a Material Change of Use (Resort Complex) over land described as Lot 1 on SP150468, located at 71-85 Port Douglas Road, Port Douglas, on the following planning grounds:

- (1) The proposal conflicts with Strategic Intent 3.2.2.2 Reinforcing Douglas Shire's Sense of Place and Identity as the Development represents an over-scaled and over development of the site that has little local context or character.**
- (2) The proposal conflicts with Theme 4- Strong Communities and Identity from the Strategic Framework, in particular the proposal does not maintain the distinctive character and unique sense of place that represents Port Douglas.**
- (3) The proposal conflicts with the Overall Outcomes of the Tourist Accommodation Zone Code;**
 - a. The proposal is not of an appropriate scale; and**
 - b. Does not achieve an attractive built form which incorporates the character of the site and surrounding area.**
- (4) The proposal Conflicts with Performance Criteria P01 (Height) of the Tourist Accommodation Zone;**
 - a. The height of the building and structures is not in keeping with the residential character of the area.**
- (5) The proposal conflicts with the Performance Criteria P01 (Setbacks) of the Tourist Accommodation Zone;**
 - a. The setbacks of the proposal do not maintain the character and amenity of the area;**
- (6) The proposal conflicts with the Performance Criteria P02 (Site Coverage) of the Tourist Accommodation Zone;**
 - a. The proportions and scale of the development are not in character with the area and local streetscape.**
- (7) The proposal conflicts with the Performance Criteria P03 (Building proportions and scale) of the Tourist Accommodation Zone;**
 - a. The proportions and scale of the development are not in character with the area and local streetscape.**
- (8) The proposal conflicts with the Performance Criteria P07 of the Tourist accommodation Zone;**
 - a. The proposal adversely affects the tropical, tourist and residential character and amenity of the area in terms of traffic.**

- (9) The proposal conflicts with the Performance Criteria P09 of the Tourist accommodation Zone;
- a. The tourist development does not include adequate ancillary services and facilities for the enjoyment of guests in terms of on-site car parking provision.
- (10) The proposal conflicts with the overall outcomes of the Landscape Values Overlay Code;
- a. The development is not consistent with the prevailing landscape character of its setting as it is visually dominant and visually intrusive.
- (11) The proposal conflicts with the Performance Criteria P03 of the Landscape Values Overlay Code;
- a. The proposal within a scenic route buffer and view corridor area does not minimise visual impacts on the setting and views in terms of the scale, height and setback of buildings.
- (12) The proposal conflicts with the overall outcomes of the Access, Parking and Service Code;
- a. The proposal provides insufficient car parking onsite.
 - b. The proposal conflicts with the preferred ultimate streetscape character and local character of the area.
- (13) The proposal conflicts with Performance Criteria PO1 of the Vehicle Parking and Access Code;
- a. The proposal provides insufficient car parking onsite.
- (14) The proposal conflicts with PO4 of the Transport Network Overlay Code;
- a. The application does not demonstrate that the development does not compromise the intended role and function or safety and efficiency of major transport corridors.
- (15) The proposal conflicts with the Performance Criteria P03 of the Multiple Dwelling, Short Term Accommodation and Retirement Facility Code;
- a. The proposal is not in keeping with the intended form and character of the local area and immediate streetscape and does not contribute to the modulation of built form.
- (16) The proposal conflicts with the Performance Criteria P014 of the Multiple Dwelling, Short Term Accommodation and Retirement Facility Code;

- a. **The retaining wall does not facilitate casual surveillance to the street and public space enabling use of private open space and does not provide a positive interface to the streetscape.**
 - (17) The proposal conflicts with the overall outcomes of the Multiple Dwelling, Short Term Accommodation and Retirement Facility Code;**
 - a. **The development is not compatible with and complementary to surrounding development, with regard to scale, bulk and streetscape patterns.**
 - (18) The development cannot be appropriately conditioned to overcome the non-compliance with the Strategic Framework or assessment benchmarks of the applicable codes.**
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EXECUTIVE SUMMARY

Council is in receipt of a development application for a material change of use of premises for a Resort complex at 79-85 Port Douglas Road, Port Douglas formally described as Lot 1 on SP150468.

The application seeks approval for Australia's first Fairmont resort with 253 rooms, food and drink outlets, function facilities, basement car parking and recreation facilities configured within a single building at up to six storeys in height.

The applicant's economic benefit report claims that the \$278m construction will create 694 jobs per annum over two years and will total \$203m in gross value added to the State economy per annum over the same period with 196 ongoing jobs supported on site.

The site is within the Tourist accommodation zone under the 2018 Douglas Shire Planning Scheme where development of a Resort complex is nominated as an Impact Assessable land use.

Public Notification of the proposal was carried out in accordance with the requirements of the Planning Act 2016 and no properly made submissions were received. There were three (3) not properly made submissions received against the application.

The proposed development is non-compliant with a significant number of planning scheme components due to its height, length, site cover, setbacks, bulk, scale and provision of facilities rendering it inconsistent with the character of the area.

The proposal is in conflict the planning intent for the site and locality and is recommended for refusal.

TOWN PLANNING CONSIDERATIONS

Proposal

Proposed is the development of a Resort complex at 71-85 Port Douglas Road, Port Douglas. The development includes 253 rooms for short term accommodation, food and drink outlet space with combined gross floor area of 1,866 square metres and function facility space with combined gross floor area of 1,866 square metres.

The proposed layout is designed within a single structure central to the site surrounded by outdoor recreation facilities and basement car parking below. Access is via two separate crossovers off Port Douglas Road. One access is for shuttles and concierge parking and the other is for service deliveries. Bicycle parking and end of trip facilities are proposed within the basement.

The proposal includes a significant landscaping plan which includes landscaping each of the six levels and recreation area on site as well as the road reserve at the front of the site.

State Planning Requirements

Schedule 10 of the Planning Regulation 2017 prescribes that the development application triggers state agency referral to the State Assessment & Referral Agency (SARA) as a concurrence agency due to the site adjoining a State controlled road (Port Douglas Road). The application was referred to the Department of Transport and Main Roads through SARA.

Council has assessed the application against the Planning Scheme in relation to natural hazards, risk and resilience to the extent it is relevant to the proposed development. In this case, assessment of the works to take place on-site against the natural hazards of flooding and acid sulphate soils requires consideration. The site characteristics show a gradual fall to the rear with a central pit constructed as part of recent demolition activities. An approval could condition the requirement of an acid sulfate soils management plan if the recommendation was for approval. In terms of flooding, this is further discussed in the body of the report.

DOUGLAS SHIRE PLANNING SCHEME ASSESSMENT

The following benchmarks are applicable to the proposed development:

Table 1.

Douglas Shire Planning Locality	Comment
Planning Zone	
Tourist Accommodation Zone	See comment below
Overlay Codes	
Acid sulphate soils overlay	See comment below
Flood and storm tide hazard overlay	See comment below
Landscape values overlay	See comment below
Transport network overlay	See comment below
Other Development Codes	
Access, parking and servicing code	See comment below
Environmental performance code	Complies
Filling and excavation code	Generally complies
Infrastructure works code	Complies
Landscaping code	Generally complies
Multiple Dwelling, Short Term Accommodation and Retirement Facility Code	See comment below

Compliance Issues

Tourist Accommodation Zone Code

Building Height:

The zone code gives the maximum building height for the zone at 13.5 metres and a requirement for no more than three storeys within Acceptable Outcome AO1. The proposed building is six storeys and ranges in height above natural ground level and the revised ground level. The applicant proposes that building height should be assessed from natural ground level (NGL). The definition of building height includes a level of ambiguity due to the options within the definition of 'ground level' with the planning scheme. Council determines the ground level definition based on the site's attributes allowable by the definition.

If the NGL was taken to be the ground level then the building ranges in height when viewed from Port Douglas Road to be 17 metres tall measured to the roofs of the shade structures and amenities building at the rooftop recreation area. The lift well structures are 19 metres above the NGL. The wedding pergola is 22 metres above natural ground level. From the Golf Course at the site's rear the pergolas on the rooftop recreation area are 20 metres above the NGL.

The applicant proposes that the building height is 14.27m when viewed from the site's frontage. This measurement is taken from the NGL to the bottom of the concrete roof slab. This excludes the roofed structures on the rooftop level.

The corresponding performance criteria PO1 requires that the height of all buildings and structures must be in keeping with the residential character of the area. It is considered that the residential character of the area is not maintained by the proposal as there are no other premises at six storeys and 17 metres in height in the Port Douglas locality or in the Tourist accommodation zone. The proposal conflicts with the overall outcome (b) from the code which requires that development is established at an appropriate scale and achieves an attractive built form which incorporates the character and natural attributes of the site and the surrounding area as integral features of the theme and design of the development.

The applicant proposes that the development complies with PO1 and overall outcome (b) from the zone codes purpose as the lowest habitable level of the proposed resort is an excavated level, the development will be extensively landscaped with planter boxes on each level and plantings across the site and in the road reserve. These proposed solutions do not support a conclusion that the development will be in keeping with the residential character of the area and is of an appropriate scale and achieves an attractive built form. Covering a building in plantings does not reduce its scale significantly, particularly when the site is relatively flat. There are no other buildings in the Shire characterised by vegetation cover hiding their appearance. This is not considered to achieve an attractive built form. Further, the applicant compares the development to Coconut Grove on Macrossan Street by claiming that it is an estimated 15.6 metres above the road to the top of its structures and that the Fairmont proposal is 15.21 to 16.31 metres above the road to the top of its structures. This comparison is like comparing apples to oranges. Coconut Grove is sited at the base of Flagstaff Hill and has the benefit of a grade to the rear and the hill as its backdrop meaning that its presence is not visually obtrusive and from ground level, being natural or revised, its height is not particularly non-compliant. Ultimately in terms of height, the 17 metre high building is not compliant with the performance outcome and purpose statement that it needs to comply with.

Building Setbacks:

AO2 of the Code give setback requirements for buildings. The code requires a minimum of 6 metres from the main street frontage. The development is generally set back more than 6 metres however, the access ramp roundabout structure appears to be a building and gives a setback of 2 metres from the site's frontage. The ramp structure relies on a retaining wall and balustrade façade over three metres in height within the setback area to the street frontage. The applicant believes this to be consistent with existing development to the south.

The proposal does not maintain the character and amenity of the area and does not achieve separation from the road frontage with this component of the design and therefore does not comply with PO2 of the code. Further, this does not achieve an attractive built form and is not in keeping with the residential character of the area as it is not comparable with anything nearby.

The proposal is generally set back 4.5 metres from the rear boundary, which is compliant.

The code also requires that buildings are set back an average of half of the height of the building at the side setback. The northern side boundary setback is six metres and the southern side boundary setback is three metres. These setbacks are to the slab for the upper ground level above the lower ground level representing the bulk of the building. The building is around 16 metres tall from NGL to the top of the balustrade garden bed on the roof at either end adjacent to each side boundary. This excludes the structures on the building's roof. The setback requirement is in the order of eight metres for the building as a whole, not for the podium level as the applicant has proposed where the building height increases, the setback from the site boundary increases. This represents a non-compliance with the acceptable solution and the performance outcome as it does not maintain the character and amenity of the area given the neighbouring allotments have compliant setbacks.

Site Coverage:

Acceptable Outcome AO2 requires that site cover is limited to 50%. Council has received a number of site coverage plans demonstrating the applicant's interpretation of the site cover definition, however, officers are not satisfied that the plans accurately communicate the overall site cover in accordance with the administrative definition. Further, the shape of the building and associated structures makes it near impossible to measure and calculate site cover without having an output model generated by the design program. Of the three (3) plans Council has received, the site cover is proposed to be at 50%, however, critical building components have been left out of each plan so the site cover is certainly greater than 50%, although the actual percentage is not known.

The applicant has a responsibility to accurately demonstrate compliance with the code and in this case they have not done so. Much of the area in question is the porte cochere ramp and questions have been raised as to whether this is in fact a building given the pavement acts as the roof of the basement car park and the structure comes up around two metres from NGL. The corresponding Performance Outcome PO2 requires that the site coverage of all buildings does not result in a built form that is bulky or visually obtrusive. Given the built form is of a single building with setback non-compliance and no breaks or opportunity for deep planting of setback areas it is determined that the performance criteria is not complied with.

Building Proportions and Scale:

Acceptable Outcome AO3.1 requires that the overall length of a building does not exceed 30 metres and the overall length of any continuous wall does not exceed 15 metres. Continuous walls do not exceed 15 metres as the building is articulated with continuous curved changes in direction. However, the building is 173 metres in length. The corresponding performance outcome for assessment due to failure of compliance with AO3.1 is PO3. This performance outcome requires that the proportions and scale of any development are in character with the area and the local streetscape. There are no buildings toward this length in Port Douglas and there are no buildings toward 173 metres or toward this scale within the local streetscape. The nearby Sheraton Mirage resort includes the central pavilion building with a dimension of 90 metres however, this has a significant set back from the road with particularly limited visibility. The Crystalbrook Marina is the longest building in Port Douglas at 130 metres but is sited with orientation away from the road, includes significant setbacks and is two (2) storeys in height.

Landscaping:

Acceptable outcome AO4.2 requires a two metre wide landscape buffer within the front setback area with minimum 75% dense planting. The architectural plans submitted as part of the Information Request Response show the porte cochere access ramp structure pulled back off the site boundary by around 1.6 allowing for an area of landscaping for deep planting. This non-compliance is relatively minor and is not a reason for refusal.

Additional requirement for assessable development:

Performance Outcome PO7 requires that development does not adversely affect the tropical, tourist and residential character and amenity of the area in terms of traffic, noise, dust, odour, lighting or other physical environmental impacts. The proposal appears to comply with all of these requirements with the exception of traffic impacts. The supply of on-site car parking is less than 50% of what the scheme requires and the configuration of a number of the spaces raises functionality concerns. The proposal appears to represent a potential traffic issue as overflow parking demand has the potential to spill out to the road verge at the frontages of neighboring developments where vehicles can fit on the road verge.

The other point of concern with the proposal, with respect to noise and dust, is the volume of excavation and material required to be taken off site. The application appears to demonstrate that over 5000 truck loads of earth need to be taken off site demonstrating the scale of the development. The applicant claims the construction period is for two years. This component of the project represents significant potential physical and environmental impacts and it is particularly concerning given there is now in the order of 8000 tonnes of crushed concrete spread across the site requiring a disposal solution.

AO9.3 requires that where a commercial service or facility offers services to persons over and above in-house guests, the commercial component provides on-site car parking for 50% of the floor area available for the use, in accordance with the relevant requirements of the parking and access code. The applicant's response to this acceptable outcome, as part of the response to the information request, is that this acceptable outcome is not applicable as the development does not offer services to persons over and above in-house guests. However, page 13 of the development application states that the resort includes public areas that are available for use by the general public which include function and event rooms, poolside areas, restaurants/ bars, cafes and kids play areas. The Gross Floor area (GFA) of private areas within the resort is 809 square metres of the 1,844 square metres of food and drink outlet GFA and 1,866 square metres of Function Facility GFA leaving a total of 3710 square metres of public gross floor area that is not adequately serviced by the car parking

supply requirement in AO9.3. The corresponding performance outcome PO9 requires that tourist development includes recreational and ancillary services and facilities for the enjoyment of guests. If on-site parking facilities are not provided adequately for the 3,710 square metres of gross floor area open for use by the public, then it is evident that the proposal does not comply with the performance outcome. Further, the proposed car parking arrangement includes a significant percentage of parking spaces for use by guests within tandem arrangements, representing a deviation from the Australian Standard. This is further discussed in the Vehicle parking and Access Code non-compliance assessment below.

Acid Sulfate Soils Overlay Code

AO1.2 of the code requires an acid sulfate soils investigation to be undertaken as part of the application. The applicant has not provided an investigation however there is a previous geotechnical report for a previous development over the site which identified potential acid sulfate spoils at one side of the site. The lack of an investigation is considered not to be a reason for refusal as it is something that could be conditioned if approval was appropriate and dealt with at Operational Works stage. Excavation requiring over 5000 truck loads of earth to leave the site is not easily managed where potential acid sulphate soils exist, however, it can be done.

Flood and Storm Tide Inundation Hazard Overlay Code

Performance Outcome PO1 requires that development is located and designed to ensure the safety of all persons, minimise damage to the development and contents of buildings; provide suitable amenity; minimise disruption to residents, recovery time, and rebuilding or restoration costs after inundation events.

The applicant has provided a certified Catchment flooding and Storm tide Study to give a recommended finished floor level for flood immunity for an 80 year design life of 3.0m AHD, inclusive of a 300mm freeboard. Councils 1% AEP level, which is the benchmark for a 1 in 100 year flood event includes 500mm of freeboard and comes in at 3.309m AHD for finished floor levels. An inconsistency for finished floor level immunity is evident here with the applicant's proposed level coming in 309mm lower than Councils nominated level presenting an issue. However, this is not a reason for refusal as it could be conditioned as part of an approval. The basement includes facilities such as plant rooms and kitchens that require flood immunity. However, stormwater appears to be prevented from entry via engineering solutions commonplace for basement levels.

Transport Network Overlay Code

AO4.1 of the code requires that development is compatible with the role and function or major transport corridors. The development is significantly undersupplied with on-site car parking space provision and is at risk of compromising the role and function of the major transport corridor, that is Port Douglas Road. PO4 requires that development does not compromise the intended role and function or safety and efficiency of major transport corridors. The applicant has not demonstrated compliance with PO4 as justification for the 44% car parking shortfall is not adequate. Given the proposal is considered to be in conflict with PO4, assessment against the overall outcomes from the purpose is required. Overall outcome (d) is considered to be in conflict with the proposal as it requires that development does not compromise the safety and efficiency of major transport infrastructure and facilities. The car parking shortfall is considered to affect the efficiency and safety of Port Douglas Road, as it is assumed that cars will spill out onto adjacent road reserve areas and informally park.

Multiple Dwelling, Short Term Accommodation and Retirement Facility Code

AO3.2 of the code requires that the development has a gross floor area of not more than 1.2 times the site area. The proposal has a GFA of 32,811 square metres and a total allowable GFA of 24,804 square metres representing a 32% increase over the acceptable outcome. The corresponding performance outcome requires that development ensures that the proportion of buildings to open space is in keeping with the intended form and character of the local area and immediate streetscape; contributes to the modulation of built form; supports residential amenity including access to breezes, natural light and sun light, supports outdoor tropical living and provides areas for deep tropical planting and/ or the retention of mature vegetation.

The applicant proposes that site cover is limited to 50% in addressing the performance outcomes in this instance. However, it is considered that PO3 is not complied with as the proposal is not in keeping with the intended form and character of the local area and immediate streetscape and does not contribute to the modulation of built form as the development is not comparable in form and character with any neighboring development and is not modular, given it is one single building.

AO14.3 requires that development for a retaining wall is stepped to minimise impact on the streetscape and pedestrian environment. The proposal includes a large retaining wall, approximately two (2) metres in height with an additional 1.6 metre tall balustrade structure. The design of this wall is not clearly demonstrated in the set of plans provided; however supplementary information provided leads to this conclusion. There is a two metre wide landscaping strip proposed between the road frontage boundary and the retaining wall supporting the porte cochere ramp and roundabout. The corresponding performance outcome PO14 requires that retaining walls and front fences must facilitate casual surveillance to the street and public space, enable use of private open space, assist in highlighting entrances to the property and provide a positive interface to the streetscape. Whilst the proposed orientation of rooms and their elevation would contribute to casual surveillance of the street there will be a significant area behind the wall that does not facilitate casual surveillance due to its height. Further the wall will not provide a positive interface to the street given its excessive height and minimal setback to the frontage boundary with ability for only 2 metres of deep planting.

The non-compliance with the performance outcomes detailed above require that the proposal is assessed against the overall outcomes in the purpose of the code. Of relevance is overall outcome (a) which requires that development is compatible with and complementary to surrounding development, with regard to scale, bulk and streetscape patterns. The proposal is not complimentary to surrounding development as its scale and bulk are significantly greater and the streetscape pattern is not maintained by the development, given the dimensions and siting of the retaining structure at the site's frontage and the gross floor area proposed, which are both in excess of anything comparable.

Landscape Values Overlay code

The site is within a Scenic route buffer/ view corridor area. AO3.1 requires that where within a scenic route buffer/ view corridor area, the height of buildings and structures is not more than identified within the acceptable outcomes of the applicable zone code. As detailed above, the height of the proposed development is in excess of the acceptable outcome, performance outcome and overall outcomes for the zone. PO3 of the code requires that development within the overlay area minimises visual impacts on the setting and views in terms of the scale, height and setback of buildings among other requirements.

The proposal is in conflict with this performance outcome despite the building being heavily landscaped with planter boxes at the edge of every level. The applicant claims that the development will not be visually obtrusive as the detailed landscaping plan has been designed to predominantly screen the development from view from the road. The landscaping plan ultimately relies on densely landscaping the road reserve to achieve this. Relying on land that is not part of the site demonstrates conflict with the code. The front boundary setback does not allow significant deep planting to achieve a high level of screening. This is exacerbated by the retaining structure and porte cochere roundabout. Ultimately landscaping the building with shrubs does not reduce the height and scale of a building of this size. The proposal complies with most of the overall outcomes from the purpose of the code, with the exception of (h) which requires that development is consistent with the prevailing landscape character of its setting and is neither visually dominant nor visually intrusive.

The scale of the development with its height, building length, site cover and setbacks is at odds with this overall outcome as the relatively tokenistic justification for compliance by landscaping and podium level stepping does not make it consistent with the character of the site's setting and does not have a material impact on building bulk or scale.

Access, Parking and Servicing Code

Acceptable outcome AO1.1 requires that the minimum number of on-site vehicle parking spaces is not less than the number prescribed for the use. The acceptable outcome requires a total of 407 parking spaces to be supplied. The proposal is for 227 parking spaces plus eight motorcycles bays, which the amended Traffic Impact Assessment (TIA) prepared by GTA Consultants claims to substitute 2% of parking, bringing the total supply to 229 spaces and 56% of what is required.

The Planning Scheme requires that car parking space demand for the Resort complex land use is taken as a cumulative rate per separate land use within the complex (food and drink outlet, function facility etc). With the development proposing components for public use and non-public use, the strict application of this may result in the over-supply of car parking spaces, as detailed in the GTA report. The use of shuttles and end to end transfers is commonplace for resorts so some dispensation can reasonably be applied. However, the extent of this needs to be justified with primary data provided in an evidence based capacity to demonstrate compliance with the corresponding performance outcome PO1, which requires that sufficient on-site parking is provided to cater for the amount and type of vehicle traffic expected to be generated by the use or uses of the site. In determining this, regard is given to:-

- a) the desired character of the area;
- b) the nature of the particular use and its specific characteristics and scale;
- c) the number of employees and the likely number of visitors to the site;
- d) the level of local accessibility;
- e) the nature and frequency of any public transport serving the area;
- f)

g)

h)

The TIA includes a car parking demand assessment to determine the actual demand for spaces. The assessment suggests that demand will exist for 75-85 staff spaces, 109-130 guest spaces and 33-46 visitor spaces based on ABS journey to work statistics, domestic and international tourist statistics and data from Accor group primarily. The range of demand is concluded to be between 217 and 261 spaces. The upper range of the parking provision assumes the high end guests (Gold Club) will drive at a rate higher than usual. However, the lack of primary data and a shortfall of 32 spaces below the upper range supply raises concern. The TIA includes a car parking demand management strategy to alleviate the demand non-compliance. Accor have provided strategies which they employ for demand management globally, which include valet services, determination of likely demand per room at booking stage and additional charges per room for parking space among other strategies.

Further, the TIA includes management strategies for staff parking demand whereby staff can be given alternative travel arrangements via shuttle from designated pick-up areas and, if necessary, guests can use staff parking. These particular measures raise a lot of concern for compliance with PO1 as the desired character of the area is for no on-street parking. The size of the development is particularly large with an undersupply of 30 spaces for the applicant's own TIA calculation and the strategies for mitigation of demand at the cost of staff parking are at odds. In addition to this the parking layout and management strategy with three space deep tandem valet parking is of particular concern.

The basement parking layout includes considerable tandem parking space rates at 40% of overall staff supply and 58% of the guest supply. The staff spaces are in tandem and the guest spaces are in tandem and triple tandem requiring valet management 24/7. There are 12 sets of triple tandem spaces and 31 sets of double tandem spaces supplied.

The management and functionality of this arrangement raises significant concern given the number of parking spaces locked up by tandem arrangement and the limited area for maneuvering to make this physically work within the tight basement aisle. It is considered that these tandem spaces are in conflict with a, b, c and d of PO1. This parking arrangement is in conflict with the AS2890 as required by the Planning Scheme and Council officers are not supportive of introducing such deviation from the standard to the Shire.

PO2 of the code requires that parking areas are compliant with the relevant Australian standards. The proposal includes a component of non-compliant isle width of 3.9m where the standard requires 5.8m. A one-way solution has been employed to vary the design from the standards. In addition, the standards do not include provision for tandem parking as proposed.

The overall outcomes of the code require that sufficient vehicle parking is provided on-site for staff, patrons, guests etc. The proposal, with the significant shortfall and management heavy solutions to parking provision, is not consistent with the development outcomes being sort by the Parking & Access code. Further, overall outcome (c) requires that on-site spaces are accessible and convenient, particularly for short term uses, the triple tandem spaces are considered to be in conflict with this.

The shortfall in provision of spaces and intense management process required to operate the tandem and triple tandem spaces is likely to impact the efficient function of the street network due to informal parking demand representing non-compliance with overall outcome (e).

The Department of Transport and Main roads has issued a concurrence agency response for the proposal given the site adjoins the State controlled Port Douglas Road. In this case, the Department has not taken issue with the double crossover proposal which has been a long standing policy position to allow only one access per lot. The Department have issued approval for two crossovers. The two crossover component of the proposal is in conflict with the preferred ultimate streetscape character and local character of the area and represents non-compliance with overall outcome (f) of the code.

Strategic Framework

The strategic framework sets the policy direction for the Planning Scheme and forms the basis for ensuring appropriate development occurs within the planning scheme area for the life of the Planning Scheme. The Strategic Framework is the highest order component of the Planning Scheme and prevails over all other components to the extent of inconsistencies. The Strategic Framework sections are less prescriptive than the lower order scheme requirements such as codes.

Where there are particular non-compliances with elements of the strategic framework, Officers cannot support applications.

The Strategic Framework is only applicable for assessment of Impact Assessable land uses such as the Resort complex use within the Tourist accommodation zone.

Overall, the proposal is compliant with the majority of the strategic framework in terms of its economic benefit to the Shire and its limited environmental impact, however significant non-compliance exists for the amenity based tests referenced below and these are not outweighed by the project's benefits.

Strategic Intent

3.2.2.2 Reinforcing Douglas Shires Sense of Place and Identity includes the following:

Port Douglas will retain its lively tropical tourism 'buzz' as a premier tourist destination. Development will be carefully planned to achieve sensitive incremental change, rather than instant, over-scaled development projects, that have little local context or character.

The proposed development introduces six (6) storeys and a building height not yet experienced in Port Douglas. Further, the scale and bulk of the development are not comparable to any other development given the proposal is for a single building in the order of 173 metres in length.

Acceptable outcome and performance outcome breaches inclusive of site cover, bulk, scale, height, setback requirements and insufficient onsite car parking spaces to accommodate the proposed development are clear indicators that the development is an over development of the site. The proposal is the first non-modular single building proposal of its scale within the Tourist Accommodation zone with nothing comparative, providing little local context and character. The proposal is in conflict with the sense of place and identity of Port Douglas for this reason.

3.7 Theme 4- Strong Communities and Identity

3.7.1 (2) The distinctive character and unique sense of place of the Shire's towns, villages and other settlement areas are maintained, promoting community pride and well-being and community safety and prosperity.

The proposal is considered to be in conflict with Strategic Outcome 2 of Theme 4 as the bulk and scale of the development within a single building arrangement is not consistent with the distinctive character and unique sense of place of Port Douglas. There are no developments of a single building nature spanning 173 in building length and averaging 17 metres in height.

Public Notification / Submissions

Public notification was carried out in accordance with section 53 of the *Planning Act 2016* for the proposed development. A 15 business day public notification period was carried out with 3 submissions received.

The submissions received were all not properly made due to non-compliance with the statutory requirements for making a submission, however, the grounds for submission are summarised below.

Building height was the primary theme of concern across the submissions with the acceptable outcome of 13.5 metres and three (3) storeys being the main issue. The acceptable outcome itself does not represent the code requirement in its entirety, however it is considered that the proposal is non-compliant with the higher order code requirements such as the relevant performance outcomes and overall outcomes discussed in the zone code assessment component of this report.

Concerns were also raised regarding the demand on Council's water and sewer network that the proposal will create. A development of this size is not unserviceable and an approval would include infrastructure charges levied to facilitate ongoing serviceability. Further, there was a particular concern raised with the amount of water needed to maintain the large scale landscaping component of the development. The development includes large stormwater retention tanks which store roof water for re-use in this application.

Greenhouse gas emissions associated with the construction processes for the development were raised as a concern, however, these are not planning considerations as planning schemes do not regulate construction materials to that end. There are components of the design which contribute to sustainable outcomes such as increased greenspace on the building to limit the need for mechanical cooling and the retention of stormwater falling on the site for re-use in gardens.

A concern with the oversupply of tourism development was also raised. The site is within the Tourist accommodation zone and the planning intent for the site and Port Douglas in general is for tourism development reflected within the strategic framework of the planning scheme and in the zone code.

Referral Agency Requirements

The State Assessment and Referral Agency has issued a concurrence agency response. This response includes conditions regulating the development with regard to the site adjoining a State Transport Corridor. A copy of the response is included in Attachment 3.

Conclusion

The proposal is significantly non-compliant with a host of planning scheme requirements which cannot be overcome by conditions. The list of non-compliances in bulk, scale, facilities provision and character render the proposal an over-development of the site which is not able to be supported by Planning Officers.

ADOPTED INFRASTRUCTURE CHARGES

The development would trigger Infrastructure Charges if it was to be approved.

COUNCIL'S ROLE

Council can play a number of different roles in certain circumstances and it is important to be clear about which role is appropriate for a specific purpose or circumstance. The implementation of actions will be a collective effort and Council's involvement will vary from information only through to full responsibility for delivery.

The following area outlines where Council has a clear responsibility to act:

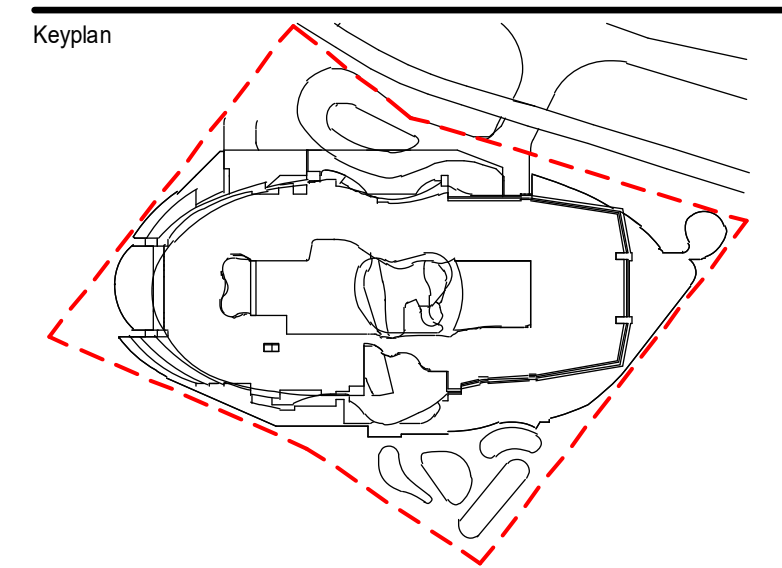
Regulator: Meeting the responsibilities associated with regulating activities through legislation or local law.

Under the *Planning Act 2016* and the *Planning Regulation 2017*, Council is the assessment manager for the application.

ATTACHMENTS

1. Attachment 1- Proposal Plans [**5.2.1** - 29 pages]
2. Attachment 2- Proposed Landscaping Plans [**5.2.2** - 15 pages]
3. Attachment 3- Concurrence Agency Response [**5.2.3** - 22 pages]

DEVELOPMENT APPLICATION SUBMISSION



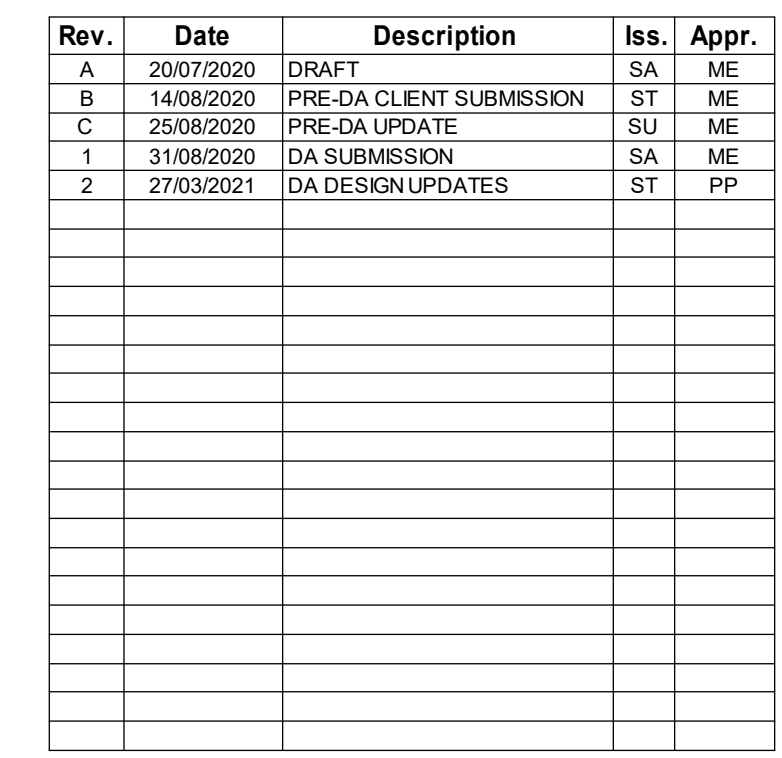
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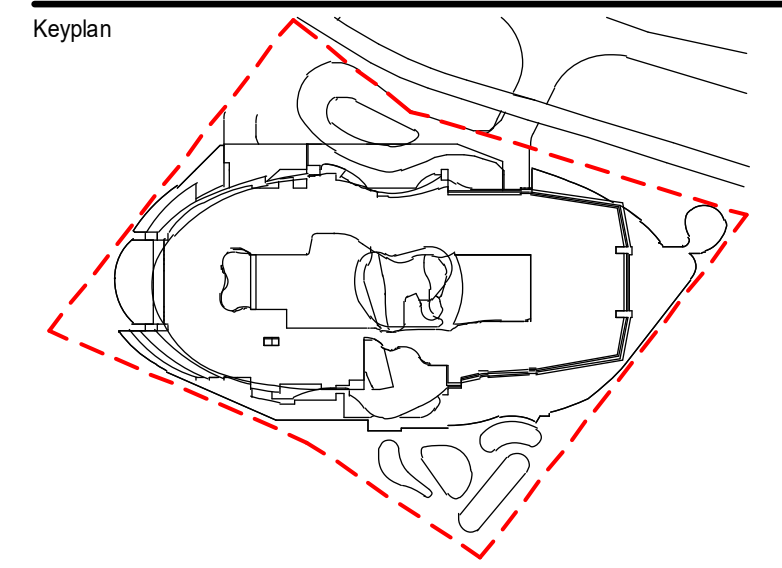
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DRAWING LIST		
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DA-0810	SHADOW DIAGRAMS - SUMMER	2
DA-0811	SHADOW DIAGRAMS - WINTER	2
DA-0900	EXTERNAL FINISHES	2
DA-0910	HOTEL PROGRAM	2
DA-0920	GFA CALCULATIONS	2

PORT DOUGLAS, QUEENSLAND



Keyplan



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Project

FAIRMONT RESORT

71-85 Port Douglas Rd, Port Douglas QLD 4877, Australia

Project Number	219194
Status	DA SUBMISSION
Date Plotted	29/03/2021 3:01:17 PM
Date Issued	27/03/2021
Scale	Not to scale

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LOCATION

Drawing Number Revision
DA-0010 **2**

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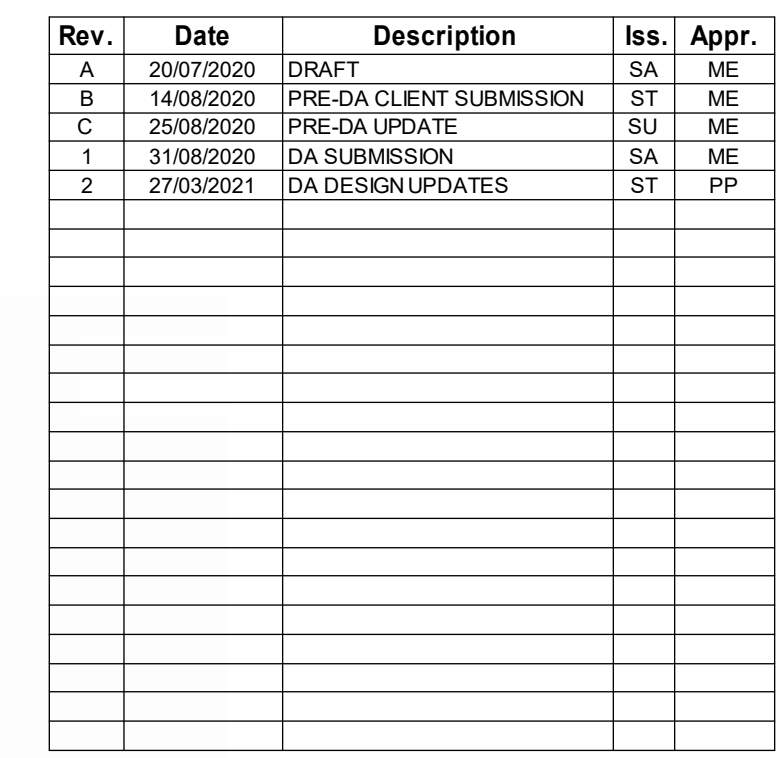
PORT DOUGLAS, QUEENSLAND

Fairmont Resort in Port Douglas sits adjacent to the famous Four Mile Beach with views to Mowbray National Park. It is uniquely located between two World Heritage listed sites – the immense Great Barrier Reef and the luscious Daintree National Park.

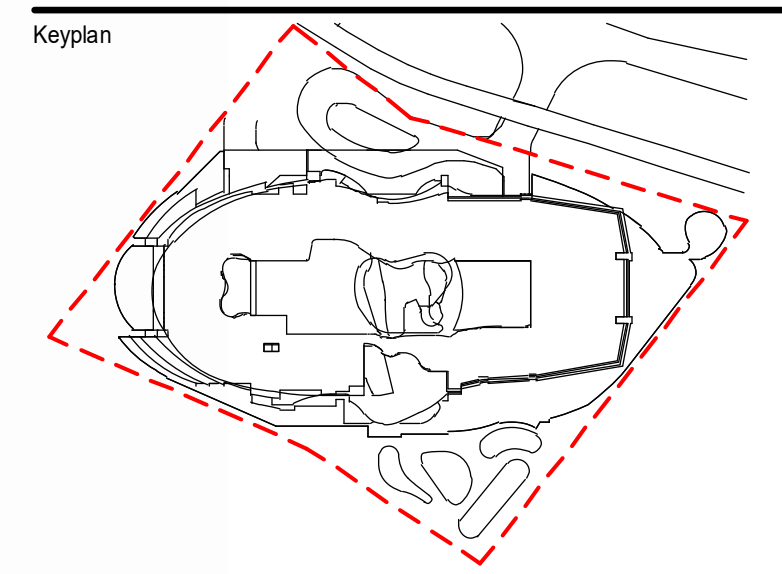
The main access road to Port Douglas is flanked by forests and beaches. The site is on the western side of Port Douglas Road. Port Douglas Road is accessed from the Captain Cook Highway to the south.

With the large Lewis Mountain rising behind the site, the topography of the area naturally slopes towards the ocean. The site however, varies in levels and slopes down from the main road.

The site is located 2km south of the village centre, nestled between the beach, Packers Creek and the Mirage Country Club. This provides a sense of tranquillity and remoteness whilst giving access to the best Port Douglas has to offer – its beach culture, boutique shops and dining. The location and its surrounding context provide uninterrupted views of Port Douglas and the breath-taking sunsets across the estuary.



Keyplan



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Project

FAIRMONT RESORT

71-85 Port Douglas Rd, Port Douglas QLD 4877, Australia

Project Number	219194
Status	DA SUBMISSION
Date Plotted	29/03/2021 3:11:20 PM
Date Issued	27/03/2021
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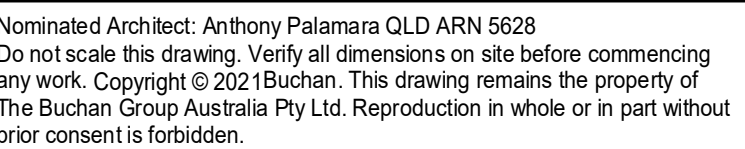
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Revision
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Keyplan



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219194

Status	DA SUBMISSION
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Date issued 27/03/2021

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Drawing Title
SITE PLAN - EXISTING

Drawing Number
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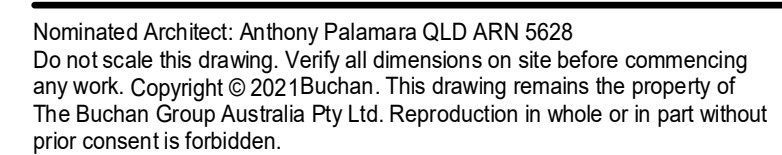
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Keyplan



File

- FOR ALL LANDSCAPE DEMOLITION,
RETENTION AND RELOCATION
REFER LANDSCAPE DWGS

NOTE: FOR DEMOLITION OF
EXISTING SERVICES WORKS
REFER SERVICES CONSULTANTS'
DRAWINGS.

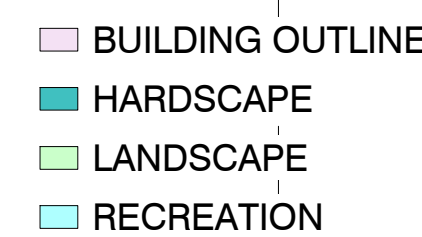
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FAIRMONT RESORT

71-85 Port Douglas Rd, Port Douglas QLD 4877, Australia

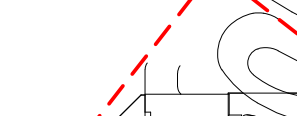
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SITE COVERAGE		
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BUILDING OUTLINE	10435 m ²	50%
HARDSCAPE	2443 m ²	12%
LANDSCAPE	6113 m ²	30%
RECREATION	1680 m ²	8%
	20672 m ²	

Keyplan

A keyplan diagram showing the layout of the building and its surrounding area. The building is a large, irregular shape with a central rectangular area. It is surrounded by a parking lot and a road. The diagram is labeled 'Keyplan' in the top left corner.

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Date Issued 12/03/2021

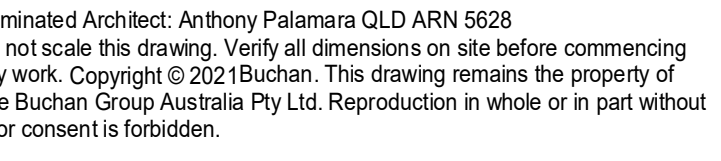
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Drawing Title

SITE COVERAGE

ypian



3

FAIRMONT RESORT
1-85 Port Douglas Rd, Port
Douglas QLD 4877, Australia

Project Number

19194

DA SUBMISSION

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Issued 27/03/2021

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Drawing Title

GRID SETOUT PLAN

Drawing Number

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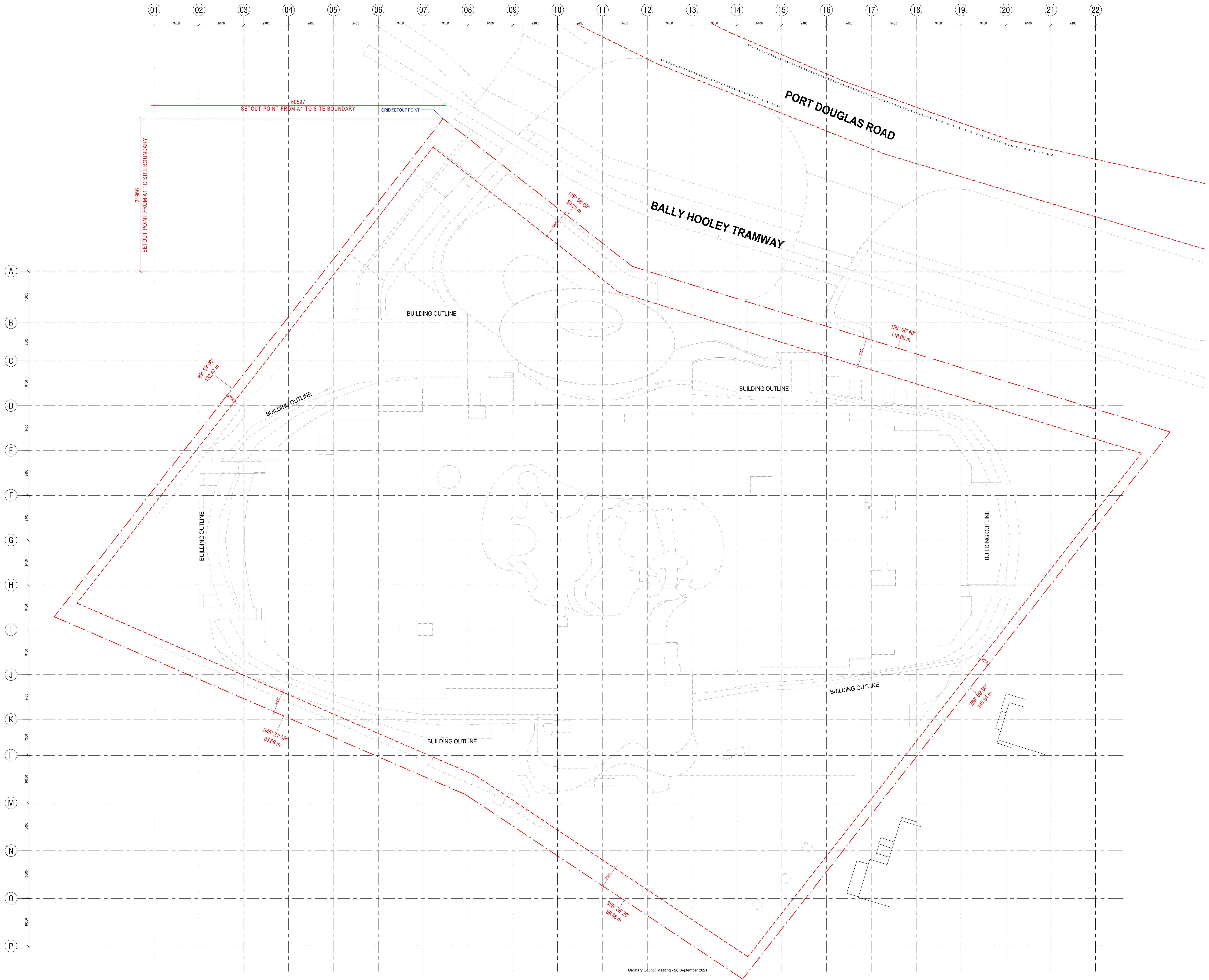
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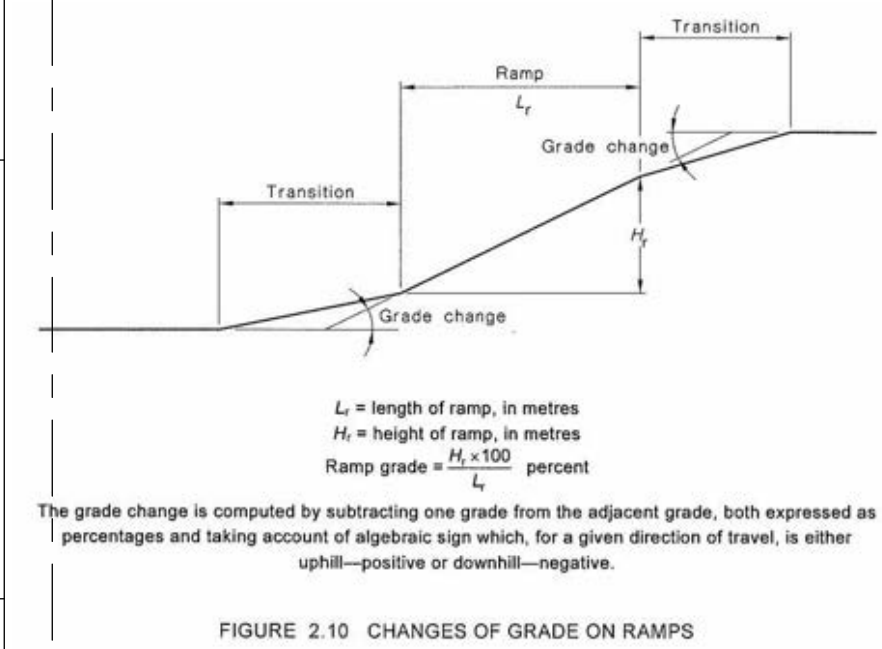
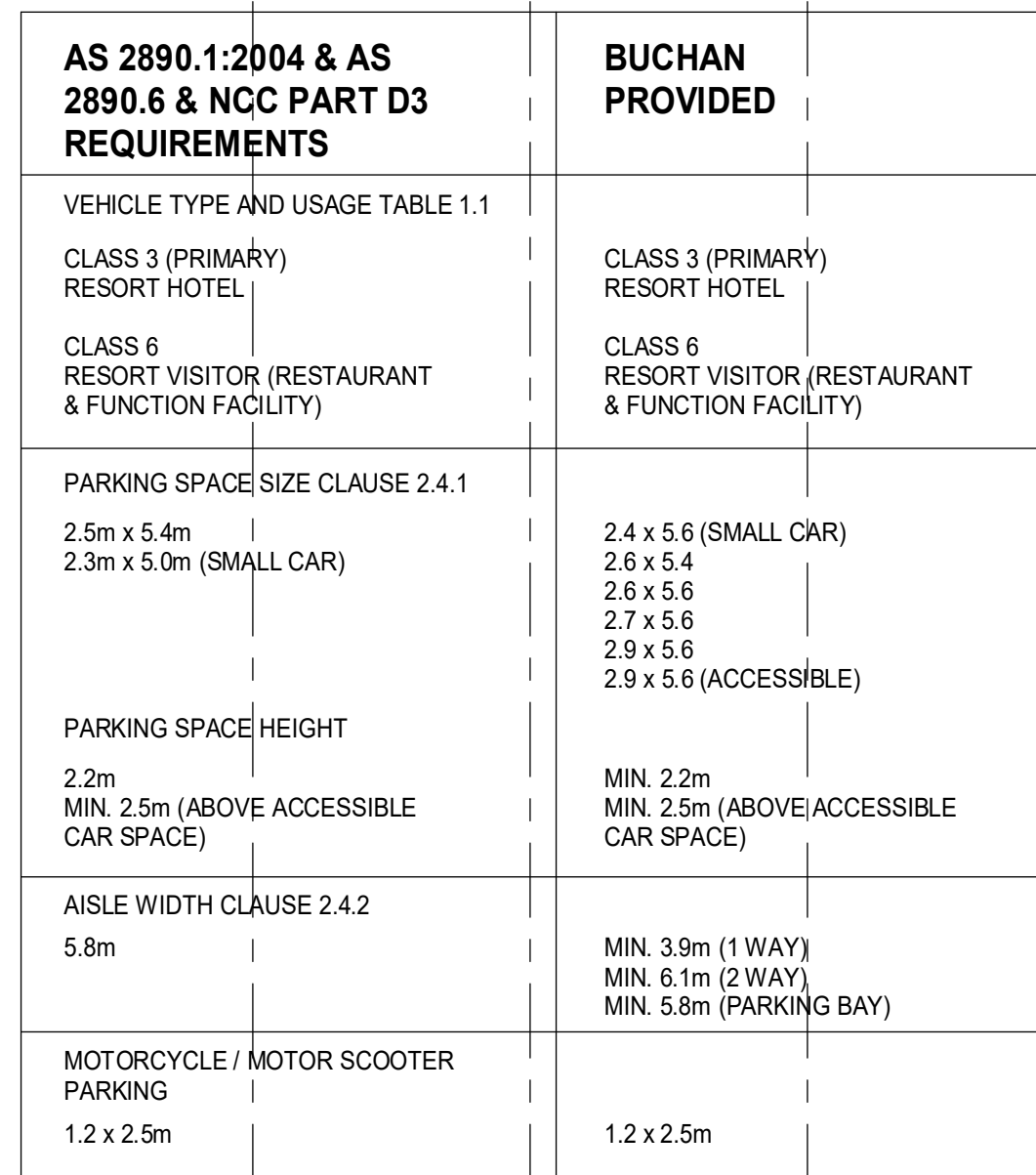
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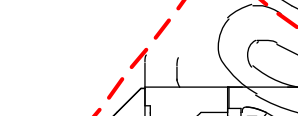
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DEVELOPMENT APPLICATION SUBMISSION





Keyplan

A keyplan diagram showing the building footprint (a large rectangle with a central circular feature) and its surrounding context, including roads and other buildings. The building footprint is outlined in red.

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Bicycle	72
Motorbike	8



Project

FAIRMONT RESORT

71-85 Port Douglas Rd, Port Douglas QLD 4877, Australia


Project Number
219194

Status **DA SUBMISSION**

Date Plotted 29/03/2021 2:41:53 PM

Date Issued 27/03/2021

Scale As indicated @A0



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Drawing Title

OVERALL PLAN

BASEMENT

RL-.0.190m

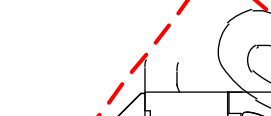
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Revision
3

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Keyplan



The keyplan shows a site with a dashed red boundary. Inside the boundary, there is a large rectangular area with a smaller rectangular area inside it, representing the proposed development. The site is surrounded by other areas, including a road and a green space.

File

EXISTING TREE TO BE RETAINED
REFER LANDSCAPE

PROPOSED TREE
REFER LANDSCAPE

PROPOSED LANDSCAPE AREA
REFER LANDSCAPE

REFER LANDSCAPE FOR ALL INTERNAL
AND EXTERNAL PLANTING TYPES AND EXTENTS

Project

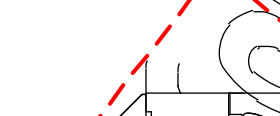
FAIRMONT RESORT

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Drawing Number **DA-0202**




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Keyplan

A keyplan diagram showing the location of the study area within the larger context of the site. The study area is highlighted by a red dashed line, indicating its position relative to the surrounding landscape and infrastructure.


File


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	PROPOSED TREE REFER LANDSCAPE
	PROPOSED LANDSCAPE AREA REFER LANDSCAPE

REFER LANDSCAPE FOR ALL INTERNAL AND EXTERNAL PLANTING TYPES AND EXTENTS

FAIRMONT RESORT
71-85 Port Douglas Rd, Port
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Date Issued	27/03/2021
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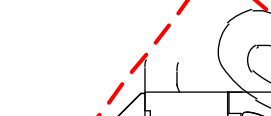
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**OVERALL PLAN
UPPER GROUND
RL.6.210m**

Drawing Number
DA-0203

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Keyplan



The keyplan shows a site with a dashed red boundary. Inside the boundary, there is a large rectangular area with a smaller rectangular area inside it, representing the proposed development. The site is surrounded by other areas, including a road and a river.

File

EXISTING TREE TO BE RETAINED
REFER LANDSCAPE

PROPOSED TREE
REFER LANDSCAPE

PROPOSED LANDSCAPE AREA
REFER LANDSCAPE

REFER LANDSCAPE FOR ALL INTERNAL
AND EXTERNAL PLANTING TYPES AND EXTENTS

Project


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DA-0205

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Keyplan



The keyplan shows a site with a dashed red boundary. Inside the boundary, there is a large rectangular area labeled 'PROPOSED DEVELOPMENT'. To the right of this area is a smaller rectangular area labeled 'EXISTING BUILDING'. The site is surrounded by other areas, including a road labeled 'A166' and a river labeled 'RIVER URE'.

File

EXISTING TREE TO BE RETAINED
REFER LANDSCAPE

PROPOSED TREE
REFER LANDSCAPE

PROPOSED LANDSCAPE AREA
REFER LANDSCAPE

REFER LANDSCAPE FOR ALL INTERNAL
AND EXTERNAL PLANTING TYPES AND EXTENTS

Project

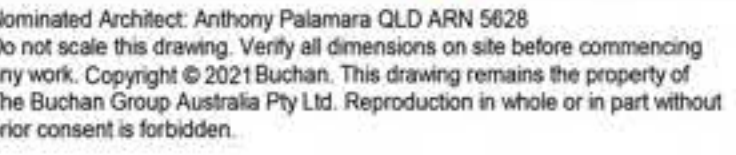
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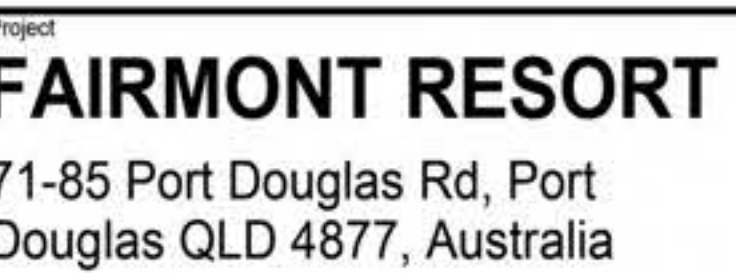
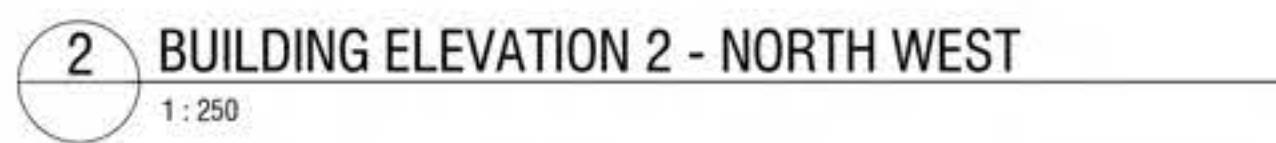
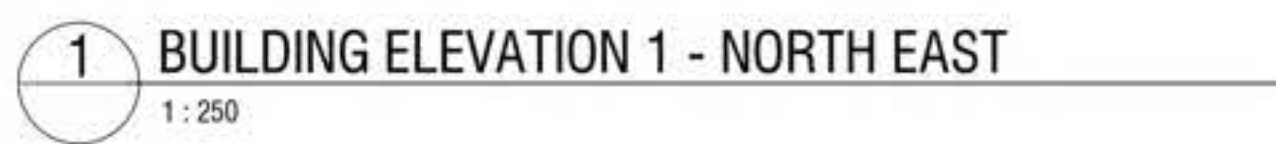
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eyplan



LEGEND



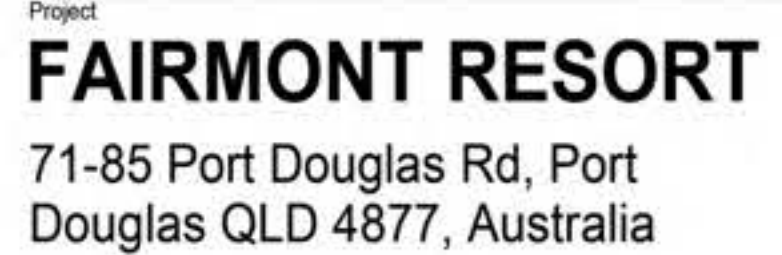
Drawing Number
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Revision
2

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File

M01	COPPER GLADDING TO STRUCTURAL ELEMENTS INCLUDING BOTH HORIZONTAL AND VERTICAL FEATURES, INCLUDING SKYLIGHTS, POOL STRUCTURES AND AWNINGS
M02	LOCALLY SOURCED SANDSTONE FOR FEATURE LANDSCAPING AND WATER FEATURE ELEMENTS
M03	DULUX EGGSHELL PINK FOR GRC SCULPTURAL PLANTER PLANTERS (VARIES)
M04	BURNISHED COPPER / SATIN BROWN FINISH FOR EXPRESSED DOORS, RAINWATER PIPES AND VARIOUS METAL WORKS INCLUDING BALUSTRADES
M05	GLASS AND METAL BALUSTRADE TO HOTEL ROOF DECK
M06	TIMBER FORMED CONCRETE TO BALCONY PARTITION WALLS WITH BRICK VARIATION AND GLASS
M07	AGED BRONZE PATINA FOR FEATURE TALL PLANTER BOXES AND SELECTED AREAS LIKE PORTE COCHERE AND THE FAB AREAS
M08	DULUX STEEPCREY FOR GRC SCULPTURAL HANGING PLANTER FINISH TALL PLANTER BOXES
M09	SOLID IRONBARK TIMBER TO SELECTED EXTERNAL WALLS AND HOTEL ENTRY DOOR FRAMING
M10	METAL/PASTE FINISH COLOUR: METEORITE SHANT FINISH COLOUR: METAL BALUSTRADE TO PLANT AREA
M11	GLASS SKYLIGHT STRUCTURE TO INTERIOR VESTIBULE
M12	TIMBER AND BAMBOO FINISH TO PORTE COCHERE



Drawing Number
DA-0404

Revision
2

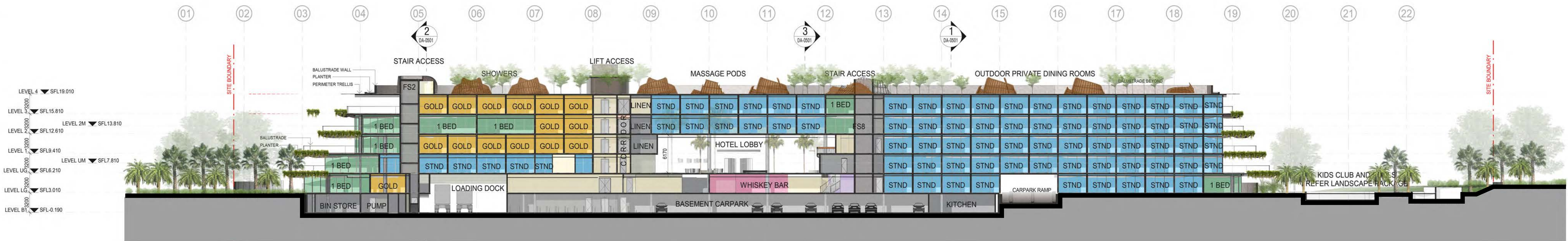
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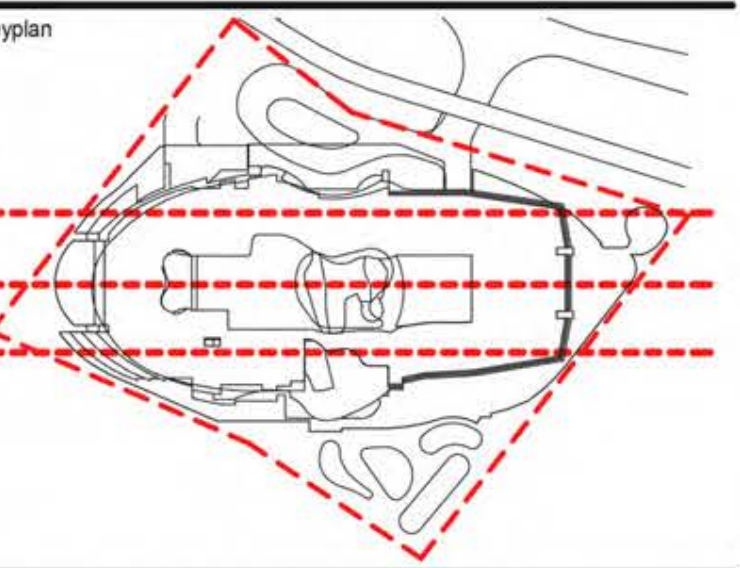


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3 SECTION 3
1: 250

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C	25/08/2020	PRE-DA UPDATE	SU	ME
1	31/08/2020	DA SUBMISSION	SA	ME
2	09/03/2021	DA DESIGN UPDATES	ST	PP



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corporation
PURE PROJECTS
PROJECT MANAGEMENT

BSE
TRUE PARTNERS

ADP
Consulting - Engineering

NORTHROP
studio

mckenzie group
carter

DURIE O DESIGN

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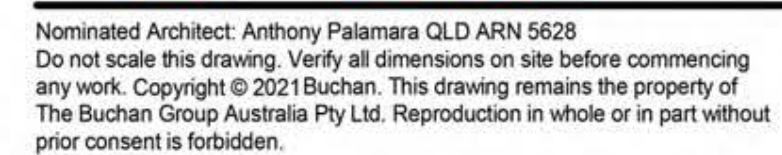
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Drawing Number
DA-0500

Revision
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Project
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219194

Status
DA SUBMISSION

Date Plotted
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Date Issued
09/03/2021

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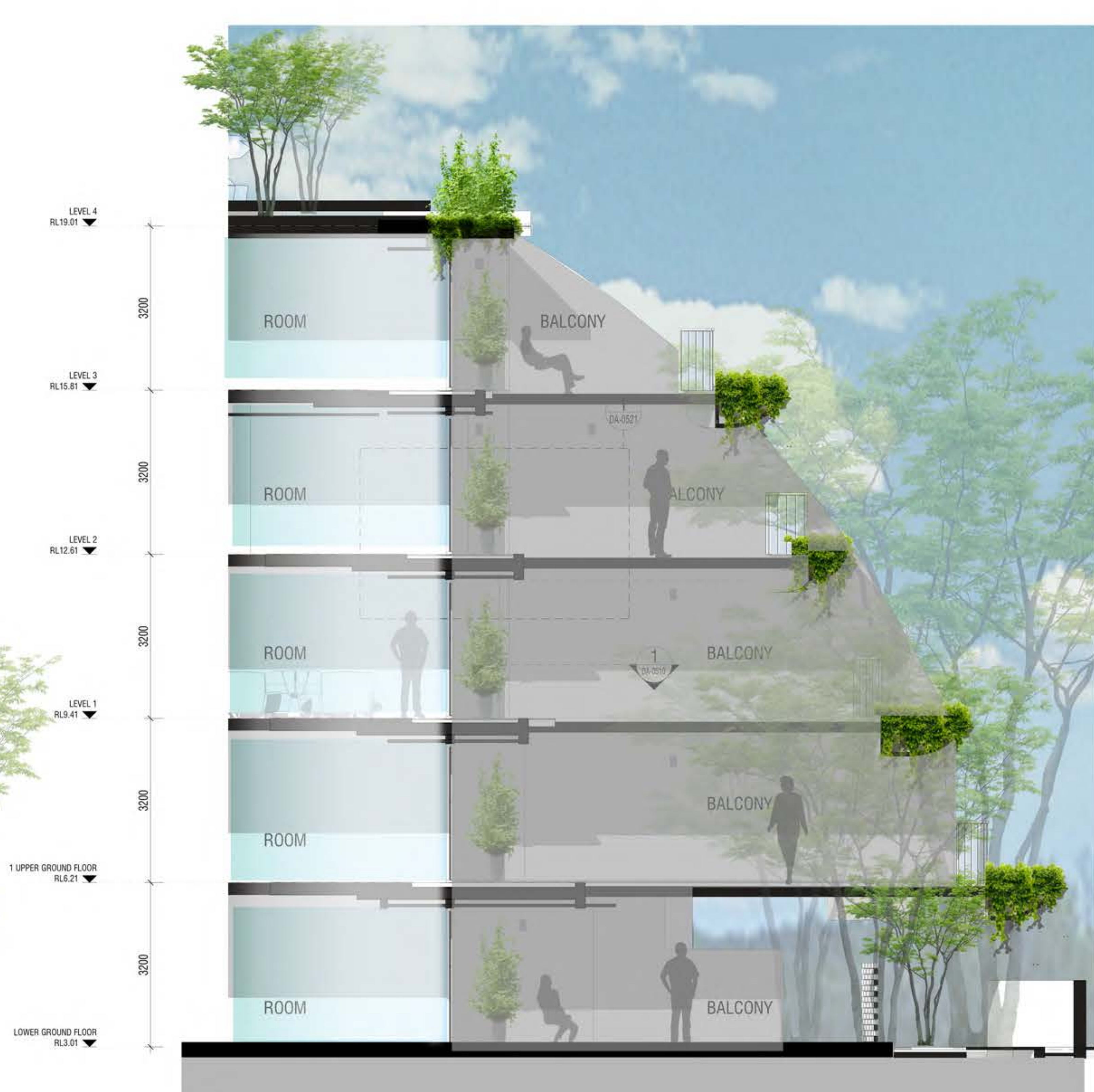
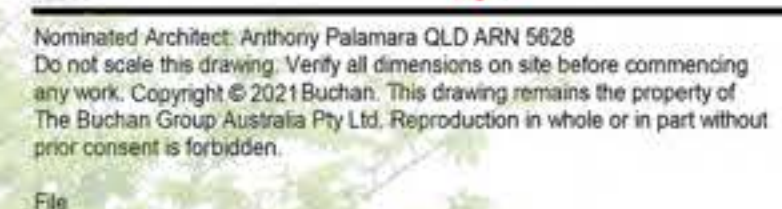
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Drawing Number
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Revision
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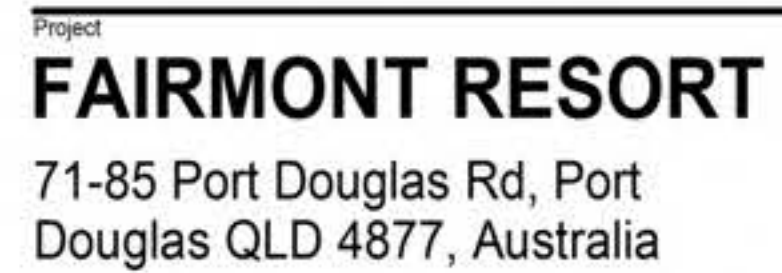
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3 TYPICAL GUEST BALCONY - SECTION
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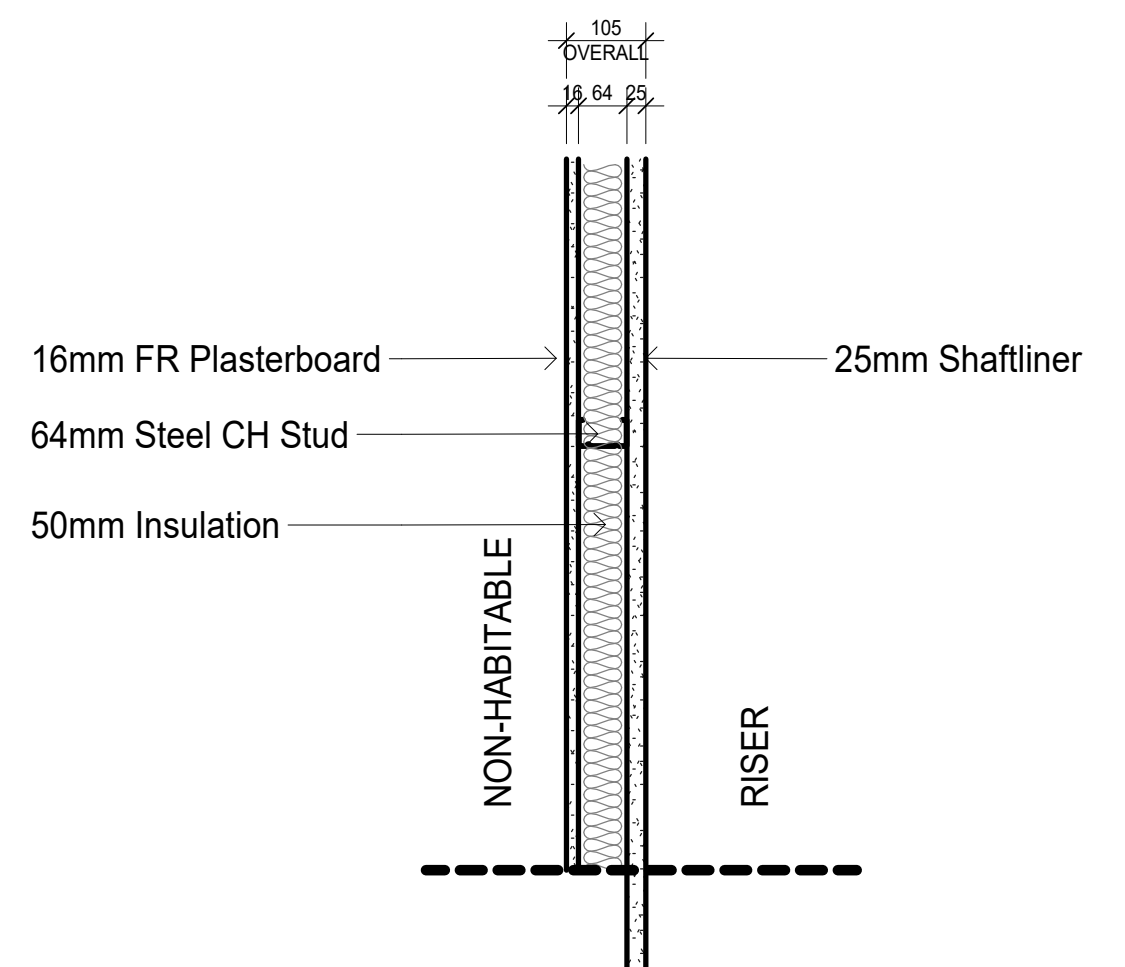
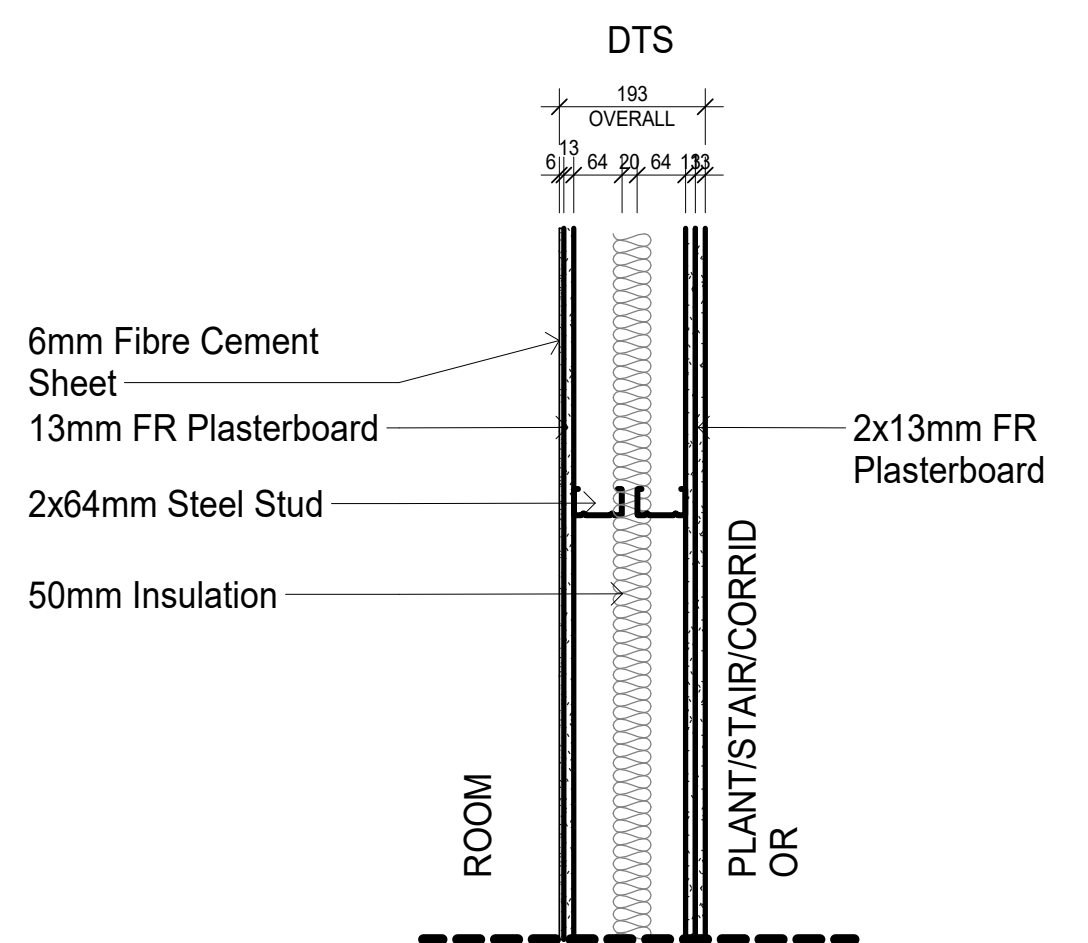
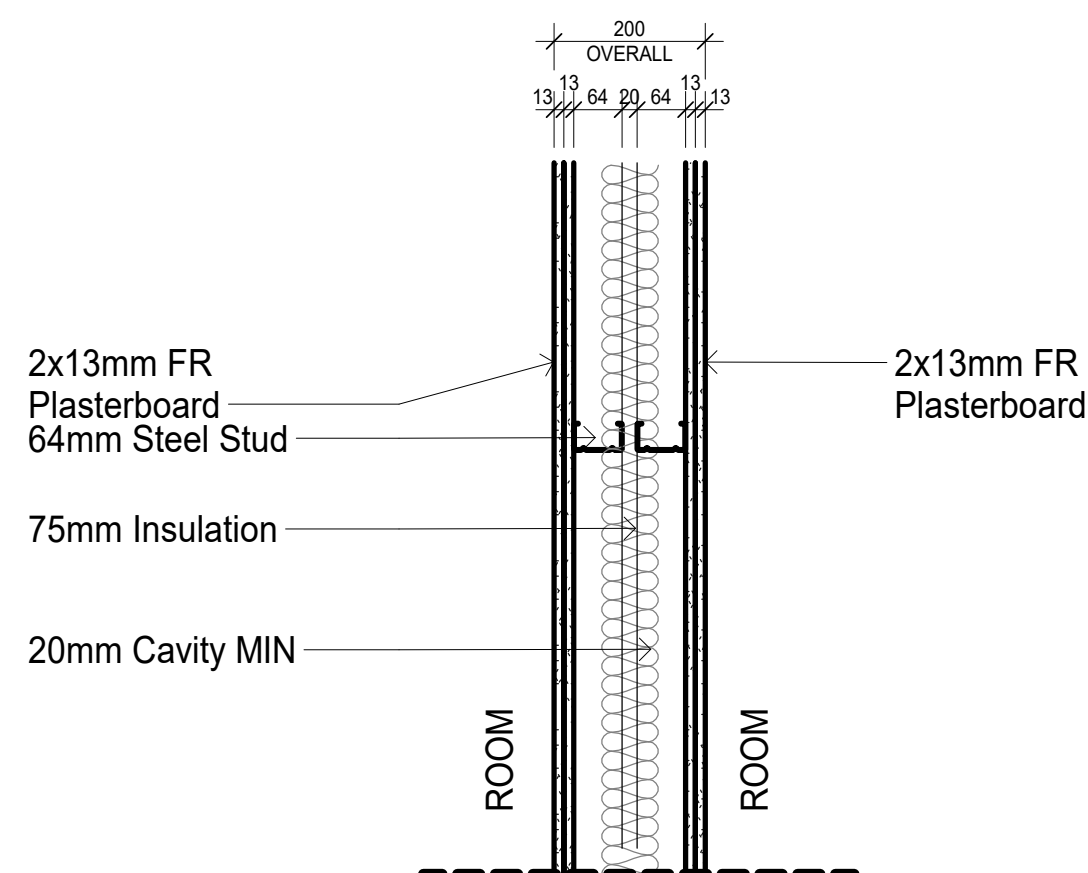
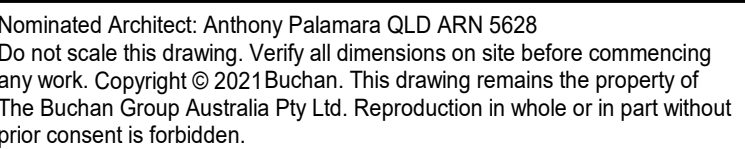


1 TYPICAL GUEST BALCONY - PLAN
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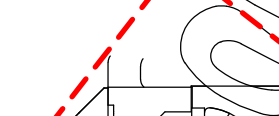


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DA-0510 **2**

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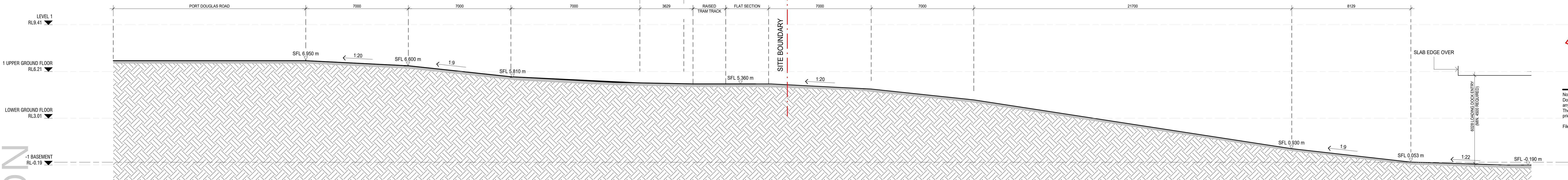


Keyplan



A keyplan diagram showing the building footprint (a large rectangle with internal divisions) and its location relative to the surrounding site. The building is outlined in black, and the entire site area is enclosed by a red dashed line. The diagram includes various site features like roads, paths, and landscaping elements.

File

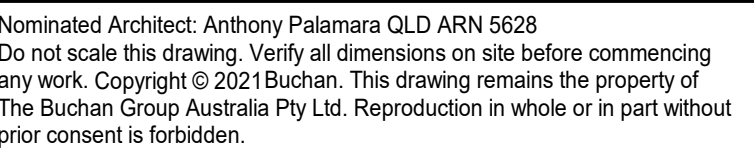




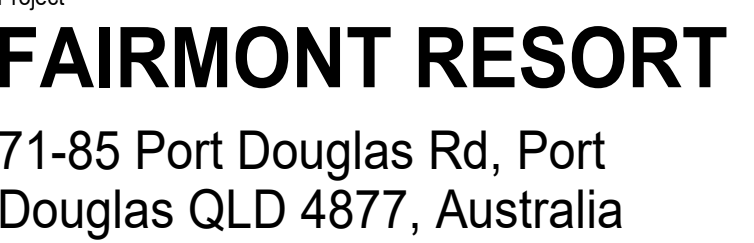
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Drawing Number **DA-0531** Revision **1**

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File



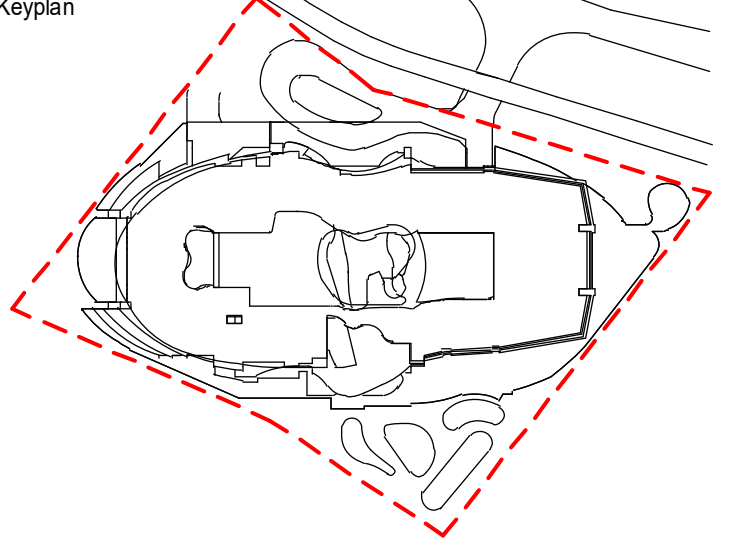
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Date Issued	19/02/2021
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DA-0532 **1**

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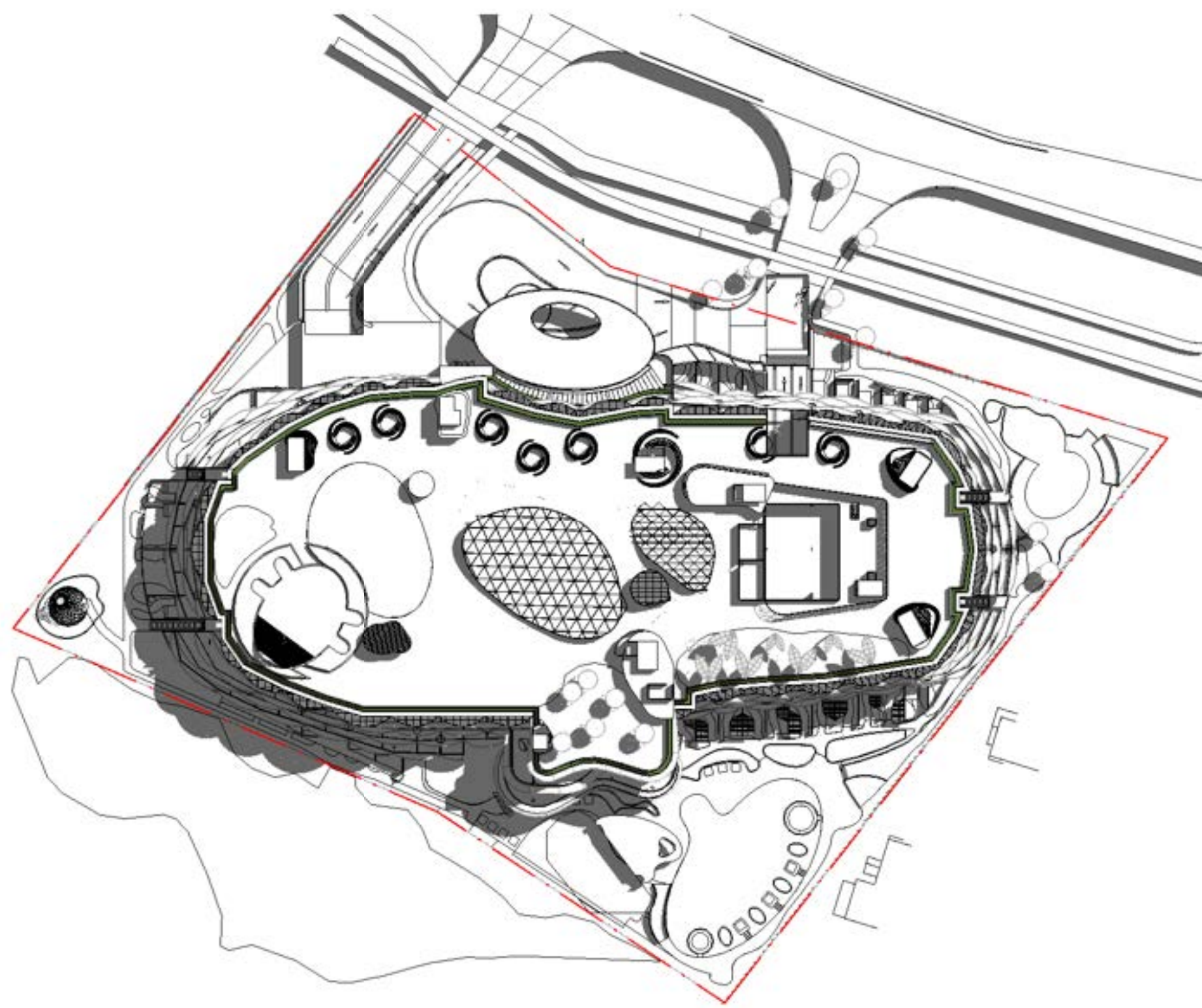
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Keyplan

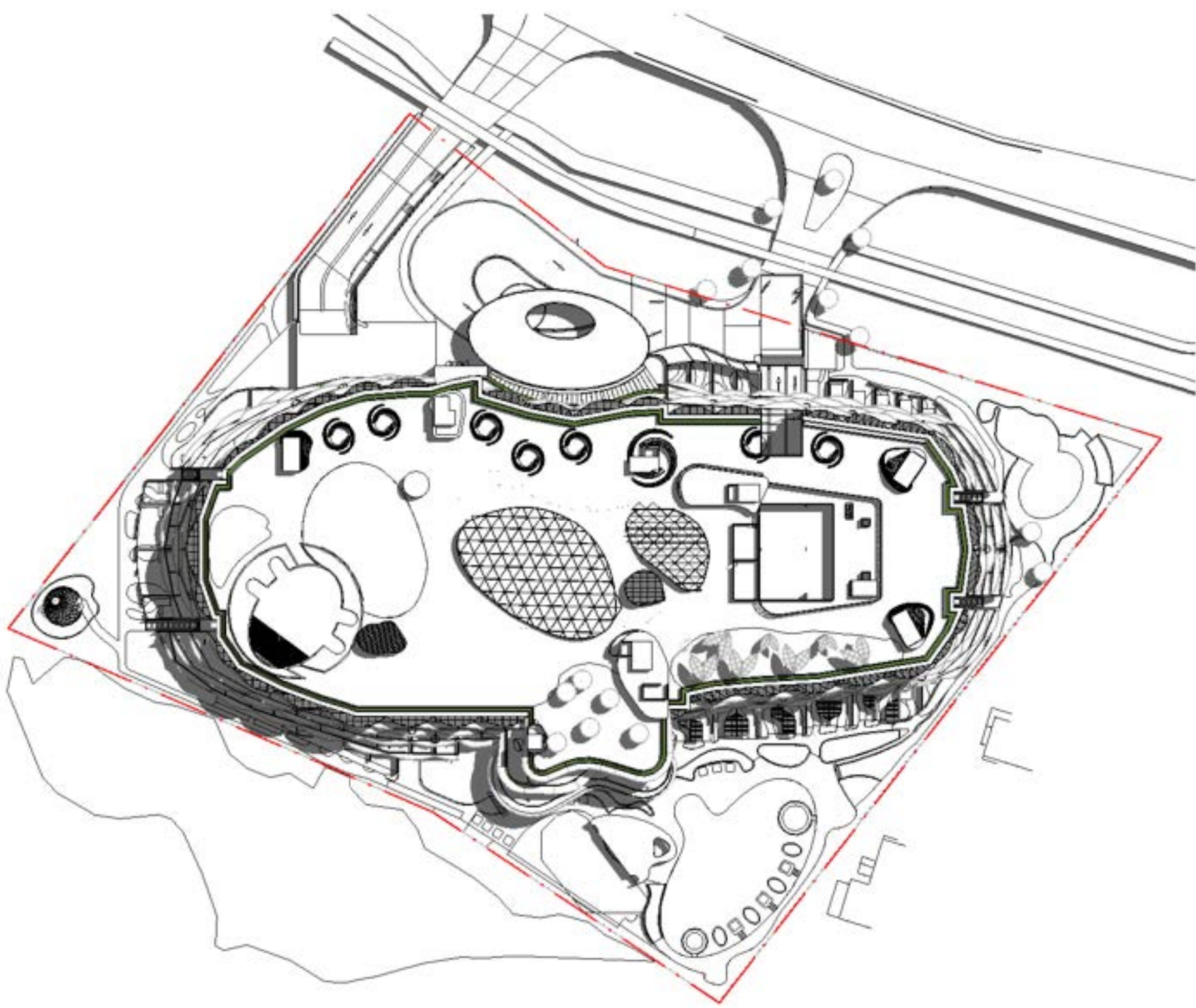


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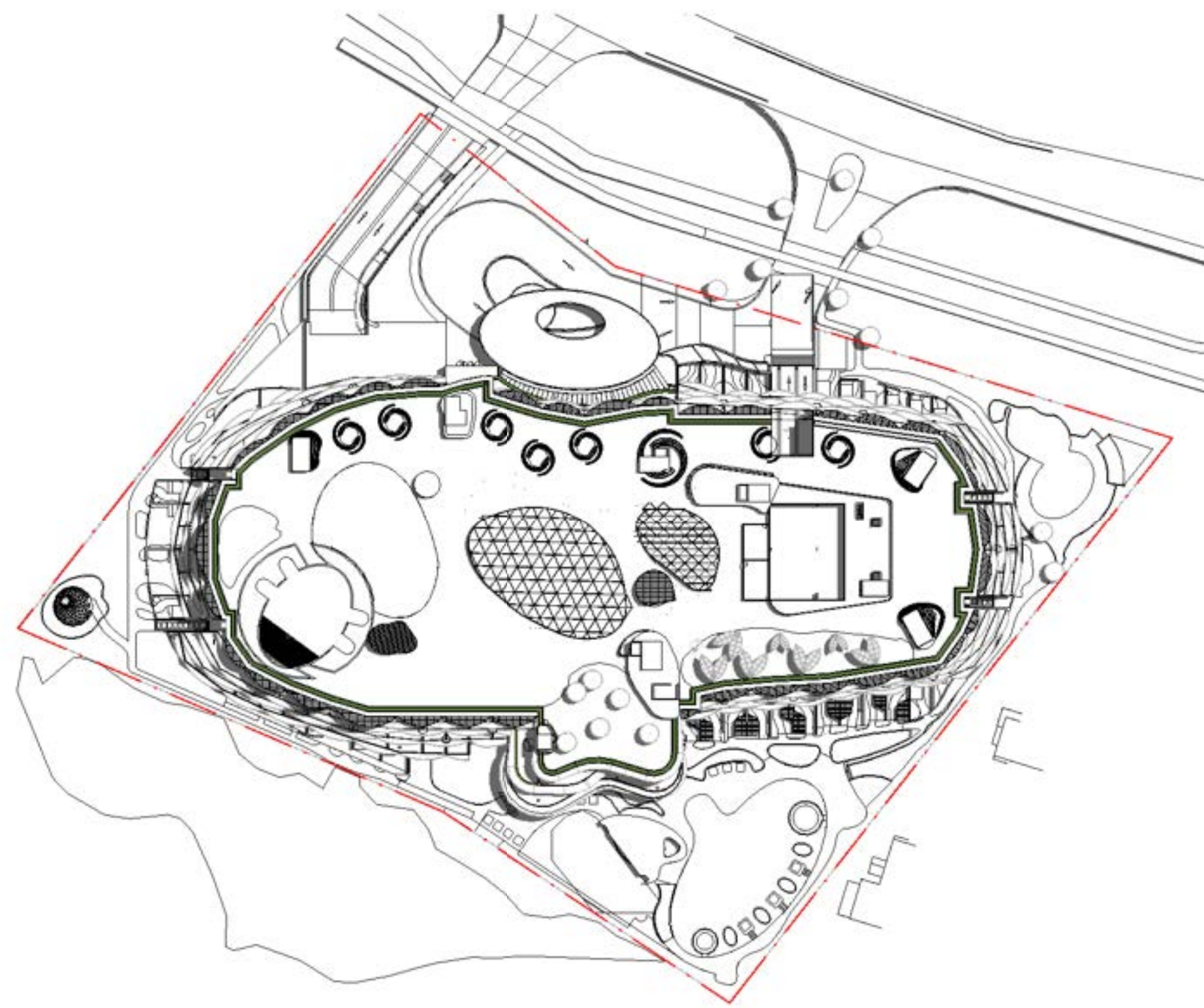
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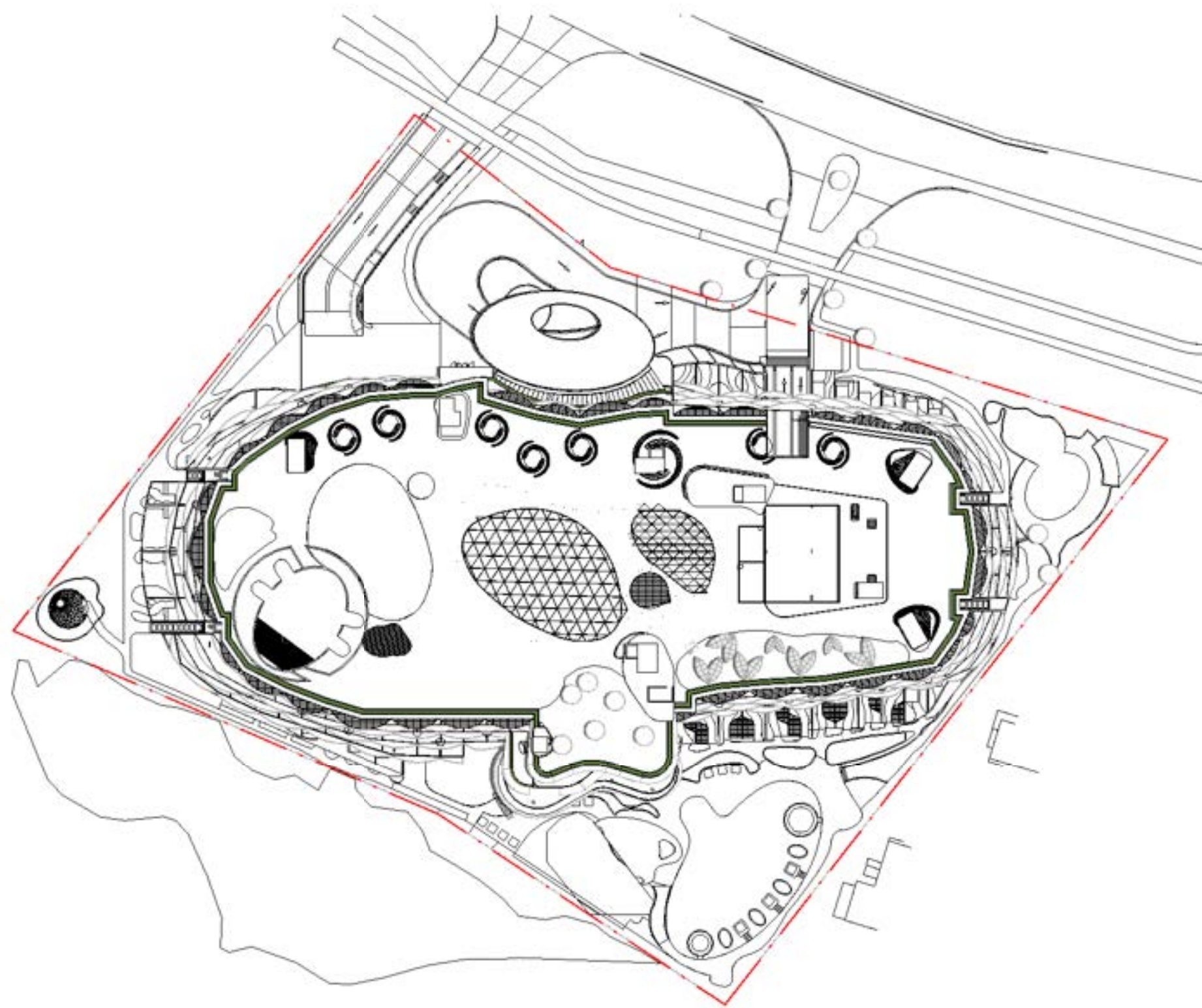
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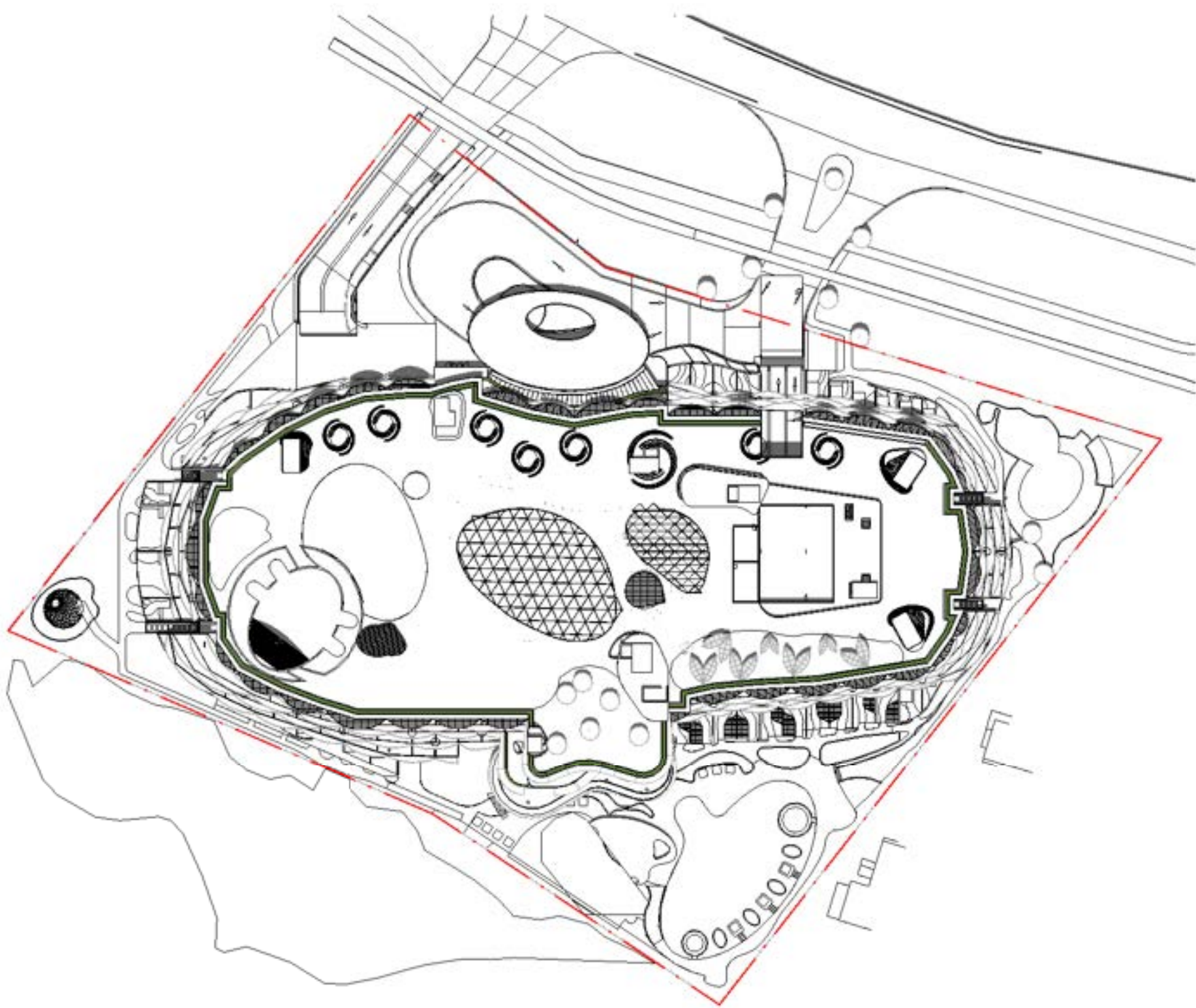
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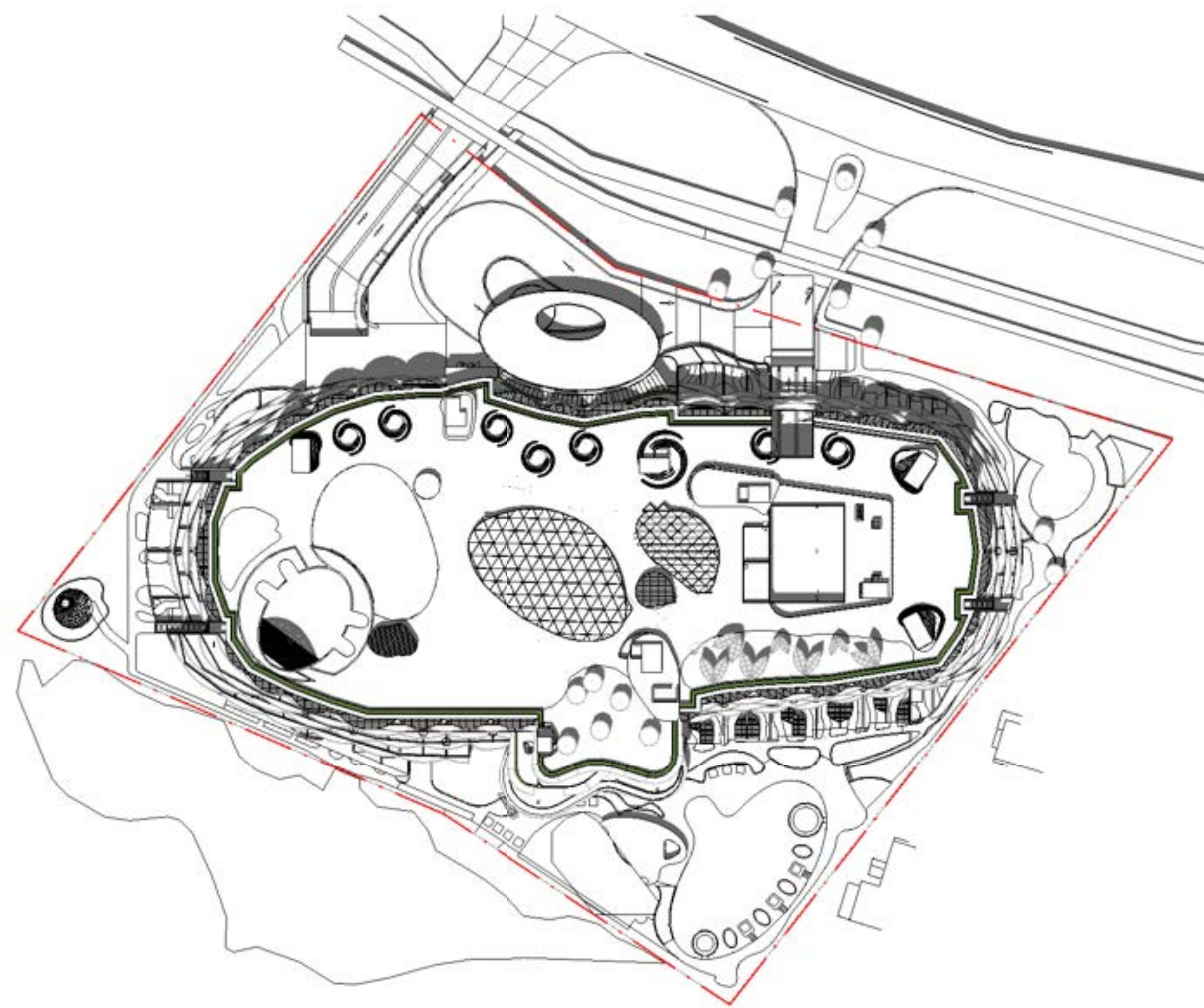
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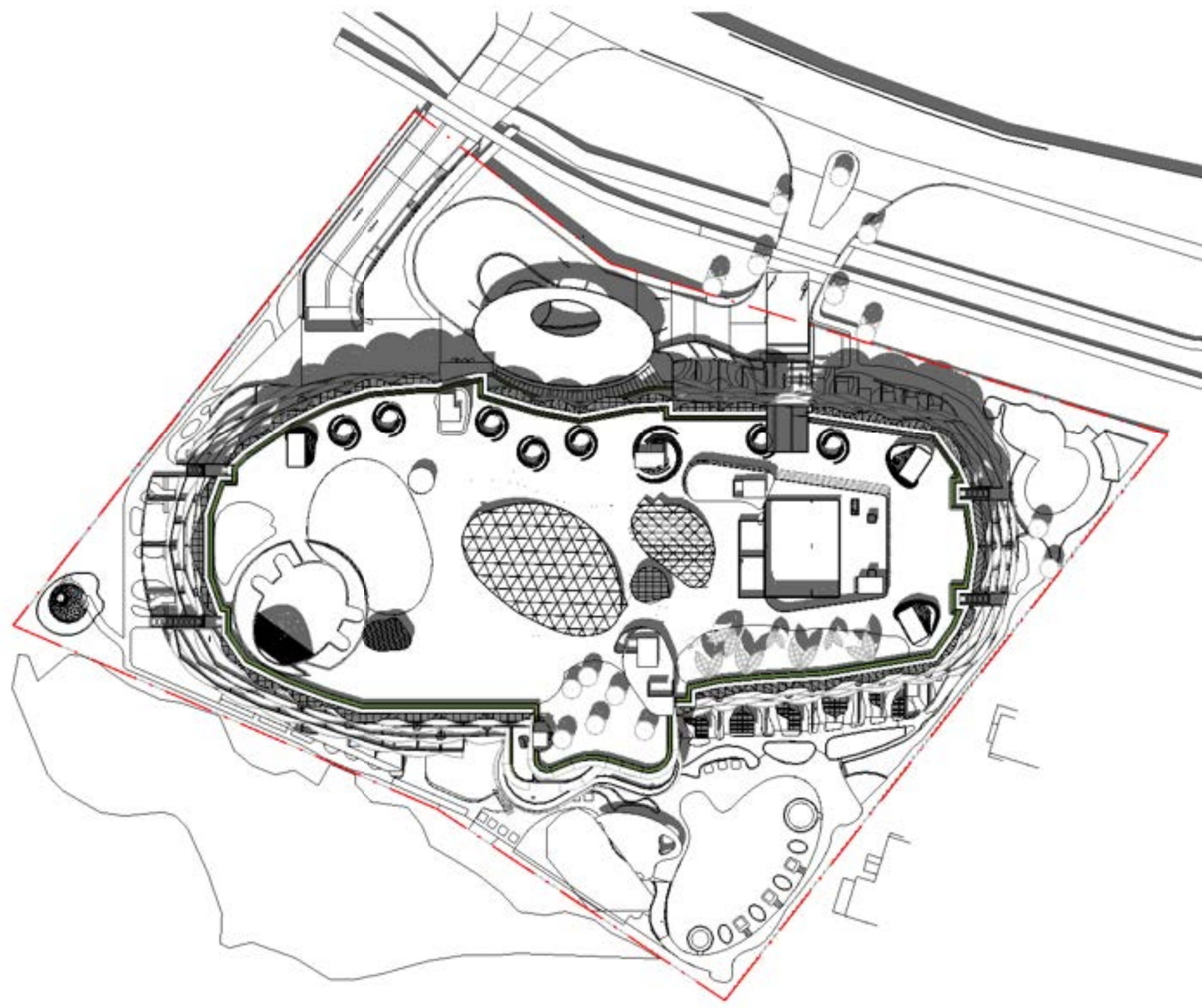
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5 SOLAR STUDY-21 DEC-1PM
1 : 1000



6 SOLAR STUDY-21 DEC-2PM
1 : 1000



7 SOLAR STUDY-21 DEC-3PM
1 : 1000



FAIRMONT RESORT
71-85 Port Douglas Rd, Port
Douglas QLD 4877, Australia

Project Number

219194

Status	DA SUBMISSION
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Date Plotted 29/03/2021 3:05:15 PM

Date Issued 27/03/2021

Scale 1:1000 @A0

Deming Title _____

SHADOW DIAGRAMS - SUMMER

Drawing Number

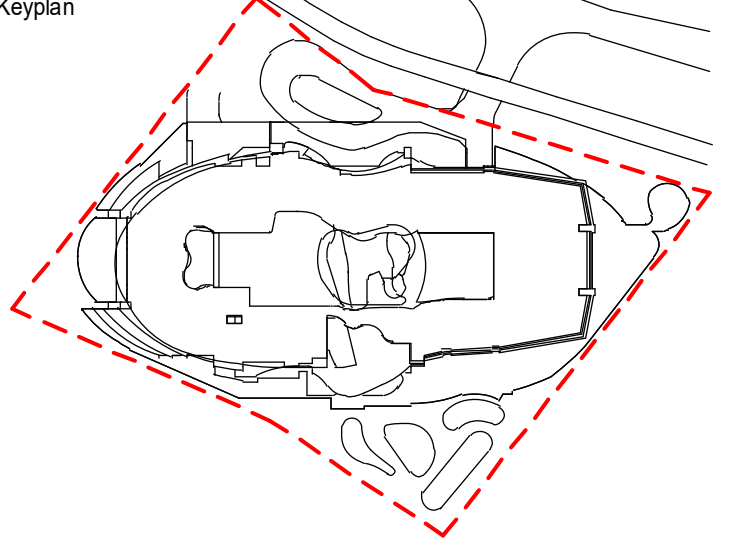
DA-081

Revision

BUCHAN

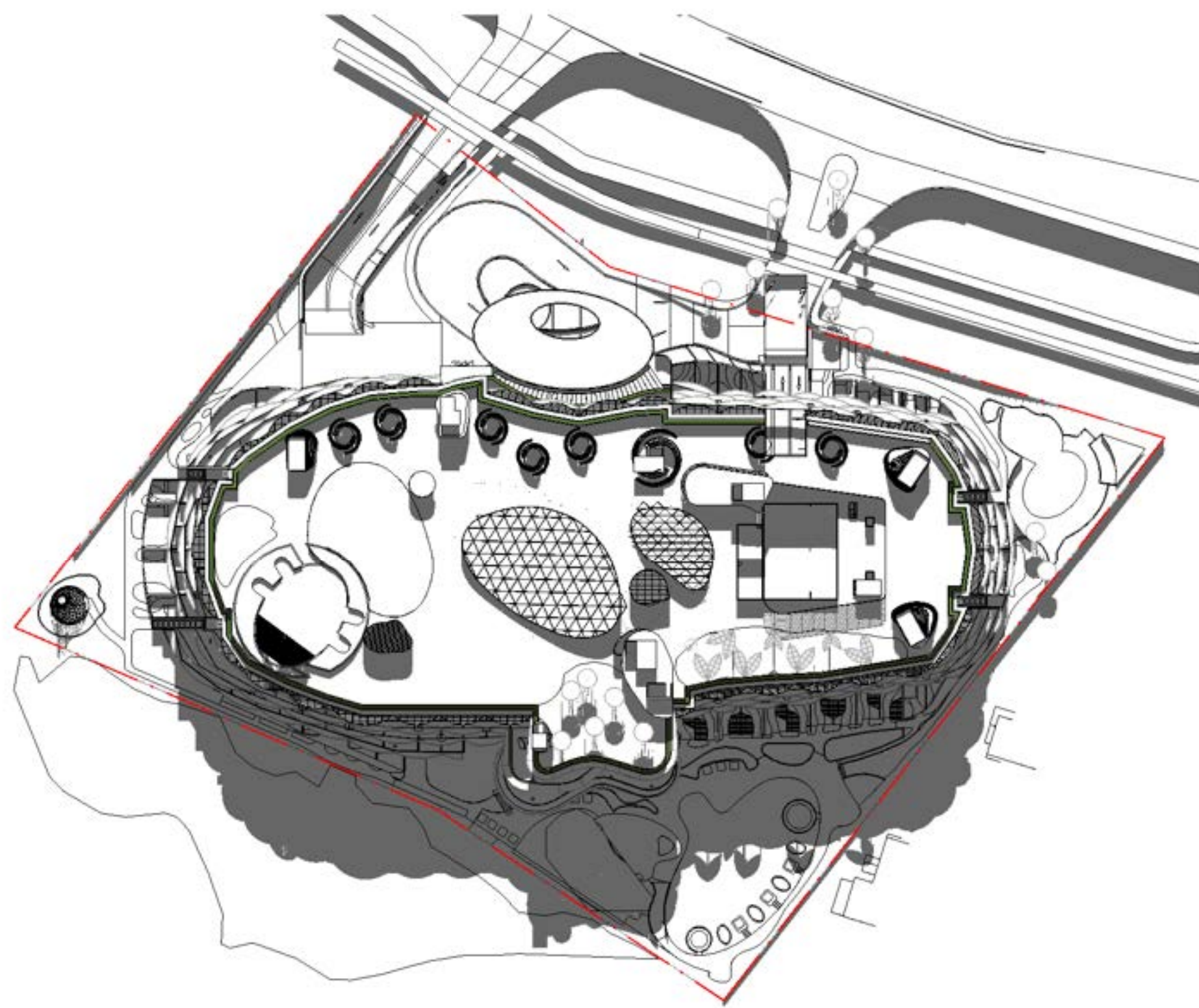
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Keyplan

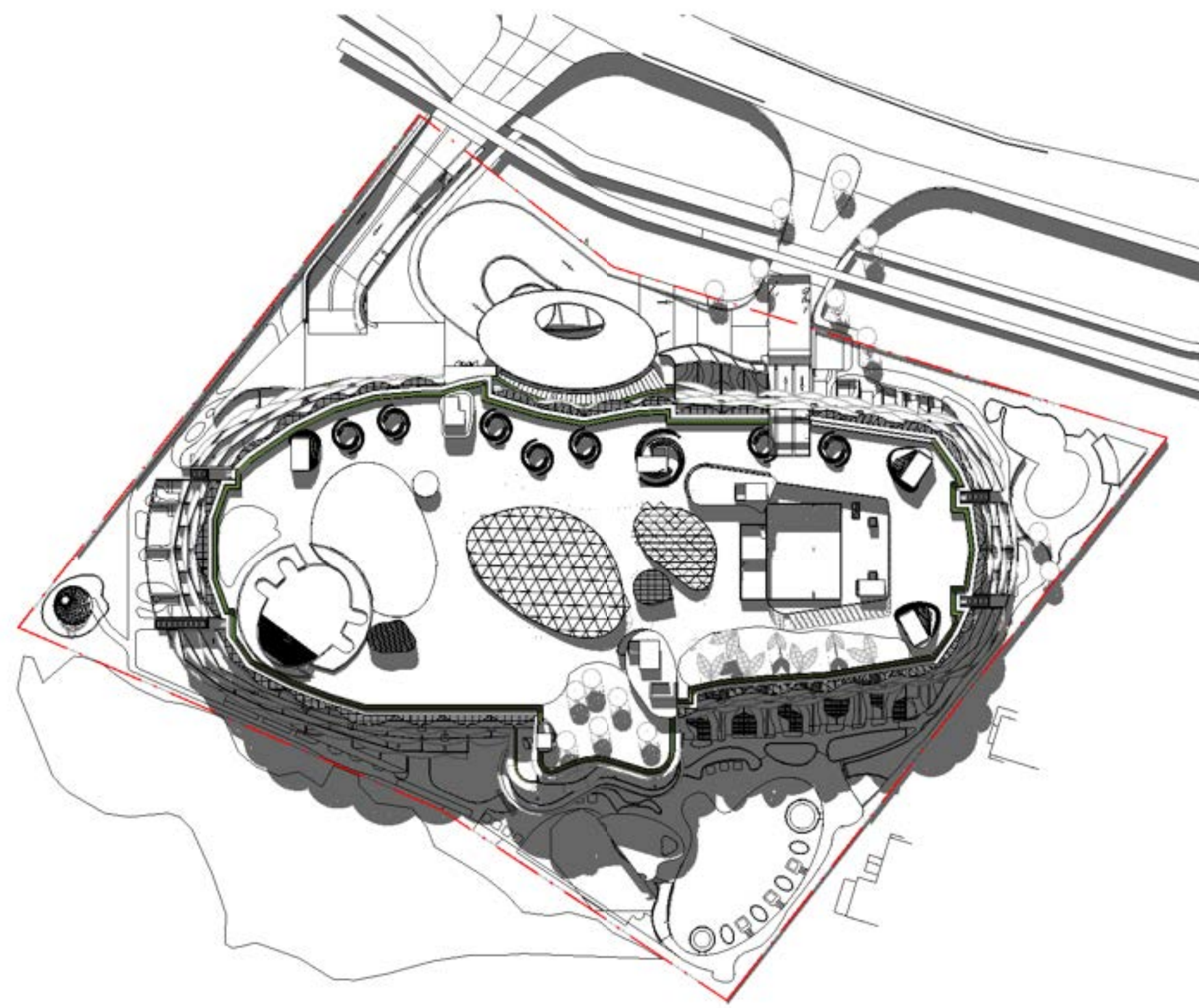


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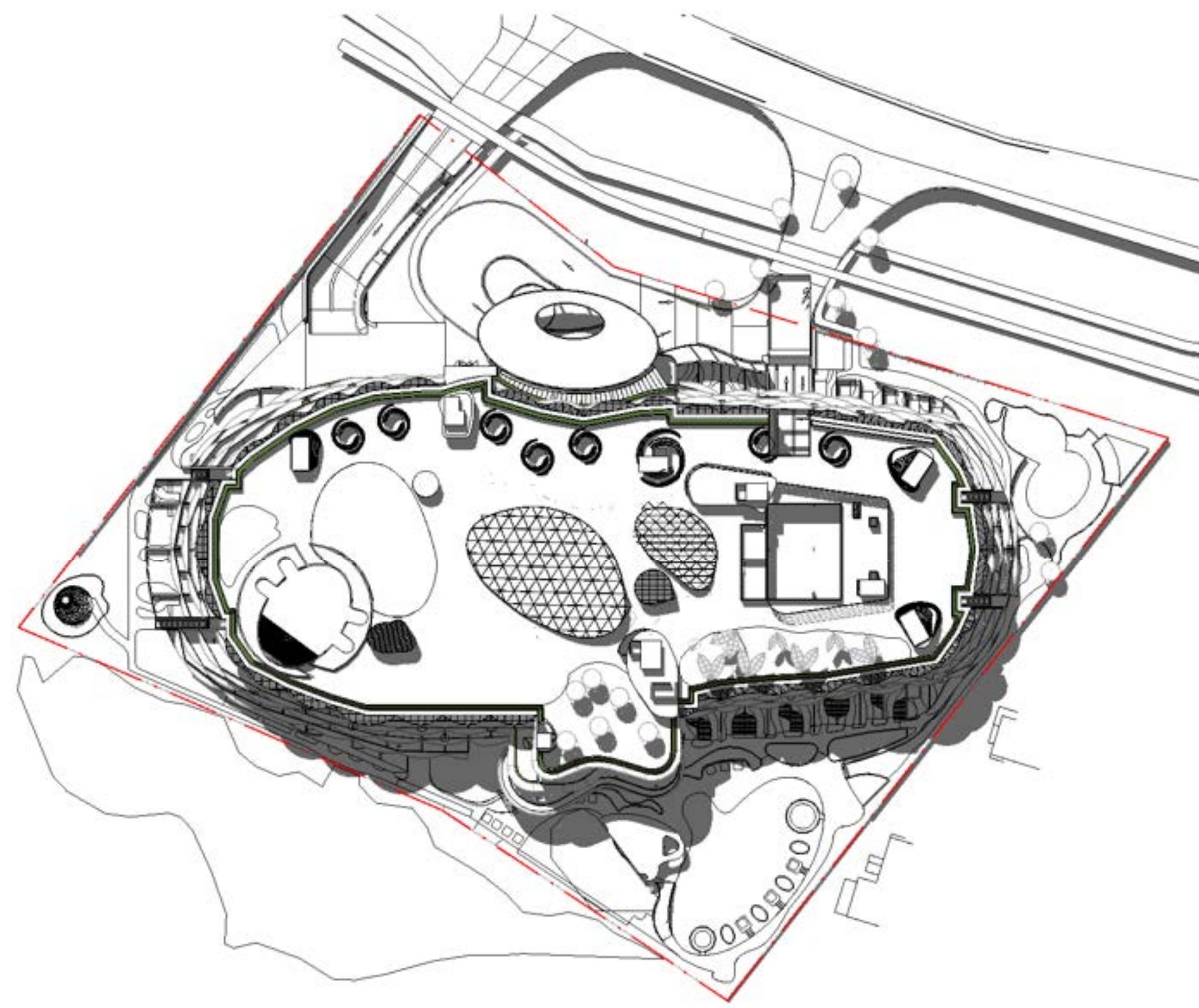
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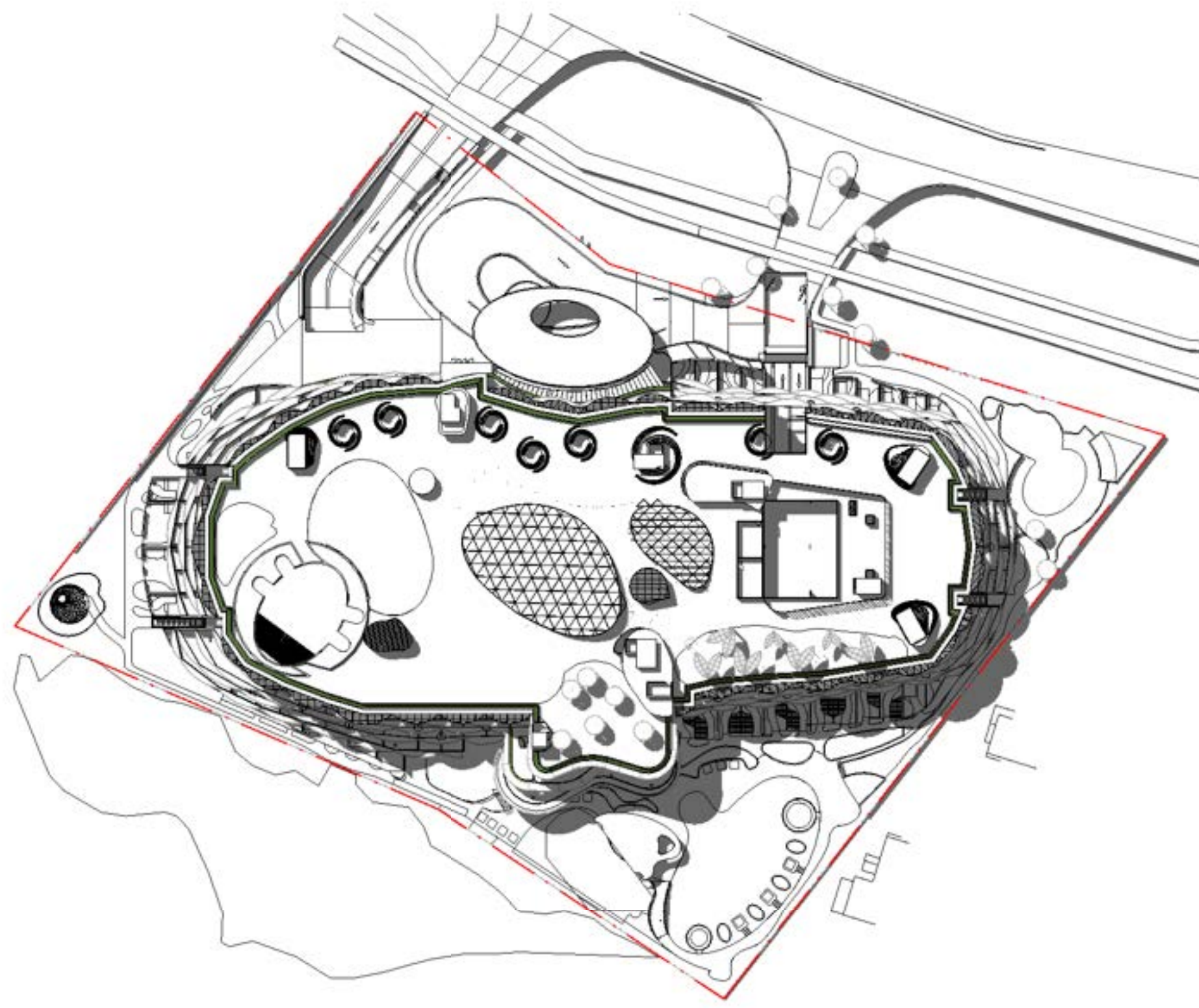
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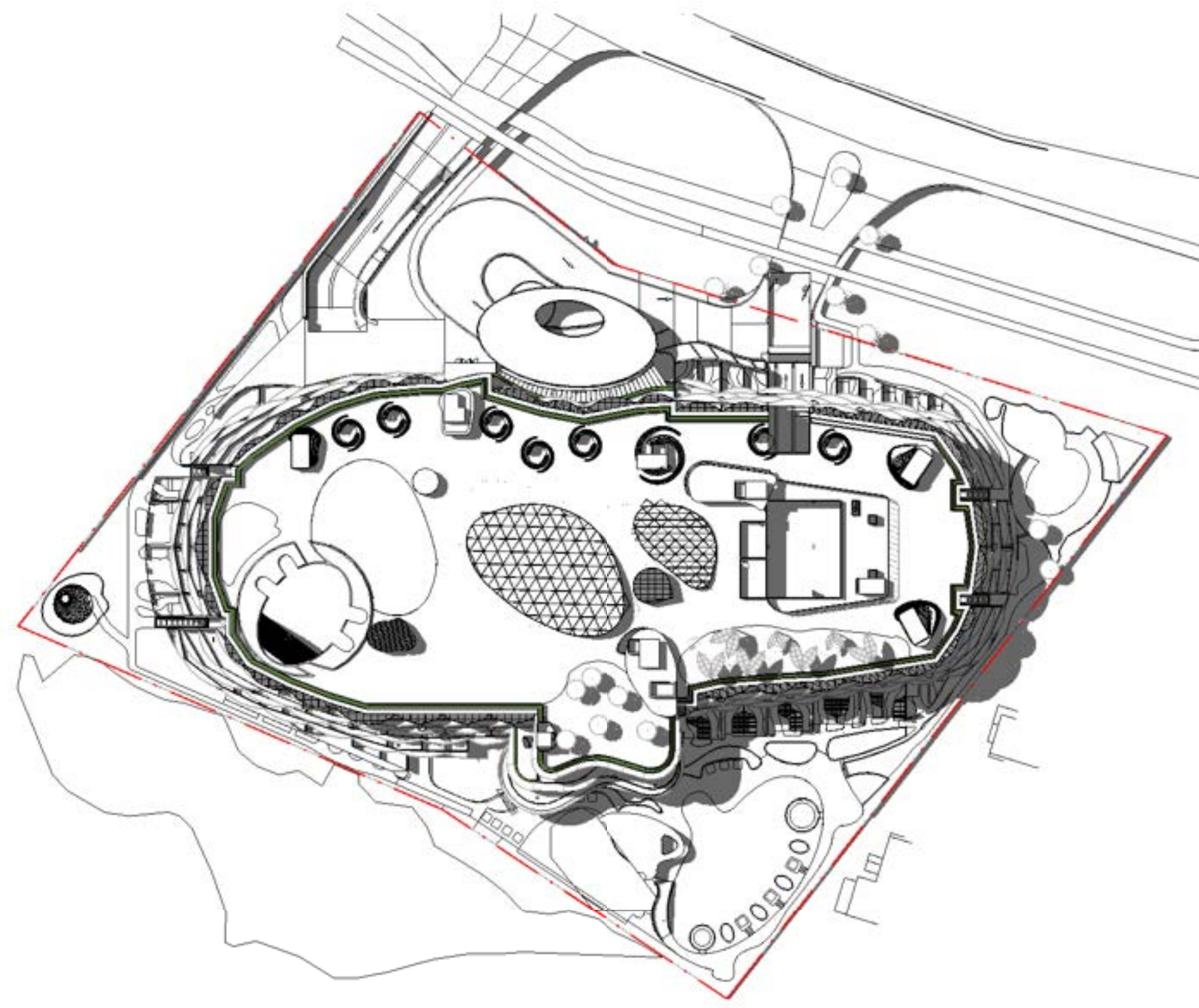
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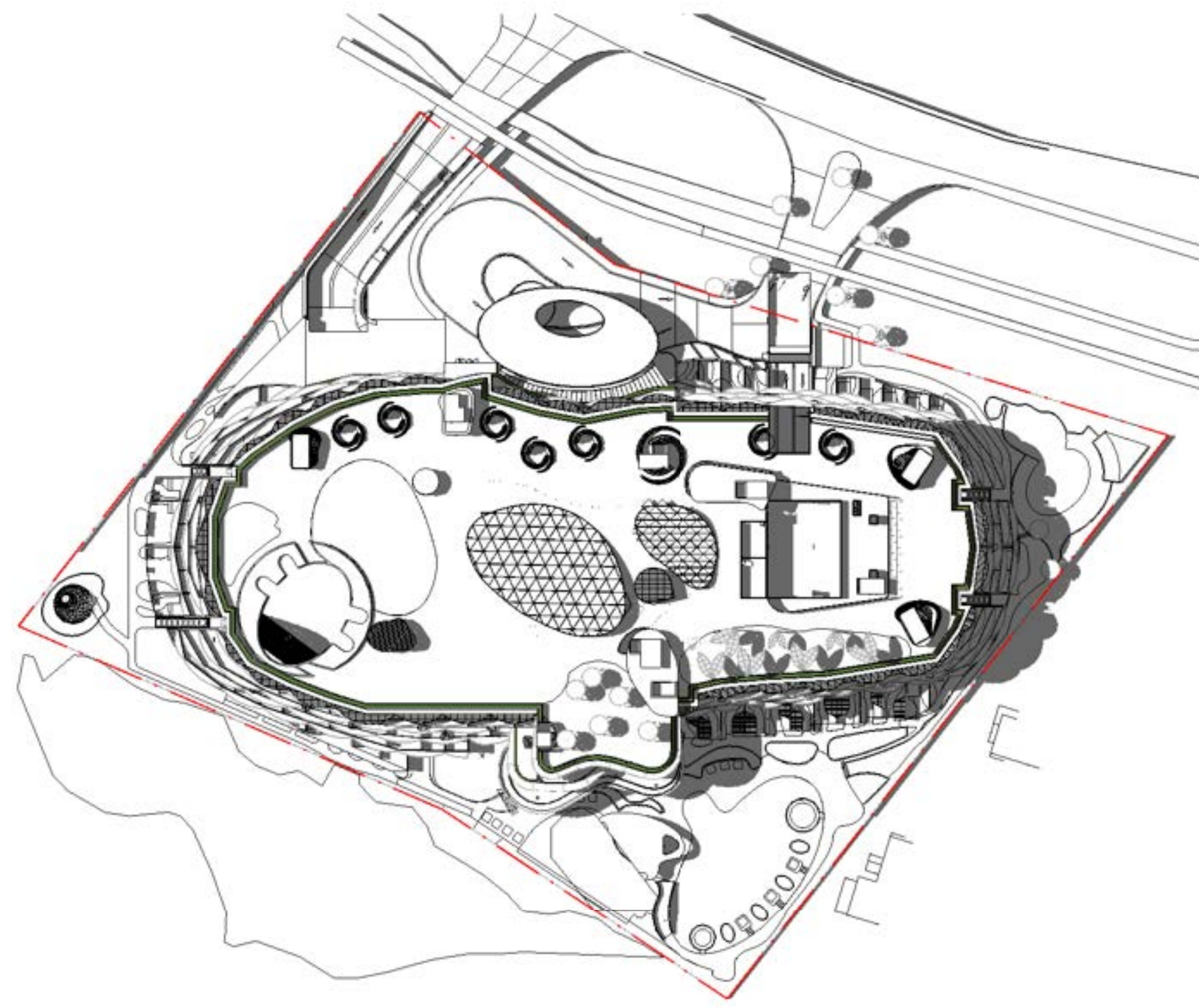
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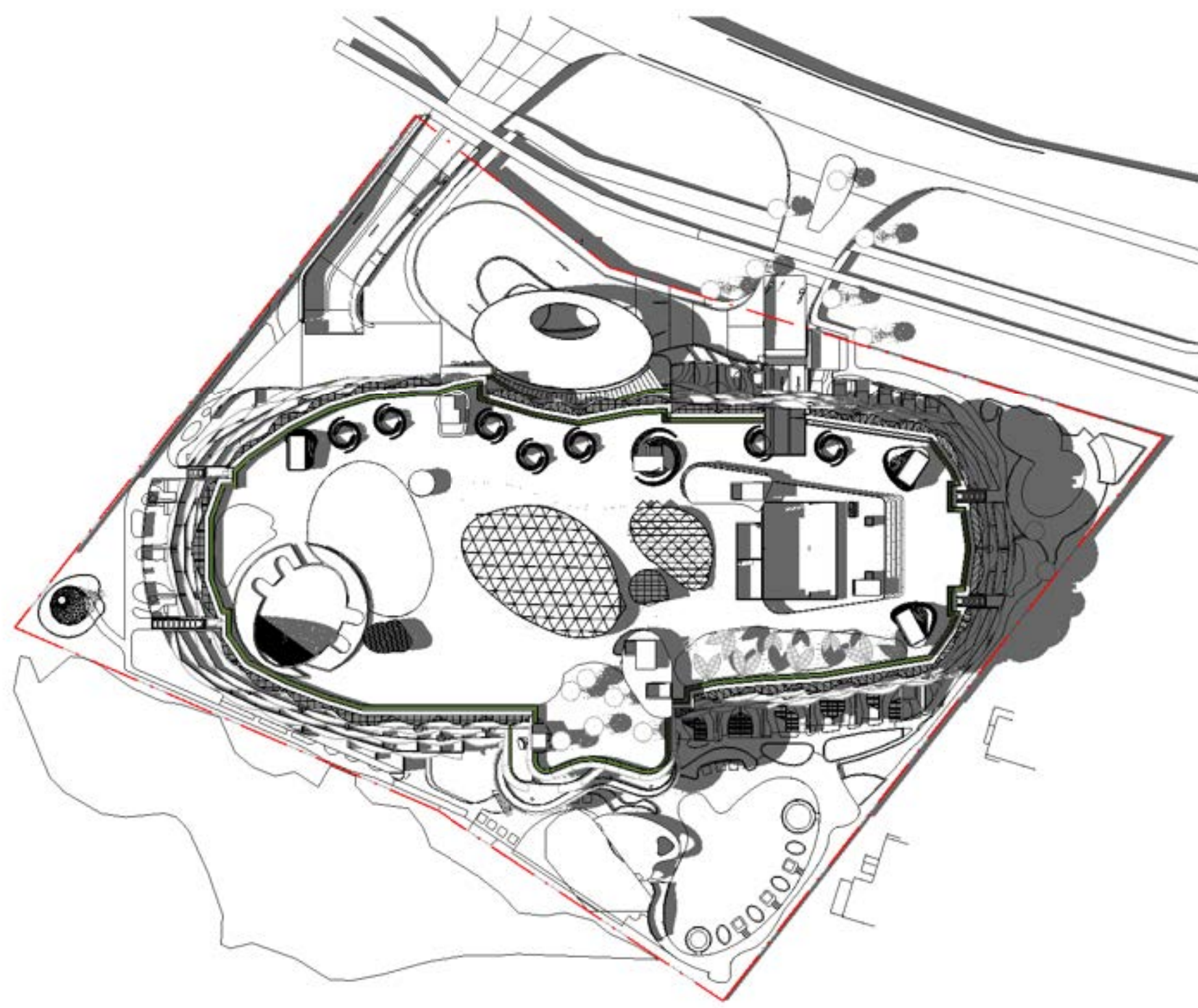
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5 SOLAR STUDY-21 JUN-1PM
1 : 1000



6 SOLAR STUDY-21 JUN-2PM
1 : 1000



7 SOLAR STUDY-21 JUN-3PM
1 : 1000



FAIRMONT RESORT
71-85 Port Douglas Rd, Port
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Project Number
219194


Status
DA SUBMISSION

Date Plotted
29/03/2021 3:08:43 PM

Date Issued
27/03/2021

Scale
1 : 1000 @A0





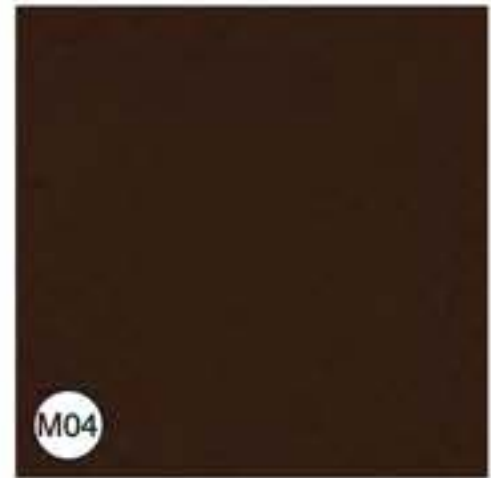
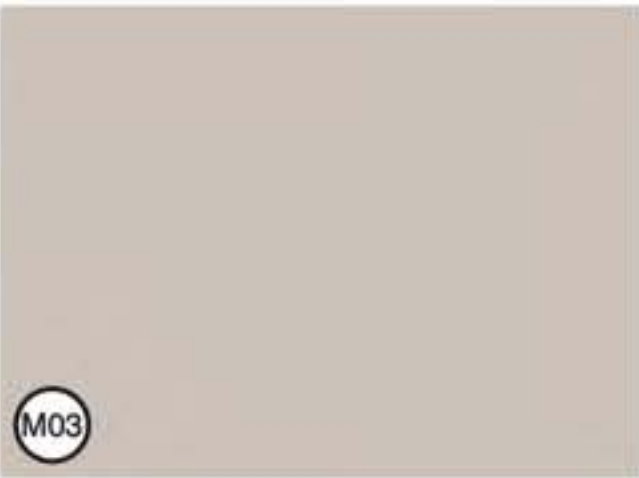
Drawing Title
SHADOW DIAGRAMS - WINTER

Drawing Number **DA-0811** Revision **2**

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Rev	By	Rev	By
A	14/08/2020	PRG-DA CLIENT SUBMISSION	ST ME
1	17/08/2020	DA SUBMISSION	SA ME
2	18/10/2020	DA SUBMISSION	ST MP



LEGEND

- M01** COPPER CLADDING TO PROMINENT STRUCTURAL ELEMENTS BOTH HORIZONTAL AND VERTICAL FEATURES INCLUDING SKYLIGHTS, POD STRUCTURES AND AWNINGS
- M02** LOCALLY SOURCED SANDSTONE FROM CAIRNS FOR FEATURE LANDSCAPING AND WATER FEATURE ELEMENTS
- M03** DULUX EGGSHELL PINK FOR GRC SCULPTURAL HANGING PLANTERS, FINISH TBA
- M04** BURNISHED COPPER | SATIN BROWN FINISH FOR EXPOSED RAINWATER PIPES, EGRESS DOORS AND VARIOUS METAL WORK INCLUDING BALUSTRADING
- M05** GLASS AND METAL BALUSTRADING TO HOTEL ROOMS
- M06** TIMBER FORMED CONCRETE TO BALCONY PARTY WALLS WITH BRICK VARIATION AND GAPS
- M07** AGED BRONZE PATINA FOR FEATURE TALL PLANTER BOXES AND SELECT AREAS LIKE THE PORTE COCHERE AND THE F&B AREAS
- M08** DULUX STEPNEY FOR GRC SCULPTURAL HANGING PLANTERS, FINISH TBA. ALSO TO BE USED IN SOFFITS TO F&B AREAS
- M09** SOLID IRONBARK TIMBER TO SELECT EXTERNAL WALLS AND HOTEL ENTRY DOOR FRAMING. ALSO USED FOR SOFFITS, POOL DECKING, AND TIMBER STRUCTURES
- M10** OPEN-WEAVED PAVING TO VARIOUS AREAS OF PUBLIC AND PRIVATE OPEN SPACE GARDEN AND LANDSCAPED AREAS.
- M11** MICACEOUS PAINT FINISH, COLOUR: METEORITE SHOWER TO SKYLIGHT FRAMING, AND METAL BALUSTRADE TO PLANT AREA



1 PART ELEVATION - NORTH EAST
1:100 (A3)

M01

M09

c|H|i|O|D|O
CORPORATION

PURE PROJECTS
PROJECT MANAGEMENT

FAIRMONT RESORT
71-85 Port Douglas Rd, Port
Douglas QLD 4877, Australia

Project Number	219194
Rev	DA SUBMISSION
Issue	14/08/2020 2:58:39 PM
Date Issued	12/03/2021
Scale	

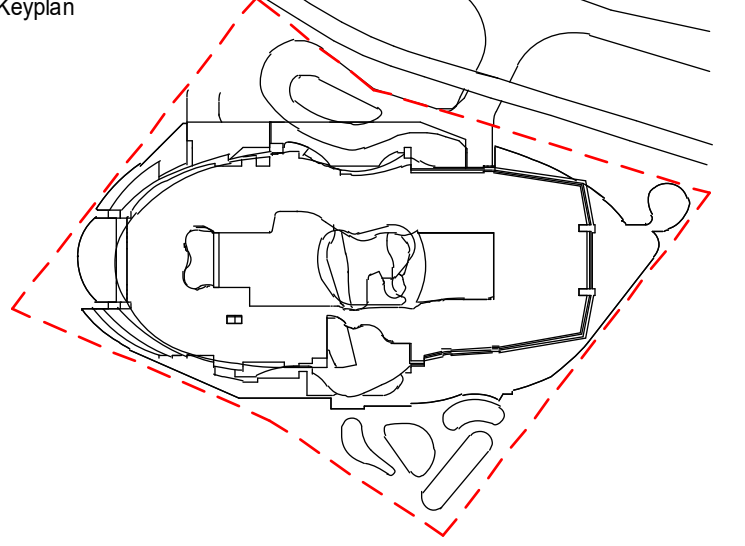
Drawing Title
EXTERNAL FINISHES

Drawing Number	DA-0900	Revision	1
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BUCHAN

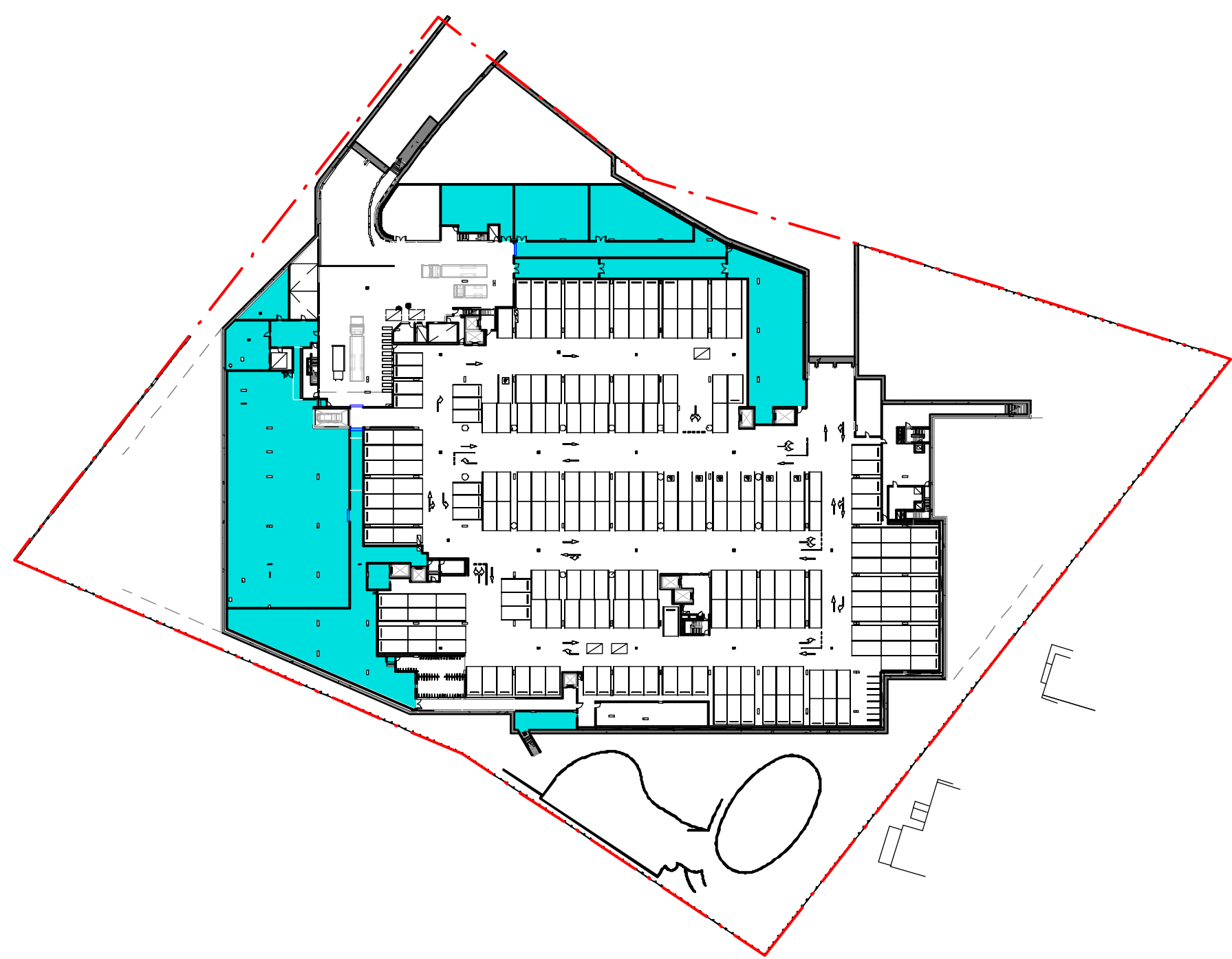
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Keyplan

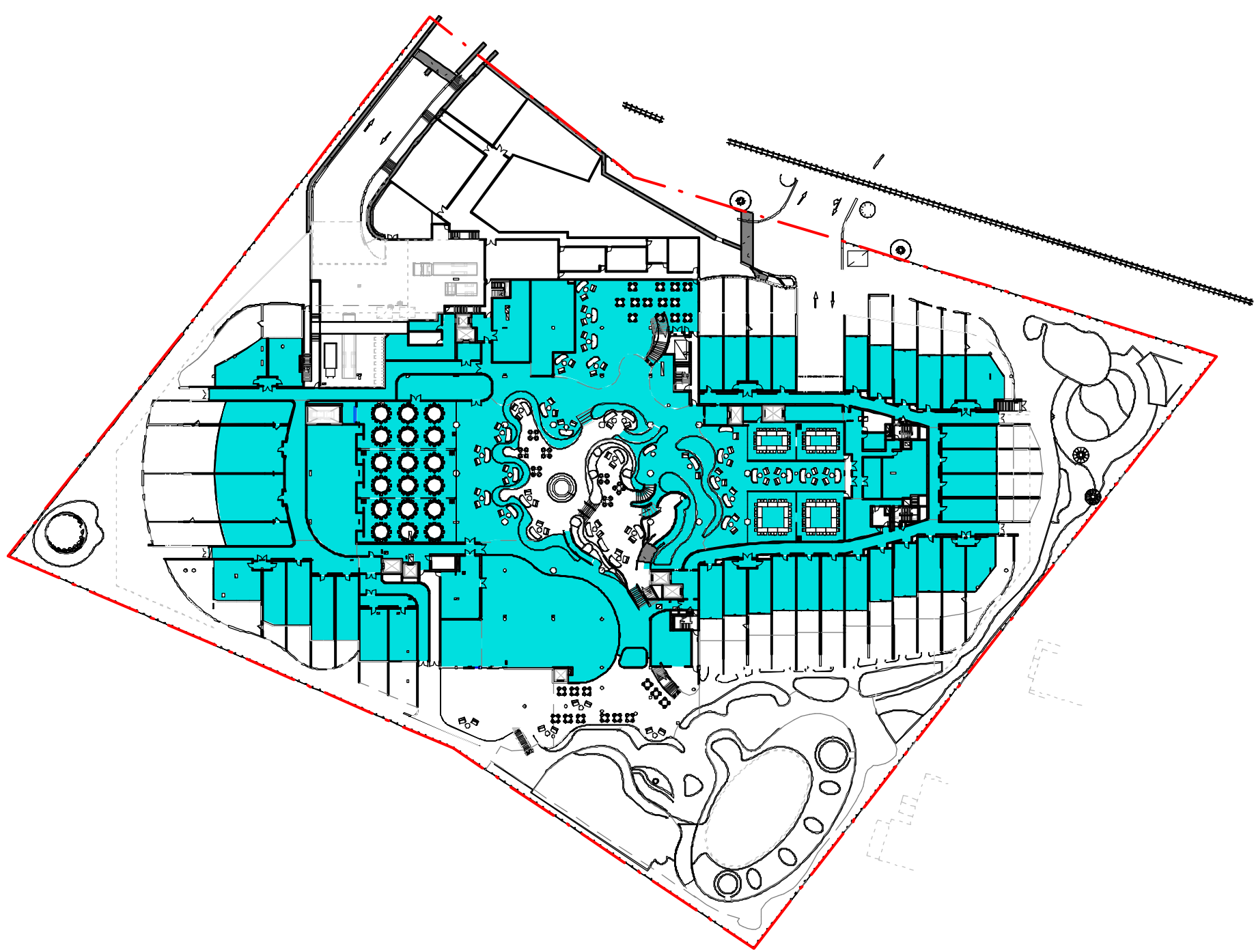


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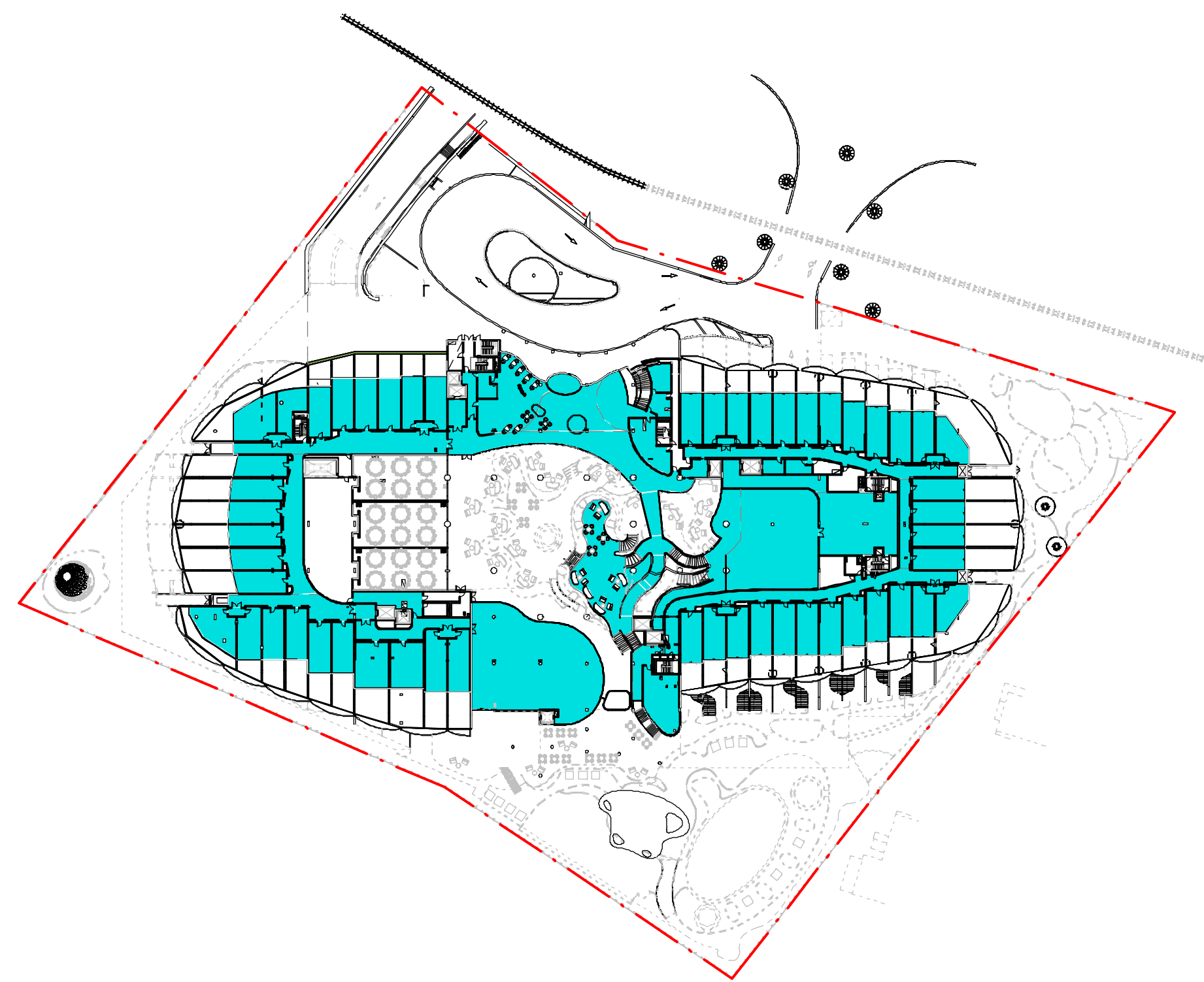
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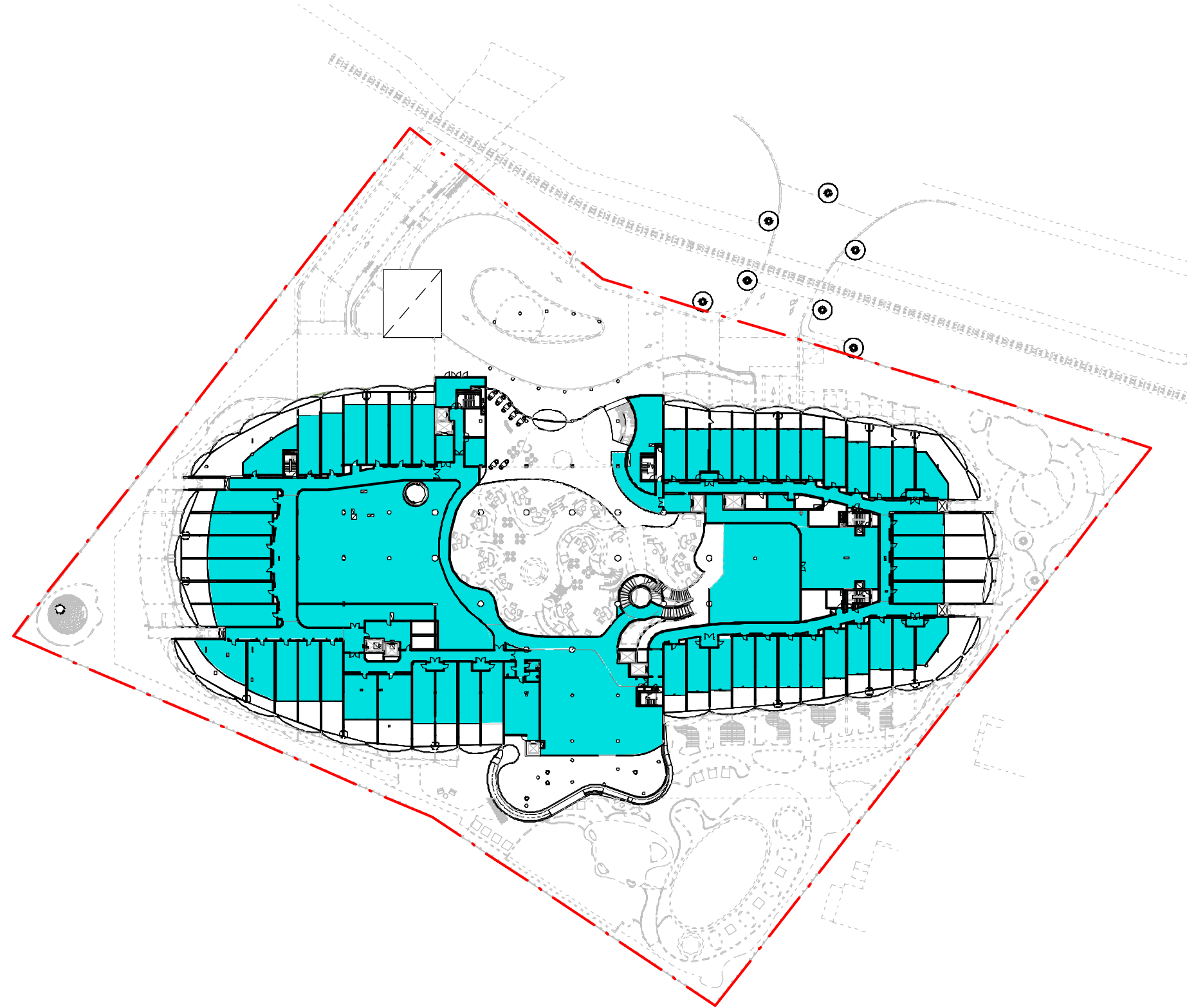
GFA - LEVEL B1
2759 m ²



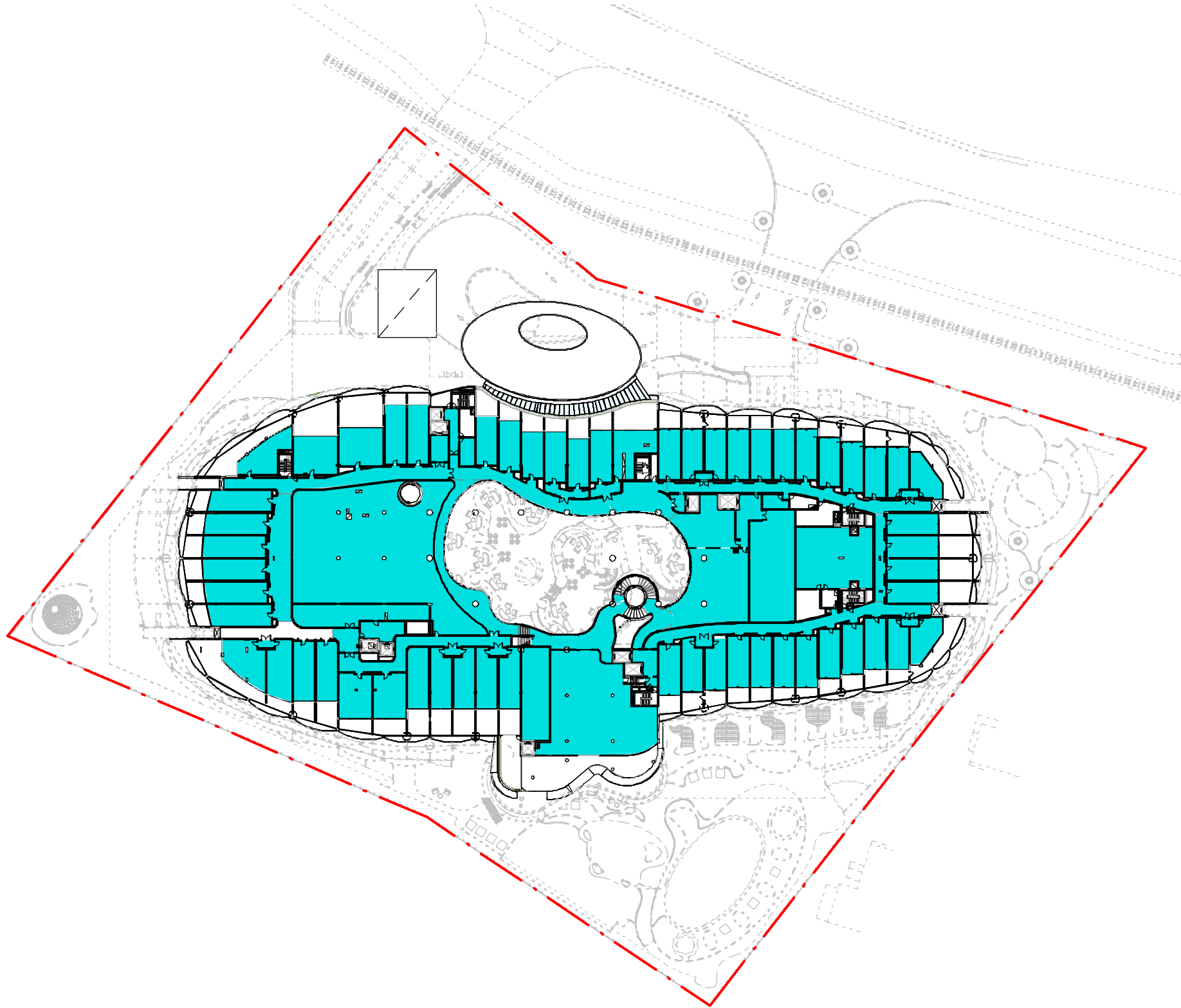
GFA - LEVEL LG
7421 m ²



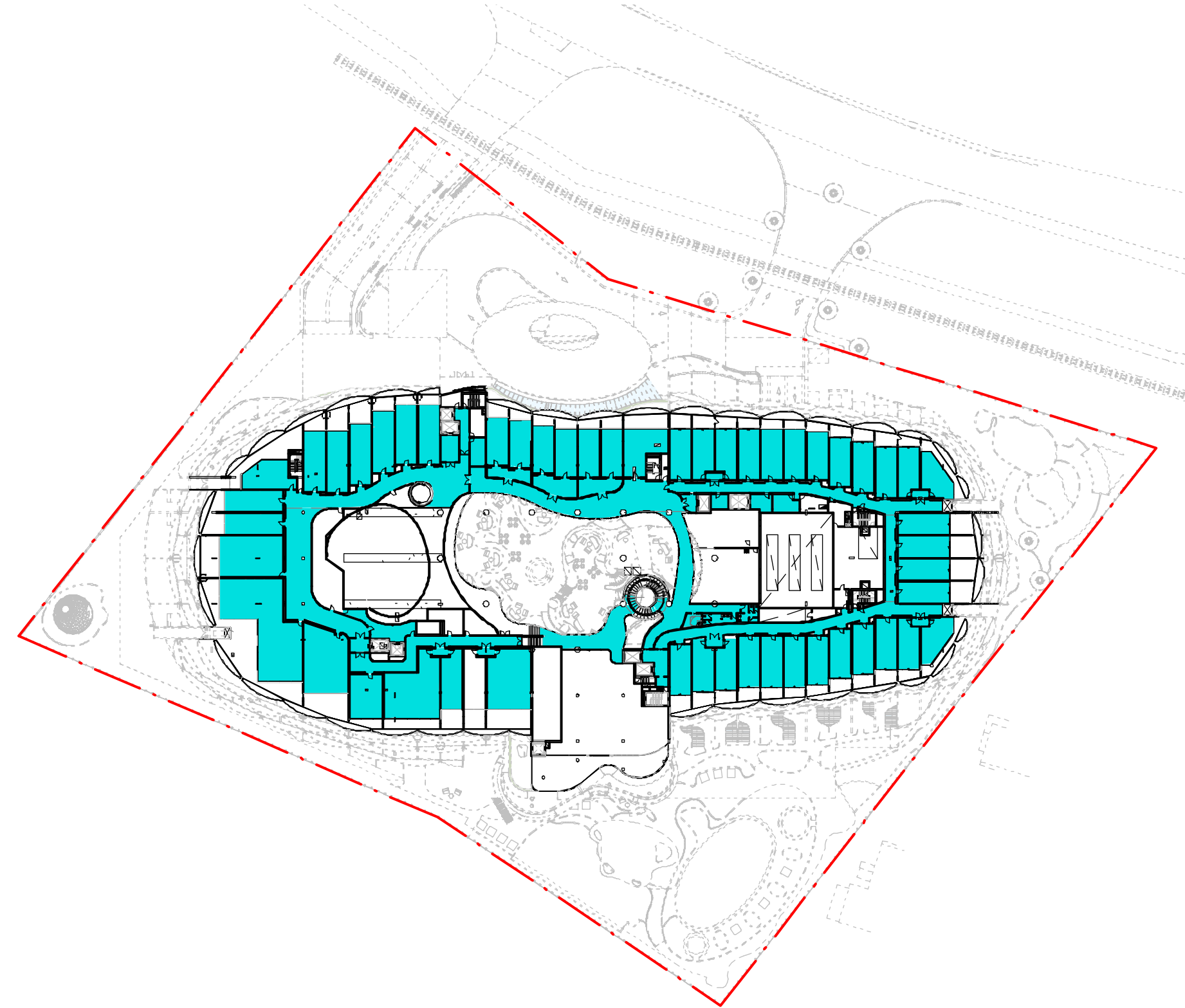
GFA - LEVEL UG
5233 m²



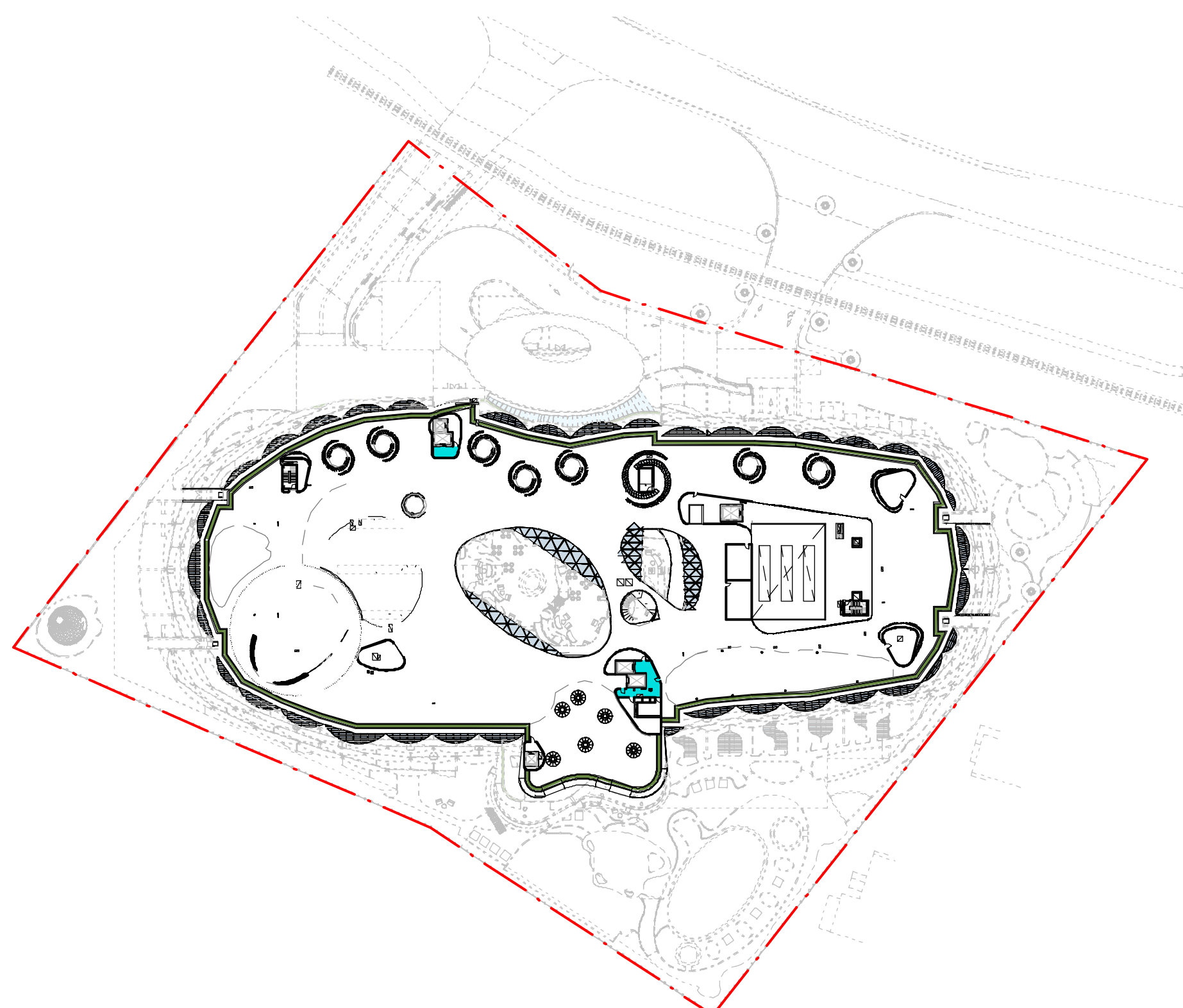
GFA - LEVEL 1
4595 m ²



GFA - LEVEL 2	
5505 m ²	



GFA - LEVEL 3
4332 m ²



GFA - LEVEL 4	
11 m²	

GFA TOTAL BY LEVEL	
TYPE	AREA
LEVEL B1	
BOH	2129 m²
CIRCULATION	64 m²
STORE	565 m²
	2759 m²

LEVEL LG	
1 BED	198 m ²
AMENITIES	203 m ²
BOH	599 m ²
CIRCULATION	1750 m ²
F & B, ALL DAY DINING, CAFE	950 m ²
FUNCTION ROOM	1500 m ²
GOLD ROOM, GOLD RECEPTION, GOLD LOUNGE	503 m ²
PRESIDENTIAL	242 m ²
RETAIL	50 m ²
STANDARD ROOM	1153 m ²
STORE	273 m ²
	7421 m ²

LEVEL UG	
1 BED	257 m²
AMENITIES	79 m²
BOH	141 m²
CIRCULATION	1726 m²
STANDARD ROOM	2233 m²
STORE	71 m²
	4507 m²

GFA TOTAL BY LEVEL	
TYPE	AREA
LEVEL UM	
BOH	263 m²
CIRCULATION	120 m²
F & B, ALL DAY DINING, CAFE	342 m²
	725 m²

LEVEL 1	
1 BED	314 m ²
AMENITIES	104 m ²
BOH	153 m ²
CIRCULATION	1146 m ²
GOLD ROOM, GOLD RECEPTION, GOLD LOUNGE	1506 m ²
LIFTS,STAIRS, RISERS, LOADING DOCK	2 m ²
STANDARD ROOM	1291 m ²
STORE	79 m ²
	4595 m ²

LEVEL 2	
1 BED	472 m ²
BOH	498 m ²
CIRCULATION	1141 m ²
F & B, ALL DAY DINING, CAFE	307 m ²
GOLD ROOM, GOLD RECEPTION, GOLD LOUNGE	896 m ²
STANDARD ROOM	1565 m ²
STORE	34 m ²
	4912 m ²

LEVEL 2M

GFA TOTAL BY LEVEL	
TYPE	AREA
BOH	111 m²
CIRCULATION	93 m²
FUNCTION ROOM	389 m²
	593 m²

LEVEL 3	
1 BED	130 m²
AMENITIES	37 m²
BOH	88 m²
CIRCULATION	1249 m²
GOLD ROOM, GOLD RECEPTION, GOLD LOUNGE	550 m²
PRESIDENTIAL	467 m²
ROYAL	227 m²
STANDARD ROOM	1548 m²
STORE	37 m²
	4332 m²

LEVEL 4	
CIRCULATION	11 m ²
	11 m ²
	29856 m ²

Project

FAIRMONT RESORT

71-85 Port Douglas Rd, Port Douglas QLD 4877, Australia

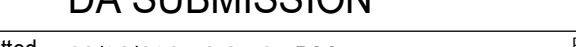
Project Number
219194

Status
DA SUBMISSION

Date Plotted
 29/03/2021 3:27:21 PM

Date Issued
 27/03/2021

Scale
 1 : 1000 @A0



Drawing Title
DETAILS
GFA CALCULATIONS

Drawing Number Revision
DA-0920 **2**

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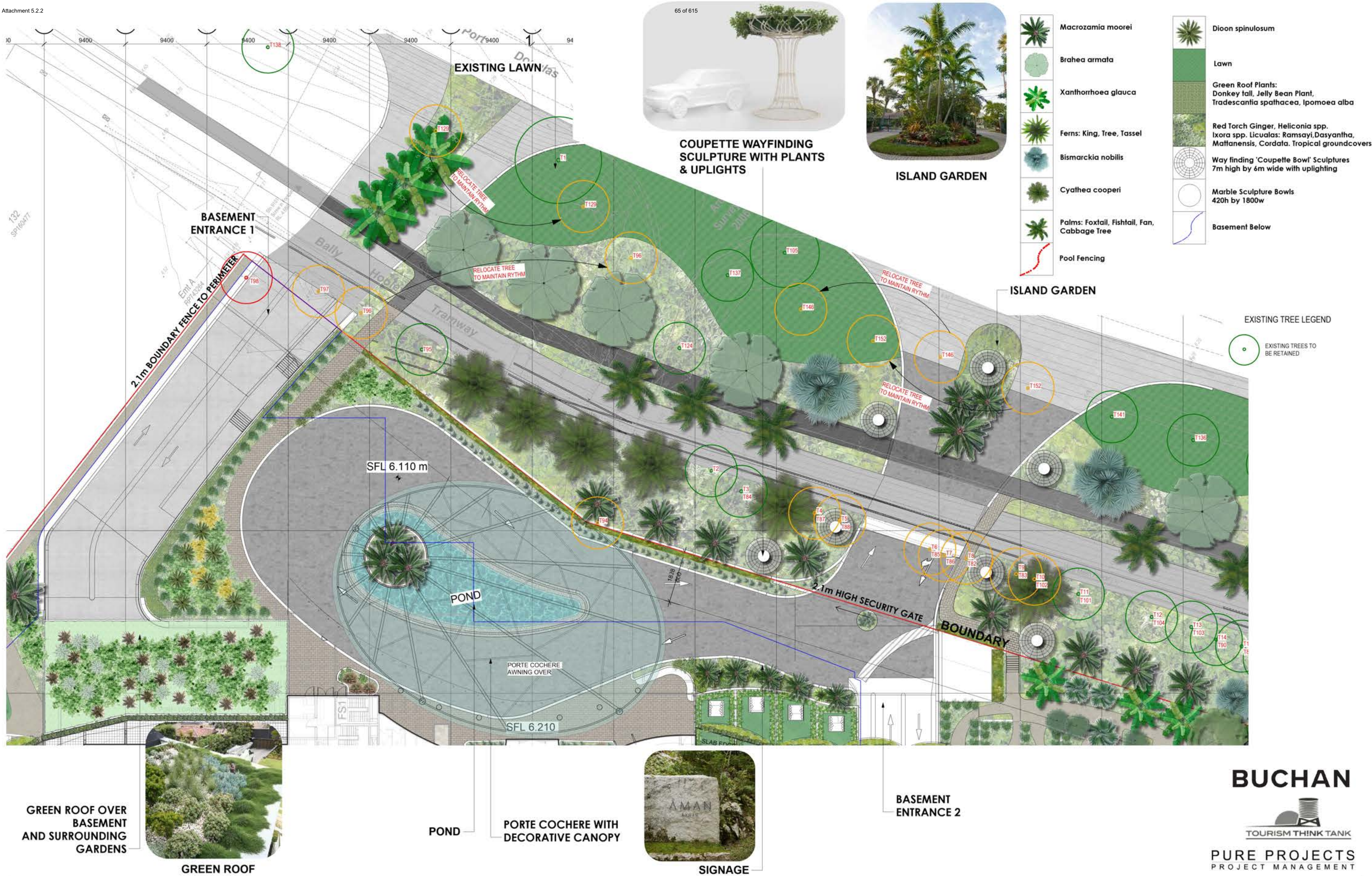
CHIDO

PROJECT
FAIRMONT RESORT PORT DOUGLAS
71-85 PORT DOUGLAS RD PORT DOUGLAS

DRAWING TITLE

**LOWER & UPPER GROUND FLOOR
LANDSCAPE PLAN**

SCALE [A1] 1:500@A1	DATE 23/02/21	DRAWN GA	CHECKED JB
JOB	DRAWING L-01	ISSUE	



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PROJECT
FAIRMONT RESORT PORT DOUGLAS
71-85 PORT DOUGLAS RD PORT DOUGLAS

DRAWING TITLE

MAIN ENTRANCE AREA

SCALE [A1] 1:200@A1	DATE 23/02/21	DRAWN GA	CHECKED JB
JOB	DRAWING L-02	ISSUE	



- Notes
1. ENTRY TO KIDS POOL
 2. WETBEDS WITH WATERFALL BEHIND
 3. WATERSLIDE THROUGH ROCKS AND PLANTING INTO A NATURAL POOL
 4. KIDS CLUB WITH GREEN ROOF
 5. DAYBED AND SWING BED AREA
 6. WATER FEATURE AT CORNER RIVER RIDE AREA
 7. WATER TERRACES
 8. GABION BASKETS WITH MACROZAMIA MOOREII AND ORCHIDS UNDER
 9. SCREEN PLANTING ALONG THE BOUNDARY



DAYBED AREA



KIDS POOL



WATERBEDS WITH NATURAL WATERFALL



GABION BASKET



CORNER RIVER SIDE



WATER TERRACE



	Macrozamia moorei		Dioon spinulosum
	Brahea armata		Lawn
	Xanthorrhoea glauca		Green Roof Plants: Donkey tail, Jelly Bean Plant, Tradescantia spathacea, Ipomoea alba
	Ferns: King, Tree, Tassel		Red Torch Ginger, Heliconia spp. Ixora spp. Licuala: Ramsay, Dasyantha, Maltanensis, Cordata. Tropical groundcovers
	Bismarckia nobilis		Way finding 'Couquette Bowl' Sculptures 7m high by 6m wide with uplighting
	Cyathea cooperi		Marble Sculpture Bowls 420h by 1800w
	Palms: Foxtail, Fishtail, Fan, Cabbage Tree		Basement Below
	Pool Fencing		
EXISTING TREE LEGEND			
	EXISTING TREES TO BE RETAINED		

<p>IMPORTANT NOTES:</p> <p>* This drawing shall be read in conjunction with all Architectural and Engineering Drawings; * All dimensions in millimeters (mm) unless otherwise stated * All dimensions to be checked on-site, written dimensions only to be used; * Refer discrepancies to the Landscape Architect * Do not scale from drawing * The Contractor shall be responsible to locate all services and utilities prior to commencement * All drawings may not be reproduced or distributed without prior permission from Durie Design</p> <p>© 2019 Durie Design. All rights reserved.</p>	<p>AMENDMENTS</p> <p>I 23/02/21 ISSUE FOR DA SUBMISSION</p>	<p>CONSULTANT</p> <p>DURIE DESIGN</p> <p>Sydney Suite 110, 117 Old Pittwater Road, Brookvale 2150 T +61 2 9313 3200</p> <p>Los Angeles 8033 Sunset Boulevard #427 CA 90046 USA T +1 323 654 2206 www.jamieduriedesign.com</p>	<p>NORTH</p>	<p>CLIENT</p> <p>CHIDO</p> <p>PROJECT</p> <p>FAIRMONT RESORT PORT DOUGLAS</p> <p>71-85 PORT DOUGLAS RD PORT DOUGLAS</p>	<p>DRAWING TITLE</p> <p>KIDS POOL AREA</p>			
					SCALE [A1]	DATE	DRAWN	CHECKED
					1:200@A1	23/02/21	GA	JB
					JOB	DRAWING	ISSUE	
						L-03		



SWIMMING POOL AREA



POOL BAR



DAYBED

	Macrozamia moorei		Dioon spinulosum
	Brahea armata		Lawn
	Xanthorrhoea glauca	Green Roof Plants: Donkey tail, Jelly Bean Plant, Tradescantia spathacea, Ipomoea alba	
	Ferns: King, Tree, Tassel		Red Torch Ginger, Heliconia spp. Ixora spp. Licuala: Ramsayi, Dasyantha, Maltanensis, Cordata. Tropical groundcovers
	Bismarckia nobilis		Way finding 'Coupette Bowl' Sculptures 7m high by 6m wide with uplighting
	Cyathea cooperi		Marble Sculpture Bowls 420h by 1800w
	Palms: Foxtail, Fishtail, Fan, Cabbage Tree		Basement Below
	Pool Fencing		



- Notes
1. F&B AREA
 2. DAYBEDS
 3. SUNKEN LOUNGE
 4. LOUNGE AND DAYBED AREA ON SHALLOW POOL
 5. STONE WALKWAYS
 6. MIXED SCREEN PLANTING
 7. POOL BAR
 8. INFINITY EDGE POOL
 9. GARDEN CAFE
 10. SCREEN PLANTING ALONG BOUNDARY

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PURE PROJECTS
PROJECT MANAGEMENT

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NORTH



CLIENT

CHIDO

PROJECT

FAIRMONT RESORT PORT DOUGLAS

71-85 PORT DOUGLAS RD PORT DOUGLAS

DRAWING TITLE

MAIN POOL AREA

SCALE [A1]

1:200@A1

DATE

23/02/21

DRAWN

GA

CHECKED

JB

JOB

DRAWING

L-04

ISSUE



OPEN LAWN



GABION BASKET



YOGA PAVILLION



LOTUS POND

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PROJECT MANAGEMENT

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PROJECT

FAIRMONT RESORT PORT DOUGLAS

71-85 PORT DOUGLAS RD PORT DOUGLAS

DRAWING TITLE

ZEN / YOGA GARDEN

SCALE [A1]

1:200@A1

JOB

DATE
23/02/21

DRAWING

L-05

DRAWN

GA

CHECKED

JB

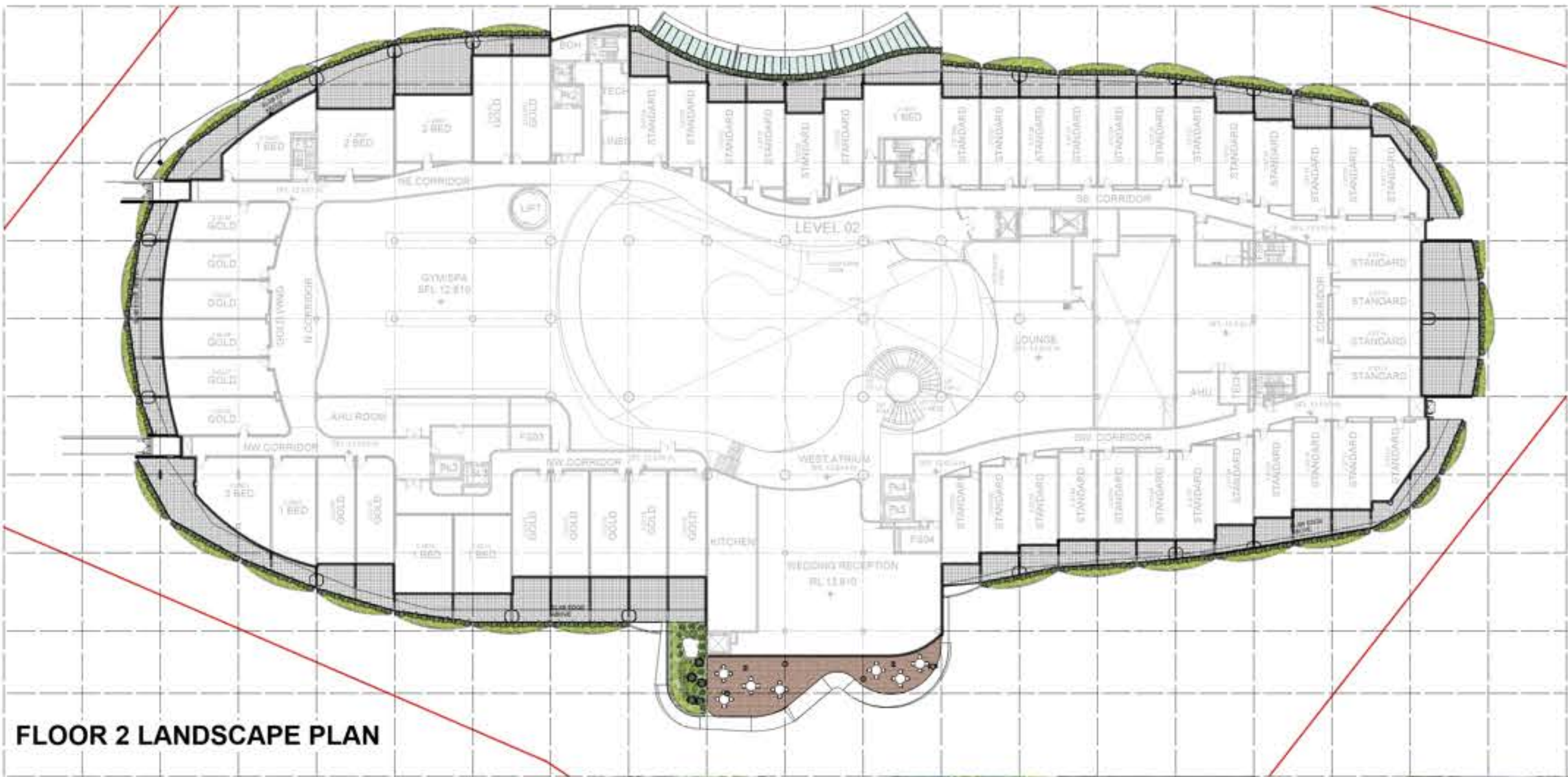
ISSUE



LOWER GROUND FLOOR LOBBY GARDEN & UPPER GROUND FLOOR PLAN



FLOOR 1 LANDSCAPE PLAN



FLOOR 2 LANDSCAPE PLAN



FLOOR 3 LANDSCAPE PLAN



- LEGEND
- Green Roof Balcony Plants:
Donkey tail, Jelly Bean Plant,
Tradescantia spathacea, Ipomoea alba
Climbers Plants
 - Ferns: King, Tree, Tassel
 - Dioon spinulosum

- EXISTING TREE LEGEND
- EXISTING TREES TO
BE RETAINED

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TOURISM THINK TANK

PURE PROJECTS
PROJECT MANAGEMENT

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PROJECT

FAIRMONT RESORT PORT DOUGLAS

71-85 PORT DOUGLAS RD PORT DOUGLAS

DRAWING TITLE

FLOOR GARDENS

SCALE [A1]

1:500@A1

JOB

DATE

23/02/21

DRAWING

L-06

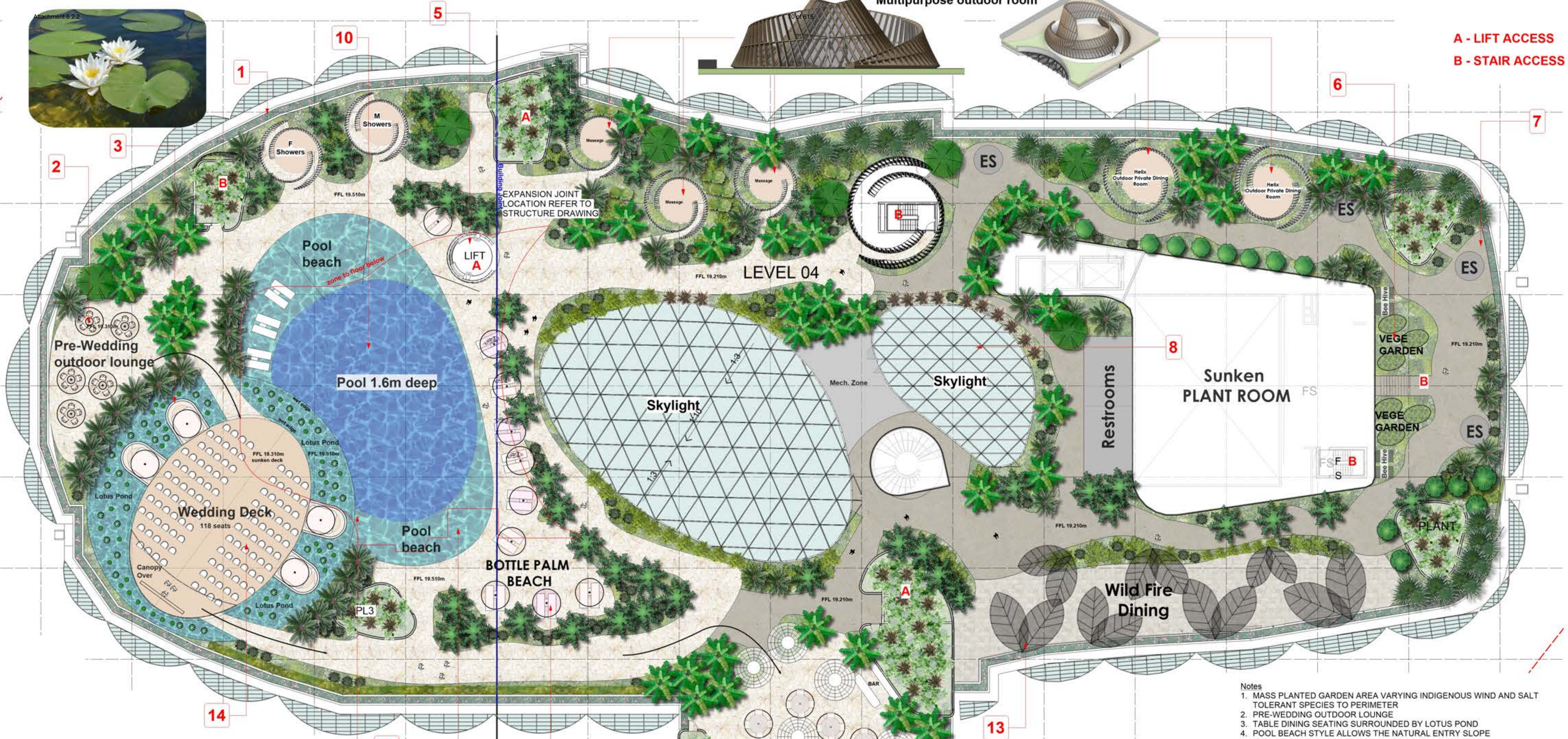
DRAWN

GA

CHECKED

JB

ISSUE



- Macrozamia moorei
- Xanthorrhoea glauca
- Cocos nucifera
- Palms: Golden Cane, Macarthur, Lady Palm, Juraca Palm
- Trees: Bauhinia alba, Bauhinia blakeana, Plumeria
- Xanthostemon crisanthus
- Hymenosporum flavum
- Green Roof Plants: Donkey tail, Jelly Bean Plant, Tradescantia spathacea, Ipomoea alba
- Perimeter Plant: Bougainvillea, Trachelospermum asiaticum, Yellow mandevilla
- Tropical shrubs & groundcovers
- Coupette Sculptures with planting and uplighting



GREEN ROOF



SUNSET BAR COUPETTE



WEDDING DECK CANOPY



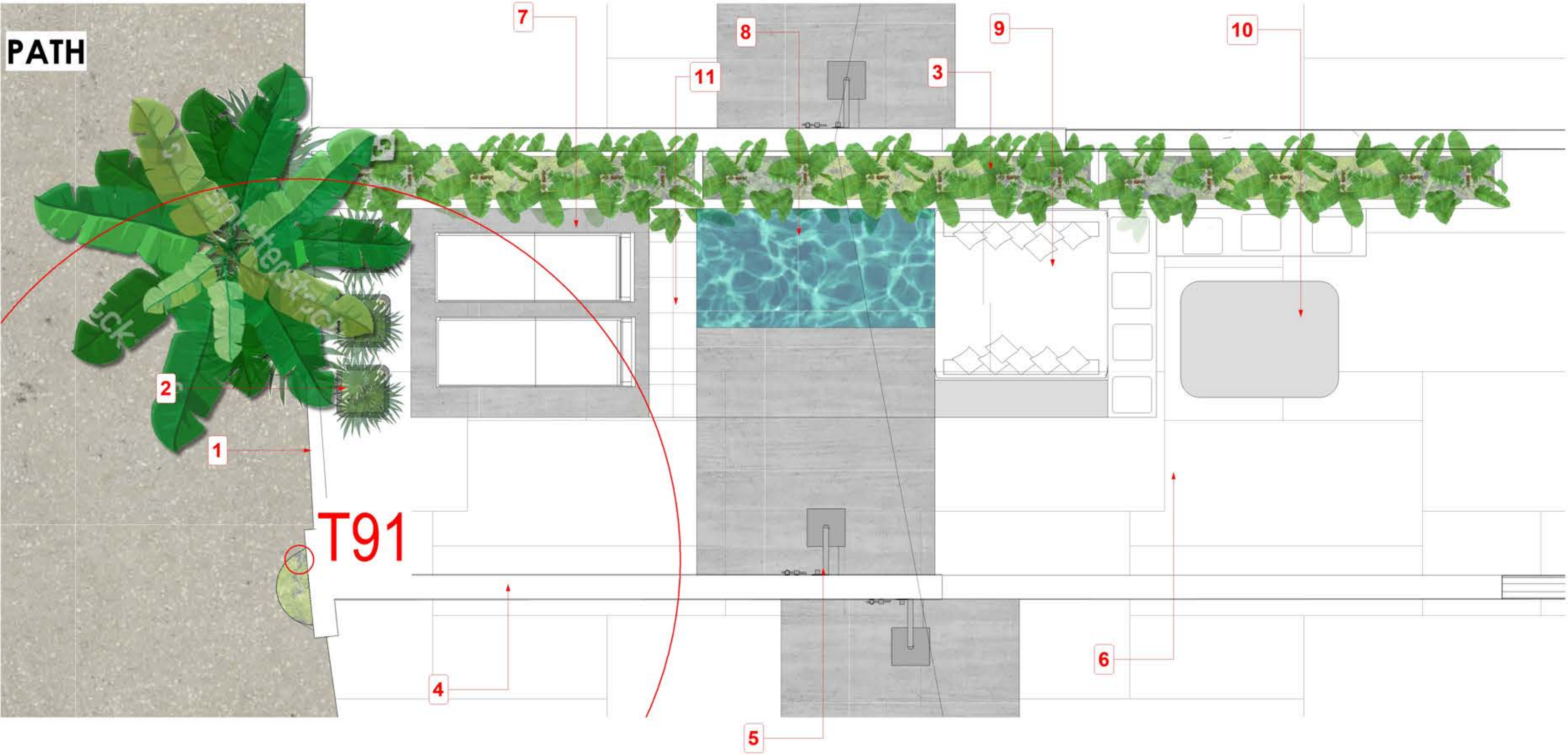
WILDFIRE DINING

- Notes
- MASS PLANTED GARDEN AREA VARYING INDIGENOUS WIND AND SALT TOLERANT SPECIES TO PERIMETER
 - PRE-WEDDING OUTDOOR LOUNGE
 - TABLE DINING SEATING SURROUNDED BY LOTUS POND
 - POOL BEACH STYLE ALLOWS THE NATURAL ENTRY SLOPE
 - LIFT ACCESS
 - VEGETABLE GARDEN WITH BEE HIVES
 - VARYING WIDTH WALKING TRACK FEATURING NUMEROUS ACTIVITIES INCLUDING - BODYWEIGHT WORKOUT EQUIPMENT, BEE HIVES, AND SEATING AREAS
 - SKYLIGHT
 - BOTTLE PALM BEACH AREA FOR SUNBEDS AND VIEWING TO THE WEST
 - POOL 1-1.6M DEEP
 - INFINITY EDGE TO LOTUS POND
 - SUNSET BAR COUPETTES
 - WILD FIRE DINING AREA
 - WEDDING/MULTIUSE DECK
- ES EXERCISE STATION
- MULTI FUNCTIONAL COUPLE'S OUTDOOR ROOMS (YOGA, MASSAGE, DINING, FUNCTIONS)



BUCHAN PURE PROJECTS
PROJECT MANAGEMENT

<div>IMPORTANT NOTES:</div> <div>* This drawing shall be read in conjunction with all Architectural and Engineering Drawings; * All dimensions in millimeters (mm) unless otherwise stated * All dimensions to be checked on-site, written dimensions only to be used; * Refer discrepancies to the Landscape Architect * Do not scale from drawing * The Contractor shall be responsible to locate all services and utilities prior to commencement * All drawings may not be reproduced or distributed without prior permission from Durie Design</div> <div>© 2019 Durie Design. All rights reserved.</div>	AMENDMENTS	<div>CONSULTANT</div> <div>DURIE DESIGN</div> <div>Sydney Suite 110, 117 Old Pittwater Road, Brookvale 2150</div> <div>T +61 2 9313 3200</div> <div>Los Angeles 8033 Sunset Boulevard #427 CA 90046 USA</div> <div>T +1 323 654 2206 www.jamieduriedesign.com</div>	<div>NORTH</div> <div></div>	<div>CLIENT</div> <div></div> <div>CHIDO</div>	<div>DRAWING TITLE</div> <div>ROOFTOP POOL</div>			
				<div>PROJECT</div> <div>FAIRMONT RESORT PORT DOUGLAS</div> <div>71-85 PORT DOUGLAS RD PORT DOUGLAS</div>	<div>SCALE [A1]</div> <div>1:200@A1</div>	<div>DATE</div> <div>23/02/21</div>	<div>DRAWN</div> <div>GA</div>	<div>CHECKED</div> <div>JB</div>
				<div>JOB</div>	<div>DRAWING</div> <div>L-07</div>	<div>ISSUE</div>		



- Notes
- 1. GATE LOCATION
 - 2. FEATURE POTS WITH FEATURE PLANTS
 - 3. PLANTER BOX
 - 4. 2.1m PRIVACY WALL
 - 5. OUTDOOR SHOWER/BATH
 - 6. STEP DOWN 150mm
 - 7. SUN LOUNGER ON TIMBER DECKING
 - 8. PLUNGE POOL 700mm HIGH WITH INFINITY EDGE AND STEP UP
 - 9. DAYBED 860 HIGH WITH STEP UP
 - 10. OUTDOOR LOUNGE
 - 11. POOL FILTER



BUCHAN



PURE PROJECTS
PROJECT MANAGEMENT

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NORTH



CLIENT

CHIDO

PROJECT

FAIRMONT RESORT PORT DOUGLAS

71-85 PORT DOUGLAS RD PORT DOUGLAS

DRAWING TITLE

PRIVATE COURTYARD

SCALE [A1]

1:25@A1

DATE

23/02/21

DRAWN

GA

CHECKED

JB

JOB

DRAWING

L-08

ISSUE



EXISTING TREE LEGEND

- EXISTING TREES TO BE RETAINED
- TREES TO BE REMOVED
- PALMS TO BE RELOCATED

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PURE PROJECTS
PROJECT MANAGEMENT

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CLIENT



PROJECT
FAIRMONT RESORT PORT DOUGLAS
71-85 PORT DOUGLAS RD PORT DOUGLAS

DRAWING TITLE

LANDSCAPE TREE RETENTION & REMOVAL PLAN

SCALE [A1] 1:500@A1	DATE 23/02/21	DRAWN GA	CHECKED JB
JOB	DRAWING L-09		ISSUE



901
Hardscape Materials
Fairmont Port Douglas

Limestone



Noce Travertine Tumbled



Silver Ash Premium Travertine



Atlantic Grey Granite - Lobby Area



Desertsand Granite Paving - Er



Silhouette Grey Granite - Entrance Driveway



Limestone



BUCHAN



PURE PROJECTS
PROJECT MANAGEMENT

<div>IMPORTANT NOTES:</div> <div><div><div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><d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INDICATIVE PLANTING SCHEDULE			
ID	Botanical Name	Common Name	Scheduled Size
Trees			
AJ	Albizia julibrissin	Persian silk tree	45L
BD	Banksia dentata	tropical banksia	45L
BC	Barringtonia calyptrate	Mango Pine	45L
BA	Bauhinia alba	White Orchid Tree, Love Tree	45L
BV	Bauhinia variegata	Orchid Tree	45L
BB2	Bauhinia x blakeana	Hongkong Orchid Tree	45L
BB	Bauhinia x blakeana	Hongkong Orchard Tree	45L
BR	Beaucarnea recurvata	Elephant Foot Tree, Ponytail	45L
BC3	Bombax ceiba	Red Silk Cotton Tree	45L
BA2	Brachychiton acerifolius	Illawarra Flame Tree	45L
BD2	Brachychiton discolor	Old Lacebark	45L
BR2	Brachychiton rupestris	Bottle Tree	45L
BC2	Brownia coccinea	Scarlet Flame Bean	45L
CF	Caesalpinia ferrea	Leopard Trees	45L
CH	Caillandra haematocephala	Powder Puff Tree	45L
CB	Cassia brewsteri	Leichhardt Bean	45L
CF2	Cassia fistula	Golden Shower Tree	45L
DT	Deplanchaea tetraphylla	Golden Bouquet Tree	45L
FB	Fagraea berteriana	Native Gardenia	45L
FB2	Ficus benjamina	Benjamin Fig	45L
FP	Ficus pseudopalma	Dracaena Fig	45L
FR	Ficus racemosa	Cluster Fig	45L
FR2	Ficus religiosa	Sacred Fig	45L
GS	Gustavia superba	Gustavia	45L
HA	Harpullia arborescens	Cooktown Tulipwood	45L
HT	Hibiscus tiliaceus	Cottonwood	45L
HF	Hymenosporum flavum	Native Frangipani	45L
MQ	Melaleuca quinquenervia	Paper Bark	45L
ME	Melicope eileryana	Pink Flowered Cork Wood	45L
PS	Pandanus solms-laubachii	Pandanus	45L
PU	Pandanus utilis	Screw Pine	45L
PP	Peltophorum pterocarpum	Yellow Flamboyant Tree	45L
PR	Peltophorum rhombifolium	Old Pittosporum	45L
PO	Plumeria obtusa	Evergreen Plumeria	45L
PR2	Plumeria rubra	Frangipani	45L
PR3	Plumeria rubra var. acutifolia	Frangipani	45L
PLP	Polyalthia longifolia pendula	Indian Mass Tree	45L
RM	Ravenala madagascariensis	Traveler's Palm	45L
SA	Syzygium angophoroides	Yarrabah Satinash	45L
SA2	Syzygium australe	Scrub Cherry	45L
SJ	Syzygium jambos "Rose apple"	Rose Apple	45L
SP	Syzygium paniculatum	Bush Cherry	45L
TC	Tabebuia chrysostricha	Golden Trumpet Tree	45L
TH	Tabebuia heterophylla	Pink Trumpet Tree	45L
TR	Tabebuia rosea	Rosy Trumpet Tree	45L
XC	Xanthostemon chrysanthus	Golden Penda	45L
Fruit Tree			
CP	Carica papaya & cvs	Pawpaw	45L
CA	Citrus australasica	Finger Lime	45L
CH2	Citrus hystrix	Kaffir Lime	45L
CM	Garcinia mangostana	Mangosteen	45L
LC	Litchi chinensis	Lychee	45L
MI	Mangifera indica- drawf cvs	Mango	45L
NL	Nephelium lappaceum	Rambutan	45L
PG	Psidium guajava	Guava	45L
TI	Tamarindus indica	Tamarind	45L
Shrub			
abu-hyb	Abutilon x hybridum	Chinese Lantern	200mm
aca-myr	Acacia myrtifolia	Myrtle Wattle	200mm
alp-coe	Alpinia coerulea	Native Ginger	200mm
alp-pur	Alpinia purpurata	Red Ginger	200mm
alp-zer	Alpinia zerumbet & cv	Shell Ginger	200mm
alp-zer2	Alpinia zerumbet 'Variegata'	Variegated Shell Ginger	200mm
ant-and	Anthurium andraenum & cvs	Flamingo Flower	200mm
bau-gal	Bauhinia galpinii	South African Orchid Tree	200mm
bau-pet	Bauhinia petersiana	Coffee Neat's Foot	200mm
bau-tom	Bauhinia tomentosa	Yellow Bauhinia	200mm
beg-met	Begonia metallica	Metal Leaf Begonia	200mm
beg-rex	Begonia rex cvs	Begonia	200mm
cae-gil	Caesalpinia gilliesii	Desert Bird of Paradise	200mm
cae-pul	Caesalpinia pulcherrima	Pride of Barbados	200mm
cal-zeb	Calathea zebrina	Zebra Plant	200mm
can-gen	Canna x generalis	Canna	200mm
cor-fru	Cordyline fruticosa & cvs	Ti Tree	200mm
cri-ped	Crinum pedunculatum	Swamp Lily	200mm
cte-bur	Ctenanthe burle-marxii	Ctenanthe	200mm
cte-lub	Ctenanthe lubbersiana	Bamburanta	200mm
dfe-seg	Dieffenbachia seguine & cvs	Spotted Dumb Cane	200mm
eie-car	Elettaria cardamomum	Cardamon	200mm
hib-sch	Hibiscus schizopetalus	Fringed Rose Mallow	200mm
ire-her	Iresine herbstii	Beefsteak Plant	200mm
ixo-coc	Ixora coccinea & cvs	Jungle Flame	200mm
lee-coc	Leea coccinea cv 'Burgundy'	West Indian Holy	200mm
lep-art	Lepironia articulata	Cigar Rush	200mm
pit-tob	Pittosporum tobira & cv	Japanese Mock Orange	200mm
plu-ind	Plumbago indica	Scarlet Leadwort	200mm
pol-gui	Polyscias guilfoylei	Geranium aralia	200mm
ron-am	Rondeletia amoena	Rondeletia	200mm
str-alb	Strelitzia alba	Great White Strelitzia	200mm
str-nic	Strelitzia nicolai	Bird of Paradise Tree	200mm
str-reg	Strelitzia reginae	Bird of Paradise	200mm
str-reg2	Strelitzia reginae var. juncea	Rush-Like Strelitzia	200mm
str-reg3	Strelitzia reginae var. parvifolia	Small Leafed Strelitzia	200mm
tha-gen	Thalia geniculata rubiginoides	Red Thalia	200mm
typ-dom	Typha domingensis	Narrow Leaf Cumbungi	200mm
typ-ori	Typha orientalis	Bullrush	200mm
yuc-gua	Yucca guatemalensis & cvs	Giant Yucca	200mm
yuc-ros	Yucca rostrata	Beaked Yucca	200mm

Palms, Bamboo and Cycads			
ang-eve	Angiopteris evecta	Giant Fern	200mm
arc-ale	Archontophoenix alexandrae	Alexandra Palm	200mm
arc-tri	Areca triandra	Triandra Palm	200mm
arc-ves	Areca vestlaria	Betel Nut	200mm
arc-aus	Arenga australasica	Native Sugar Palm	200mm
asp-aus	Asplenium australasicum	Bird's Nest Fern	200mm
asp-bul	Asplenium bulbiferum	Mother Spleenwort	200mm
asp-nid	Asplenium nidus	Bird's Nest Fern	200mm
bam-fun	Bambusa funghomii	Canebrake	200mm
bam-lak	Bambusa lako	Timor Black Bamboo	200mm
bam-mul	Bambusa multiplex 'Fernleaf'	Fernleaf Hedge Bamboo	200mm
bam-ven	Bambusa ventricosa	Buddha Belly Bamboo	200mm
bis-nob	Bismarckia nobilis	Bismarck Palm	200mm
ble-bra	Blechnum brassiliense	Water Fern	200mm
but-cap	Butia capitata	Jelly Palm, Wine Palm	200mm
but-cap-1	Carludovica palmata "Panama Hat"	Panama Hat Palm	200mm
cha-ele	Chamaedorea elegans	Parlor Palm	200mm
cha-met	Chamaedorea metallica	Metallic Palm	200mm
cha-sei	Chamaedorea seifrizii	Reed Palm, Cane Palm	200mm
cha-hum	Chamaerops humilis & cvs	Dwarf Fan Palm	200mm
chr-lut	Chrysalidocarpus lutescens	Golden Cane Palm	200mm
coc-cri	Coccothrinax crinita	Old Man Palm	200mm
coc-nuc	Cocos nucifera	Coconut	200mm
coc-nuc2	Cocos nucifera 'Malay Dwarf'	Dwarf Coconut Palm	200mm
cyc-med	Cycas media	Zamia Palm	200mm
cyc-tho	Cycas thouarsii	Malagasy Cycad	200mm
cyp-pap	Cyperus papyrus	Papyrus, Egyptian Paper Reed	200mm
cyr-ren	Cyrtostachys renda	Lipstick Palm	200mm
den-asp	Dendrocalamus asper	Black Asper Bamboo	200mm
dio-alb	Dictyosperma album	Princess Palm, Hurricane Palm	200mm
dio-edu	Dioon edule	Mexican Fern Palm	200mm
dyp-dec	Dypsis decaryi	Triangle Palm	200mm
dyp-lut	Dypsis lutescens	Golden Feather Palm	200mm
enc-alt	Encephalartos altensteinii	Prickly Cycad	200mm
enc-fer	Encephalartos ferox	Zululand Cycad	200mm
enc-hir	Encephalartos hirsutus	Hairy Cycad	200mm
enc-hor	Encephalartos horridus	Ferocious Blue Cycad	200mm
enc-leh	Encephalartos lehmannii	Karoo Cycad	200mm
eut-edu	Euterpe edulis	Jucara Palm	200mm
eut-ole	Euterpe oleracea	Assai Palm	200mm
eut-pre	Euterpe precatoria	Mountain Cabbage Palm	200mm
gig-atr	Gigantochloa atrovioleacea	Tropical Black Bamboo	200mm
gig-rob	Gigantochloa robusta	Robust Bamboo	200mm
hyd-wen	Hydriastele wendlandiana	Florence Falls Palm	200mm
hyo-lag	Hyophorbe lagenicaulis	Bottle Palm	200mm
hyo-ver	Hyophorbe verschaffeltii	Spindle Palm	200mm
lep-hop	Lepidozamia hopei	Zamia	200mm
lic-gra	Licuala grandis	Vanuatu Fan Palm	200mm
lic-ram	Licuala ramsayi	Native Fan Palm	200mm
liv-mue	Livistona muelleri	Cabbage Tree Palm	200mm
mac-moo	Macrozamia moorei	Springsure Zamia	200mm
med-arg	Medemia argun	Argun Palm	200mm
mic-mus	Microsorium musifolium "Crocodyllus"	Crocodyllus Fern	200mm
nan-rit	Nannorrhops ritchiana	Mazzari Palm	200mm
pho-bor	Phoenicophorium borsigianum	Lattianier Feuille	200mm
pla-bif	Platynerium bifurcatum	Elkhorn Fern	200mm
pla-sup	Platynerium superbum	Staghorn Fern	200mm
pri-pac	Pritchardia pacifica	Fiji Fan Palm	200mm
pri-pac2	Pritchardia pacifica2	Fiji Fan Palm	200mm
pri-thu	Pritchardia thurstonii	Lau Fan Palm	200mm
pse-sar	Pseudophoenix sargentii	Cherry Palm	200mm
pty-ele	Ptychosperma elegans	Alexander Palm	200mm
pty-mac	Ptychosperma macarthurii	Macarthur Palm	200mm
rha-hum	Rhapis humilis	Slender Lady Palm	200mm
roy-reg	Roystonea regia	Royal Palm	200mm
thy-max	Thyrsostachys maxima	Tiger Grass	200mm
thy-sia	Thyrsostachys siamensis	Monastery Bamboo	200mm
tra-for	Trachycarpus fortunei	Chinese Windmill Palm	200mm
vei-joa	Veitchia joannis	Veitchia	200mm
vei-mer	Veitchia merrillii	Manila Palm	200mm
ver-spl	Verschaaffeltia splendida	Seychelles Stilt Palm	200mm
wod-bif	Wodyetia bifurcata	Foxtail Palm	200mm
zam-fur	Zamia furfuracea	Cardboard Palm	200mm
zam-lod	Zamia loddigesii	Cycad	200mm
zam-pum	Zamia pumila	Florida Arrowroot	200mm

Groundcovers and Climbers			
bou-cvs	Bougainvillea cvs	Bougainvillea	150mm
can-ros	Canavalia rosea	Canavalia	150mm
cos-pro	Costus productus	Spiral Flag	150mm
cym-spp	Cymbidium spp. & hybrids	Orchid	150mm
den-spp	Dendrobium spp. & hybrids	Dendrobium Orchid	150mm
dis-lax	Distictis laxiflora	Vanilla Trumpet Vine	150mm
fit-ver	Fittonia verschaffeltii	Nerve Plant	150mm
hoy-aus	Hoya australis	Hoya, Wax Flower	150mm
ipo-alb	Ipomoea alba	Moonflower	150mm
man-lax	Mandevilla laxa	Chilean Jasmine	150mm
man-san	Mandevilla sanderi & cvs	Chilean Jasmine	150mm
pas-coc	Passiflora coccinea	Red Flowering Passionfruit	150mm
pen-lut	Pentalinon luteum	Yellow Mandevilla	150mm
pen-lan	Pentas lanceolata & cvs	Star Cluster	150mm
pet-vol	Petrea volubilis	Purple Wreath	150mm
phi-xan	Philodendron 'Xanadu'	Xanadu Philodendron	150mm
sed-mor	Sedum morganianum	Donkey Tail	150mm
sed-rub	Sedum rubrotinctum	Jelly Bean Plant	150mm
sol-max	Solandra maxima	Golden Chalice Vine	150mm
ste-flo	Stephanotis floribunda	Madagascar Jasmine	150mm
til-tec	Tillandsia tectorum	Air Plant	150mm
til-usn	Tillandsia usneoides	Spanish Moss	150mm
tra-asi	Trachelospermum asiaticum & cvs	Trachelospermum	150mm
tra-jas	Trachelospermum jasminoides	Star Jasmine	150mm
tra-pal	Tradescantia pallida	Purple Heart	150mm
tra-spa	Tradescantia spathacea	Moses in the Cradle	150mm
yuc-whi	Yucca whipplei	Our Lord's Candle	150mm
Accents			

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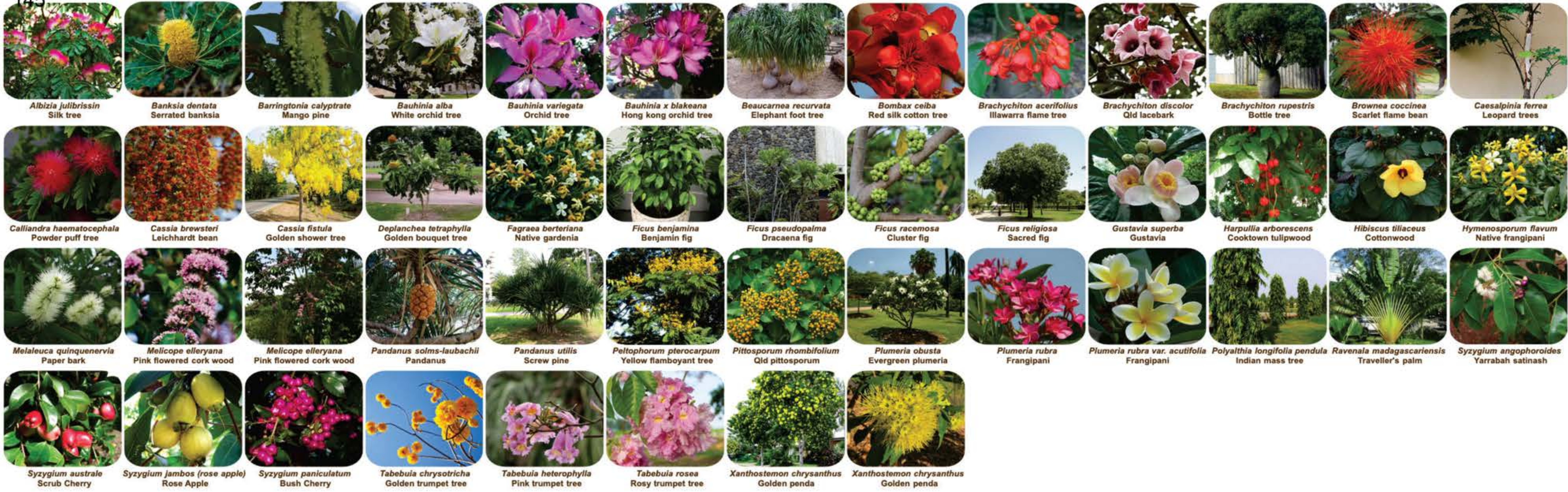


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TREES

145



SHRUB



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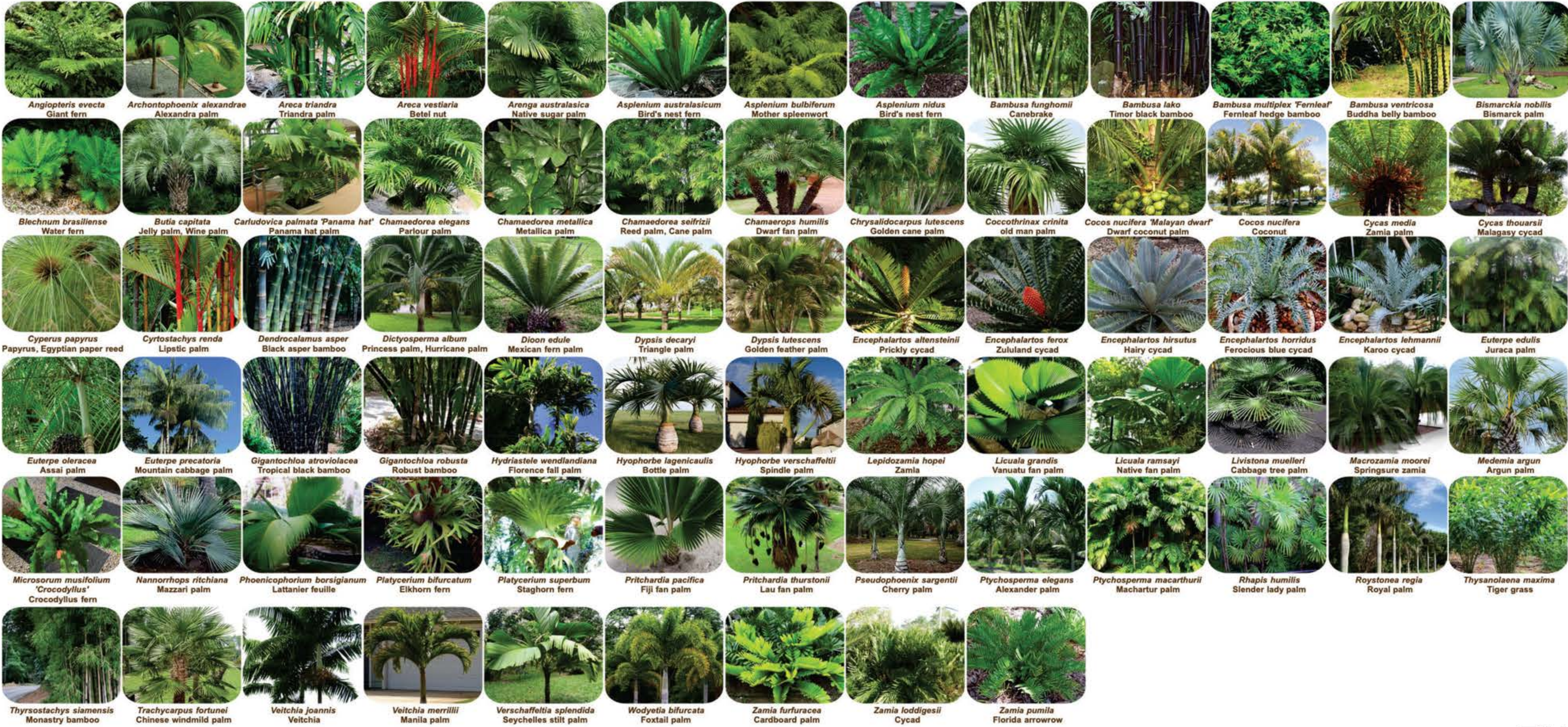
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					PROJECT	SCALE [A1]	DATE	DRAWN	CHECKED
		H 23/02/21 ISSUE FOR DA SUBMISSION			FAIRMONT RESORT PORT DOUGLAS	N.T.S@A1	23/02/21	GA	JB
				71-85 PORT DOUGLAS RD PORT DOUGLAS	JOB	DRAWING		ISSUE	
						L-12			

146 FRUIT TREES



PALMS, BAMBOO & CYCADS



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	H 23/02/21 ISSUE FOR DA SUBMISSION				PROJECT FAIRMONT RESORT PORT DOUGLAS 71-85 PORT DOUGLAS RD PORT DOUGLAS	SCALE [A1] N.T.S@A1	DATE 23/02/21	DRAWN GA
					JOB	DRAWING L-13	ISSUE	

GROUNDCOVERS & CLIMBERS



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					SCALE [A1] N.T.S@A1	DATE 23/02/21	DRAWN GA	CHECKED JB
					PROJECT FAIRMONT RESORT PORT DOUGLAS 71-85 PORT DOUGLAS RD PORT DOUGLAS	JOB	DRAWING L-14	ISSUE



EXISTING TREES TO
BE RETAINED



TOURISM THINK TANK

PURE PROJECTS
PROJECT MANAGEMENT



SARA reference: 2101-20652 SRA
Council reference: MCUI2020_3711/1

21 May 2021

Chief Executive Officer
Douglas Shire Council
PO Box 723
Mossman Qld 4873
enquiries@douglas.qld.gov.au

Attention: Mr Daniel Lamond

Dear Mr Lamond

SARA response—Fairmont Resort Complex, Port Douglas

(Referral agency response given under section 56 of the *Planning Act 2016*)

The development application described below was confirmed as properly referred by the State Assessment and Referral Agency on 20 January 2021.

Response

Outcome:	Referral agency response – with conditions.
Date of response:	21 May 2021
Conditions:	The conditions in Attachment 1 must be attached to any development approval.
Advice:	Advice to the applicant is in Attachment 2 .
Reasons:	The reasons for the referral agency response are in Attachment 3 .

Development details

Description:	Development permit	Material Change of Use for a Resort Complex
SARA role:	Referral Agency	
SARA trigger:	Schedule 10, Part 9, Division 4, Subdivision 1, Table 1. Item .1 - Infrastructure - State transport infrastructure (Planning Regulation 2017)	
	Schedule 10, Part 9, Division 4, Subdivision 2, Table 4, Item 1 - State	

transport corridors and future state transport corridors

SARA reference: 2101-20652 SRA

Assessment Manager: Douglas Shire Council

Street address: 71-85 Port Douglas Road, Port Douglas

Real property description: Lot 1 on 1SP150468

Applicant name: Chiodo Corporation

Applicant contact details: PO Box 930
Townsville QLD 4810
Erin.Campbell@ghd.com

State-controlled road access permit: This referral included an application for access to a state-controlled road under section 62A(2) of Transport Infrastructure Act 1994. Below are the details of the decision:

- Approved
- Reference: TMR21-032012 (500-1509)
- Date: 31 March 2021

Further information on the road access permit can be obtained by contacting the Department of Transport and Main Roads at Far.North.Queensland.IDAS@tmr.qld.gov.au.

Representations

An applicant may make representations to a referral agency, at any time before the application is decided, about changing a matter in the referral agency response (s.30 Development Assessment Rules) Copies of the relevant provisions are in **Attachment 4**.

A copy of this response has been sent to the applicant for their information.

For further information please contact Danielle Harris, Principal Planner, on 3452 7654 or via email DAAT@dsdmip.qld.gov.au who will be pleased to assist.

Yours sincerely



Steve Conner
Executive Director

cc Chiodo Corporation, Erin.Campbell@ghd.com

enc Attachment 1 - Referral agency conditions
Attachment 2 - Advice to the applicant
Attachment 3 - Reasons for referral agency response
Attachment 4 – Change representation provisions
Attachment 5 - Approved plans and specifications

Attachment 1—Referral agency conditions

(Under section 56(1)(b)(i) of the *Planning Act 2016* the following conditions must be attached to any development approval relating to this application) (Copies of the plans and specifications referenced below are found at Attachment 5)

No.	Conditions	Condition timing
Material change of use		
The chief executive administering the <i>Planning Act 2016</i> nominates the Director-General of Department of Transport and Main Roads to be the enforcement authority for the development to which this development approval relates for the administration and enforcement of any matter relating to the following condition(s):		
1.	<p>(a) The development must provide at least one parking bay that is capable of accommodating a vehicle that is suitable for use by people with disabilities.</p> <p>(b) The parking bay required in part (a) of this condition must be in accordance with the following:</p> <ul style="list-style-type: none"> Disability Standards for Accessible Public Transport 2002 - subsection 31(1) of the <i>Disability Discrimination Act 1992</i>; AS1428.1 – Design for Access and Mobility; and AS2890.6 – Parking Facilities, Part 6: Off-street parking for people with disabilities be sign posted with a passenger loading zone signs - R5-23 (passengers – 15 minute). 	(a) and (b) Prior to the commencement of use and to be maintained at all times
2.	<p>(a) The development must provide a private 'bus set down' facility and 'bus park' and be able to provide forward gear manoeuvring on site for a single unit ridged bus of 14.5m in length.</p> <p>(b) The private 'bus set down' required in part (a) of this condition must be in accordance with the <i>Disability Standards for Accessible Public Transport 2002</i> made under subsection 31(1) of the <i>Disability Discrimination Act 1992</i> and must include the following:</p> <ul style="list-style-type: none"> capacity for 1 x single unit rigid bus of 14.5m in length with a bus bay length of 27m, width of 3m and departure length of 10m two (2) bus zone signs R5-20, one at either end of the private bus zone, in accordance with AS1742.11 – Manual of uniform traffic control devices, Part 11: Parking controls access and hardstand for waiting and boarding/alighting passengers. <p>(c) The private 'bus park' required in part (a) of this condition must include the following:</p> <ul style="list-style-type: none"> 1 bus parking bay for a single minibus of 7m in length bus zone signs R5-20 in accordance with AS1742.11 – Manual of uniform traffic control devices, Part 11: Parking controls. 	(a) – (c) Prior to the commencement of use and to be maintained at all times
3.	(a) The development must be carried in accordance with Section	(a) At all times.

	<p>5.2 and 5.2.1 – Stormwater Management of the CIVIL ENGINEERING REPORT prepared by Northrop, DATE: 31.08.20, Ref: SY200372-CR01, Rev: 1.</p> <p>(b) Provide certification from a Registered Professional Engineer of Queensland certification, with supporting documentation, to far.north.queensland.idas@tmr.qld.gov.au within the Department of Transport and Main Roads, confirming that the development has been designed and constructed in accordance with part (a) of this condition.</p>	(b) Prior to the commencement of use.
4.	<p>(a) The existing vehicular property access located between Lot 1 on SP150468 and Port Douglas Road must be permanently closed and removed.</p> <p>(b) The table drain between the pavement edge and the property boundary must be reinstated in accordance with TMR's Road Planning and Design Manual and standard drawing at no cost to the Department of Transport and Main Roads'.</p>	Prior to the commencement of use
5.	<p>(a) The road access locations are to be located generally in accordance with TMR Layout Plan (6504 – 2.65km), prepared by Queensland Government Transport and Main Roads, DATE: 31/03/2021, File Ref: TMR21-32012 (500-1509), Issue: A.</p> <p>(b) Road access works comprising of the following must be provided at the road access locations:</p> <ul style="list-style-type: none"> • a channelised right-turn treatment with a short turn slot (CHR(S)) and rural AUL(S) treatment with a short left-turn lane at the changed access; and • a sealed Type C - Rural Property Access at the new left-in / left-out access. <p>(c) The road access works must be designed and constructed in accordance with Queensland Government Property Access Drawing and Austroads Guide to Road Design Part 4: Intersections and Crossings – General and Part 4A: Unsignalised and Signalised intersections, specifically:</p> <ul style="list-style-type: none"> • Figure 8.3: Rural AUL(S) treatment with a short left-turn lane; • Figure A 29: Channelised right-turn treatment with a short turn slot (CHR(S)) on a two-lane rural road; and • Standard Drawing No1807, Type C - Rural Property Access, Date 07/2020. 	<p>(a) At all times.</p> <p>(b) and (c) Prior to the commencement of use.</p>

Attachment 2—Advice to the applicant

General advice	
1.	Terms and phrases used in this document are defined in the Planning Act 2016 its regulation or the State Development Assessment Provisions (SDAP) [v2.6]. If a word remains undefined it has its ordinary meaning.
Advertising devices	
2.	<p>Advertising advice should be obtained from the Department of Transport and Main Roads (DTMR) if the development intends to include the erection, alteration or operation of an advertising sign or another advertising device that would be visible from a state-controlled road, and beyond the boundaries of the state-controlled road, and reasonably likely to create a traffic hazard for the state-controlled road.</p> <p>Note: DTMR has powers under section 139 of the Transport Operations (Road Use Management - Accreditation and Other Provisions) Regulation 2015 to require removal or modification of an advertising sign and/or a device which is deemed that it creates a danger to traffic.</p>
Transport noise corridor	
3.	<p>Mandatory Part (MP) 4.4 of the Queensland Development Code (QDC) commenced on 1 September 2010 and applies to building work for the construction or renovation of a residential building in a designated transport noise corridor. MP4.4 seeks to ensure that the habitable rooms of Class 1, 2, 3 and 4 buildings located in a transport noise corridor are designed and constructed to reduce transport noise. Transport noise corridor means land designated under Chapter 8B of the Building Act 1975 as a transport noise corridor. Information about transport noise corridors is available at state and local government offices.</p> <p>A free online search tool can be used to find out whether a property is located in a designated transport noise corridor. This tool is available at the State Planning Policy Interactive Mapping System website: https://spp.dsdip.esriaustraliaonline.com.au/geoviewer/map/planmaking and allows searches on a registered lot number and/or property address to determine whether and how the QDC applies to the land. Transport Noise Corridors are located under Information Purposes within Transport Infrastructure of the State Planning Policy (SPP) mapping system.</p>
Road works approval	
	<p>In accordance with section 33 of the <i>Transport Infrastructure Act 1994</i> (TIA), an applicant must obtain written approval from Department of Transport and Main Roads (DTMR) to carry out road works, including road access works on a state-controlled road. Please contact DTMR on 4045 7144 to make an application under section 33 of the TIA to carry out road works. This approval must be obtained prior to commencing any works on the state-controlled road reserve. The approval process may require the approval of engineering designs of the proposed works, certified by a Registered Professional Engineer of Queensland (RPEQ).</p> <p>The road works approval process takes time – please contact Transport and Main Roads as soon as possible to ensure that gaining approval does not delay construction.</p>

Attachment 3—Reasons for referral agency response

(Given under section 56(7) of the *Planning Act 2016*)

The reasons for the department's decision are:

- The proposed development will not impact upon safety and efficiency of the state-controlled road
- The proposed development has been conditioned to ensure the construction of the development will not impact the state-controlled road
- The proposed development complies with State Development Assessment Provisions (SDAP) Version 2.6 – State code 1: Development in a state-controlled road environment and State code 6: Protection of State transport networks.

Material used in the assessment of the application:

- The development application material and submitted plans
- *Planning Act 2016*
- Planning Regulation 2017
- The *State Development Assessment Provisions* (version 2.6), as published by the department
- The Development Assessment Rules
- SARA DA Mapping system.

Attachment 4—Change representation provisions

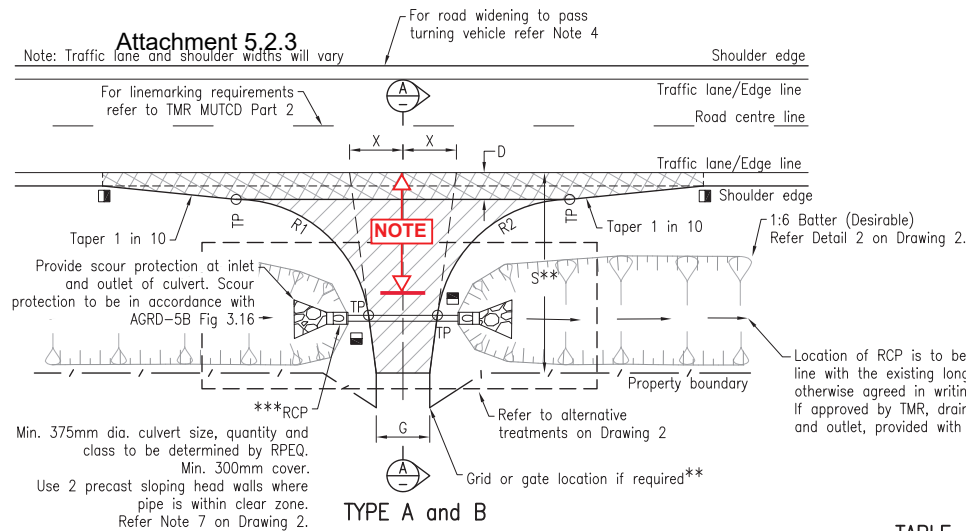
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Attachment 5—Approved plans and specifications

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Attachment 5.2.3

Note: Traffic lane and shoulder widths will vary



87 of 615

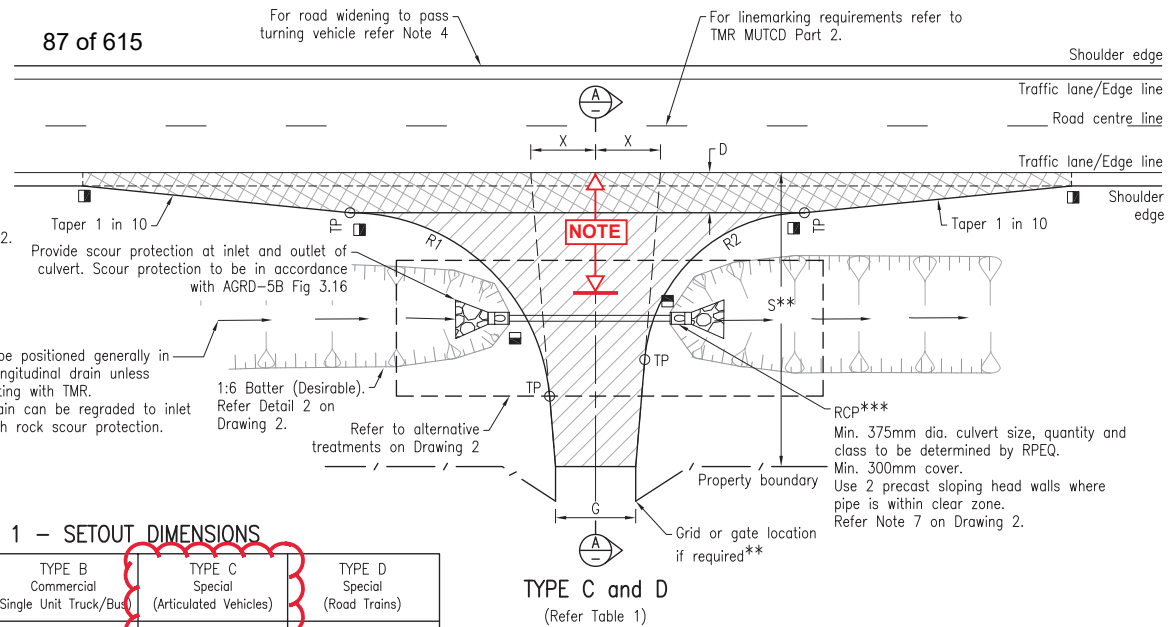


TABLE 1 – SETOUT DIMENSIONS

	TYPE A Residential (Car/Service Vehicle)	TYPE B Commercial (Single Unit Truck/Bus)	TYPE C Special (Articulated Vehicles)	TYPE D Special (Road Trains)
R1	10m	10m	15m	20m
R2	10m	10m	12m	12m
D	2m	2m	3m	3m
X	3m	5m	4m	
S	12m	15m	22m	30m**
G	4–6m ϕ	4–6m ϕ	6m	6m
ϕ 6m Minimum width for two-way two-lane access.				

LEGEND

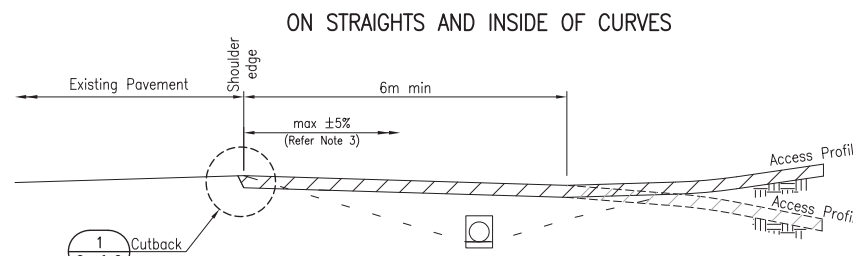
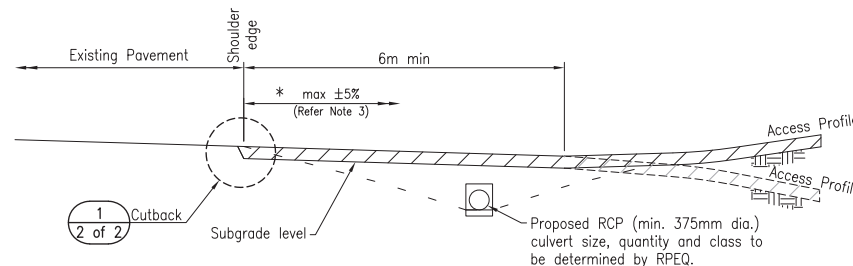
- Pavement Type 1 – Bitumen surfacing, 2 coat bitumen seal. Pavement depth and type to match existing or proposed through road pavement. Refer to Table 2 for minimum depths.
- Pavement Type 2 – Gravel, unbound pavement. Refer to Table 2 for depths. Access may be required to be sealed for up to 10m width from edge line (to minimise gravel on through road) to be determined by the RPEQ.
- * Maintain existing shoulder crossfall and superelevation.
- ** Length 'S' to property boundary by TMR. Where length 'S' is greater than the road reserve boundary, then fencing and grid/gate shall be recessed at the cost of owner from property boundary to ensure vehicle does not impede through lane.
- *** RCBC (min. size 600x300) can be used instead of RCP, or invert option where table drain is of insufficient depth for a culvert.
- Denotes Road Edge Guide Post
The Filled in portion denotes a red reflector and the open portion a white reflector.

TABLE 2 – MINIMUM PAVEMENT DETAILS AND DEPTH

	TYPE A Residential (Car/Service Vehicle)	TYPE B Commercial (Single Unit Truck/Bus)	TYPE C & D Special (Articulated Vehicles)
Sealed Pavement Base Course	150mm(Min.) Type 2.2 or match existing	200mm(Min.) Type 2.2 or match existing	280mm(Min.) Type 2.2 or match existing
Unsealed Pavement Base Course	150mm(Min.) Type 2.4 or match existing	200mm(Min.) Type 2.4 or match existing	#

NOTE:

- NOTE: In all cases, bitumen seal to extend a minimum of 10m from road edge.**
 - Where access is located on curves, intersections or is Type C, or excessive screwing motion will occur, pavement seal to extend to property boundary at the owner's cost to the engineer's/designer's discretion.
- # Bitumen sealed pavement only.
- Type 3.1 or 4.3 or match existing is permissible if Type 2.2/2.4 is unable to be used.



ON OUTSIDE OF SUPERELEVATED CURVES

Ordinary Council Meeting 20 September 2021

NOTES:

- Details shown on this drawing are the minimum layout requirements for a private rural property access. For additional requirements and other design considerations refer to Sections 7.2.1 and 7.2.3 of the AGRD-4 (2009).
- For sight distance requirements refer to Section 3.4 of the RPDM (2nd Edition) Volume 3 Supplement to AGRD-4A, and Section 3 of the AGRD-4A (2010).
- Vertical clearance checks to be carried out for proposed vehicle in accordance with AS 2890.2 – Parking Facilities Off-Street Commercial Vehicle Facilities.
- RPEQ or designer to conduct traffic impact assessment to determine if turning treatments are required. Urban right-turn treatments maybe appropriate, refer to Section 7.5 of the AGRD-4A (2010) for pavement widening requirements. Pavement type to match existing or minimums specified in Table 2 of this drawing.
- This drawing is to be read in conjunction with Drawing 2 of 2.
- All dimensions in metres and are minimum unless specified.

REFERENCED DOCUMENTS:

Departmental Standard Drawings:

- 1243 Precast Culvert Headwalls – Headwall Connections for Culverts
- 1305 Pipe Culverts – Headwall and Apron for Pipe Diameter 375 to 675
- 1359 Culverts – Installation, Bedding and Filling/Backfilling Against/Over Culverts

Departmental Documents:

- RPDM Road Planning and Design Manual (2nd Edition)
- MRTS03 Drainage, Retaining Structures and Protective Treatment
- Austroads Guide to Road Design:
- AGRD-4 (2009) Part 4: Intersections and Crossings – General (2009)
- AGRD-4A (2010) Part 4A: Unsignalised and Signalised Intersections (2010)
- AGRD-5B (2013) Part 5B: Drainage – Open Channels, Culverts and Floodways (2013)

Department of Transport and Main Roads			
PROPERTY ACCESS		Standard Drawing No	
RURAL PROPERTY ACCESS		1807	
DRAWING 1 OF 2		Date 07/2020	
A3		Not to Scale	
A			

Attachment 5.2.3

Provide scour protection at inlet and outlet of culvert. Scour protection to be in accordance with AGRD-5B Fig 3.16

RCBC
Min. 600x300mm RCBC, quantity, class and cover to be determined by RPEQ.
Use 2 sloping head walls where RCBC is within clear zone. Refer Note 7.

For Access Setout refer Table 1 on Drawing 1

RC BOX CULVERT PLAN VIEW

88 of 615

Table drain to join existing alignment.

1:6 Batter (Desirable)
Refer Detail 2.

Concrete/pavement/bitumen surface Invert. Refer to Section C for invert crossing details.

Table drain profile to be flush with invert crossing finished surface.

INVERT CROSSING PLAN VIEW

LEGEND



Pavement Type 2 – Gravel, unbound pavement. Refer to Table 2 of Drawing 1 for depths. Access may be required to be sealed for up to 10m width from edge line (to minimize gravel on through road) to be determined by the RPEQ.



Invert crossing surface

* Maintain existing shoulder crossfall and superelevation.

NOTES:

- This drawing is to be read in conjunction with Drawing 1 of 2.
- Minimum longitudinal fall for concrete or bitumen invert is 0.3%.
- 1 in 6 grade can be further levelled for larger design vehicles. Ensure sufficient area for drainage remains. Dimensions to be based on stormwater flow rate for appropriate design ARI event to ensure invert crossing can meet required capacity. Type 22 and Type 28 inverts can be used if drainage design criteria is met.
- Vertical clearance checks to be carried out for small rigid vehicle to ensure adequate transition between change in grade. Refer to AS 2890.2.
- For pavement or bitumen surfacing inverts, refer Table 2 on Drawing 1 for minimum depths.
- Concrete access to have minimum N32 concrete, 100mm thick on 100mm thick sub-base gravel. Concrete access to be reinforced with SL72 mesh with minimum 40mm top cover.
- Culvert clear zone varies with location and speed environment. Refer to TMR Road Planning and Design Manual – Supplement to AGRD Part 6, and Austroads Guide to Road Design – Part 6.

REFERENCED DOCUMENTS:

Departmental Standard Drawings:

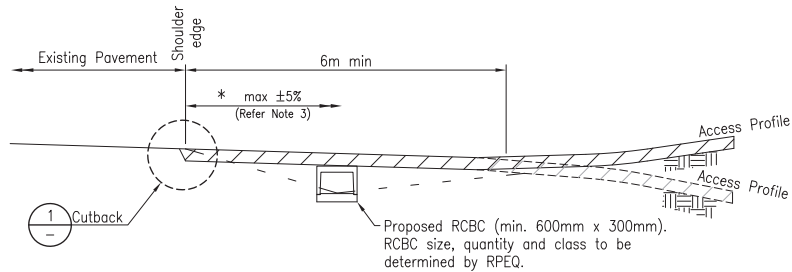
1260 R C Box Culverts and Slab Link Box Culverts –
Culverts Height = 375 To 600

1033 Kerb and Channel – Profiles

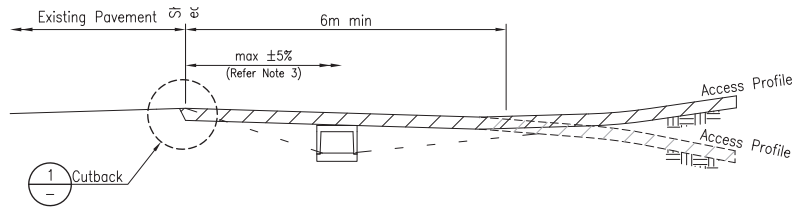
Australian Standards Documents:

AS2890.2 Parking Facilities – Off-Street Commercial Vehicle Facilities

Department of Transport and Main Roads			
PROPERTY ACCESS			
RURAL PROPERTY ACCESS		Standard Drawing No 1807	
DRAWING 2 OF 2		Date 07/2020	

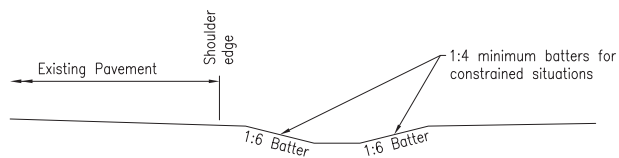


ON STRAIGHTS AND INSIDE OF CURVES



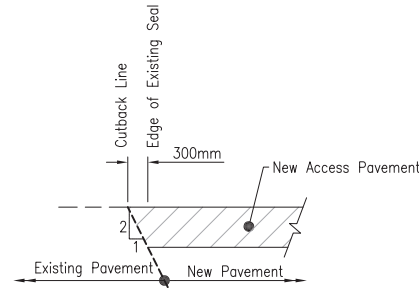
ON OUTSIDE OF SUPERELEVATED CURVES

SECTION B



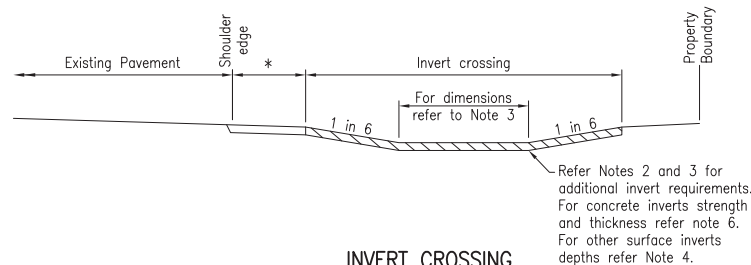
BATTER DETAIL

DETAIL 2



CUTBACK DETAIL

DETAIL 1



INVERT CROSSING

SECTION C

Ordinary Council Meeting - 28 September 2021



Our ref TMR21-032012 (500-1509)
 Your ref
 Enquiries Ronald Kaden

Department of
Transport and Main Roads

31 March 2021

Decision Notice – Permitted Road Access Location (s62(1) *Transport Infrastructure Act 1994*)

This is not an authorisation to commence work on a state-controlled road¹

Development application reference number MCUI2020_3711/1, lodged with Douglas Shire Council involves constructing or changing a vehicular access between Lot 1SP150468, the land the subject of the application, and Port Douglas Road (a state-controlled road).

In accordance with section 62A(2) of the *Transport Infrastructure Act 1994* (TIA), this development application is also taken to be an application for a decision under section 62(1) of TIA.

Applicant Details

Name and address Chiodo Corporation Operations Pty Ltd
 C/- GHD
 PO Box 930
 Townsville QLD 4810

Application Details

Address of Property 71-85 Port Douglas Road, Port Douglas QLD 4877
 Real Property Description 1SP150468
 Aspect/s of Development Material Change of Use for Development Permit for Tourist Resort

Decision (given under section 67 of TIA)

It has been decided to approve the application, subject to the following conditions:

No.	Conditions of Approval	Condition Timing
1	Resort Access The permitted road access location is approximately 80 metres from the southern boundary of Lot 1SP150468, in accordance with: 1. TMR Layout Plan (6504 - 2.65km) Issue A 31/03/2021	At all times.
2	The use of the permitted road access location is to be restricted to: a) Design vehicles up to a maximum size Three axle truck or bus - Class 4 Medium Length (up to 14.5m) Heavy Vehicle**	At all times.

¹ Please refer to the further approvals required under the heading 'Further approvals'

No.	Conditions of Approval	Condition Timing
	Note: ** as described in Austroads Vehicle Classification System	
3	<p>Road access works comprising a Channelised right-turn treatment (short) and auxiliary left-turn lane treatment (short) must be provided at the permitted access location, generally in accordance with:</p> <ul style="list-style-type: none"> • Austroads Guide to Road Design Part 4, Figure A 29: CHR(S), and • Austroads Guide to Road Design Part 4A, Figure 8.3, (AUL)S 	Prior to commencement of use
4	<p>Service Vehicle Access The permitted road access location is approximately 10 metres from the northern boundary of Lot 1SP150468, in accordance with: 1. TMR Layout Plan (6504 - 2.65km) Issue A 31/03/2021</p>	At all times.
5	<p>The use of the permitted road access location is to be restricted to:</p> <ol style="list-style-type: none"> Left-In, Left-Out vehicle movements only, Design vehicles up to a maximum size Four axle articulated or rigid vehicle & trailer - Class 7 Long (up to 19.0m) Heavy Vehicle** <p>Note: ** as described in Austroads Vehicle Classification System</p>	At all times.
6	<p>Road Access Works comprising a rural property access must be provided at the permitted road access location, generally in accordance with;</p> <ol style="list-style-type: none"> Type C Property Access drawing prepared by Queensland Government dated 07/2020 reference 1807 Issue A (with bitumen seal amendments). 	Prior to commencement of use
7	<p>No additional access Direct access is prohibited between Port Douglas Road and Lot 1SP150468 at any other location other than the permitted road access location described in Conditions 1 and 4.</p>	At all times.
8	<p>Removal of existing works The existing road access works situated approximately 50 metres from the northern boundary of Lot 1SP150468 (approximately 170m south of Mirage Roundabout) must be removed and all kerb and channel/ table drain/ footpath reinstated between the pavement edge and the property boundary in accordance with TMR's Road Planning and Design Manual and standard drawings.</p>	Prior to commencement of use

Reasons for the decision

The reasons for this decision are as follows:

- Currently the subject site (Lot 1 on SP150468) has road frontage and vehicle access via Port Douglas Road, a state-controlled road.

- b) The applicant intends to remove the existing access and create two independent access locations, one for resort guests and the other for service vehicles.
- c) The proposed development will be increasing traffic generation and will require a changed vehicle access between Lot 1 on SP150468 (the subject site) and Port Douglas Road.
- d) Therefore, a decision under section 62 of the Act is required as no prior approval for road access location is evidenced.

Please refer to **Attachment A** for the findings on material questions of fact and the evidence or other material on which those findings were based.

Information about the Decision required to be given under section 67(2) of TIA

1. There is no guarantee of the continuation of road access arrangements, as this depends on future traffic safety and efficiency circumstances.
2. In accordance with section 70 of the TIA, the applicant for the planning application is bound by this decision. A copy of section 70 is attached as **Attachment B**, as required, for information.

Further information about the decision

1. In accordance with section 67(7) of TIA, this decision notice:
 - a) starts to have effect when the development approval has effect; and
 - b) stops having effect if the development approval lapses or is cancelled; and
 - c) replaces any earlier decision made under section 62(1) in relation to the land.
2. In accordance with section 485 of the TIA and section 31 of the *Transport Planning and Coordination Act 1994* (TPCA), a person whose interests are affected by this decision may apply for a review of this decision only within 28 days after notice of the decision was given under the TIA. A copy of the review provisions under TIA and TPCA are attached in **Attachment C** for information.
3. In accordance with section 485B of the TIA and section 35 of TPCA a person may appeal against a reviewed decision. The person must have applied to have the decision reviewed before an appeal about the decision can be lodged in the Planning and Environment Court. A copy of the Appeal Provisions under TIA and TPCA is attached in **Attachment C** for information.

Further approvals

The Department of Transport and Main Roads also provides the following information in relation to this approval:

1. Road Access Works Approval Required – Written approval is required from the department to carry out road works that are road access works (including driveways) on a state-controlled road in accordance with section 33 of the TIA. This approval must be obtained prior to commencing any works on the state-controlled road. The approval process may require the approval of engineering designs of the proposed works, certified by a Registered Professional Engineer of Queensland (RPEQ). Please contact the department to make an application.

If further information about this approval or any other related query is required, Mr Ronald Kaden, Technical Officer (Development Control) should be contacted by email at ron.p.kaden@tmr.qld.gov.au or on (07) 4045 7151.

Yours sincerely

A handwritten signature in black ink, appearing to read 'Peter McNamara', written in a cursive style.

Peter McNamara
Principal Engineer (Civil)

Attachments: Attachment A – Decision evidence and findings
Attachment B - Section 70 of TIA
Attachment C - Appeal Provisions
Attachment D - Permitted Road Access Location Plan

Attachment A
Decision Evidence and Findings

Evidence or other material on which findings were based:

Title of Evidence / Material	Prepared by	Date	Reference no.	Version / Issue
TMR Layout Plan (6504 - 2.65km)	Queensland Government Transport and Main Roads	31 March 2021	TMR21-32012 (500-1509)	A

Attachment B**Section 70 of TIA***Transport Infrastructure Act 1994*

Chapter 6 Road transport infrastructure

Part 5 Management of State-controlled roads

70 Offences about road access locations and road access works, relating to decisions under s 62(1)

- (1) This section applies to a person who has been given notice under section 67 or 68 of a decision under section 62(1) about access between a State-controlled road and adjacent land.
- (2) A person to whom this section applies must not—
- (a) obtain access between the land and the State-controlled road other than at a location at which access is permitted under the decision; or
 - (b) obtain access using road access works to which the decision applies, if the works do not comply with the decision and the noncompliance was within the person's control; or
 - (c) obtain any other access between the land and the road contrary to the decision; or
 - (d) use a road access location or road access works contrary to the decision; or
 - (e) contravene a condition stated in the decision; or
 - (f) permit another person to do a thing mentioned in paragraphs (a) to (e); or
 - (g) fail to remove road access works in accordance with the decision.

Maximum penalty—200 penalty units.

- (3) However, subsection (2)(g) does not apply to a person who is bound by the decision because of section 68.

Attachment C

Appeal Provisions

Transport Infrastructure Act 1994
Chapter 16 General provisions

485 Internal review of decisions

- (1) A person whose interests are affected by a decision described in schedule 3 (the *original decision*) may ask the chief executive to review the decision.
- (2) The person is entitled to receive a statement of reasons for the original decision whether or not the provision under which the decision is made requires that the person be given a statement of reasons for the decision.
- (3) The *Transport Planning and Coordination Act 1994*, part 5, division 2—
 - (a) applies to the review; and
 - (b) provides—
 - (i) for the procedure for applying for the review and the way it is to be carried out; and
 - (ii) that the person may apply to QCAT to have the original decision stayed.

485B Appeals against decisions

- (1) This section applies in relation to an original decision if a court (the appeal court) is stated in schedule 3 for the decision.
- (2) If the reviewed decision is not the decision sought by the applicant for the review, the applicant may appeal against the reviewed decision to the appeal court.
- (3) The *Transport Planning and Coordination Act 1994*, part 5, division 3—
 - (a) applies to the appeal; and
 - (b) provides—
 - (i) for the procedure for the appeal and the way it is to be disposed of; and
 - (ii) that the person may apply to the appeal court to have the original decision stayed.
- (4) Subsection (5) applies if—
 - (a) a person appeals to the Planning and Environment Court against a decision under section 62(1) on a planning application that is taken, under section 62A(2), to also be an application for a decision under section 62(1); and

- (b) a person appeals to the Planning and Environment Court against a decision under the Planning Act on the planning application.
- (5) The court may order—
 - (a) the appeals to be heard together or 1 immediately after the other; or
 - (b) 1 appeal to be stayed until the other is decided.
- (6) Subsection (5) applies even if all or any of the parties to the appeals are not the same.
- (7) In this section—

original decision means a decision described in schedule 3.

reviewed decision means the chief executive's decision on a review under section 485.

Transport Planning and Coordination Act 1994
Part 5, Division 2 – Review of Original Decisions

31 Applying for review

- (1) A person may apply for a review of an original decision only within 28 days after notice of the original decision was given to the person under the transport Act.
- (2) However, if—
 - (a) the notice did not state the reasons for the original decision; and
 - (b) the person asked for a statement of the reasons within the 28 days mentioned in subsection (1)the person may apply within 28 days after the person is given the statement of the reasons.
- (3) In addition, the chief executive may extend the period for applying.
- (4) An application must be written and state in detail the grounds on which the person wants the original decision to be reviewed.

32 Stay of operation of original decision

- (1) If a person applies for review of an original decision, the person may immediately apply for a stay of the decision to the relevant entity.
- (2) The relevant entity may stay the original decision to secure the effectiveness of the review and any later appeal to or review by the relevant entity.
- (3) In setting the time for hearing the application, the relevant entity must allow at least 3 business days between the day the application is filed with it and the hearing day.
- (4) The chief executive is a party to the application.
- (5) The person must serve a copy of the application showing the time and place of the hearing and any document filed in the relevant entity with it on the chief executive at least 2 business days before the hearing.
- (6) The stay—
 - (a) may be given on conditions the relevant entity considers appropriate; and
 - (b) operates for the period specified by the relevant entity; and
 - (c) may be revoked or amended by the relevant entity.
- (7) The period of a stay under this section must not extend past the time when the chief executive reviews the original decision and any later period the relevant entity allows the applicant to enable the applicant to appeal against the decision or apply for a review of the decision as provided under the QCAT Act.

(8) The making of an application does not affect the original decision, or the carrying out of the original decision, unless it is stayed.

(9) In this section—

relevant entity means—

- (a) if the reviewed decision may be reviewed by QCAT—QCAT; or
- (b) if the reviewed decision may be appealed to the appeal court—the appeal court.

35 Time for making appeals

(1) A person may appeal against a reviewed decision only within—

- (a) if a decision notice is given to the person—28 days after the notice was given to the person; or
- (b) if the chief executive is taken to have confirmed the decision under section 34(5)—56 days after the application was made.

(2) However, if—

- (a) the decision notice did not state the reasons for the decision; and
- (b) the person asked for a statement of the reasons within the 28 days mentioned in subsection (1)(a);

the person may apply within 28 days after the person is given a statement of the reasons.

(3) Also, the appeal court may extend the period for appealing.

Development Assessment Rules—Representations about a referral agency response

The following provisions are those set out in sections 28 and 30 of the Development Assessment Rules¹ regarding **representations about a referral agency response**

Part 6: Changes to the application and referral agency responses

28 Concurrence agency changes its response or gives a late response

- 28.1. Despite part 2, a concurrence agency may, after its referral agency assessment period and any further period agreed ends, change its referral agency response or give a late referral agency response before the application is decided, subject to section 28.2 and 28.3.
- 28.2. A concurrence agency may change its referral agency response at any time before the application is decided if—
- (a) the change is in response to a change which the assessment manager is satisfied is a change under section 26.1; or
 - (b) the Minister has given the concurrence agency a direction under section 99 of the Act; or
 - (c) the applicant has given written agreement to the change to the referral agency response.²
- 28.3. A concurrence agency may give a late referral agency response before the application is decided, if the applicant has given written agreement to the late referral agency response.
- 28.4. If a concurrence agency proposes to change its referral agency response under section 28.2(a), the concurrence agency must—
- (a) give notice of its intention to change its referral agency response to the assessment manager and a copy to the applicant within 5 days of receiving notice of the change under section 25.1; and
 - (b) the concurrence agency has 10 days from the day of giving notice under paragraph (a), or a further period agreed between the applicant and the concurrence agency, to give an amended referral agency response to the assessment manager and a copy to the applicant.

¹ Pursuant to Section 68 of the *Planning Act 2016*

² In the instance an applicant has made representations to the concurrence agency under section 30, and the concurrence agency agrees to make the change included in the representations, section 28.2(c) is taken to have been satisfied.

Part 7: Miscellaneous

30 Representations about a referral agency response

30.1. An applicant may make representations to a concurrence agency at any time before the application is decided, about changing a matter in the referral agency response.³

³ An applicant may elect, under section 32, to stop the assessment manager's decision period in which to take this action. If a concurrence agency wishes to amend their response in relation to representations made under this section, they must do so in accordance with section 28.