



INFORMATION RESPONSE – TO DSC AND SARA

**MATERIAL CHANGE OF USE SERVICE INDUSTRY, CARETAKER'S
RESIDENCE, INDOOR SPORT & ENTERTAINMENT, SHOPPING FACILITY**

1-5 DICKSON ST, CRAIGLIE

LODGEMENT DATE

24 October 2017

**prepared by
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**on behalf of
PDM Timber Pty Ltd**

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Application Summary

Applicant Details	
Proposal	Development Permit for Material Change of Use Service Industry, Caretaker's Residence, Indoor Sport and Entertainment, Shopping Facility (second hand sales /repair)
Applicant	PDM Timbers
Address	1-5 Dickson Street, Craiglie
Real Property Description	Lot 10 RP746523
Lot Size	4,847m ²
Planning Area	Industry
Current Use	Mix of uses – many of which have been established Service Industry, Caretaker's Residence, Indoor Sport and Entertainment, Shopping Facility
Level of Assessment	Impact
Applicable Codes	<ol style="list-style-type: none"> 1. Desired Environmental Outcomes 2. Strategic Framework (draft scheme) 3. Port Douglas & Environs Locality Code 4. Commercial Planning Area Code 5. Multi Unit Housing / Holiday Accommodation Code 6. Acid Sulfate Soils Code 7. Filling and excavation Code 8. Landscaping Code 9. Vehicle Parking and Access Code 10. Design and Siting of Advertising Devices code
Referral Agencies	DILGP / QTMR for development within 25m of a state controlled road (trigger 10.9.4.2.4.1)

1.0 INTRODUCTION

The site has been used for a range of industrial and service industrial uses dating back to before the first (1981) Planning Scheme. The site has a range of existing use rights, and this application is made to clarify the historical – existing and proposed uses.

The ongoing use of the site, the changes in planning schemes and legislation, and the loss of local / corporate memory is such that this application is being made essentially to bring the use of the site into a contemporary set of definitions – i.e. in response to the passage of time.

The application is for the development of Service Industry, Caretaker’s Residence, Indoor Sport and Entertainment (*already approved), Shopping Facility at 1 – 5 Dickson Street Craiglie (Lot 10 RP746523).

The 4,847m² site is in the Port Douglas Environs Locality and the Industry Planning Area, the use is Impact Assessable. Proposal plans are provided in **Appendix 1**.



Figure 1: Air photo of Site and Locality

1.2 Existing Uses

The existing uses include:

- caretaker’s residence (established prior to 2002).
- old Shire Hall moved to the site in the mid 2000s which has been used for indoor sport and entertainment uses such as kindy-gym and gymnastics (refer to attached correspondence in **Appendix 2**).
- second hand sales and repair (Sentimental Salvage) which includes the restoration of old pinball and parlour machines, and sale of bric-a-brac of similar nature. This is defined as shopping facility in the planning scheme and shown as commercial on the plan.
- coach parking for Port Douglas tourism businesses – this is consistent with the definition of service industry (provided below) and is consistent with the Service Industry uses for the locality.
- industrial shed tenancies of between 50-100m², which are generally used for Service industries, which are defined in the Planning Scheme as:

Existing Use	Existing / Retained	New	Demolish
Sport and Rec Hall	195m ²		
Commercial	75m ²	72m ²	
Caretaker’s Residence		100m ²	72m ²
Service industry shed	528m ²		
Office		15m ²	
Office			90m ²
Service industry shed	152m ²		
Service industry shed		492m ²	
Ablution block		20m ²	15m ²
Totals	950m²	699m²	177m²

Service Industry: Means any premises used, or intended to be used for trades and services that cater to the tourist and marine activities in Port Douglas. This includes the manufacturing of goods on the premises, depots for receiving goods to be serviced and any administration and minor sales functions associated with the use, where these are carried out on the same Site and are ancillary to the Service Industry activity. Service Industry uses are limited to uses, which are allied to tourist and marine activities in Port Douglas.

The term may include but is not limited to the following activities:

- Limousine/bus depot;

- Cleaning or detailing of motor vehicles;
- Catering business;
- Servicing of small items and appliances such as: - Bicycles; - Cameras; - Electrical appliances for domestic or office use; and - Marine equipment;
- Printing;
- Fishing gear manufacturing;
- Marine engineering;
- Bulk storage and ancillary sales of: - Indoor/Outdoor furniture; - Hardware supplies; - Raw materials; - Plants and Landscaping supplies;

Any off-Site effects do not cause any detriment to the amenity of the area. In particular, the noise levels generated, any dust, fumes, odours or other emissions produced from the Site, the appearance of the Site and any traffic generated by the activities on the Site must be managed so as not to cause detriment to adjoining Sites.



Figure 2: Site and Surrounding Uses

1.3 Proposed Uses

The applicant / owner of the site has regular requests for the use of storage sheds of 50m² to 100m². This part of the application is in response to this acknowledged market requirement. The proposal (summarised in the table above) and is to:

- Build a new 100m² caretaker’s residence in a new building on the south-western corner of the site, providing improved amenity and services.

- Convert the old caretaker's into commercial (72m²) adjacent to the existing commercial area (75m²) – which is likely to be the continuation of the existing sales and repair and second hand uses – or may ultimately become more service industry. A degree of flexibility in the uses with frontage to Owen Street is sought, as uses with good frontage are likely to be attractive to uses where a sales-customer interaction is desirable. These uses are well suited to the locality and form a part of the fabric of any town.
- Demolish the existing office (90m²) and replace with a 15²m² service industry shed.
- Build a dedicated ablution block.
- Build a new 492m² Service industry shed which is likely to be used for any general service industry use including storage sheds.

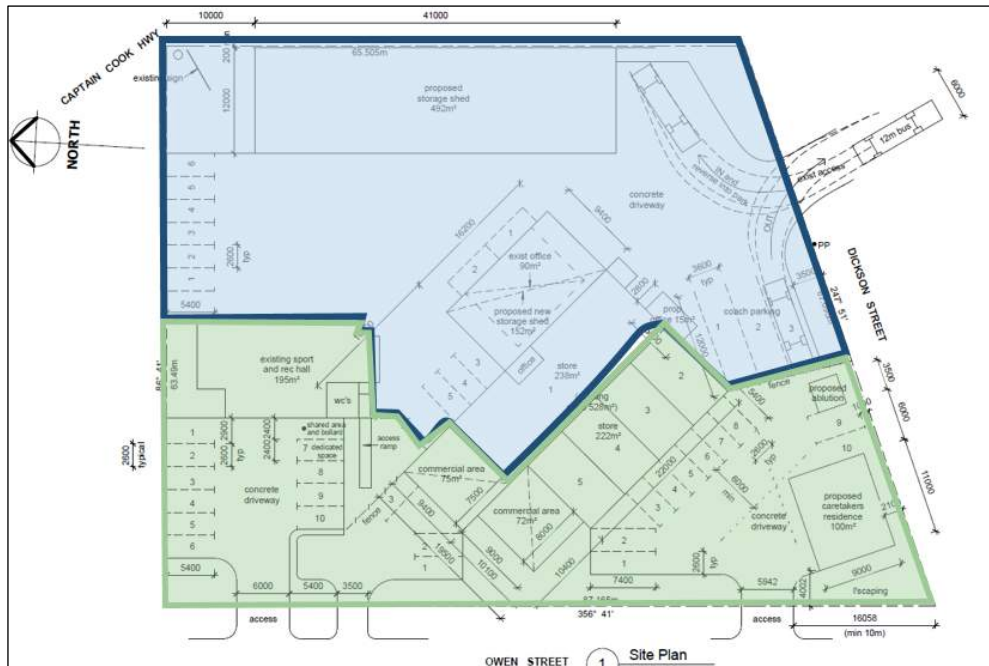
1.4 The Site and Access / Egress

The site is accessed from Owen Street, Dickson Street and has a nominal (very narrow) frontage to the Captain Cook Highway – access is not physically possible from the site on to the Highway (**Figure 1**).

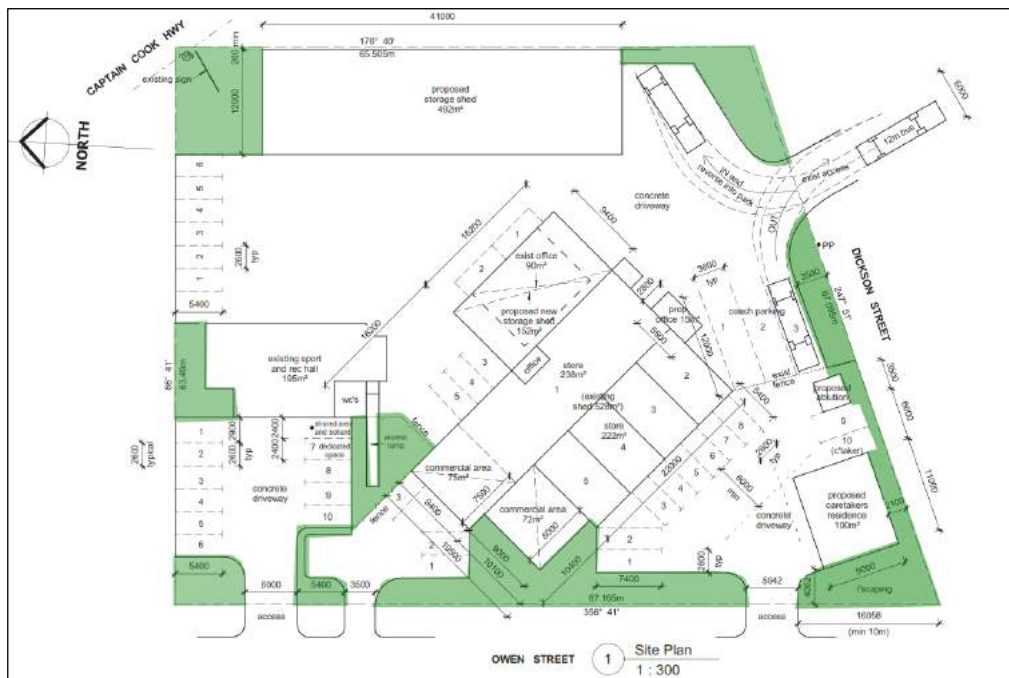
The uses on Dickson Street include Storage Sheds, Telstra, the original farmer shed all on the southern side of the street and the subject site and NQ Homes on the northern side of the street. Dickson Street does not connect to Owen Street.

Owen Street is a cul-de-sac and does not connect through to the Captain Cook highway. All traffic from Owen Street, accesses the highway from Beor St. Adjacent uses on Owen Street include Reece Plumbing and mechanical repairs to the north of the site.

Given the mix of uses on the 4,847m² site, there is a separation of the uses across the site by fences and buildings. The extract of the proposal plan (**Figure 3**) shows the division of uses across the site – particularly in relation to the access / egress.



Figures 3a & 3b: Extract of Site Plan showing egress and landscaping



2.0 PLANNING CONSIDERATIONS

2.1 State Assessment and Referral

The *Planning Regulations* and State Mapping (**Figures 4 and 5**) set out the matters of interest to the State for development assessment. Where the State is a Referral Agency for a development application the *State Development Assessment Provisions* (SDAP) apply.

The site is mapped for the following State interests:

1. State-controlled roads (trigger 10.9.4.2.4.1): area within 25m of State controlled road. Referral is required. The SDAP codes are addressed in (**Appendix 3**).



Figure 4: Mapping layers for State Matters of Interest



Figure 5: State-controlled road noise corridor mapping

2.2 Planning Scheme Assessment

In considering the proposal against the relevant Codes, there are Performance Criteria and Acceptable Solutions which are to be considered:

1. Assessable development must demonstrate that the Performance Criteria can be achieved.
2. The Acceptable Solutions nominated in the Codes are just one means by which the Performance Criteria may be achieved.

The proposal satisfies the Purpose and Overall Outcomes of the Planning Scheme Codes, as identified in **Section 3**.

Table 2: Summary of applicable codes

Planning Scheme Code	Complies	Comment
Port Douglas and Environs Locality	Yes	The development utilises the existing infrastructure including road works.
Industry Planning Area	Yes	The development complies with the performance criteria & is a positive contribution to the locality and local economy.
Caretaker's Residence	Yes	The development complies with the performance criteria.
Acid Sulfate Soil	Yes	The development complies with the performance criteria.
Filling and Excavation	Yes	The development complies with the performance criteria.
Landscaping	Yes	The development meets the requirements.
Vehicle Parking and Access	Yes	The proposal exceeds these provisions.
Advertising Devices	Yes	The development meets the requirements where applicable.

3.0 ASSESSMENT AGAINST DOUGLAS PLANNING SCHEME CODES

3.1 Desired Environmental Outcomes

The Desired Environmental Outcomes (DEOs) represent the policy intent for the Planning Scheme and are grouped into 3 core matters. As the proposed use is impact assessable, the DEOs are a relevant consideration for this application; and the provisions that are applicable to this residential infill development are addressed as follows.

1. Ecological Processes and Natural Systems.
 - a. The proposal is not on or adjacent to a major area of environmental significance, natural habitat or critical conservation area.
 - b. Each of the overlays in the current scheme and draft scheme have been reviewed and the site is not mapped for any environmental or heritage overlays.
 - c. The development is not likely to have a negative impact upon the environmental values of the Shire.
 - d. The site is not on or within a waterway – river or wetland. The development contains appropriate design and engineering solutions to manage water quality.
 - e. The development satisfies the design criteria of the applicable codes.
 - f. The development is not affected by the requirements of WTMA or GBRMPA.

2. Economic Development
 - a. The development does not reduce or impact upon GQAL or primary production.
 - b. The development will support the tourism and commercial base of the Shire and does not impact upon rural activities.
 - c. The development does not reduce or impact upon the natural resources of the Shire such as extractive resources, water and forestry.
 - d. The development does reinforce the values of the Shire, as the development is undertaken in accordance with the preferred pattern of development i.e.

- consolidating growth and employment opportunities primarily in the urban location of Port Douglas.
- e. The development contributes to the efficient use of the physical infrastructure networks including, circulation of traffic, without compromising the Captain Cook Highway as the scenic entry corridor to the Shire.
 - f. The development provides access to services and facilities to provide convenience to residents.
3. Cultural, Economic, Physical and Social Well-being of the Community
- a. The site does not contain or adjoin or affect a place of cultural or heritage significance (Indigenous or European).
 - b. The proposal does not detract from the distinctive character and unique sense of place of the town.
 - c. The development is likely to contribute to community pride and well-being, community safety and prosperity.

Draft Strategic Framework: The draft Strategic Framework shows all of Port Douglas within an urban area. Section 3.2.2.1 relates to planning stewardship and recognises that the planning scheme has a very important role to play in ensuring that the Shire's growth is aimed at minimising human impact on the environment while continuing to support economic progress and social well-being. Essentially this entails strong stewardship and the pursuit of planning strategies that:

- a. conserve the Shire's natural environment and outstanding biodiversity values;
- b. utilise the region's natural resources efficiently, in particular water and energy, while reducing waste;
- c. ensure the region's atmosphere remains clear and clean;
- d. contain urban growth to a defined footprint and promote the best use of land that is allocated for that purpose;
- e. support development of a diverse thriving economy that complements the region's green and environmentally responsible image.
- f. provide opportunities that ensure everyone is able to participate, live, work and benefit in the healthy cohesive environment that the Shire promotes.

The development is consistent with the relevant provisions / strategic intent of the Draft Douglas Shire Strategic Framework.

3.3 Port Douglas and Environs Locality Code

The purpose of this Code is, amongst other things, to facilitate the achievement of the following outcomes for the Locality:

- consolidate Port Douglas as the major tourist accommodation and tourist service centre;
- ensure that tourist development and associated landscaping is of high quality which reflects and complements the image of Port Douglas as a tropical seaside resort town of international renown;
- consolidate the area between Macrossan Street and Marina Mirage as the major tourist, retail, dining and entertainment centre;
- ensure that all forms of development complement the tropical image of the town by incorporating attractive design and architectural features;
- encourage the expansion of residential areas that are pleasant, functional, distinctive and in visually well-defined areas;
- protect existing and future residential areas from the intrusion of tourist accommodation and activity;
- protect sensitive environments and natural features which give Port Douglas its distinctive character and identity, in particular Four Mile Beach, Dicksons Inlet and Flagstaff Hill;
- maintain the distinct rural hinterland, dominant natural environment of the western escarpment, and the existing vegetated hillside of Flagstaff Hill;
- protect primary functions of the port (marine and fishing activities) from incompatible land uses and acknowledge the industrial and commercial land uses associated with the maritime industry, while also providing secondary opportunities for recreational use by residents and tourists.

Performance Criteria	Acceptable Solutions	Comment
	the wall of the Building: P1 Tourist and Residential – (High Scale); and P2 Commercial – (High Scale, within the Tourist Centre and on the low side of Macrossan Street, through to Warner Street).	
P2 Development is connected to all urban services.	A2.1 Development is connected to available urban services by underground connections, wherever possible. AND/OR Contributions are paid when applicable in accordance with the requirements of Planning Scheme Policy No 11 – Water Supply and Sewerage Headworks and Works External Contributions.	Complies The site is connected to all urban services.
P3 Landscaping of development Sites complements the existing tropical seaside resort town character of Port Douglas and creates a dominant tropical vegetated streetscape.	A3.1 Landscaping of a development Site complies with Planning Scheme Policy No 7 – Landscaping, with particular emphasis on appropriate species for Port Douglas.	Complies with performance criteria The site is landscaped. The proposal plan Figure 3b shows the extent of landscaped areas.
P4 Development Sites are provided with efficient and safe vehicle Access and manoeuvring areas on Site and to the Site, to an acceptable standard for the Locality.	A4.1 All Roads, driveways and manoeuvring areas on Site and adjacent to the Site are designed and maintained to comply with the specifications set out in the Planning Scheme Policy No 6 – FNQROC Development Manual.	Complies The access points exist. The proposal plan Figure 3a shows the of access points and the catchments from the site.
Provisions relating to <i>Tourist Centres</i> , <i>Local Centres</i> , <i>Special Management Areas</i> and <i>Residential Development Outside Tourist Centre</i> have not been included here as they are not relevant to this application.		
Other Development		
P19 Industrial development is limited to Service Industry and is located in existing or identified Industrial areas and is of a scale and intensity of development which is acceptable in	A19.1 Service Industry development is located in the identified Industrial areas of: □ Special Management Area 3 - Service Industry Precincts	Complies The site is in Special Management Area 3 – Craiglie.

Performance Criteria	Acceptable Solutions	Comment
the Locality.	(Craiglie); and □ Special Management Area 4 - Service Industry Precincts (Mahogany Street)	
Provisions relating to <i>Community Facilities</i> have not been included here as they are not relevant to this application.		
Protection of Scenic Amenity and Natural Values		
P21 The views and vistas of Four Mile Beach from the intersection of Davidson Street and Macrossan Street to the beach front are maintained.	A21.1 Any development in Macrossan Street between Davidson Street and the beach front, outside the Tourist Centre, is designed with Macrossan Street as the Main Street Frontage and the Buildings are Setback 6 metres from the Main Street Frontage.	Not applicable The site is not located near Four Mile Beach or the intersection of Davidson Street and Macrossan Street.
P22 Development does not adversely impact on areas of sensitive natural vegetation, foreshore areas, Watercourses and areas of tidal inundation which contribute the Scenic Amenity and natural values of the locality	No Acceptable Solution.	Not applicable The site does not contain areas of sensitive natural vegetation, foreshore areas, or a watercourse.

3.4 Industry Planning Area Code

The purpose of this Code is to:


- provide for the establishment of Industry, Class A and Class B and Service Industry on appropriate land with regard to Site suitability, accessibility, surrounding land uses, and location of utilities and services;
- ensure that Industry achieves appropriate environmental standards;
- ensure that industrial Buildings have a high standard of layout and building design that provides an efficient, safe and attractive working environment;
- ensure that Industry, Class A and Class B and Service Industry do not adversely impact on surrounding land uses and Setback areas provide landscaped buffers to adjacent incompatible land uses;
- ensure that Landscaping provides an attractive streetscape and screens utility, storage and car parking from the street; and
- ensure that industrial land uses are protected from encroachment of incompatible land use activities.

Comment


The proposed development complies with the purpose and intent of the code particular the development is of an appropriate scale and achieves an attractive built form which incorporates the character and natural attributes of the surrounding area.

Elements of the Code

Performance Criteria		Acceptable Solution	Comment
<i>Consistent and Inconsistent Uses</i>			
P1	The establishment of uses is consistent with the outcomes sought for the Industry Planning Area.	A1.1 Uses identified as inconsistent uses in the Assessment Tables are not established in the industry Planning Area.	Complies with performance criteria. All uses are permitted uses in the Planning Area with the exception of indoor sport and entertainment – which has approval and the existing second hand sales and repair which Council has indicated may be Shopping Facility, however it may also reasonably be considered to be a service industry.
P2	A Caretaker’s Residence is only established in association	A2.1 Only one Caretaker’s Residence is established on	Complies

Performance Criteria	Acceptable Solution	Comment
with an industrial use or activity operating as the primary use on the Site.	the parent Site in association with an industrial use or activity located on one industrial allotment identified on a Standard Format Plan.	
Site Coverage		
<p>P3 The Site Coverage of Buildings ensures that there is sufficient area for the provision of services and Landscaping..</p>	<p>A3.1 The Site Coverage of all Buildings does not exceed 60% of the Site area.</p>	<p>Complies Refer to the discussion in section 1.3 of this report. The site cover is calculated by 950m² existing buildings 699m² new buildings 177m² to be demolished 1,826m² on a 4,847m² site, therefore the site cover is 37%</p>
Design and Siting		
<p>P4 The siting of industrial Buildings/structures contributes to the desired amenity of the area and protects the amenity of other land uses.</p>	<p>A4.1 Buildings/structures on Sites with Frontage to a State-Controlled Road, are Setback 8 metres from the Road Frontage.</p> <p>A4.2 In other cases, Buildings/structures are Setback: □ 6 metres from the Main Street Frontage; and □ 4 metres from any secondary street Frontage.</p> <p>A4.3 Where the Site has a common boundary with land in an Industry Planning Area, the Building/structure may be built to the side and rear boundaries where the Building Code requirements are satisfied.</p> <p>HOWEVER Where the Building Code requirements are not satisfied, Buildings are Setback 2.5 metres or a quarter of the Height of the Building/structure, whichever is the greater, from side and rear boundaries.</p> <p>A4.4 Where the Site adjoins land not</p>	<p>Complies with performance criteria Setbacks are as follows: - 10m to state controlled road - 4m to Owen St (note this is just a narrow point – the bulk of the building ins setback, at least 9.5m - 2.1m to Dickson St which is actually not a constructed road in this area.</p>  <p>The siting of industrial</p>

Performance Criteria	Acceptable Solution	Comment
	<p>in an Industry Planning Area or land developed partially or wholly for a residential use, the Building/structure is Setback 2.5 metres or a quarter of the Height of the Building/structure, whichever is the greater, from the common boundary.</p> <p>A4.5 The Building/structure is sited to maximise energy conservation, natural cooling and shading from summer sun, with the use of high quality materials and non-reflective roof materials.</p>	<p>Buildings/structures contributes to the desired amenity of the area and protects the amenity of other land uses.</p>
Loading / Unloading Facilities		
<p>P5 The transport of goods and materials to and from industrial Sites does not adversely affect the movement of traffic on the Roads adjacent to the Site.</p>	<p>A5.1 All delivery/pick up vehicles are contained wholly within the Site when being loaded/unloaded.</p> <p>A5.2 Sufficient manoeuvring area is provided on Site to allow a single unit truck to ingress and egress the Site in a forward gear.</p> <p>A5.3 Site Access is limited to one Access point for each street Frontage.</p> <p>OR</p> <p>If the Site has Frontage to the Captain Cook Highway and another Road, Access is limited to the secondary Road.</p> <p>A5.4 Where two Access points to the street Frontage are necessitated, to facilitate manoeuvrability of large industrial vehicles, the accesses are separated by a minimum distance of 10 metres.</p>	<p>Complies</p> <p>Complies</p> <p>Complies with performance criteria</p> <p>The site has an 87.165m² frontage to Owen St and 2 accesses is reasonable given the planning scheme would permit 1 industrial lot every 25m, which would allow at least 3 accesses onto Owen Street.</p> <p>Not applicable</p>
Landscaping and Amenity		
<p>P6 Industrial Sites are landscaped to enhance the amenity of industrial areas and provide a</p>	<p>A6.1 A minimum of 20% of the area of the Site is landscaped.</p> <p>A6.2 Dense Planting along any Road</p>	<p>Complies with performance criteria</p> <p>The site has 37% site cover and has</p>

Performance Criteria	Acceptable Solution	Comment
<p>pleasant working environment.</p>	<p>Frontage is a minimum width of 3 metres.</p> <p>EXCEPT THAT</p> <p>Dense Planting along the Road Frontage is a minimum of 4 metres in width where adjacent to the Captain Cook Highway.</p> <p>A6.3 Any Setback areas from side and rear boundaries where the Site adjoins land not in an Industry Planning Area or land developed partially or wholly for a residential use, are landscaped with Dense Planting in accordance with all the relevant requirements of the Landscaping Code and Planning Scheme Policy No 7 – Landscaping.</p> <p>A6.4 Areas used for loading and unloading, storage, utilities and car parking are screened from public view by a combination of Landscaping and screen fencing.</p>	<p>2m to 3m landscaping along both frontages, and landscaping to the highway and internal. Road reserves to Owen and Dickson Street contain extensive landscaping. The site is landscaped to enhance the amenity of industrial areas and provide a pleasant working environment.</p>  <p>Not applicable</p> <p>Complies</p>
<p>P7 Industrial areas are not characterised by a proliferation</p>	<p>A7.1 Signage complies with the Design and Siting of</p>	<p>Complies with performance criteria</p>

Performance Criteria	Acceptable Solution	Comment
of advertising signs and/or the use of large advertising signs.	Advertising Devices Code. AND No wall signs are located on the walls of industrial Buildings facing the Captain Cook Highway or any other State-Controlled Road.	Signage is historically approved no changes are proposed.

	<p>provided with a Landscaping and Recreation Area at the rate of 30m² for the first bedroom plus 15m² for each additional bedroom, and</p> <ul style="list-style-type: none"> ▫ at least 35m² is provided as private Recreation Area which is directly accessible from the residence and which has a minimum dimension of 3 metres. <p>A2.3 The private Recreation Area is fenced or screened to facilitate the private enjoyment of the space by the occupants of the Caretaker's Residence.</p> <p>A2.4 The Caretaker's Residence is provided with:</p> <ul style="list-style-type: none"> ▫ an outdoor service court with a minimum area of 5m² to facilitate clothes drying facilities; ▫ an area for general storage; ▫ an area for the storage of a garbage receptacle; and ▫ a designated covered car parking space 	<p>space of 12m x 4m minimum, in one location. The residence adjoins Dickson Street which is not constructed in the locality, thus there is over 20m of landscaped area between the Residence and any building on any neighbouring property to the south.</p> <p>Will be complied with</p> <p>Complies The caretaker's parking space is not proposed to be covered.</p>
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	<p>not generated and acidity is neutralised;</p> <ul style="list-style-type: none"> • untreated Acid Sulfate Soils are not taken off-Site unless this is to an alternative location for treatment; and • surface and groundwater flows from areas containing Acid Sulfate Soils do not release leachate containing acid or metal contaminants into the environment. 	
<p>Identification and Management of Acid Sulfate Soils</p>		
<p>P2 The location and extent of Acid Sulfate Soils are identified on the development Site and appropriately management so as to avoid the release of acid and associated metal contaminants into the environment.</p>	<p>A2.1 No Acceptable Solution</p> <p>(Information that the Council may request to demonstrate Compliance with the Performance Criteria is outlined in Planning Scheme Policy No 9 – Reports and Information the Council May Request, for code and impact assessable development).</p>	<p>Will be complied with as applicable</p>

3.8 Filling and Excavation Code

The purpose of this Code is to ensure that filling and excavation do not:

- affect visual/scenic amenity values of the Shire;
- cause flooding and drainage problems;
- impact upon the environment of an area;
- cause land instability; or
- adversely impact upon utility services.

Comment

Filling and excavation are not anticipated for this development.

Elements of the Code

Performance Criteria	Acceptable Measure	Comment
<i>Filling and Excavation – General</i>		
<p>P1 All filling and excavation work does not create a detrimental impact on the slope stability, erosion potential or visual amenity of the Site or the surrounding area.</p>	<p>A1.1 The height of cut and/or fill, whether retained or not, does not exceed 2 metres in height.</p> <p style="text-align: center;">AND</p> <p>Cuts in excess of those stated in A1.1 above are separated by benches / terraces with a minimum width of 1.2 metres that incorporate drainage provisions and screen planting.</p> <p>A1.2 Cuts are supported by batters, retaining or rock walls and associated benches / terraces are capable of supporting mature vegetation.</p> <p>A1.3 Cuts are screened from view by the siting of the Building / structure, wherever possible.</p> <p>A1.4 Topsoil from the Site is retained from cuttings and reused on benches / terraces.</p>	<p>Not applicable Not applicable to this proposed extension to an existing use – on what is a flat and well developed site.</p> <p>Not applicable As above.</p> <p>Not applicable As above.</p> <p>Not applicable As above.</p>

Performance Criteria	Acceptable Measure	Comment
	<p>A1.5 No crest of any cut or toe of any fill, or any part of any retaining wall or structure, is located closer than 600 mm to any boundary of the property, unless the prior written approval of the adjoining landowner and the Council, has been obtained.</p> <p>A1.6 Non-retained cut and / or fill on slopes are stabilised and protected against scour and erosion by suitable measures, such as grassing, Landscaping or other protective/aesthetic measures.</p>	<p>Not applicable As above.</p> <p>Not applicable As above.</p>
Visual Impact and Site Stability		
<p>P2 Filling and excavation are carried out in such a manner that the visual/scenic amenity of the area and the privacy and stability of adjoining properties is not compromised.</p>	<p>A2.1 The extent of filling or excavation does not exceed 40% of the Site area or 500m² whichever is the lesser. EXCEPT THAT</p> <p>A2.1 does not apply to reconfiguration of 5 lots or more.</p> <p>A2.2 Filling and excavation does not occur within 2 metres of the Site boundary.</p>	<p>Not applicable As above.</p> <p>Not applicable</p> <p>Not applicable As above.</p>
Flooding and Drainage		
<p>P3 Filling and excavation does not result in a change to the run off characteristics of a Site which then have a detrimental impact upon the Site or nearby land or adjacent Road reserves.</p>	<p>A3.1 Filling and excavation does not result in the ponding of water on a Site or adjacent land or Road reserves.</p> <p>A3.2 Filling and excavation does not result in an increase in the flow of water across a Site or any other land or Road reserves.</p> <p>A3.3 Filling and excavation does not result in an increase in the volume of water or concentration of water in a Watercourse and overland flow paths.</p> <p>A3.4</p>	<p>Not applicable As above.</p> <p>Not applicable As above.</p> <p>Not applicable As above.</p> <p>Not applicable</p>

Performance Criteria	Acceptable Measure	Comment
	Filling and excavation complies with the specifications set out in the Planning Scheme Policy No 6 – FNQROC Development Manual.	As above.
Water Quality		
P4 Filling and excavation does not result in a reduction of the water quality of receiving waters.	A4.1 Water quality is maintained to comply with the specifications set out in the Planning Scheme Policy No 6 – FNQROC Development Manual.	Will be complied with to the extent relevant to this development.

3.9 Landscaping Code

The purpose of this Code is to:

- ensure that new Landscaping incorporates plants which encourage Biodiversity;
- maintain and strengthen the tropical and native landscape character of the Shire through high quality landscape works;
- ensure that Landscaping enhances the visual quality and unique identity of different parts of the Shire by featuring endemics;
- create attractive streetscapes and public spaces through landscape design and the use of street trees and shade trees;
- ensure that native species are incorporated into Landscaping, as a means of providing continuity between developed and undeveloped areas;
- ensure that existing vegetation on Site is retained, protected during works and integrated with the built environment;
- ensure preferred plant species are selected in accordance with the Plant Species Schedule in Planning Scheme Policy No 7 – Landscaping; and
- ensure that Landscaping screens Buildings to reduce their bulk and to enhance the landscape character of the Shire.

Elements of the Code

Performance Criteria	Acceptable Measure	Comment
<i>Landscape Design</i>		
P1 Landscape design satisfies the purpose and the detailed requirements of this Code.	A1.1 Landscaping is undertaken in accordance with a Landscape Plan drawn to scale which complies with and illustrates all the relevant requirements of this Code and Planning Scheme Policy No 7 – Landscaping. AND Landscaping is maintained in accordance with the requirements specified in this Code and Planning Scheme Policy No 7 – Landscaping.	Complies with performance criteria The site has 37% site cover and has existing landscaping. The site has 2m to 3m landscaping along both frontages, and landscaping to the highway and internal. Road reserves to Owen and Dickson Street contain extensive landscaping. The site is landscaped to enhance the amenity of industrial areas and provide a pleasant working environment.

Performance Criteria	Acceptable Measure	Comment
Landscape Character and Planting		
<p>P2 Landscaping contributes to a sense of place, is functional to the surroundings and provides dominant visual interest and form.</p>	<p>A2.1 A minimum of 80% of the proposed landscape area is open to the sky for sunlight and ventilation.</p> <p>A2.2 The percentage of native or endemic species utilised in the Landscaping is as specified in the Locality Code. OR Where not specified in the Locality Code, in accordance with Planning Scheme Policy No. 7 – Landscaping.</p> <p>A2.3 Landscaping includes planting layers comprised of canopy, middle storey, screening and groundcovers, with palm trees used as accent plants only.</p>	<p>Complies</p> <p>Complies</p> <p>Complies to the extent relevant for this use in this locality</p>
<p>P3 Landscaping is consistent with the existing landscape character of the area and native vegetation existing on the Site is to be retained wherever possible and integrated with new Landscaping.</p>	<p>A3.1 Existing native vegetation on Site is retained and incorporated into the Site design, wherever possible.</p> <p>A3.2 Any mature vegetation on the Site which is removed or damaged during development of the Site is replaced with advanced native species.</p> <p>A3.3 Where there is an existing landscape</p>	<p>Complies to the extent relevant</p> <p>Will be complied with</p> <p>Not applicable / complies</p>

Performance Criteria	Acceptable Measure	Comment
	<p>character in a street or locality which results from existing vegetation, similar species are planted on Site or on the street.</p> <p>A3.4 Street trees are 100% native species which enhance the landscape character of the streetscape, with species chosen from the Plant Species Schedule in Planning Scheme Policy No 7 – Landscaping.</p>	<p>Complies The trees exist, no change proposed.</p>
<p>P4 Plant species are selected with consideration to the scale and form of development, screening, buffering, streetscape, shading and the locality of the area.</p>	<p>A4.1 Species are selected in accordance with the Plant Species Schedule in Planning Scheme Policy No 7 – Landscaping.</p>	<p>Complies The trees exist, no change proposed</p>
<p>P5 Shade planting is provided in car parking areas where uncovered or open, and adjacent to driveways and internal Roadways.</p>	<p>A5.1 Where car parking areas are uncovered or open, shade trees are planted at regular intervals (a minimum of 1 shade tree is provided for every 5 car parks) throughout the car parking areas, and adjacent to driveways and internal Roadways.</p> <p>A5.2 A minimum of 1 shade tree is provided for every 10 metres along a driveway or internal Roadway.</p> <p>A5.3 Landscape beds and trees are protected by garden edging, bollards or wheel stops.</p> <p>A5.4 Trees within car parking areas have a minimum planting area the equivalent of 1 car parking bay, with a minimum topsoil depth of 0.8 metre.</p>	<p>Not applicable to this scale of development.</p> <p>Complies / Not applicable to this scale of development.</p> <p>Will be complied with</p> <p>Will be complied with as appropriate</p>
Screening		
<p>P6 Fences along street frontages are articulated with appropriate Landscaping.</p>	<p>A6.1 Perimeter fencing to any street Frontage complies with the relevant Planning Area Code.</p>	<p>Will be complied with as appropriate</p>

Performance Criteria	Acceptable Measure	Comment
	<p>A6.2 Trees, shrubs and groundcovers are planted within any recessed areas along the fence line.</p>	<p>Not applicable</p>
<p>P7 Landscaping within Recreation Areas of residential development are functional, well designed and enhance the residential amenity.</p>	<p>A7.1 One shade tree is provided for each private open space or private Recreation Area.</p> <p>A7.2 Tree species provide 30% shade over the area within 5 years.</p> <p>A7.3 A minimum of 50% of the Landscaping and Recreational Area is landscaped, with trees, shrubs, groundcovers, minimising large expanses of hardstand areas and structures.</p> <p>A7.4 Plants are located to provide shelter and shade to Habitable Rooms and outdoor Recreation Areas from the hot summer sun.</p>	<p>Will be complied with</p> <p>Will be complied / complies</p> <p>Complies</p> <p>Will be complied with.</p>
<p>P8 Undesirable features are screened with Landscaping.</p>	<p>A8.1 Landscaping of Dense Planting is planted along and near retaining walls, long blank walls of Buildings, mechanical and air-conditioning units, clothes drying areas, bin enclosures and other utility structures with appropriate trees, shrubs and groundcovers.</p>	<p>Complies with performance criteria</p>
<p>P9 The environmental values of the Site and adjacent land are enhanced.</p>	<p>A9.1 Landscaping using similar endemic or native species, is planted on-Site on land adjoining an area of natural environmental value.</p>	<p>Complies with performance criteria</p>
<p>Streetscape and Site Amenity</p>		
<p>P10 Landscaping for residential development enhances the streetscape and the visual appearance of the development.</p>	<p>A10.1 Dense Planting along the front of the Site incorporates:</p> <ul style="list-style-type: none"> shade canopy trees to provide shade to the Frontage of the Site within 5 years of planting; 	<p>Not applicable This does not apply to the commercial development</p>

Performance Criteria	Acceptable Measure	Comment
	<ul style="list-style-type: none"> • landscape screening of blank walls; • low shrubs, groundcovers and mulch to completely cover unsealed ground. <p>A10.2 Dense Planting to the rear of the Site incorporates:</p> <ul style="list-style-type: none"> • 1 shade tree for an average of every 75m², growing to the Building eave Height within 5 years of planting; • screening shrubs to grow to 3 metres in Height within 2 years of planting; • low shrubs, groundcovers and mulch to completely cover unsealed ground. <p>A10.3 Dense Planting to the side boundaries incorporates:</p> <ul style="list-style-type: none"> • trees planted for an average of every 10 metres where adjacent to a Building; • low shrubs, groundcovers and mulch to completely cover unsealed ground 	<p>Not applicable This does not apply to the commercial development.</p> <p>Not applicable This does not apply to the commercial.</p>
<p>P11 Landscaping for non-residential development enhances the streetscape and the visual appearance of the development.</p>	<p>A11.1 Dense Planting along the front boundary of the Site where a Building is Setback from the front alignment, incorporates:</p> <ul style="list-style-type: none"> • shade canopy trees to provide shade to the Frontage of the Site within 5 years of planting where appropriate; • landscape screening of blank walls; • low shrubs, groundcovers and mulch to completely cover unsealed ground. <p>A11.2 Dense Planting to the rear of the Site where a Building is Setback from the</p>	<p>Complies to the extent appropriate for this use in this location.</p> <p>Complies to the extent appropriate for this use in this location.</p>

Performance Criteria	Acceptable Measure	Comment
	<p>boxes in accordance with Australian Standards, with 1 hose cock within each area.</p> <p>A12.3 Turf areas are accessible by standard lawn maintenance equipment.</p> <p>A12.4 Plant species are selected with long life expectancy and minimal maintenance requirements where on-Site management will be limited.</p> <p>A12.5 Mulching is provided to all garden beds to reduce weed growth and to retain water, and is to be replenished every year in the ongoing maintenance program.</p>	<p>Will be complied with – as appropriate.</p> <p>Will be complied with to the extent applicable to an existing use – on what is a flat and well developed site</p> <p>Will be complied with</p>
<p>P13 Stormwater runoff is minimised and re-used in Landscaping through water infiltration, where appropriate.</p>	<p>A13.1 Adequate drainage is provided to all paving, turf and garden beds, including the use of swales, spoon drains, subsurface drainage, field gullies, rock or pebble lined Watercourses and stormwater connections.</p> <p>A13.2 Overland flow paths are not to be restricted by Landscaping works.</p> <p>A13.3 Water runoff is re-used through draining of hard surface areas towards permeable surfaces, turf, garden beds and by minimising impervious surfaces on the Site.</p>	<p>Will be complied with</p> <p>Will be complied with</p> <p>Will be complied with</p>
Safety		
<p>P14 Tree species and their location accommodate vehicle and pedestrian sight lines.</p>	<p>A14.1 Trees located near pathways, driveways, access points, parking areas and street corners have a minimum 3.0 metres of clear trunk.</p>	<p>Will be complied with</p>
<p>P15 The landscape design enhances personal safety and reduces the</p>	<p>A15.1 Security and foot lighting is provided to all common areas, including car</p>	<p>Will be complied with</p>

Performance Criteria	Acceptable Measure	Comment
potential for crime and vandalism.	<p>parks, entries, driveways and pathways.</p> <p>A15.2 Hard surfaces are stable, non-slippery and useable in all weathers.</p> <p>A15.3 Bushfire hazard is minimised with planting of bushfire resistant species near bushfire prone areas, (refer to the Bushfire Risk Overlay on the relevant Locality Map).</p> <p>A15.4 Lighting for bicycle paths is provided in accordance with the relevant Australian Standards</p>	<p>Will be complied with</p> <p>Not applicable to this scale of development</p> <p>Not applicable to this scale of development</p>
Utilities and Services		
<p>P16 The location and type of plant species does not adversely affect the function and accessibility of services and facilities and service areas.</p>	<p>A16.1 Plant species are selected and sited with consideration to the location of overhead and underground services.</p> <p>A16.2 All underground services are to be located under pathways and below the eaves of the Building.</p> <p>A16.3 Irrigation control devices are located in the common Landscaping and Recreation Area.</p> <p>A16.4 Landscaping is located to enable trade persons to Access and view meters and other mechanical equipment within the Site.</p> <p>A16.5 Landscaping does not limit Access for service vehicles or rubbish trucks to utility areas, bin enclosures or docking areas.</p> <p>A16.6 Landscaping near electric lines or substations is designed and developed so that any vegetation at maturity or Landscaping structures or works do not exceed 40 metres in</p>	<p>Complies</p> <p>Will be complied with</p> <p>Will be complied with</p> <p>Complies</p> <p>Complies</p> <p>Will be complied with / not applicable</p>

3.10 Vehicle Parking and Access Code

The purpose of this Code is to ensure that:

- sufficient vehicle parking is provided on-Site to cater for all types of vehicular traffic accessing and parking on the Site, including staff, guests, patrons, residents and short term delivery vehicles;
- sufficient bicycle parking and end of trip facilities are provided on-Site to cater for customer and staff.
- on-Site parking is provided so as to be accessible and convenient, particularly for any short term use;
- the provision of on-Site parking, loading/unloading facilities and the provision of Access to the Site, do not impact on the efficient function of the street network or on the area in which the development is located; and
- new vehicle Access points are safely located and are not in conflict with the preferred ultimate streetscape character and local character and do not unduly disrupt any current or future on-street parking arrangements

Comment

The parking rates per use are identified in the table below. The site has 3 coaster bus parking spaces and 34 vehicle parking spaces. It is also reasonable to assume a further 10 parking spaces within the existing and proposed storage sheds. The site has ample parking for the proposed uses.

Par	Existing / Retained	New	Demolish	Parking Rate	Parking Requirement
Sport and Rec Hall	195m ²			1 space / 15m ²	13 – approved with 10
Commercial	75m ²	72m ²		1 space / 25m ²	5.8
Caretaker's Residence		100m ²	72m ²	1 space	1
Service Industry Shed	528m ²			1 space / 90m ²	5.86
Office		15m ²		1 space / 25m ²	0.6
Office			90m ²		0.0
Service Industry Shed	152m ²			1 space / 90m ²	1.68
Service Industry Shed		492m ²		1 space / 90m ²	5.46
Ablution block		20m ²	15m ²	nil	0.0
Totals	950m²	699m²	177m²		33.4 required 30.4 = 31

Elements of the Code

Performance Criteria	Acceptable Measure	Comment
Vehicle Parking Numbers		
<p>P1 Sufficient parking spaces are provided on the Site to accommodate the amount and type of vehicle traffic expected to be generated by the use or uses of the Site, having particular regard to:</p> <ul style="list-style-type: none"> • the desired character of the area in which the Site is located; • the nature of the particular use and its specific characteristics and scale; • the number of employees and the likely number of visitors to the Site; • the level of local accessibility; • the nature and frequency of any public transport serving the area; • whether or not the use involves the retention of an existing Building and the previous requirements for car parking for the Building; • whether or not the use involves an identified Valuable Conservation Feature and Valuable Site; and • whether or not the use involves the retention of significant vegetation. 	<p>A1.1 The minimum number of vehicle parking spaces provided on the Site is not less than the number prescribed in Schedule 1* of this Code for the particular use or uses. Where the number of spaces calculated from the Schedule is not a whole number, the number of spaces provided is the next highest whole number.</p>	<p>Complies The building area is: 950m² existing buildings 699m² new buildings <u>177m²</u> to be demolished 1,826m²</p> <p>The parking rates per use are identified in the table above. The site has 3 coaster bus parking spaces and 34 vehicle parking spaces. It is also reasonable to assume a further 10 parking spaces within the existing and proposed storage sheds. The site has ample parking for the proposed uses.</p> <p>All vehicles can exit in a forward gear.</p>
Parking for People with Disabilities		
<p>P2 Parking spaces are provided to meet the needs of vehicle occupants with disabilities</p>	<p>A2.1 For parking areas with a total number of ordinary vehicle spaces less than 50, wheelchair accessible spaces are provided as follows:</p> <ul style="list-style-type: none"> • Medical, higher education, entertainment facilities and shopping centres – 2 spaces; • All other uses – 1 space. <p>A2.2 For parking areas with 50 or more ordinary vehicle spaces, wheelchair accessible spaces are provided as follows:</p> <ul style="list-style-type: none"> • Medical, higher education, 	<p>Complies The mobility impaired space is provided adjacent to the sport and rec hall.</p> <p>Not applicable</p>

Performance Criteria	Acceptable Measure	Comment
	entertainment facilities and shopping centres – 3% (to the closest whole number) of the total number of spaces required; <ul style="list-style-type: none"> All other uses – 2% (to the closest whole number) of the total number of spaces required. 	
Motor Cycles		
P3 In recognition that motorcycles are low Road-space transport, a proportion of the parking spaces provided may be for motorcycles. The proportion provided for motor cycles is selected so that: <ul style="list-style-type: none"> ordinary vehicles do not demand parking in the spaces reserved for motor cycles due to capacity constraints; and, it is a reflection of the make-up of the likely vehicle fleet that uses the parking; and, it is not a reflection of the lower cost of providing motorcycle parking. 	A3.1 Parking for motorcycles is substituted for ordinary vehicle parking to a maximum level of 2% per cent of total ordinary parking. AND The motorcycle parking complies with other elements of this Code.	Not applicable
Compact Vehicles		
P4 A proportion of the parking spaces provided may be for compact vehicles. The proportion of total parking provided for compact vehicles is selected considering: <ul style="list-style-type: none"> compact vehicles spaces are not available to non-compact vehicles; and, it is a reflection of the proportion of the likely vehicle fleet that uses the parking; and, compact vehicle spaces are located so as to be proximate to pedestrian destinations such that they present significant inclination 	A4.1 For parking areas exceeding 100 spaces for short term users or 50 spaces for long-term users, parking is provided for compact vehicles as a substitute for ordinary vehicle parking so that: <ul style="list-style-type: none"> compact vehicle parking does not exceed 10% of total vehicle parking required; and, the parking location is proximate to the entry locations for parking users; and, the parking provided complies with other elements of this Code. 	Not applicable

Performance Criteria	Acceptable Measure	Comment
for use by users of compact vehicles; and, <ul style="list-style-type: none"> the scale of parking spaces, likely users and the likely degree of familiarity with the availability of such spaces. 		
Bicycles Parking		
P5 Sufficient bicycle parking spaces with appropriate security and end of trip facilities are provided on-Site to accommodate the amount of bicycles expected to be generated by the use or uses.	A5.1 The minimum number of bicycle parking spaces provided on Site is not less than the number prescribed in Schedule 1 of this Code, for the particular use or uses.	Complies There is room for bicycle parking across the site.
Vehicular Access to the Site		
P6 The location of Access points minimises conflicts and is designed to operate efficiently and safely taking into account: <ul style="list-style-type: none"> the amount and type of vehicular traffic; the type of use (eg long-stay, short-stay, regular, casual); Frontage Road traffic conditions; the nature and extent of future street or intersection improvements; current and future on-street parking arrangements; the capacity of the adjacent street system; and the available sight distance. 	A6.1 The location of the Access points is in accordance with the provisions of the relevant Australian Standards. AND Where the Site has Frontage to more than one street, the Access is from the lowest order street. A6.2 All redundant Accesses must be removed and a suitable barrier Erected to prevent further use of the Access. A6.3 Only one Access point is to be provided to each Site unless stated otherwise in another Code.	Complies Complies Not applicable Refer to discussion in the industry planning area code. The site has an 87.165m ² frontage to Owen St and 2 accesses is reasonable given the planning scheme would permit 1 industrial lot every 25m, which would allow at least 3 accesses onto Owen Street
Accessibility and Amenity for Users		
P7 On-Site vehicle parking is provided where it is convenient, attractive and safe to use, and does not detract from an attractive or existing streetscape character.	A7.1 Short term visitor parking is provided at the front or on the main approach side of the Site, with easy Access to the Building entry, where such provision is in keeping with the	Complies

Performance Criteria	Acceptable Measure	Comment
	materials of any adjacent footpath.	
Access for People with Disabilities		
P11 Access for people with disabilities is provided to the Building from the parking area and from the street.	A11.1 Access for people with disabilities is provided in accordance with the relevant provisions of the Australian Standards.	Complies
Access for Pedestrians		
P12 Access for pedestrians is provided to the Building from the parking area and from the street.	A12.1 Defined, safe pedestrian pathways are provided to the Building entry from the parking area and from the street.	Complies
Access for Cyclists		
P13 Access for cyclists is provided to the Building or to bicycle parking area from the street.	A13.1 Access pathways for cyclists are provided in accordance with the relevant provisions of the Australian Standards. AND Where Access for cyclists is shared with Access for pedestrians and vehicles, the shared use is identified by signage and linemarking.	Not applicable to this scale of development
Dimensions of Parking Spaces		
P14 Parking spaces must have adequate areas and dimensions to meet user requirements.	A14.1 Car parking for the disabled, ordinary car parking spaces and motorcycle parking spaces meet the requirements of the relevant Australian Standards. AND Parking spaces for special vehicles that are classified in accordance with the relevant Australian Standards meet the requirements of that Standard. AND Parking spaces for standard sized buses have the following minimum dimensions: <ul style="list-style-type: none"> • width: 4 metres • length: 20 metres 	Complies as far as relevant to this scale of development Complies Complies to the extent relevant (12m coaster busses) not 20m busses.

Performance Criteria	Acceptable Measure	Comment
	<ul style="list-style-type: none"> • clear Height: 4 metres. <p>AND</p> <ul style="list-style-type: none"> • Parking spaces for compact vehicles have the following minimum dimensions: • 15 per cent less in width measurements than required by Australian Standards for any ordinary vehicle; and, • 20 per cent less in length measurements than required by Australian Standards for any ordinary vehicle. <p>AND</p> <ul style="list-style-type: none"> • Parking spaces for special vehicles meet the requirements dictated by the vehicle dimensions and manoeuvring characteristics and provide sufficient clearance to obstructions and adjacent vehicles to achieve a level of service to users equivalent to that specified by the relevant Australian Standards. <p>A14.2 Parking spaces for bicycles meet the requirement of the relevant Australian Standard.</p>	<p>Not applicable</p> <p>Not applicable</p> <p>Not applicable</p>
On-Site Driveways, Manoeuvring Areas and Parking/Standing Areas		
<p>P15 On-Site driveways, manoeuvring areas and vehicle parking / standing areas are designed, constructed and maintained such that they:</p> <ul style="list-style-type: none"> • are at gradients suitable for intended vehicle use; • consider the shared movements of pedestrians and cyclists; • are effectively drained and surfaced; and • are available at all times they are required. 	<p>A15.1 On-Site driveways, vehicle manoeuvring and loading / unloading areas:</p> <ul style="list-style-type: none"> • are sealed in urban areas: <p>AND</p> <ul style="list-style-type: none"> • upgraded to minimise noise, dust and runoff in other areas of the Shire in accordance with the relevant Locality Code; • have gradients and other design features in accordance with the provisions of the relevant Australian Standards; and 	<p>Complies</p>

Performance Criteria	Acceptable Measure	Comment
	<ul style="list-style-type: none"> drain adequately and in such a way that adjoining and downstream land is not adversely affected. <p>A15.2 Parking areas are kept and used exclusively for parking and are maintained in a suitable condition for parking.</p>	<p>Will be complied with</p>
Vehicle Circulation, Queuing and Set Down Areas		
<p>P16 Sufficient area or appropriate circulation arrangements are provided to enable all vehicles expected to use the Site to drive on and off the Site in forward gear.</p>	<p>A16.1 Circulation and turning areas comply with the provisions of the relevant Australian Standards.</p>	<p>Complies</p>
<p>P17 An on-Site circulation system provides safe and practical Access to all parking, loading/unloading and manoeuvring areas.</p>	<p>A17.1 Circulation driveways comply with the provisions of the relevant Australian Standards.</p>	<p>Complies</p>
<p>P18 Where vehicle queuing, set down or special vehicle parking is expected, sufficient queuing or parking area is provided to enable vehicles to stand without obstructing the free flow of moving traffic or pedestrian movement.</p>	<p>A18.1 Queuing and set down areas comply with the relevant Australian Standard and any relevant AUSTROAD Guidelines.</p>	<p>Not applicable to this scale of development</p>

3.11 Design and Siting of Advertising Devices Code

The purpose of this Code is to:

- ensure that Advertising Devices do not adversely impact on the streetscape or detract from the amenity of the locality;
- ensure that Advertising Devices are appropriate to the scale of surrounding Buildings and the locality;
- ensure that any Advertising Devices which are incorporated in the Site design of a development or the architecture of a Building, complement the Building or development;
- limit the number of Advertising Devices to avoid excessive signage throughout the Shire; and
- ensure that Advertising Devices do not dominate the surrounding vegetation, Landscaping or natural features of the environment and scenic amenity values of the Shire.

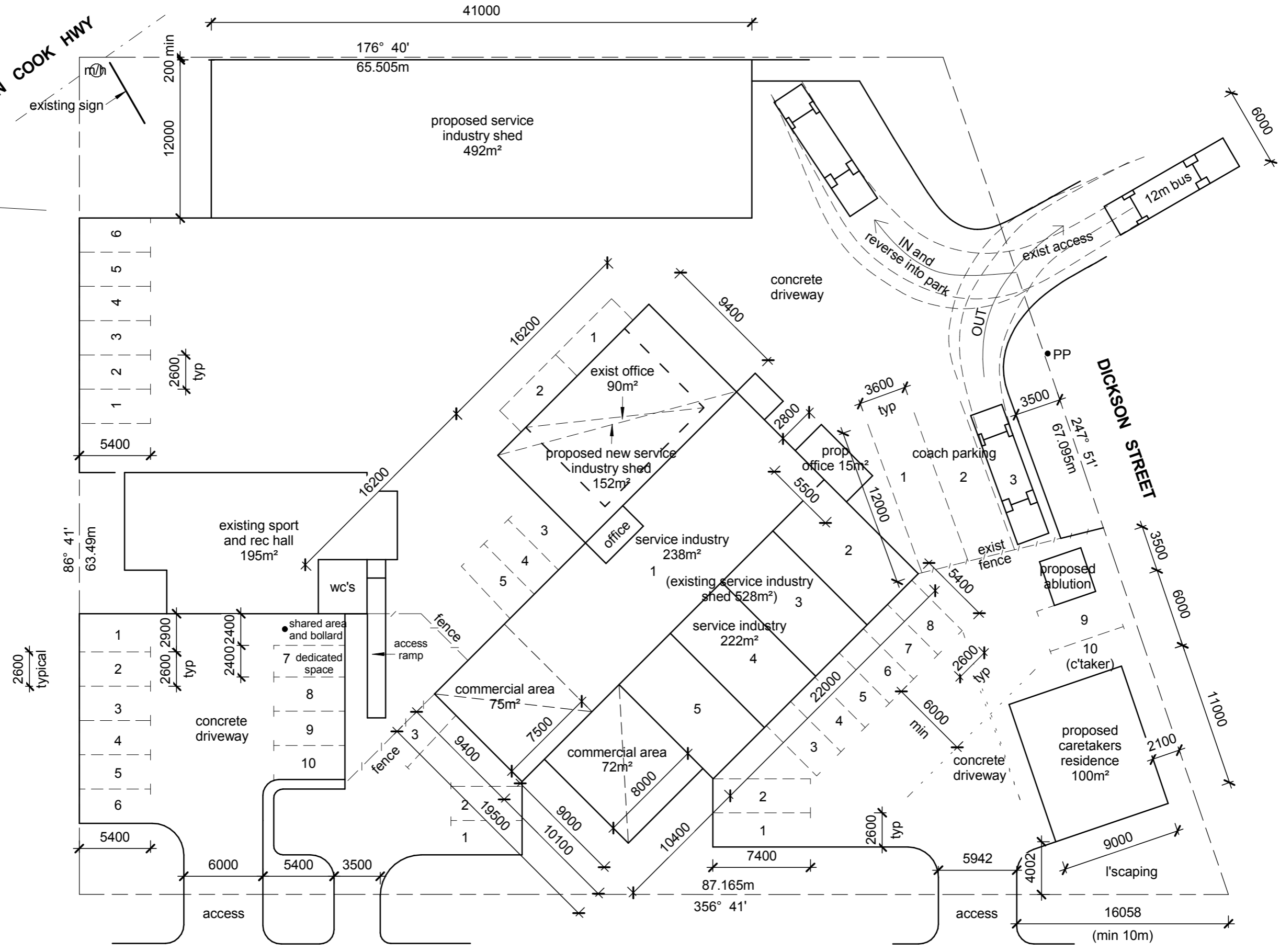
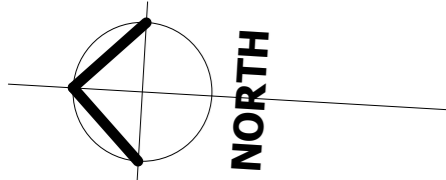
Elements of the Code

Performance Criteria	Acceptable Measure	Comment
<i>Signage Type</i>		
P1 Advertising Devices are subservient in scale to the primary use of the Site and relate to the use/s carried out on the Site	A1.2 Where a Below Awning Sign : <ul style="list-style-type: none"> • maximum one per business, or one per Frontage; • maximum Height of 0.6 metres • ground clearance not less than 2.6 metres • maximum width of 0.3 metres; • maximum length of 2.5 metres and does not project beyond the awning. 	Will be complied with
	A1.5 Where a Fascia Sign located on the fascia of an awning: <ul style="list-style-type: none"> • maximum of one fascia sign per business or one per Frontage; • maximum Height above Ground Level of 2.5 metres; • does not project above or below the fascia of the Building; • does not project within 0.45 metre Setback from the face of the kerb or where no kerb exists, 0.30 	Will be complied with

Performance Criteria	Acceptable Measure	Comment
	<p>metre from the fascia.</p> <p>A1.10 Where a Wall Sign:</p> <ul style="list-style-type: none"> • maximum of one wall sign on any building facade or boundary wall; • maximum area of 4 m²; • maximum length of 3m; • maximum Height of 2m and sited at ground floor level of a Building or boundary wall; • does not project further than 0.10 metres from the face of the wall. <p>A1.12 Where an Indirectly Illuminated Sign:</p> <ul style="list-style-type: none"> • artificial light limited to illuminating the face of the sign; • does not cause light spillage from the source of external illumination; • complies with other relevant requirements for the particular type of Advertising Device, which are specified in this Code; • not located within a State-Controlled Road or on a Council Road. 	<p>Will be complied with</p> <p>Will be complied with</p>
Signage Location		
<p>P2 Advertising Devices are located in appropriate areas, relative to the land uses in the area and the amenity and character of the area.</p>	<p>A2.1 Particular types of Advertising Devices are considered appropriate in the following locations: Residential, Rural and Rural Settlement Areas:</p> <ul style="list-style-type: none"> • Home Activity/Home Based Business Sign; and • Directional Sign <p>Tourist and Residential Areas:</p> <ul style="list-style-type: none"> • Directional Sign; • Projecting Wall Sign; • Symbol; • Wall Sign; and • Indirectly Illuminated Sign. 	<p>The signage is existing. No changes proposed. Will be complied with as applicable</p>

APPENDIX 1 PROPOSAL PLANS

Drawing or Document	Reference	Date
Site Plan – PDM Timber	Greg Skyring Design 703-17 Sheet 1 of 4 Rev C	24.10. 2017
Ablution Floor Plan, Elevations	Greg Skyring Design 703-17 Sheet 2 of 4 Rev C	24.10. 2017
Shed Floor Plan, Elevations	Greg Skyring Design 703-17 Sheet 3 of 4 Rev C	24.10. 2017
Caretakers Floor Plan, Elevations	Greg Skyring Design 703-17 Sheet 4 of 4 Rev C	24.10. 2017



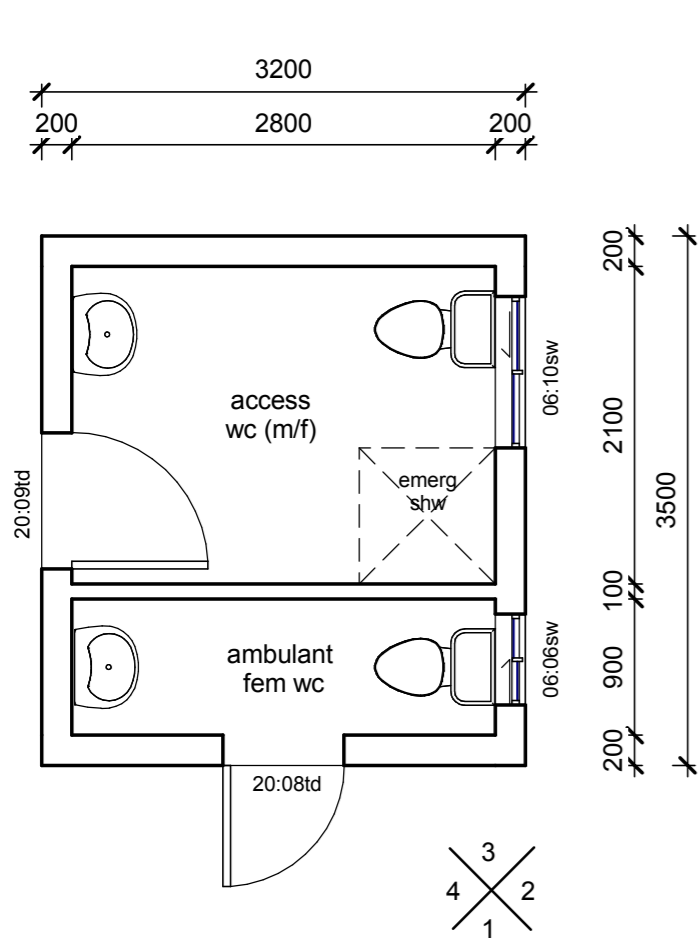
1 Site Plan
1 : 300

GREG SKYRING
Design and DRAFTING Pty. Ltd.
Lic Under QBSA Act 1991 - No 1040371
11 Noli Close,
Mossman Q. 4873
Phone/Fax: (07) 40982061
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Email: greg@skyringdesign.com.au

PROJECT
Proposed Car Parking and Building Layout,
1-5 Dickson Street,
L10 RP746523
CRAIGLIE

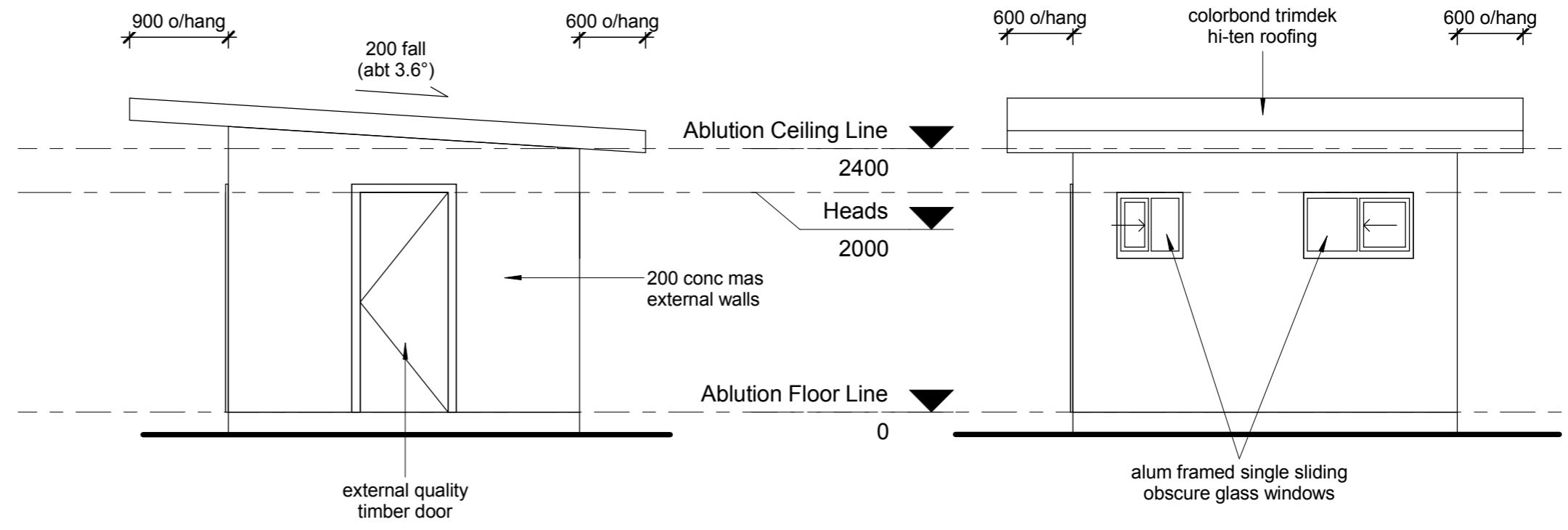
CLIENT
PDM Timber
SCALES
1 : 300
PLAN TITLE
Site Plan

WIND CLASS	PLAN NUMBER 703-17	SHEET 1 of 4
DATE OF ISSUE 24.10.17	REV C	



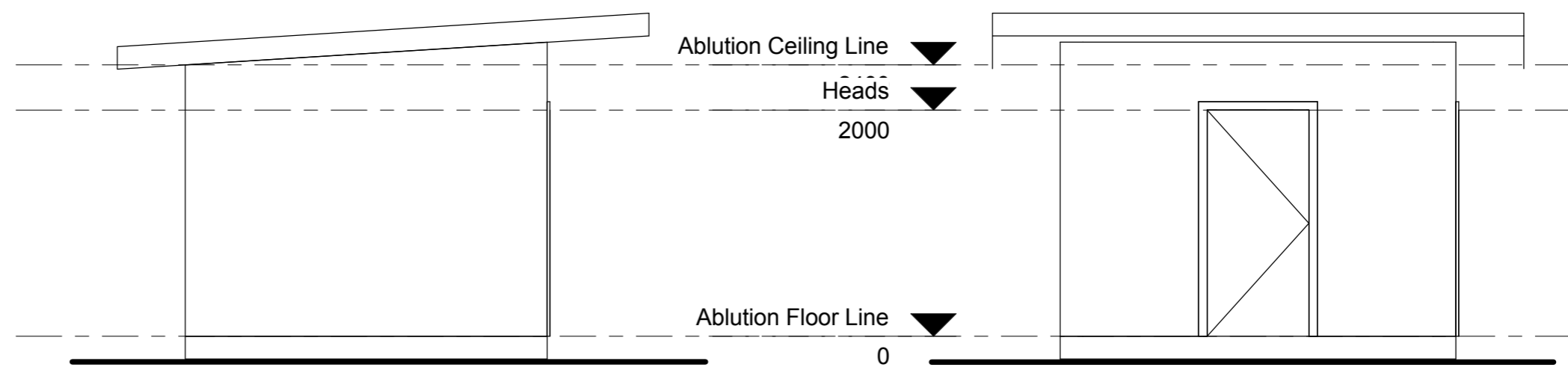
ELEVATION LAYOUT

1 Floor Plan Ablution
1 : 50



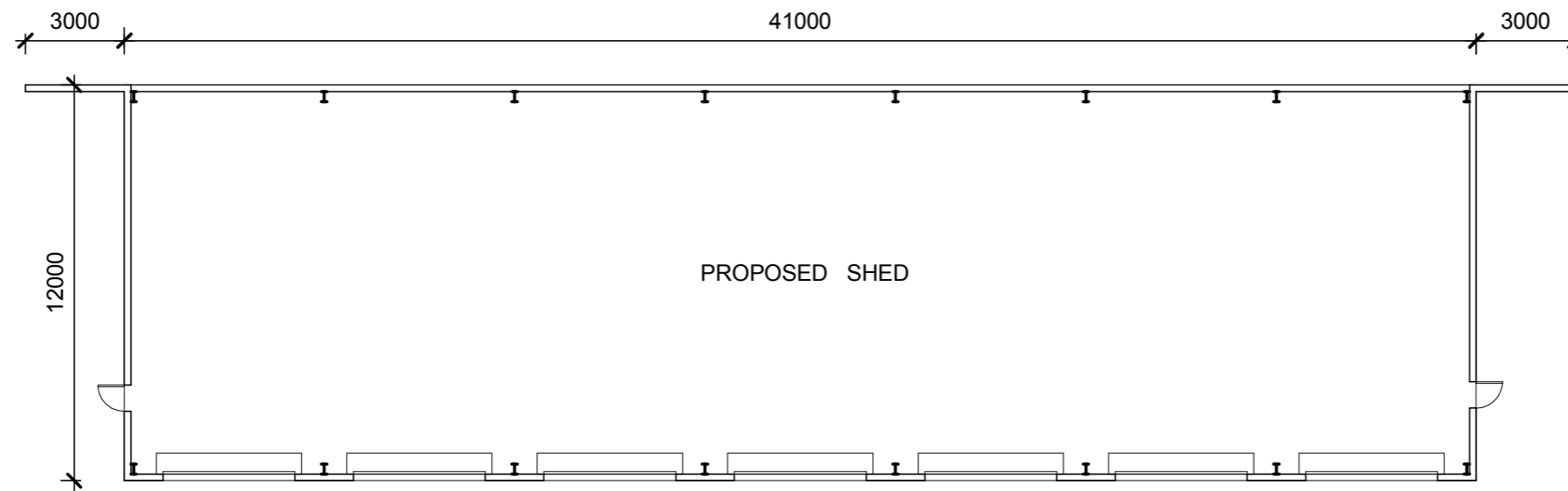
2 Elevation 1 - Ablution
1 : 50

3 Elevation 2 - Ablution
1 : 50



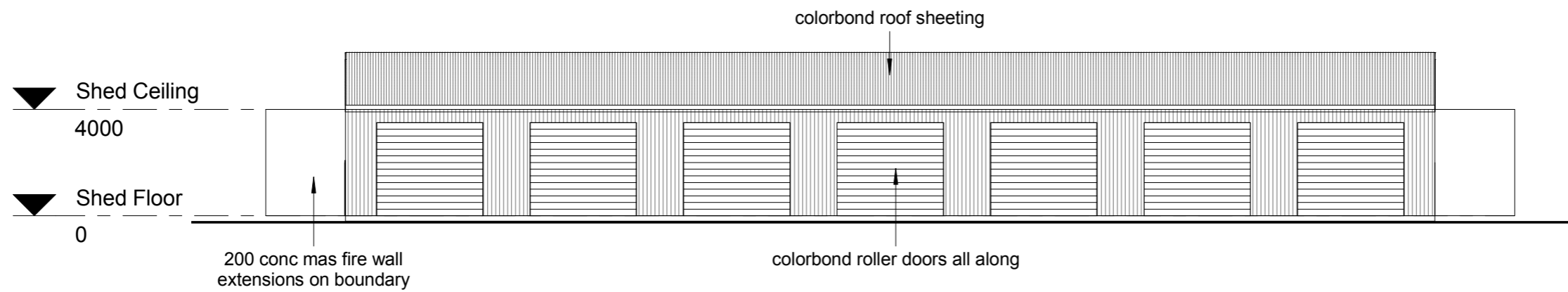
4 Elevation 3 - Ablution
1 : 50

5 Elevation 4 - Ablution
1 : 50

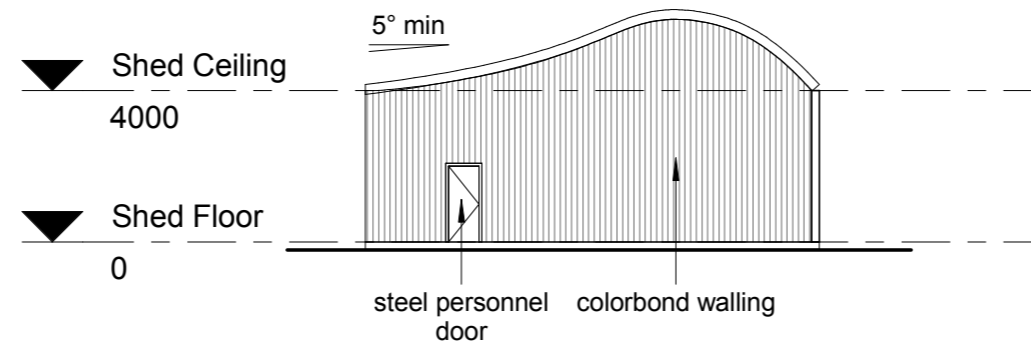


1
2
ELEVATION
LAYOUT

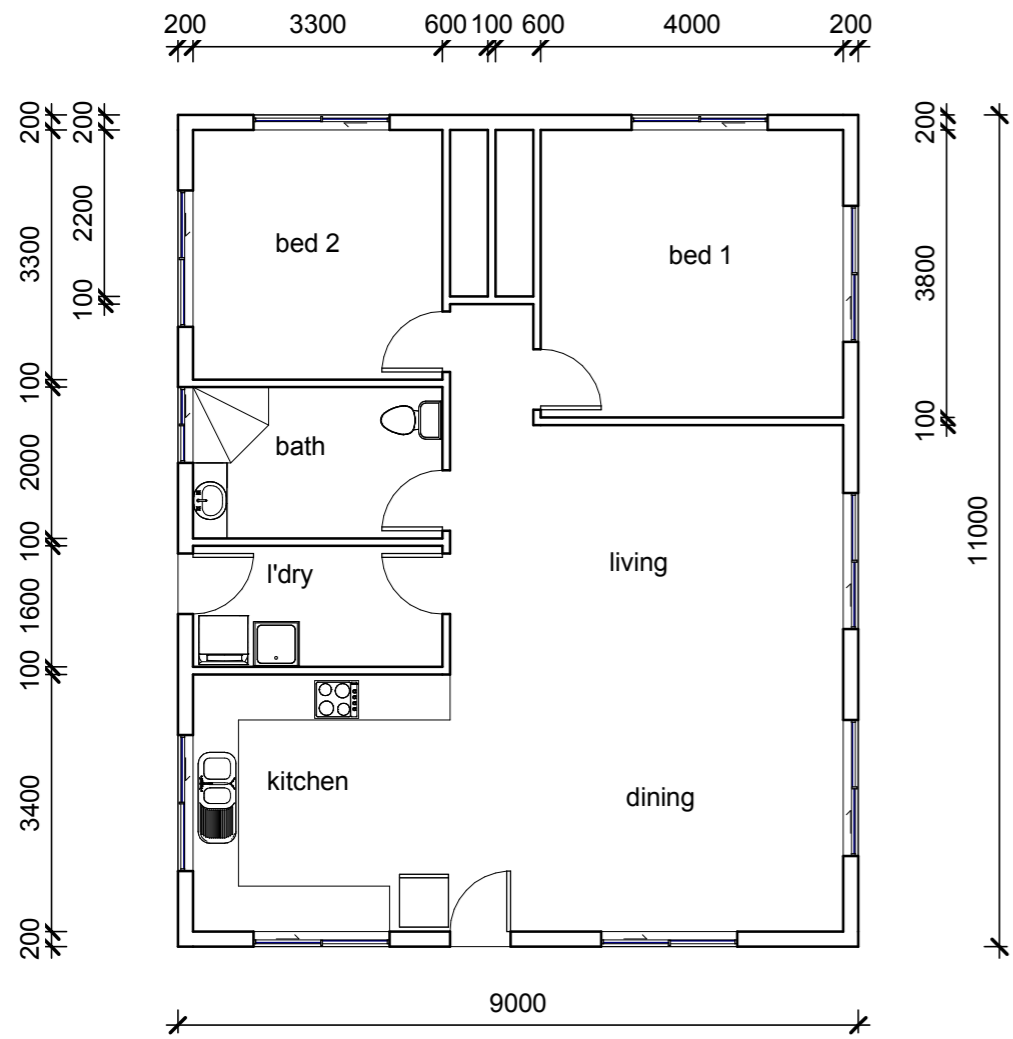
1 Floor Plan Shed
1 : 200



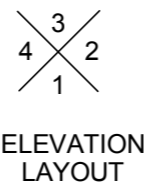
2 Shed Elevation - 1
1 : 200



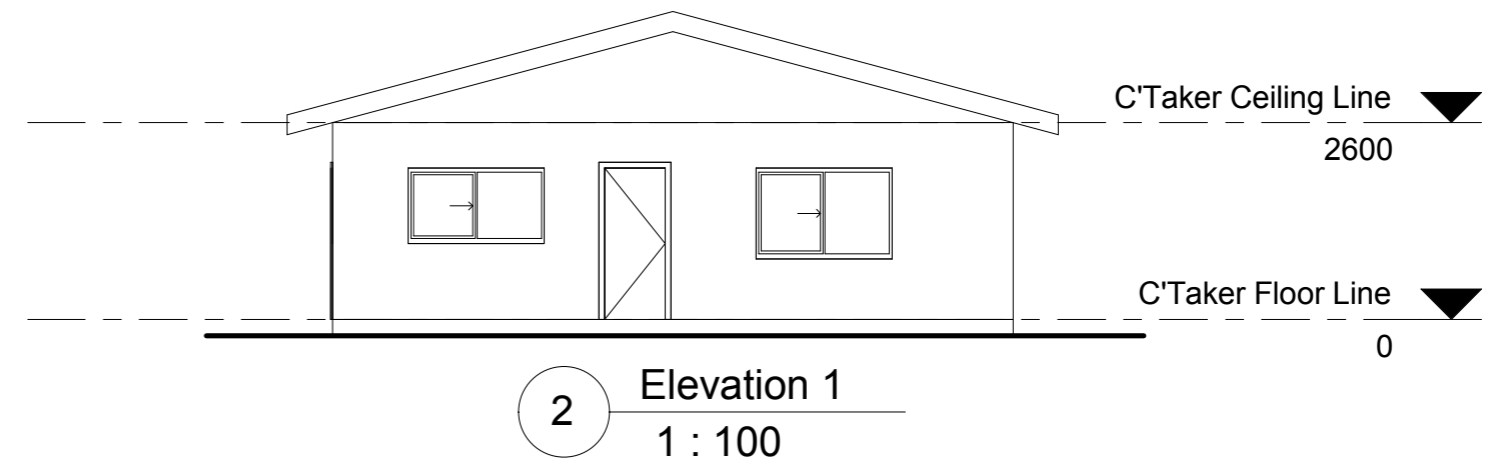
3 Shed Elevation 2
1 : 200



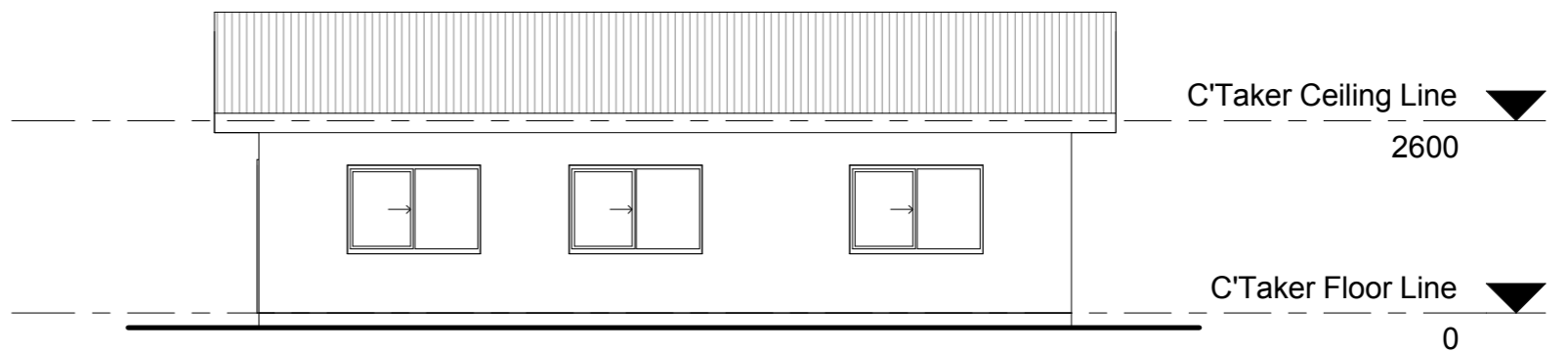
1 Floor Plan C'Takers
1 : 100



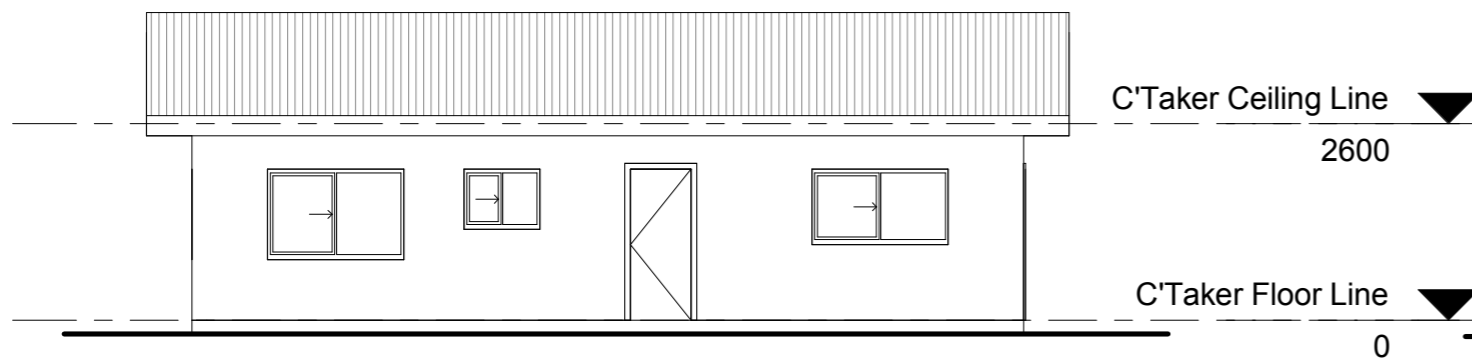
ELEVATION LAYOUT



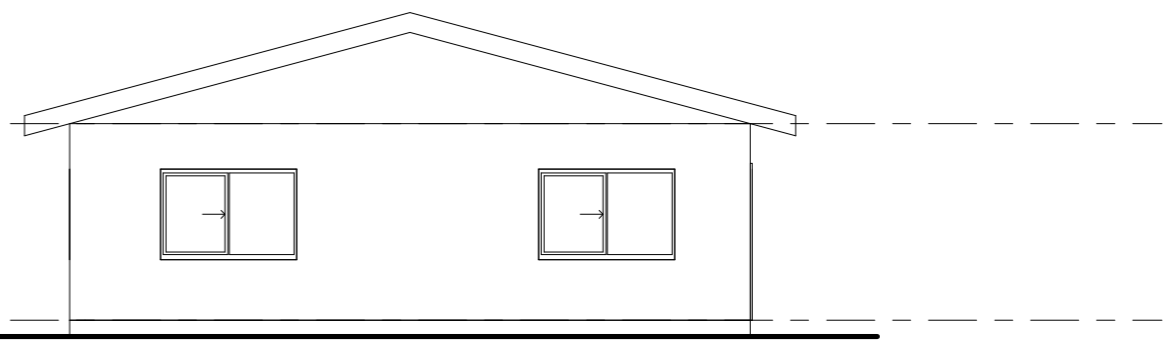
2 Elevation 1
1 : 100



3 Elevation 2
1 : 100



5 Elevation 4
1 : 100



4 Elevation 3
1 : 100

MCU: 1-5 DICKSON ST, CRAIGLIE



APPENDIX 2: HISTORICAL CORRESPONDENCE



ENQUIRIES: Leon Doutre
PHONE: (07) 4044 3243
FAX: (07) 4044 3836
YOUR REF: As Below
OUR REF: 8/20/1 (3723906)

11 September 2012

Planz Town Planning Pty Ltd
17 Atherton Street
Whitfield QLD 4870

ATTN: Ms Nikki Huddy

Dear Madam,

RE: CARETAKER'S RESIDENCE AT LOT 10 DICKSON STREET, CRAIGLIE

Council refers to your letter requesting confirmation of the lawful establishment of a Caretaker's Residence on an industrial property at Dickson Street, Craiglie, on land described as Lot 10 on RP746523. It is understood that you are seeking permission from Council for the existing Caretaker's Residence on the property to be transferred on the site, from behind the large Shed that provides the base of the timber business, to the 'old town hall' building in the centre of the site.

From a review of Council records it would appear that a Caretaker's Residence has been present on the site for well over 10 years. The plans and information attached to this are limited, but the use is verified. This would therefore negate the need for any new development application to supply headworks contributions for the Caretaker's land use. It is recognised that the 'old town hall' building has also been on the site since the mid-2000s.

Officers concur that the hall is likely to be capable of complying with the relevant performance criteria under the Douglas Shire Planning Scheme 2008 and any approval would focus more on the justification of the use and on the provision of services to the residence. Officers therefore recognise that there may be little net benefit in requiring a formal Development Application for the change. That notwithstanding, Council still seek some assurances from you as to how the Caretaker's Residence would effectively function from the hall building and on the decommissioning of the former building used for this purpose.

The new residence is still required to gain a Development Permit for Building Works in addition to the relevant plumbing approvals for the provision of potable water and connection to sewer. The landowner is also required to provide Council with an


43.2012.4724

1/3

updated Certificate of Classification, evidencing that the internal changes made on the site have been certified.

Should you require any further information or assistance, please contact Leon Doutre of Council's Development Assessment Team on telephone number (07) 4044 3243.

Yours faithfully


Neil Beck

A/Manager Development Assessment

APPENDIX 3 SDAP CODES

State code 1: Development in a state-controlled road environment

Table 1.2.1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
Buildings and structures		
PO1 The location of buildings, structures, infrastructure, services and utilities does not create a safety hazard in a state-controlled road, or cause damage to, or obstruct road transport infrastructure	AO1.1 Buildings, structures, infrastructure, services and utilities are not located in a state-controlled road. AND	Complies
	AO1.2 Buildings, structures, infrastructure, services and utilities can be maintained without requiring access to a state-controlled road.	Complies
PO2 The design and construction of Buildings and structures does not create a safety hazard by distracting users of a state-controlled road.	AO2.1 Facades of buildings and structures facing a state-controlled road are made of non-reflective materials. OR	Complies
	AO2.2 Facades of buildings and structures do not reflect point light sources into the face of oncoming traffic on a state-controlled road. AND	Complies
	AO2.3	Complies

Performance outcomes	Acceptable outcomes	Response
	External lighting of buildings and structures is not directed into the face of oncoming traffic on a state-controlled road and does not involve flashing or laser lights. AND	
	AO2.4 Advertising devices visible from a state-controlled road are located and designed in accordance with the Roadside advertising guide, Department of Transport and Main Roads, 2013.	Complies Signage is historical no changes proposed
PO3 Road, pedestrian and bikeway bridges over a state-controlled road are designed and constructed to prevent projectiles from being thrown onto a state-controlled road.	AO3.1 Road, pedestrian and bikeway bridges over a state-controlled road include throw protection screens in accordance with section 4.9.3 of the Design criteria for bridges and other structures manual, Department of Transport and Main Roads, 2014.	Not applicable
Filling, excavation and retaining structures		
PO4 Filling and excavation does not interfere with, or result in damage to, infrastructure or services in a state-controlled road. Note: Information on the location of services and public utility plants in a state-controlled road can be obtained from the Dial Before You Dig service. Where development will impact on an existing or future service or public utility plant in a state-controlled road such that the	No acceptable outcome is prescribed.	Complies No filling or excavation proposed.

Performance outcomes	Acceptable outcomes	Response
<p>service or public utility plant will need to be relocated, the alternative alignment must comply with the standards and design specifications of the relevant service or public utility provider, and any costs of relocation are to be borne by the developer.</p>		
<p>PO5 Filling, excavation, building foundations and retaining structures do not undermine, or cause subsidence of, a state-controlled road.</p> <p>Note: To demonstrate compliance with this performance outcome, it is recommended an RPEQ certified geotechnical assessment, prepared in accordance with Volume 3 of the Road Planning And Design Manual 2nd edition, Department of Transport and Main Roads, 2016, is provided.</p>	<p>No acceptable outcome is prescribed.</p>	<p>Complies</p>
<p>PO6 Filling, excavation, building foundations and retaining structures do not cause ground water disturbance in a state-controlled road.</p> <p>Note: To demonstrate compliance with this performance outcome, it is recommended an RPEQ certified geotechnical assessment, prepared in accordance with Volume 3 of the Road planning and design manual 2nd edition, Department of Transport and Main Roads, 2016, is provided.</p>	<p>No acceptable outcome is prescribed.</p>	<p>Complies The site drains away from the road.</p>
<p>PO7 Excavation, boring, piling, blasting or fill compaction during construction of a development does not result in ground movement or vibration impacts that would cause damage or nuisance to a state-controlled road, road transport infrastructure or road works.</p>	<p>No acceptable outcome is prescribed.</p>	<p>Complies No filling or excavation proposed.</p>

Performance outcomes	Acceptable outcomes	Response
<p>Note: To demonstrate compliance with this performance outcome, it is recommended an RPEQ certified geotechnical assessment, prepared in accordance with Volume 3 of the Road Planning And Design Manual 2nd edition, Department of Transport and Main Roads, 2016, is provided.</p>		
<p>PO8 Development involving the haulage of fill, extracted material or excavated spoil material exceeding 10,000 tonnes per year does not damage the pavement of a state-controlled road.</p> <p>Note: It is recommended a pavement impact assessment is provided in accordance with the Guide to Traffic Impact Assessment, Department of Transport and Main Roads, 2017.</p>	<p>AO8.1 Fill, extracted material and spoil material is not transported to or from the development site on a state-controlled road.</p>	<p>Complies No filling or excavation proposed.</p>
<p>PO9 Filling and excavation associated with the construction of vehicular access to a development does not compromise the operation or capacity of existing drainage infrastructure for a state-controlled road.</p>	<p>No acceptable outcome is prescribed.</p>	<p>Will be complied with as appropriate No filling or excavation proposed.</p>
<p>PO10 Fill material used on a development site does not result in contamination of a state-controlled road.</p>	<p>AO10.1 Fill material is free of contaminants including acid sulfate content.</p> <p>Note: Soils and rocks should be tested in accordance with AS 1289.0 – Methods of testing soils for engineering purposes and AS 4133.0-2005 – Methods of testing rocks for engineering purposes.</p>	<p>Will be complied with as appropriate No filling or excavation proposed.</p>

Performance outcomes	Acceptable outcomes	Response
	AND	
	AO10.2 Compaction of fill is carried out in accordance with the requirements of AS 1289.0 2000 – Methods of testing soils for engineering purposes.	Will be complied with as appropriate
PO11 Filling and excavation does not cause wind-blown dust nuisance in a state-controlled road.	AO11.1 Compaction of fill is carried out in accordance with the requirements of AS 1289.0 2000 – Methods of testing soils for engineering purposes. AND	Will be complied with as appropriate
	AO11.2 Dust suppression measures are used during filling and excavation activities such as wind breaks or barriers and dampening of ground surfaces.	Will be complied with
Stormwater and drainage		
PO12 Development does not result in an actionable nuisance, or worsening of, stormwater, flooding or drainage impacts in a state-controlled road.	No acceptable outcome is prescribed.	Complies The site drains away from the road.
PO13	AO13.1 Development does not create any new points of discharge to a state-controlled road.	Will be complied with The site drains away from the road.

Performance outcomes	Acceptable outcomes	Response
Run-off from the development site is not unlawfully discharged to a state-controlled road.	AND	
	AO13.2 Stormwater run-off is discharged to a lawful point of discharge. Note: Section 3.4 of the Queensland Urban Drainage Manual, Department of Energy and Water Supply, 2013, provides further information on lawful points of discharge. AND	Will be complied with
	AO13.3 Development does not worsen the condition of an existing lawful point of discharge to the state-controlled road.	Will be complied with
PO14 Run-off from the development site during construction does not cause siltation of stormwater infrastructure affecting a state-controlled road.	AO14.1 Run-off from the development site during construction is not discharged to stormwater infrastructure for a state-controlled road.	Will be complied with
Vehicular access to a state-controlled road		
PO15 Vehicular access to a state-controlled road that is a limited access road is consistent with government policy for the management of limited access roads.	AO15.1 Development does not require new or changed access to a limited access road. Note: Limited access roads are declared by the transport chief executive under section 54 of the Transport Infrastructure Act 1994 and are identified in the DA mapping system. OR	Not a limited access road
	AO15.2	Not a limited access road

Performance outcomes	Acceptable outcomes	Response
	<p>A new or changed access to a limited access road is consistent with the limited access policy for the state-controlled road.</p> <p>Note: Limited access policies for limited access roads declared under the Transport Infrastructure Act 1994 can be obtained by contacting the relevant Department of Transport and Main Roads regional office.</p> <p>AND</p>	
	<p>AO15.3</p> <p>Where a new or changed access is for a service centre, access is consistent with the Service centre policy, Department of Transport and Main Roads, 2013 and the Access policy for roadside service centre facilities on limited access roads, Department of Transport and Main Roads, 2013, and the Service centre strategy for the state-controlled road.</p> <p>Note: The Service centre policy, Department of Transport and Main Roads, 2013, Access policy for roadside service centre facilities, Department of Transport and Main Roads, 2013 and the relevant Service centre strategy for a state-controlled road can be accessed by contacting the relevant Department of Transport and Main Roads regional office.</p>	<p>Not applicable</p>
<p>PO16</p> <p>The location and design of vehicular access to a state-controlled road (including access to a limited access road) does not create a safety hazard for users of a state-controlled road or result in a worsening of operating conditions on a state-controlled road.</p>	<p>AO16.1</p> <p>Vehicular access is provided from a local government road.</p>	<p>Not applicable</p>
	<p>OR all of the following acceptable outcomes apply:</p> <p>AO16.2</p>	<p>Will be complied with</p>

Performance outcomes	Acceptable outcomes	Response
<p>Note: Where a new or changed access between the premises and a state-controlled road is proposed, the Department of Transport and Main Roads will need to assess the proposal to determine if the vehicular access for the development is safe. An assessment can be made by Department of Transport and Main Roads as part of the development assessment process and a decision under section 62 of Transport Infrastructure Act 1994 issued where sufficient information is provided.</p>	<p>Vehicular access for the development is consistent with the function and design of the state-controlled road. AND</p>	
	<p>AO16.3 Development does not require new or changed access between the premises and the state-controlled road. Note: A decision under section 62 of the Transport Infrastructure Act 1994 outlines the approved conditions for use of an existing vehicular access to a state-controlled road. Current section 62 decisions can be obtained from the relevant Department of Transport and Main Roads regional office. AND</p>	Complies
	<p>AO16.4 Use of any existing vehicular access to the development is consistent with a decision under section 62 of the <i>Transport Infrastructure Act 1994</i>. Note: The development which is the subject of the application must be of an equivalent use and intensity for which the section 62 approval was issued and the section 62 approval must have been granted no more than 5 years prior to the lodgement of the application. AND</p>	Not applicable
	<p>AO16.5 Onsite vehicle circulation is designed to give priority to entering vehicles at all times so vehicles do not queue in a road intersection or on the state-controlled road.</p>	Complies
PO17	AO17.1	Not applicable

Performance outcomes	Acceptable outcomes	Response
<p>Vehicular access to a state-controlled road or local government road (and associated road access works) are located and designed to not damage or interfere with public passenger transport infrastructure, public passenger services or pedestrian or cycle access to public passenger transport infrastructure and public passenger services.</p>	<p>Vehicular access and associated road access works are not located within 5 metres of existing public passenger transport infrastructure. AND</p>	
	<p>AO17.2 The location and design of vehicular access for a development does not necessitate the relocation of existing public passenger transport infrastructure. AND</p>	<p>Not applicable</p>
	<p>AO17.3 On-site vehicle circulation is designed to give priority to entering vehicles at all times so vehicles using a vehicular access do not obstruct public passenger transport infrastructure and public passenger services or obstruct pedestrian or cycle access to public passenger transport infrastructure and public passenger services. AND</p>	<p>Not applicable</p>
	<p>AO17.4 The normal operation of public passenger transport infrastructure or public passenger services is not interrupted during construction of the development.</p>	<p>Not applicable</p>
<p>Vehicular access to local roads within 100 metres of an intersection with a state-controlled road</p>		
<p>PO18</p>	<p>AO18.1</p>	<p>Complies</p>

Performance outcomes	Acceptable outcomes	Response
<p>The location and design of vehicular access to a local road within 100 metres of an intersection with a state-controlled road does not create a safety hazard for users of a state-controlled road.</p>	<p>Vehicular access is located as far as possible from the state-controlled road intersection. AND</p>	<p>The access on to Dickson St is 50m from the State Controlled Road. Dickson Street is not constructed completely past this point.</p>
	<p>AO18.2 Vehicular access is in accordance with volume 3, parts, 3, 4 and 4A of the Road Planning And Design Manual, 2nd edition, Department of Transport and Main Roads, 2016. AND</p>	<p>Not applicable</p>
	<p>AO18.3 Onsite vehicle circulation is designed to give priority to entering vehicles at all times so vehicles do not queue in the intersection or on the state-controlled road.</p>	<p>Not applicable</p>
<p>Planned upgrades</p>		
<p>PO19 Development does not impede delivery of planned upgrades of state-controlled roads.</p>	<p>AO19.1 Development is not located on land identified by the Department of Transport and Main Roads as land required for the planned upgrade of a state-controlled road. Note: Land required for the planned upgrade of a state-controlled road is identified in the DA mapping system. OR</p>	<p>Not applicable</p>
	<p>AO19.2 Development is sited and designed so that permanent buildings, structures,</p>	<p>Not applicable</p>

Performance outcomes	Acceptable outcomes	Response
	infrastructure, services or utilities are not located on land identified by the Department of Transport and Main Roads as land required for the planned upgrade of a state-controlled road.	
	OR all of the following acceptable outcomes apply: AO19.3 Structures and infrastructure located on land identified by the Department of Transport and Main Roads as land required for the planned upgrade of a state-controlled road are able to be readily relocated or removed without materially affecting the viability or functionality of the development. AND	<p>Not applicable</p>
	AO19.4 Vehicular access for the development is consistent with the function and design of the planned upgrade of the state-controlled road. AND	<p>Not applicable</p>
	AO19.5 Development does not involve filling and excavation of, or material changes to, land required for a planned upgrade to a state-controlled road. AND	<p>Not applicable</p>
	AO19.6	<p>Not applicable</p>

Performance outcomes	Acceptable outcomes	Response
	Land is able to be reinstated to the pre-development condition at the completion of the use.	
Network impacts		
<p>PO20 Development does not result in a worsening of operating conditions on the state-controlled road <u>network</u>.</p> <p>Note: To demonstrate compliance with this performance outcome, it is recommended that an RPEQ certified traffic impact assessment is provided, prepared in accordance with the Guide to Traffic Impact Assessment, Department of Transport and Main Roads, 2017.</p>	No acceptable outcome is prescribed.	Complies
<p>PO21 Development does not impose traffic loadings on a state-controlled road which could be accommodated on the local road network.</p>	<p>AO21.1 The layout and design of the development directs traffic generated by the development to the local road network.</p>	<p>Complies Refer to discussion in Section 1.4 of this report.</p>
<p>PO22 Upgrade works on, or associated with, a <u>state-controlled road</u> are built in accordance with Queensland road design standards.</p>	<p>AO22.1 Upgrade works required as a result of the development are designed and constructed in accordance with the Road planning and design manual, 2nd edition, Department of Transport and Main Roads, 2016.</p> <p>Note: Road works in a state-controlled road require approval under section 33 of the <i>Transport Infrastructure Act 1994</i> before the works commence.</p>	Will be complied with