

3 February 2025



Chief Executive Officer  
Douglas Shire Council  
64-66 Front Street  
MOSSMAN QLD 4873

Via email: [enquiries@douglas.qld.gov.au](mailto:enquiries@douglas.qld.gov.au)

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**RE: COMBINED DEVELOPMENT APPLICATION FOR RECONFIGURING A LOT (1 LOT INTO 2 LOTS) AND A MATERIAL CHANGE OF USE (2 x DWELLING HOUSES) OVER LAND AT 12 DAVIDSON STREET, PORT DOUGLAS, MORE FORMALLY DESCRIBED AS LOT 706 ON CROWN PLAN PTD2092**

Aspire Town Planning and Project Services acts on behalf of Davidson Developments Pty Ltd (the 'Landowner' and 'Applicant'). On behalf of the Applicant, we are pleased to submit this Development Application, made in accordance with Sections 50 and 51 of the Planning Act 2016, seeking a Development Permit for Reconfiguring a Lot (1 into 2 Lots) and a Material Change of Use (Dwelling House) at 12 Davidson Street, Port Douglas.

The application has been prepared with reference to relevant legislative requirements and includes the following supporting documentation:

- Attachment 1 – Duly completed DA Form 1; and
- Attachment 2 – Town Planning Report, addressing the applicable planning considerations.

**Application Fees & Payment Request**

Under the Douglas Shire Council Fees and Charges Schedule 2024/25, the applicable Development Application fee has been calculated as \$2,240.00, comprising:

- Reconfiguring a Lot (1 into 2 Lots): \$1,524.00
- Material Change of Use (Dwelling House): \$716.00

We kindly request that Council issue an invoice for the total amount, to be paid directly by the Applicant.

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PO BOX 1040, MOSSMAN QLD 4873  
M. 0418826560  
E. [admin@aspireqld.com](mailto:admin@aspireqld.com)  
W. [www.aspireqld.com](http://www.aspireqld.com)  
ABN. 79 851 193 691

**Further Information & Site Inspection**

We appreciate your time in reviewing this application and look forward to Council's assessment. Should you require any further information or wish to arrange a site inspection, please do not hesitate to contact the undersigned at your earliest convenience.

Regards,

A handwritten signature in black ink, consisting of a large, stylized loop followed by a horizontal line and a small flourish.

Daniel Favier

**Senior Town Planner**

**ASPIRE Town Planning and Project Services**

## **Attachment I**

**Duly completed DA Form I**

# DA Form 1 – Development application details

Approved form (version 1.6 effective 2 August 2024) made under section 282 of the Planning Act 2016.

This form **must** be used to make a development application **involving code assessment or impact assessment**, except when applying for development involving only building work.

For a development application involving **building work only**, use *DA Form 2 – Building work details*.

For a development application involving **building work associated with any other type of assessable development (i.e. material change of use, operational work or reconfiguring a lot)**, use this form (*DA Form 1*) and parts 4 to 6 of *DA Form 2 – Building work details*.

Unless stated otherwise, all parts of this form **must** be completed in full and all required supporting information **must** accompany the development application.

One or more additional pages may be attached as a schedule to this development application if there is insufficient space on the form to include all the necessary information.

This form and any other form relevant to the development application must be used to make a development application relating to strategic port land and Brisbane core port land under the *Transport Infrastructure Act 1994*, and airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*. For the purpose of assessing a development application relating to strategic port land and Brisbane core port land, any reference to a planning scheme is taken to mean a land use plan for the strategic port land, Brisbane port land use plan for Brisbane core port land, or a land use plan for airport land.

**Note:** All terms used in this form have the meaning given under the Planning Act 2016, the Planning Regulation 2017, or the Development Assessment Rules (DA Rules).

## PART 1 – APPLICANT DETAILS

1) Applicant details	
Applicant name(s) (individual or company full name)	Davidson Street Developments Pty Ltd
Contact name (only applicable for companies)	c/- Daniel Favier T/A Aspire Town Planning and Project Services
Postal address (P.O. Box or street address)	PO Box 1040
Suburb	Mossman
State	QLD
Postcode	4873
Country	Australia
Contact number	0418 826 560
Email address (non-mandatory)	admin@aspireqld.com
Mobile number (non-mandatory)	
Fax number (non-mandatory)	
Applicant's reference number(s) (if applicable)	2024-10-39 - Davidson Developments Pty Ltd - 12 Davidson Street, Port Douglas
1.1) Home-based business	
<input type="checkbox"/> Personal details to remain private in accordance with section 264(6) of <i>Planning Act 2016</i>	
2) Owner's consent	
2.1) Is written consent of the owner required for this development application?	
<input type="checkbox"/> Yes – the written consent of the owner(s) is attached to this development application	
<input checked="" type="checkbox"/> No – proceed to 3)	



## PART 2 – LOCATION DETAILS

### 3) Location of the premises (complete 3.1) or 3.2), and 3.3) as applicable)

**Note:** Provide details below and attach a site plan for any or all premises part of the development application. For further information, see [DA Forms Guide: Relevant plans](#).

#### 3.1) Street address and lot on plan

- ☒ Street address **AND** lot on plan (all lots must be listed), **or**  
☐ Street address **AND** lot on plan for an adjoining or adjacent property of the premises (appropriate for development in water but adjoining or adjacent to land e.g. jetty, pontoon. All lots must be listed).

a)	Unit No.	Street No.	Street Name and Type	Suburb
		12	Davidson Street	Port Douglas
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)
	4877	706	Crown Plan PTD2092	Douglas
b)	Unit No.	Street No.	Street Name and Type	Suburb
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)

#### 3.2) Coordinates of premises (appropriate for development in remote areas, over part of a lot or in water not adjoining or adjacent to land e.g. channel dredging in Moreton Bay)

**Note:** Place each set of coordinates in a separate row.

- ☐ Coordinates of premises by longitude and latitude

Longitude(s)	Latitude(s)	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

- ☐ Coordinates of premises by easting and northing

Easting(s)	Northing(s)	Zone Ref.	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> 54 <input type="checkbox"/> 55 <input type="checkbox"/> 56	<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

#### 3.3) Additional premises

- ☐ Additional premises are relevant to this development application and the details of these premises have been attached in a schedule to this development application  
☒ Not required

#### 4) Identify any of the following that apply to the premises and provide any relevant details

- ☐ In or adjacent to a water body or watercourse or in or above an aquifer

Name of water body, watercourse or aquifer:

- ☐ On strategic port land under the *Transport Infrastructure Act 1994*

Lot on plan description of strategic port land:

Name of port authority for the lot:

- ☐ In a tidal area

Name of local government for the tidal area (if applicable):

Name of port authority for tidal area (if applicable)

<input type="checkbox"/> On airport land under the <i>Airport Assets (Restructuring and Disposal) Act 2008</i>
Name of airport: <input type="text"/>
<input type="checkbox"/> Listed on the Environmental Management Register (EMR) under the <i>Environmental Protection Act 1994</i>
EMR site identification: <input type="text"/>
<input type="checkbox"/> Listed on the Contaminated Land Register (CLR) under the <i>Environmental Protection Act 1994</i>
CLR site identification: <input type="text"/>

#### 5) Are there any existing easements over the premises?

*Note: Easement uses vary throughout Queensland and are to be identified correctly and accurately. For further information on easements and how they may affect the proposed development, see [DA Forms Guide](#).*

- ☐ Yes – All easement locations, types and dimensions are included in plans submitted with this development application
- ☒ No

## PART 3 – DEVELOPMENT DETAILS

### Section 1 – Aspects of development

#### 6.1) Provide details about the first development aspect

a) What is the type of development? *(tick only one box)*

- ☐ Material change of use    ☒ Reconfiguring a lot    ☐ Operational work    ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☒ Development permit    ☐ Preliminary approval    ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☒ Code assessment    ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal (e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):

Reconfiguring a Lot (1 Lot into 2 Lots)

e) Relevant plans

**Note:** Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms guide: Relevant plans](#).

- ☒ Relevant plans of the proposed development are attached to the development application

#### 6.2) Provide details about the second development aspect

a) What is the type of development? *(tick only one box)*

- ☒ Material change of use    ☐ Reconfiguring a lot    ☐ Operational work    ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☒ Development permit    ☐ Preliminary approval    ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☒ Code assessment    ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal (e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):

2 x Dwelling House

e) Relevant plans

**Note:** Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).

- ☒ Relevant plans of the proposed development are attached to the development application

**6.3) Additional aspects of development**

- ☐ Additional aspects of development are relevant to this development application and the details for these aspects that would be required under Part 3 Section 1 of this form have been attached to this development application
- ☒ Not required

**6.4) Is the application for State facilitated development?**

- ☐ Yes - Has a notice of declaration been given by the Minister?
- ☒ No

**Section 2 – Further development details****7) Does the proposed development application involve any of the following?**

Material change of use	<input checked="" type="checkbox"/> Yes – complete division 1 if assessable against a local planning instrument
Reconfiguring a lot	<input checked="" type="checkbox"/> Yes – complete division 2
Operational work	<input type="checkbox"/> Yes – complete division 3
Building work	<input type="checkbox"/> Yes – complete <i>DA Form 2 – Building work details</i>

**Division 1 – Material change of use**

**Note:** This division is only required to be completed if any part of the development application involves a material change of use assessable against a local planning instrument.

**8.1) Describe the proposed material change of use**

Provide a general description of the proposed use	Provide the planning scheme definition (include each definition in a new row)	Number of dwelling units (if applicable)	Gross floor area (m <sup>2</sup> ) (if applicable)
Dwelling House	Dwelling House	2	

**8.2) Does the proposed use involve the use of existing buildings on the premises?**

- ☐ Yes
- ☒ No

**8.3) Does the proposed development relate to temporary accepted development under the Planning Regulation?**

- ☐ Yes – provide details below or include details in a schedule to this development application
- ☒ No

Provide a general description of the temporary accepted development	Specify the stated period dates under the Planning Regulation

**Division 2 – Reconfiguring a lot**

**Note:** This division is only required to be completed if any part of the development application involves reconfiguring a lot.

**9.1) What is the total number of existing lots making up the premises?**

1

**9.2) What is the nature of the lot reconfiguration? (tick all applicable boxes)**

<input checked="" type="checkbox"/> Subdivision (complete 10)	<input type="checkbox"/> Dividing land into parts by agreement (complete 11)
<input type="checkbox"/> Boundary realignment (complete 12)	<input type="checkbox"/> Creating or changing an easement giving access to a lot from a constructed road (complete 13)

10) Subdivision				
10.1) For this development, how many lots are being created and what is the intended use of those lots:				
Intended use of lots created	Residential	Commercial	Industrial	Other, please specify:
Number of lots created	2			

10.2) Will the subdivision be staged?	
<input type="checkbox"/> Yes – provide additional details below <input checked="" type="checkbox"/> No	
How many stages will the works include?	
What stage(s) will this development application apply to?	

11) Dividing land into parts by agreement – how many parts are being created and what is the intended use of the parts?				
Intended use of parts created	Residential	Commercial	Industrial	Other, please specify:
Number of parts created				

12) Boundary realignment			
12.1) What are the current and proposed areas for each lot comprising the premises?			
Current lot		Proposed lot	
Lot on plan description	Area (m <sup>2</sup> )	Lot on plan description	Area (m <sup>2</sup> )
12.2) What is the reason for the boundary realignment?			

13) What are the dimensions and nature of any existing easements being changed and/or any proposed easement? (attach schedule if there are more than two easements)				
Existing or proposed?	Width (m)	Length (m)	Purpose of the easement? (e.g. pedestrian access)	Identify the land/lot(s) benefitted by the easement

### Division 3 – Operational work

**Note:** This division is only required to be completed if any part of the development application involves operational work.

14.1) What is the nature of the operational work?	
<input type="checkbox"/> Road work <input type="checkbox"/> Drainage work <input type="checkbox"/> Landscaping <input type="checkbox"/> Other – please specify:	<input type="checkbox"/> Stormwater <input type="checkbox"/> Earthworks <input type="checkbox"/> Signage <input type="checkbox"/> Water infrastructure <input type="checkbox"/> Sewage infrastructure <input type="checkbox"/> Clearing vegetation
14.2) Is the operational work necessary to facilitate the creation of new lots? (e.g. subdivision)	
<input type="checkbox"/> Yes – specify number of new lots:	
<input type="checkbox"/> No	
14.3) What is the monetary value of the proposed operational work? (include GST, materials and labour)	
\$	

## PART 4 – ASSESSMENT MANAGER DETAILS

15) Identify the assessment manager(s) who will be assessing this development application

Douglas Shire Council

16) Has the local government agreed to apply a superseded planning scheme for this development application?

- ☐ Yes – a copy of the decision notice is attached to this development application
- ☐ The local government is taken to have agreed to the superseded planning scheme request – relevant documents attached
- ☒ No

## PART 5 – REFERRAL DETAILS

17) Does this development application include any aspects that have any referral requirements?

**Note:** A development application will require referral if prescribed by the Planning Regulation 2017.

- ☐ No, there are no referral requirements relevant to any development aspects identified in this development application – proceed to Part 6

Matters requiring referral to the **Chief Executive of the Planning Act 2016:**

- ☐ Clearing native vegetation
- ☐ Contaminated land (*unexploded ordnance*)
- ☐ Environmentally relevant activities (ERA) (*only if the ERA has not been devolved to a local government*)
- ☐ Fisheries – aquaculture
- ☐ Fisheries – declared fish habitat area
- ☐ Fisheries – marine plants
- ☐ Fisheries – waterway barrier works
- ☐ Hazardous chemical facilities
- ☐ Heritage places – Queensland heritage place (*on or near a Queensland heritage place*)
- ☐ Infrastructure-related referrals – designated premises
- ☒ Infrastructure-related referrals – state transport infrastructure
- ☐ Infrastructure-related referrals – State transport corridor and future State transport corridor
- ☐ Infrastructure-related referrals – State-controlled transport tunnels and future state-controlled transport tunnels
- ☐ Infrastructure-related referrals – near a state-controlled road intersection
- ☐ Koala habitat in SEQ region – interfering with koala habitat in koala habitat areas outside koala priority areas
- ☐ Koala habitat in SEQ region – key resource areas
- ☐ Ports – Brisbane core port land – near a State transport corridor or future State transport corridor
- ☐ Ports – Brisbane core port land – environmentally relevant activity (ERA)
- ☐ Ports – Brisbane core port land – tidal works or work in a coastal management district
- ☐ Ports – Brisbane core port land – hazardous chemical facility
- ☐ Ports – Brisbane core port land – taking or interfering with water
- ☐ Ports – Brisbane core port land – referable dams
- ☐ Ports – Brisbane core port land – fisheries
- ☐ Ports – Land within Port of Brisbane's port limits (*below high-water mark*)
- ☐ SEQ development area
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – tourist activity or sport and recreation activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – community activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – indoor recreation
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – urban activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – combined use
- ☐ SEQ northern inter-urban break – tourist activity or sport and recreation activity
- ☐ SEQ northern inter-urban break – community activity
- ☐ SEQ northern inter-urban break – indoor recreation
- ☐ SEQ northern inter-urban break – urban activity

<input type="checkbox"/> SEQ northern inter-urban break – combined use <input type="checkbox"/> Tidal works or works in a coastal management district <input type="checkbox"/> Reconfiguring a lot in a coastal management district or for a canal <input type="checkbox"/> Erosion prone area in a coastal management district <input type="checkbox"/> Urban design <input type="checkbox"/> Water-related development – taking or interfering with water <input type="checkbox"/> Water-related development – removing quarry material <i>(from a watercourse or lake)</i> <input type="checkbox"/> Water-related development – referable dams <input type="checkbox"/> Water-related development – levees <i>(category 3 levees only)</i> <input type="checkbox"/> Wetland protection area
<b>Matters requiring referral to the local government:</b> <input type="checkbox"/> Airport land <input type="checkbox"/> Environmentally relevant activities (ERA) <i>(only if the ERA has been devolved to local government)</i> <input type="checkbox"/> Heritage places – Local heritage places
<b>Matters requiring referral to the Chief Executive of the distribution entity or transmission entity:</b> <input type="checkbox"/> Infrastructure-related referrals – Electricity infrastructure
<b>Matters requiring referral to:</b> <ul style="list-style-type: none"> <li>• The <b>Chief Executive of the holder of the licence</b>, if not an individual</li> <li>• The <b>holder of the licence</b>, if the holder of the licence is an individual</li> </ul> <input type="checkbox"/> Infrastructure-related referrals – Oil and gas infrastructure
<b>Matters requiring referral to the Brisbane City Council:</b> <input type="checkbox"/> Ports – Brisbane core port land
<b>Matters requiring referral to the Minister responsible for administering the Transport Infrastructure Act 1994:</b> <input type="checkbox"/> Ports – Brisbane core port land <i>(where inconsistent with the Brisbane port LUP for transport reasons)</i> <input type="checkbox"/> Ports – Strategic port land
<b>Matters requiring referral to the relevant port operator, if applicant is not port operator:</b> <input type="checkbox"/> Ports – Land within Port of Brisbane's port limits <i>(below high-water mark)</i>
<b>Matters requiring referral to the Chief Executive of the relevant port authority:</b> <input type="checkbox"/> Ports – Land within limits of another port <i>(below high-water mark)</i>
<b>Matters requiring referral to the Gold Coast Waterways Authority:</b> <input type="checkbox"/> Tidal works or work in a coastal management district <i>(in Gold Coast waters)</i>
<b>Matters requiring referral to the Queensland Fire and Emergency Service:</b> <input type="checkbox"/> Tidal works or work in a coastal management district <i>(involving a marina (more than six vessel berths))</i>

<b>18) Has any referral agency provided a referral response for this development application?</b>		
<input type="checkbox"/> Yes – referral response(s) received and listed below are attached to this development application <input checked="" type="checkbox"/> No		
Referral requirement	Referral agency	Date of referral response
Identify and describe any changes made to the proposed development application that was the subject of the referral response and this development application, or include details in a schedule to this development application <i>(if applicable)</i> .		

## PART 6 – INFORMATION REQUEST

### 19) Information request under the DA Rules

☒ I agree to receive an information request if determined necessary for this development application

☐ I do not agree to accept an information request for this development application

**Note:** By not agreeing to accept an information request I, the applicant, acknowledge:

- that this development application will be assessed and decided based on the information provided when making this development application and the assessment manager and any referral agencies relevant to the development application are not obligated under the DA Rules to accept any additional information provided by the applicant for the development application unless agreed to by the relevant parties
- Part 3 under Chapter 1 of the DA Rules will still apply if the application is an application listed under section 11.3 of the DA Rules or
- Part 2 under Chapter 2 of the DA Rules will still apply if the application is for state facilitated development

Further advice about information requests is contained in the [DA Forms Guide](#).

## PART 7 – FURTHER DETAILS

**20) Are there any associated development applications or current approvals? (e.g. a preliminary approval)**

☐ Yes – provide details below or include details in a schedule to this development application

☒ No

List of approval/development application references	Reference number	Date	Assessment manager
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			

**21) Has the portable long service leave levy been paid? (only applicable to development applications involving building work or operational work)**

☐ Yes – a copy of the receipted QLeave form is attached to this development application

☐ No – I, the applicant will provide evidence that the portable long service leave levy has been paid before the assessment manager decides the development application. I acknowledge that the assessment manager may give a development approval only if I provide evidence that the portable long service leave levy has been paid

☒ Not applicable (e.g. building and construction work is less than \$150,000 excluding GST)

Amount paid	Date paid (dd/mm/yy)	QLeave levy number (A, B or E)
\$		

**22) Is this development application in response to a show cause notice or required as a result of an enforcement notice?**

☐ Yes – show cause or enforcement notice is attached

☒ No

## 23) Further legislative requirements

### Environmentally relevant activities

23.1) Is this development application also taken to be an application for an environmental authority for an **Environmentally Relevant Activity (ERA)** under section 115 of the *Environmental Protection Act 1994*?

- ☐ Yes – the required attachment (form ESR/2015/1791) for an application for an environmental authority accompanies this development application, and details are provided in the table below
- ☒ No

**Note:** Application for an environmental authority can be found by searching "ESR/2015/1791" as a search term at [www.qld.gov.au](http://www.qld.gov.au). An ERA requires an environmental authority to operate. See [www.business.qld.gov.au](http://www.business.qld.gov.au) for further information.

Proposed ERA number:		Proposed ERA threshold:	
Proposed ERA name:			

- ☐ Multiple ERAs are applicable to this development application and the details have been attached in a schedule to this development application.

### Hazardous chemical facilities

23.2) Is this development application for a **hazardous chemical facility**?

- ☐ Yes – *Form 536: Notification of a facility exceeding 10% of schedule 15 threshold* is attached to this development application
- ☒ No

**Note:** See [www.business.qld.gov.au](http://www.business.qld.gov.au) for further information about hazardous chemical notifications.

### Clearing native vegetation

23.3) Does this development application involve **clearing native vegetation** that requires written confirmation that the chief executive of the *Vegetation Management Act 1999* is satisfied the clearing is for a relevant purpose under section 22A of the *Vegetation Management Act 1999*?

- ☐ Yes – this development application includes written confirmation from the chief executive of the *Vegetation Management Act 1999* (s22A determination)
- ☒ No

**Note:** 1. Where a development application for operational work or material change of use requires a s22A determination and this is not included, the development application is prohibited development.  
2. See <https://www.qld.gov.au/environment/land/vegetation/applying> for further information on how to obtain a s22A determination.

### Environmental offsets

23.4) Is this development application taken to be a prescribed activity that may have a significant residual impact on a **prescribed environmental matter** under the *Environmental Offsets Act 2014*?

- ☐ Yes – I acknowledge that an environmental offset must be provided for any prescribed activity assessed as having a significant residual impact on a prescribed environmental matter
- ☒ No

**Note:** The environmental offset section of the Queensland Government's website can be accessed at [www.qld.gov.au](http://www.qld.gov.au) for further information on environmental offsets.

### Koala habitat in SEQ Region

23.5) Does this development application involve a material change of use, reconfiguring a lot or operational work which is assessable development under Schedule 10, Part 10 of the Planning Regulation 2017?

- ☐ Yes – the development application involves premises in the koala habitat area in the koala priority area
- ☐ Yes – the development application involves premises in the koala habitat area outside the koala priority area
- ☒ No

**Note:** If a koala habitat area determination has been obtained for this premises and is current over the land, it should be provided as part of this development application. See koala habitat area guidance materials at [www.desi.qld.gov.au](http://www.desi.qld.gov.au) for further information.



### **Water resources**

23.6) Does this development application involve **taking or interfering with underground water through an artesian or subartesian bore, taking or interfering with water in a watercourse, lake or spring, or taking overland flow water under the *Water Act 2000***?

☐ Yes – the relevant template is completed and attached to this development application and I acknowledge that a relevant authorisation or licence under the *Water Act 2000* may be required prior to commencing development

☒ No

**Note:** Contact the Department of Resources at [www.resources.qld.gov.au](http://www.resources.qld.gov.au) for further information.

DA templates are available from [planning.statedevelopment.qld.gov.au](http://planning.statedevelopment.qld.gov.au). If the development application involves:

- Taking or interfering with underground water through an artesian or subartesian bore: complete DA Form 1 Template 1
- Taking or interfering with water in a watercourse, lake or spring: complete DA Form 1 Template 2
- Taking overland flow water: complete DA Form 1 Template 3.

### **Waterway barrier works**

23.7) Does this application involve **waterway barrier works**?

☐ Yes – the relevant template is completed and attached to this development application

☒ No

DA templates are available from [planning.statedevelopment.qld.gov.au](http://planning.statedevelopment.qld.gov.au). For a development application involving waterway barrier works, complete DA Form 1 Template 4.

### **Marine activities**

23.8) Does this development application involve **aquaculture, works within a declared fish habitat area or removal, disturbance or destruction of marine plants**?

☐ Yes – an associated resource allocation authority is attached to this development application, if required under the *Fisheries Act 1994*

☒ No

**Note:** See guidance materials at [www.daf.qld.gov.au](http://www.daf.qld.gov.au) for further information.

### **Quarry materials from a watercourse or lake**

23.9) Does this development application involve the **removal of quarry materials from a watercourse or lake under the *Water Act 2000***?

☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development

☒ No

**Note:** Contact the Department of Resources at [www.resources.qld.gov.au](http://www.resources.qld.gov.au) and [www.business.qld.gov.au](http://www.business.qld.gov.au) for further information.

### **Quarry materials from land under tidal waters**

23.10) Does this development application involve the **removal of quarry materials from land under tidal water under the *Coastal Protection and Management Act 1995***?

☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development

☒ No

**Note:** Contact the Department of Environment, Science and Innovation at [www.desi.qld.gov.au](http://www.desi.qld.gov.au) for further information.

### **Referable dams**

23.11) Does this development application involve a **referable dam** required to be failure impact assessed under section 343 of the *Water Supply (Safety and Reliability) Act 2008* (the *Water Supply Act*)?

☐ Yes – the 'Notice Accepting a Failure Impact Assessment' from the chief executive administering the *Water Supply Act* is attached to this development application

☒ No

**Note:** See guidance materials at [www.resources.qld.gov.au](http://www.resources.qld.gov.au) for further information.

### **Tidal work or development within a coastal management district**

23.12) Does this development application involve **tidal work or development in a coastal management district**?

- ☐ Yes – the following is included with this development application:
- ☐ Evidence the proposal meets the code for assessable development that is prescribed tidal work (*only required if application involves prescribed tidal work*)
  - ☐ A certificate of title

☒ No

**Note:** See guidance materials at [www.desi.qld.gov.au](http://www.desi.qld.gov.au) for further information.

### **Queensland and local heritage places**

23.13) Does this development application propose development on or adjoining a place entered in the **Queensland heritage register** or on a place entered in a local government's **Local Heritage Register**?

☐ Yes – details of the heritage place are provided in the table below

☒ No

**Note:** See guidance materials at [www.desi.qld.gov.au](http://www.desi.qld.gov.au) for information requirements regarding development of Queensland heritage places. For a heritage place that has cultural heritage significance as a local heritage place and a Queensland heritage place, provisions are in place under the Planning Act 2016 that limit a local categorising instrument from including an assessment benchmark about the effect or impact of, development on the stated cultural heritage significance of that place. See guidance materials at [www.planning.statedevelopment.qld.gov.au](http://www.planning.statedevelopment.qld.gov.au) for information regarding assessment of Queensland heritage places.

Name of the heritage place:		Place ID:	
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### **Decision under section 62 of the Transport Infrastructure Act 1994**

23.14) Does this development application involve new or changed access to a state-controlled road?

☐ Yes – this application will be taken to be an application for a decision under section 62 of the *Transport Infrastructure Act 1994* (subject to the conditions in section 75 of the *Transport Infrastructure Act 1994* being satisfied)

☒ No

### **Walkable neighbourhoods assessment benchmarks under Schedule 12A of the Planning Regulation**

23.15) Does this development application involve reconfiguring a lot into 2 or more lots in certain residential zones (except rural residential zones), where at least one road is created or extended?

☐ Yes – Schedule 12A is applicable to the development application and the assessment benchmarks contained in schedule 12A have been considered

☒ No

**Note:** See guidance materials at [www.planning.statedevelopment.qld.gov.au](http://www.planning.statedevelopment.qld.gov.au) for further information.

## **PART 8 – CHECKLIST AND APPLICANT DECLARATION**

### **24) Development application checklist**

I have identified the assessment manager in question 15 and all relevant referral requirement(s) in question 17	<input checked="" type="checkbox"/> Yes
<b>Note:</b> See the Planning Regulation 2017 for referral requirements	
If building work is associated with the proposed development, Parts 4 to 6 of <a href="#">DA Form 2 – Building work details</a> have been completed and attached to this development application	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> Not applicable
Supporting information addressing any applicable assessment benchmarks is with the development application	
<b>Note:</b> This is a mandatory requirement and includes any relevant templates under question 23, a planning report and any technical reports required by the relevant categorising instruments (e.g. local government planning schemes, State Planning Policy, State Development Assessment Provisions). For further information, see <a href="#">DA Forms Guide: Planning Report Template</a> .	<input checked="" type="checkbox"/> Yes
Relevant plans of the development are attached to this development application	
<b>Note:</b> Relevant plans are required to be submitted for all aspects of this development application. For further information, see <a href="#">DA Forms Guide: Relevant plans</a> .	<input checked="" type="checkbox"/> Yes
The portable long service leave levy for QLeave has been paid, or will be paid before a development permit is issued (see 21)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> Not applicable

## 25) Applicant declaration

- ☒ By making this development application, I declare that all information in this development application is true and correct
- ☒ Where an email address is provided in Part 1 of this form, I consent to receive future electronic communications from the assessment manager and any referral agency for the development application where written information is required or permitted pursuant to sections 11 and 12 of the *Electronic Transactions Act 2001*

**Note:** It is unlawful to intentionally provide false or misleading information.

**Privacy** – Personal information collected in this form will be used by the assessment manager and/or chosen assessment manager, any relevant referral agency and/or building certifier (including any professional advisers which may be engaged by those entities) while processing, assessing and deciding the development application. All information relating to this development application may be available for inspection and purchase, and/or published on the assessment manager's and/or referral agency's website.

Personal information will not be disclosed for a purpose unrelated to the *Planning Act 2016*, Planning Regulation 2017 and the DA Rules except where:

- such disclosure is in accordance with the provisions about public access to documents contained in the *Planning Act 2016* and the Planning Regulation 2017, and the access rules made under the *Planning Act 2016* and Planning Regulation 2017; or
- required by other legislation (including the *Right to Information Act 2009*); or
- otherwise required by law.

This information may be stored in relevant databases. The information collected will be retained as required by the *Public Records Act 2002*.

## PART 9 – FOR COMPLETION OF THE ASSESSMENT MANAGER – FOR OFFICE USE ONLY

Date received:  Reference number(s):

### Notification of engagement of alternative assessment manager

Prescribed assessment manager	
Name of chosen assessment manager	
Date chosen assessment manager engaged	
Contact number of chosen assessment manager	
Relevant licence number(s) of chosen assessment manager	

### QLeave notification and payment

**Note:** For completion by assessment manager if applicable

Description of the work	
QLeave project number	
Amount paid (\$)	Date paid (dd/mm/yy)
Date receipted form sighted by assessment manager	
Name of officer who sighted the form	

## **Attachment 2**

### **Town Planning Report**





# Town Planning Report

12 DAVIDSON STREET, PORT DOUGLAS

**3 February 2025**

**ASPIRE Town Planning and Project Services**

**Authored by: Daniel Favier**

**Ref: 2024-10-39 - Davidson Developments Pty Ltd - 12**

**Davidson Street, Port Douglas**

This Town Planning Report is intended for the exclusive use of our Client "Davidson Developments Pty Ltd" and is provided for informational purposes only. The information contained herein has been prepared based on sources and data believed to be reliable and accurate at the time of preparation. However, Aspire Town Planning and Project Services does not warrant the accuracy, completeness, or currency of the information and disclaims any responsibility for any errors or omissions, or for any loss or damage incurred by any party as a result of reliance on this information.

The conclusions and recommendations contained in this report are based on our professional judgment and interpretation of the current planning policies and regulations. It is important to note that planning regulations and policies are subject to change, and this report should not be construed as a guarantee of any future planning outcomes.

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# Executive Summary

This report supports a Development Application for a combined Reconfiguration of a Lot (1 Lot into 2 Lots) and a Material Change of Use (2 x Dwelling Houses) over 12 Davidson Street, Port Douglas. The site is included within the Tourist Accommodation Zone under the Douglas Shire Planning Scheme 2018 v1 and is located in a well-established residential and tourist accommodation precinct. The proposal seeks to subdivide the existing lot into two separate lots and construct two contemporary, single storey Dwelling Houses, each designed to be high-quality, architecturally responsive, and well-integrated with the surrounding built and natural environment.

The subject site is 1,012m<sup>2</sup> in area and is currently an undeveloped lot situated within a prime urban setting in Port Douglas. It is surrounded by a mix of residential dwellings, tourist accommodation developments, and commercial uses, consistent with the intent of the Tourist Accommodation Zone. The site has frontage to Davidson Street, providing direct access to existing infrastructure and services, and is located within walking distance of Four Mile Beach, Macrossan Street, and various local amenities. The site's topography, landscape values, and environmental characteristics have been carefully considered in the design response, ensuring a development that is sympathetic to its surroundings.

The application seeks approval for a reconfiguration of the existing lot into two equal-sized lots, facilitating the construction of two high-quality, single storey, Dwelling Houses. Each dwelling has been individually designed to maximise privacy, natural light, and ventilation, while incorporating modern architectural elements that complement the character of Port Douglas. The proposal includes landscaped outdoor areas, private open spaces, and high-end finishes, ensuring a premium residential outcome that aligns with the expectations for development in this high-value coastal area.

The dwellings have been designed to minimise site disturbance. The rear outdoor entertainment areas will be constructed with elevated structures to accommodate the natural topography and overland stormwater flow paths. The built form is articulated to reduce bulk and scale, ensuring a visually attractive streetscape.

The development is generally consistent with the Douglas Shire Planning Scheme 2018 (v1) and aligns with the intent of the Tourist Accommodation Zone, which supports a mix of tourist and residential accommodation in high-amenity locations. The proposal complies with the Acceptable Outcomes and Performance Outcomes of the Reconfiguring a Lot and Dwelling House Codes, demonstrating a high standard of urban design, site responsiveness, and seamless integration with the surrounding built environment. Where deviations from specific provisions occur, justification has been provided to demonstrate compliance with the overall intent of the planning scheme. The design appropriately

manages stormwater, access, and servicing requirements, while preserving the amenity and character of the surrounding area.

Both lots will be accessed directly from Davidson Street, ensuring safe and efficient vehicle access. Each dwelling will have onsite car parking provisions, including garages and visitor spaces. The development will be connected to all essential services, including reticulated water, sewerage, stormwater, electricity, and telecommunications. A stormwater management strategy has been prepared to ensure overland flow paths are maintained, and drainage infrastructure will be designed to mitigate any potential impacts on adjoining properties.

The proposed Reconfiguration of a Lot and Material Change of Use delivers a well-considered and high-quality residential development that is consistent with the planning framework. The subdivision supports infill development within an established urban area, maximising the efficient use of existing infrastructure and services. The design ensures a high level of residential amenity while complementing the coastal and tropical character of Port Douglas. Overall, the proposal represents a logical and appropriate development outcome, delivering low-impact and high quality residential housing within the Tourist Accommodation Zone, in a manner that respects the environmental, character, and amenity values of the site and its surrounds. On this basis, it is recommended that the application be approved subject to reasonable and relevant conditions.

# 1.0 Summary

Table 1: Application Summary.

Development Details	Information
Street Address	12 Davidson Street, Port Douglas
Lot and Plan	Lot 706 on Crown Plan PTD2092
Land Owner	Davidson Developments Pty Ltd <i>See Attachment 1 – Certificate of Title</i>
Site Area	1,012m <sup>2</sup>
Road Frontages	20.182m to Davidson Street
Easements	Nil
Environmental and Contaminated Land Register	To the best of our knowledge at the time of writing, the site is not listed on the EMR or CLR
Current Approved Use	Vacant (Existing dwelling house demolished in 2024)
Proposal	Reconfiguration of a Lot: 1 Lot into 2 Lots Material Change of Use: 2 x Dwelling Houses
Proposed Lot Sizes	Lot 1: 506m <sup>2</sup> Lot 2: 506m <sup>2</sup>
Approvals Sought	Development Permit
Level of Assessment	Code Assessable
Planning Scheme Zone	Tourist Accommodation
Local Plan	Port Douglas Craiglie Local Plan
Overlays	Acid Sulfate Soils (<5m AHD) Landscape Values (Scenic Route Buffer; View Corridor) Transport Network: <ul style="list-style-type: none"> <li>- Noise Corridor (Category 1 and 2)</li> <li>- Pedestrian Cycle (Principal Route)</li> <li>- Road Hierarchy (Access Road)</li> </ul>
Regional Plan Designation	Urban Footprint
State Planning Policy	Appropriately reflected within the Douglas Shire Planning Scheme 2018 v1
State Development Assessment Provisions	Nil
Referral	State Assessment and Referral Agency (SARA): <ul style="list-style-type: none"> <li>- Infrastructure-Related Referral – State Transport Infrastructure</li> </ul>
Planning Regulation 2016 Schedule 12A – Assessment Benchmarks for Particular Reconfiguring a Lot	Not applicable
Infrastructure & Services	The site has access to reticulated water, sewerage, stormwater, electricity, and telecommunications
Car Parking Provision	Each dwelling will provide on-site car parking, including 2 garage spaces per dwelling
Public Notification Requirements	Not required (Code Assessable development)



## 2.0 Site Description

The subject site, located at 12 Davidson Street, Port Douglas, is formally described as Lot 706 on Crown Plan PTD2092 and is included within the Tourist Accommodation Zone under the Douglas Shire Planning Scheme 2018 (v1). The site has a total area of 1,012m<sup>2</sup> and is situated within an established urban environment that supports a mix of residential, short-term accommodation, and commercial uses, characteristic of the Port Douglas township.



Image 1: Subject Site Locality (Sources: QLDGlobe, 2025)

### 2.1 Site Context and Surrounding Land Uses

The site is located on the eastern side of Davidson Street, a key arterial road within Port Douglas that provides access to tourist accommodation, low-density residential dwellings, and commercial developments. The surrounding area is well serviced by existing infrastructure and provides convenient access to local amenities, retail precincts, and recreational spaces, including Four Mile Beach and Macrossan Street, the town's primary commercial and entertainment strip.



## 2.2 Directly Adjoining and Surrounding Uses

To the north, east, south and west is a mix of short-term accommodation, permanent residences, boutique resorts and commercial properties consistent with the Tourist Accommodation Zone designation. Adjoining buildings are typically 3 storeys in height and reasonably setback from the common boundary.

## 2.3 Topography, Vegetation, and Site Characteristics

The site is generally flat with a gentle slope that facilitates efficient stormwater drainage to the east. It does not contain any significant watercourses, wetlands, or environmentally sensitive areas. Vegetation on the site is limited, with some landscaped elements and trees contained within the adjoining properties along the site boundaries. The lot has been cleared following the demolition of the existing dwelling in 2024, providing a vacant and development-ready parcel.



Photograph 1: Site View from Davidson Street

## **2.4 Access and Infrastructure Services**

The site has 20.182 metres of frontage to Davidson Street, ensuring direct access to the local road network. Davidson Street is a sealed, two-lane road that provides safe vehicle and pedestrian movement, linking the site to Macrossan Street, Port Douglas Road, and other key routes. The proposed development will be serviced by existing water, sewerage, stormwater, electricity, and telecommunications infrastructure, which are readily available within the road reserve.

## **2.5 Planning Constraints and Overlays**

The subject site is included in the Urban Footprint under the Far North Queensland Regional Plan 2009-2031, reinforcing its suitability for urban development. While the Douglas Shire Planning Scheme 2018 does not identify any environmental constraints directly affecting the site, consideration has been given to potential stormwater and landscape values when designing the proposed development.

## 3.0 Proposal

This Development Application seeks approval for a Development Permit for a combined Reconfiguration of a Lot (1 Lot into 2 Lots) and a Material Change of Use (2 x Dwelling Houses) over land at 12 Davidson Street, Port Douglas (Lot 706 on Crown Plan PTD2092).

In response to a Prelodgement Enquiry, Council Officers suggested that the proposal may constitute a Dual Occupancy. However, it is maintained that the proposed development consists of two separate Dwelling Houses, each located on its own freehold lot following subdivision. The resulting buildings will be physically separated, independent, and functionally distinct, ensuring that the development remains consistent with the definition and intent of a Dwelling House under the Douglas Shire Planning Scheme 2018.

The site is 1,012m<sup>2</sup> in area and is included within the Tourist Accommodation Zone under the Douglas Shire Planning Scheme 2018 v1. The proposal aims to subdivide the existing lot into two separate freehold lots and establish two high-quality, architecturally designed Dwelling Houses, each with private open space, onsite parking, and complementary landscaping.

The development has been carefully designed to be responsive to the site conditions, ensuring minimal impact on surrounding properties and integration with the established urban environment. The proposal aligns with the planning intent for the Tourist Accommodation Zone, which supports a mix of permanent and short-term accommodation in well-serviced locations such as this.

### **Reconfiguration of a Lot (1 into 2 Lots)**

The proposed reconfiguration will create two new freehold lots, each designed to accommodate an individual detached Dwelling House. The subdivision is consistent with the character of the area, which features similarly sized residential lots supporting single dwellings within the vicinity of the site. The proposed lot layout ensures efficient use of urban land while maintaining appropriate lot dimensions for dwelling construction.

- Lot 1: Approximately 506m<sup>2</sup>, with primary frontage to Davidson Street.
- Lot 2: Approximately 506m<sup>2</sup>, also with frontage to Davidson Street.

Both lots will have direct road access, ensuring safe and efficient vehicle entry and exit. No new roads or easements are required as part of the subdivision.

It is acknowledged that, in relation to the titling of the new lots, Council may impose conditions requiring that a Building Permit for the Dwelling Houses be obtained, the slab poured, and at least three courses of blocks laid before the new lots can be titled.

### **Material Change of Use (2 x Dwelling Houses)**

The proposal includes the construction of two architecturally designed Dwelling Houses, one on each newly created lot. The dwellings have been designed to be climatically responsive, taking advantage of natural ventilation, shading, and energy-efficient materials. Although single storey the built form includes elements in the design to give a larger appearance from the street, consistent with character of the surrounding area, ensuring that the development blends seamlessly with adjoining premises.

### **Design and Built Form**

Each dwelling is designed as a single-storey contemporary home, ensuring a functional and accessible living environment suitable for occupants of all generations. The applicant has deliberately opted for a single-level design to enhance universal accessibility, recognising that stairs can present mobility challenges, while lifts may be cost-prohibitive. The proposed design incorporates:

- Open-plan living areas that maximise natural light and airflow.
- Openings within the roof to further maximise natural light.
- Private open space in the form of rear courtyards and landscaped garden areas.
- Onsite parking, including secure garages and additional visitor parking.
- Modern architectural finishes that reflect the coastal character of Port Douglas.

While the internal floor layouts of the two dwellings are identical, the building façades have been uniquely designed to create visual distinction and individuality. This, combined with the placement of garages on opposing sides and the integration of strategic landscaping, ensures that the development presents as two separate dwellings rather than a ‘cookie-cutter’ design. The thoughtful approach to façade articulation, material selection, and site layout enhances the streetscape appeal and reinforces a sense of architectural diversity, contributing to the character of the area.

The front entry includes glass louvers and door to facilitate greater interaction with the street and casual surveillance.

The building setbacks, height, and site cover are consistent with the locality ensuring the development does not create any adverse amenity impacts on adjoining properties.

### **Access and Parking**

Each lot will have direct access to Davidson Street, with individual driveways providing safe and convenient vehicle movement. The development includes enclosed garages for resident parking, with additional space for visitor vehicles. The proposed parking arrangements comply with Council requirements for Dwelling Houses, ensuring sufficient off-street parking is available.



The design of the dwellings carefully incorporates garage doors in a discreet manner, ensuring they do not dominate the streetscape or detract from the architectural appeal of the development. As a result, the development maintains a high-quality, visually appealing frontage, reinforcing the sense of separate and distinct dwellings rather than a garage-focused streetscape.

### **Landscaping and Open Space**

The development will incorporate strategically designed landscaping to enhance the visual appeal of the site, provide privacy, and complement the surrounding streetscape, ensuring seamless integration with the established character of Port Douglas. Each dwelling will include private outdoor areas, designed to maximise natural shade, cooling effects, and residential amenity for future occupants.

A mix of tropical planting species, consistent with the coastal and subtropical character of Port Douglas, will be incorporated throughout the site. Additionally, Davidson Street features a 15m wide grass verge adjacent to the site, which presents an opportunity for supplementary landscaping to provide additional screening for the development. This approach aligns with existing landscaping treatments along Davidson Street, ensuring continuity with the established urban landscape and reinforcing the green, vegetated aesthetic of the area. The inclusion of landscaping within the road verge is consistent with adjoining and surrounding properties, further enhancing the streetscape quality and visual integration of the development.

### **Infrastructure and Services**

The site is located within a well-established urban area and is serviced by reticulated water, sewerage, electricity, stormwater drainage, and telecommunications. The development will connect to these existing networks, ensuring efficient servicing and infrastructure provision.

A stormwater management strategy has been incorporated into the design, ensuring that overland flow paths are maintained and that the development does not negatively impact adjoining properties or Council infrastructure.

### **Compliance with Planning Intent**

The proposed subdivision and Dwelling Houses are consistent with the intent of the Tourist Accommodation Zone, which supports both permanent residential dwellings and visitor accommodation. The proposal:

- Supports infill development in an established township location.
- Is compatible with the character and amenity of the surrounding area.
- Utilises existing infrastructure and road networks, ensuring efficient service delivery.
- Provides high-quality residential accommodation, contributing to housing diversity in Port Douglas.

Given the site’s zoning, location, and surrounding land uses, the proposed subdivision and Dwelling Houses represent a logical, well-integrated, and high-quality development outcome for 12 Davidson Street, Port Douglas. On this basis, approval of the development application is sought.



**Image 2: Proposed Development Perspectives**

# 4.0 Statutory Town Planning Framework

## 4.1 Planning Act 2016

The *Planning Act 2016* (the 'Planning Act') is the statutory instrument for the State of Queensland under which, amongst other matters, Development Applications are assessed by Local Governments. The Planning Act is supported by the *Planning Regulation 2017* (the 'Planning Regulation'). The following sections of this report discuss the parts of the Planning Act and Planning Regulation applicable to the assessment of a development application.

### 4.1.1 Approval and Development

Pursuant to Sections 49, 50 and 51 of the Planning Act, the Development Application seeks a Development Permit for Reconfiguring a Lot (1 Lot into 2 Lots) and Material Change of Use (Dwelling House).

### 4.1.2 Application

The proposed development is:

- development that is located completely in a single local government area;
- development made assessable under a local categorising instrument; and
- for a Material Change of Use.

In accordance with Section 48 of the Planning Act and Schedule 8, Table 2, Item 1 of the Planning Regulation, the development application is required to be made to the applicable Local Government, in this instance being Douglas Shire Council (the 'Council').

### 4.1.3 Referral

Section 54(2) of the Planning Act and Section 22 and Schedules 9 and 10 of the Planning Regulation provide for the identification of the jurisdiction of referral agencies, to which a copy of the Development Application must be provided. A review of the Planning Regulation confirms the Development Application triggers referral to the State Assessment and Referral Agency under:

- Schedule 10, Part 9, Division 4, Subdivision 2, Table 1: Reconfiguring a lot near a State transport corridor

Assessment against the State code 1: Development in a state-controlled road environment is included under ***Attachment 4: Statement of Code Compliance Against the State Development Assessment Code***.



#### 4.1.4 Public Notification

Section 53(1) of the Planning Act provides that an applicant must give notice of a Development Application where any part is subject to Impact Assessment or where it is an application, which includes a variation request.

The Development Application is subject to Code Assessment and therefore Public Notification of the Development Application is not required.

#### 4.1.5 Assessment Framework

As noted within this report, the proposed development triggers a Code Assessable Development Application. Section 45(3) of the *Planning Act* provides that:

- “(3) A code assessment is an assessment that must be carried out only—*
- (a) against the assessment benchmarks in a categorising instrument for the development; and*
  - (b) having regard to any matters prescribed by regulation for this paragraph.”*

The Douglas Shire Planning Scheme 2018 v1.0, as the applicable local categorising instrument, is discussed in greater detail in the following sections of this report.

Section 26 of the *Planning Regulation* provides the following assessment benchmarks for the purposes of Section 45(3)(a) of the *Planning Act*:

- “(1) For section 45(3)(a) of the Act, the code assessment must be carried out against the assessment benchmarks for the development stated in schedules 9 and 10.*
- (2) Also, if the prescribed assessment manager is the local government, the code assessment must be carried out against the following assessment benchmarks—*
- (a) the assessment benchmarks stated in—*
    - (i) the regional plan for a region, to the extent the regional plan is not identified in the planning scheme as being appropriately integrated in the planning scheme; and*
    - (ii) the State Planning Policy, part E, to the extent part E is not identified in the planning scheme as being appropriately integrated in the planning scheme; and*
    - (iii) any temporary State planning policy applying to the premises;*

(b) *if the local government is an infrastructure provider—the local government’s LGIP.*

(3) *However, an assessment manager may, in assessing development requiring code assessment, consider an assessment benchmark only to the extent the assessment benchmark is relevant to the development.”*

Section 27 of the *Planning Regulation* provides matters for the purposes of Section 45(3)(b) of the *Planning Act*:

“(1) *For section 45(3)(b) of the Act, the code assessment must be carried out having regard to—*

(a) *the matters stated in schedules 9 and 10 for the development; and*

...

(d) *if the prescribed assessment manager is a person other than the chief executive—*

(i) *the regional plan for a region, to the extent the regional plan is not identified in the planning scheme as being appropriately integrated in the planning scheme; and*

(ii) *the State Planning Policy, to the extent the State Planning Policy is not identified in the planning scheme as being appropriately integrated in the planning scheme; and*

(iii) *for designated premises—the designation for the premises; and*

(e) *any temporary State planning policy applying to the premises; and*

(f) *any development approval for, and any lawful use of, the premises or adjacent premises; and*

(g) *the common material.*

(2) *However—*

(a) *an assessment manager may, in assessing development requiring code assessment, consider a matter mentioned in subsection (1) only to the extent the assessment manager considers the matter is relevant to the development; and*

(b) *if an assessment manager is required to carry out code assessment against assessment benchmarks in an instrument stated in subsection (1), this section does not require the assessment manager to also have regard to the assessment benchmarks.”*

The following sections of this report discuss the applicable assessment benchmarks and applicable matters in further detail.

## **4.2 Far North Queensland Regional Plan 2009-2031**

The Far North Queensland Regional Plan 2009 - 2031 ('the Regional Plan') is intended to guide and manage the region's development and to address key regional environmental, social, economic and urban objectives. The site falls within the area to which the Urban Footprint applies. The Regional Plan is identified in the Planning Scheme as being appropriately integrated in the scheme. Further detailed assessment against the Regional Plan is therefore not applicable.

## **4.3 State Planning Policy**

The State Planning Policy ('the SPP') was released on 2 December 2013 and replaced all previous State Planning Policies. The SPP has since been revised, with new versions released on 2 July 2014, 29 April 2016 and 3 July 2017. The April 2016 version of the SPP is identified in the Planning Scheme as being appropriately integrated. Whilst the SPP has been amended since April 2016 version, it is considered that the policy content and outcomes contained within the SPP, to the extent they are relevant and applicable to the proposed development, have not been sufficiently amended to require the reconsideration of the SPP separately.

## **4.4 Temporary State Planning Policies**

There are currently no temporary State Planning Policies in effect in Queensland.

## **4.5 Schedule 12A Planning Regulation**

Schedule 12A of the Planning Regulation does not apply as the Development Application does not meet the triggering criteria.

## **4.6 Douglas Shire Planning Scheme 2018 v1.0**

The Planning Scheme came into effect on 2 January 2018 and is the applicable planning scheme to the Douglas Local Government Area. It is noted that the Planning Scheme was drafted under the

*Sustainable Planning Act 2009* ('the SPA'). The interpretation of the Planning Scheme with respect to the proposed development is therefore based on the transitional provisions of the Planning Act.

#### **4.6.1 Zone**

The subject site is located within the Tourist Accommodation Zone. The purpose of the Tourist Accommodation Zone is to *"provide for short-term accommodation supported by community uses and small-scale services and facilities in locations where there are tourist attractions."*

The proposed Reconfiguration of a Lot and Dwelling House is consistent with the overall outcomes of the Tourist Accommodation Zone Code by contributing to a diverse range of accommodation options that complement the tourism function of the area. While the development comprises two Dwelling Houses, these are designed to be suitable for either permanent residents or short-term accommodation, ensuring flexibility to support tourist needs in line with the intent of the zone. Acknowledging that further approval may be required to establish Short Term Accommodation use rights.

The built form and scale of the development are appropriate to the surrounding context, incorporating architectural elements that respond to the tropical climate, such as open-plan layouts, generous outdoor spaces, natural ventilation, and shaded areas to enhance comfort. The design reflects the coastal character of Port Douglas, incorporating natural materials, articulated façades, and landscaping treatments that integrate the dwellings into the existing urban and environmental setting. The landscaping will be of a high quality, reinforcing the dominance of tropical vegetation and enhancing the streetscape appeal of Davidson Street, where a 15-metre-wide grass verge provides additional opportunities for screening and integration with the surrounding environment. The development is fully serviced by existing infrastructure and contributes to the ongoing enhancement of the Port Douglas township, ensuring that the proposal meets the intent of the planning scheme while maintaining a balance between residential and tourist accommodation opportunities.

An assessment of the proposed development against the Tourist Accommodation Zone Code is included under ***Attachment 3 – Statement of Code Compliance against the Planning Scheme.***

#### **4.6.2 Local Plan**

The subject site is located within the Port Douglas Craiglie Local Plan, however is not located within a Local Plan Precinct or Sub Precinct.

An assessment of the proposed development against the Local Plan Code is included under ***Attachment 3 – Statement of Code Compliance against the Planning Scheme.***

### **4.6.3 Overlays**

Review of the Planning Scheme Mapping confirms the following applicable Overlays:

- Acid Sulfate Soils (<5m AHD)
- Landscape Values Overlay (Scenic Route Buffer; View Corridor)
- Transport Network Overlay (Noise Corridor Category 1 and 2; Pedestrian Cycle Principle Route; Road Hierarchy Access Road)

It is noted that the Landscape Values Overlay does not trigger for a Reconfiguring a Lot or a Dwelling House and therefore assessment against this code is omitted.

### **4.6.4 Category of Assessment**

Pursuant to Part 5 of the Planning Scheme, a Development Application for a Material Change of Use (Dwelling House) in the Tourist Accommodation Zone is Self Assessable. However, development for Reconfiguring a Lot is Code Assessable and therefore a Development Application for a Development Permit is required to Douglas Shire Council.

The category of assessment of the proposed development is not otherwise altered by the Planning Scheme.

### **4.6.5 Assessment Criteria**

A detailed assessment against the following relevant assessment criteria is provided in Attachment 3.

Zone Code

- Tourist Accommodation Zone Code

Local Plan Code

- Port Douglas Craiglie Local Plan Code

## Overlay Codes

- Acid Sulfate Soils Overlay
- Transport Network Overlay

## Use Code

- Dwelling House (Complies with Acceptable Outcomes – detailed assessment not included)

## Development Codes

- Access, Parking and Services
- Environmental Performance
- Filling and Excavation Code
- Infrastructure Works Code
- Landscaping Code
- Reconfiguring a Lot Code

An assessment of the proposed development against the Overlay Codes is included under ***Attachment 3 – Statement of Code Compliance against the Planning Scheme.***

## 5.0 Conclusion

The proposed Reconfiguration of a Lot (1 Lot into 2 Lots) and Material Change of Use (2 x Dwelling Houses) at 12 Davidson Street, Port Douglas represents a logical, well-integrated, and high-quality development outcome that aligns with the intent and objectives of the Douglas Shire Planning Scheme 2018 v1. The proposal seeks to subdivide the existing 1,012m<sup>2</sup> lot into two freehold lots and construct two architecturally designed, single-storey Dwelling Houses, delivering a modern and accessible residential offering within the Tourist Accommodation Zone.

The development is consistent with the established character of the area, which accommodates a mix of residential and short-term accommodation uses. The proposed lot configuration and built form are sympathetic to the surrounding context, ensuring that the development complements the streetscape and existing urban fabric of Port Douglas. The low-density nature of the proposal, combined with high-quality landscaping, ensures that the development contributes positively to the visual amenity and tropical aesthetic of the locality.

The site benefits from existing infrastructure and road access, with direct frontage to Davidson Street, where a 15-metre-wide grass verge provides an opportunity for additional screening and integration with the surrounding landscape character. The proposal incorporates sustainable design principles, including passive cooling strategies, natural ventilation, and climate-responsive materials, making it well-suited to the tropical conditions of Far North Queensland.

The development application is Code Assessable, and generally complies with the Acceptable Outcomes and Performance Outcomes of the Reconfiguring a Lot and Dwelling House Codes under the planning scheme. Where minor variations arise, justifications have been provided to demonstrate alignment with the overall planning intent. The proposed subdivision and Dwelling Houses will not result in any adverse impacts on the surrounding properties, infrastructure, or environmental features of the site.

Furthermore, the proposal is well-serviced by existing utilities, including water, sewerage, stormwater drainage, electricity, and telecommunications. Appropriate measures have been incorporated into the design to ensure stormwater management is effectively addressed, preserving the functionality of overland flow paths and minimising potential impacts on adjoining properties.

In summary, the proposal delivers a high-quality residential development that enhances housing diversity, contributes to the urban consolidation objectives of the Douglas Shire, and aligns with the Tourist Accommodation Zone. Given the site's strategic location, planning compliance, and careful design response, the proposal represents an appropriate and desirable development outcome.

On this basis, it is recommended that the application be approved subject to reasonable and relevant conditions.



# **Attachment 1**

## **Certificate of Title**

Queensland Titles Registry Pty Ltd  
ABN 23 648 568 101

<b>Title Reference:</b>	<b>20971138</b>	<b>Search Date:</b>	30/01/2025 12:57
<b>Date Title Created:</b>	25/10/1974	<b>Request No:</b>	50718838
<b>Previous Title:</b>	20025184		

#### ESTATE AND LAND

Estate in Fee Simple

LOT 706 CROWN PLAN PTD2092  
Local Government: DOUGLAS

#### REGISTERED OWNER

Dealing No: 723598649 14/10/2024

DAVIDSON DEVELOPMENTS PTY LTD A.C.N. 679 448 134

#### EASEMENTS, ENCUMBRANCES AND INTERESTS

1. Rights and interests reserved to the Crown by  
Deed of Grant No. 10332026 (ALLOT 6 SEC 7)

#### ADMINISTRATIVE ADVICES

NIL

#### UNREGISTERED DEALINGS

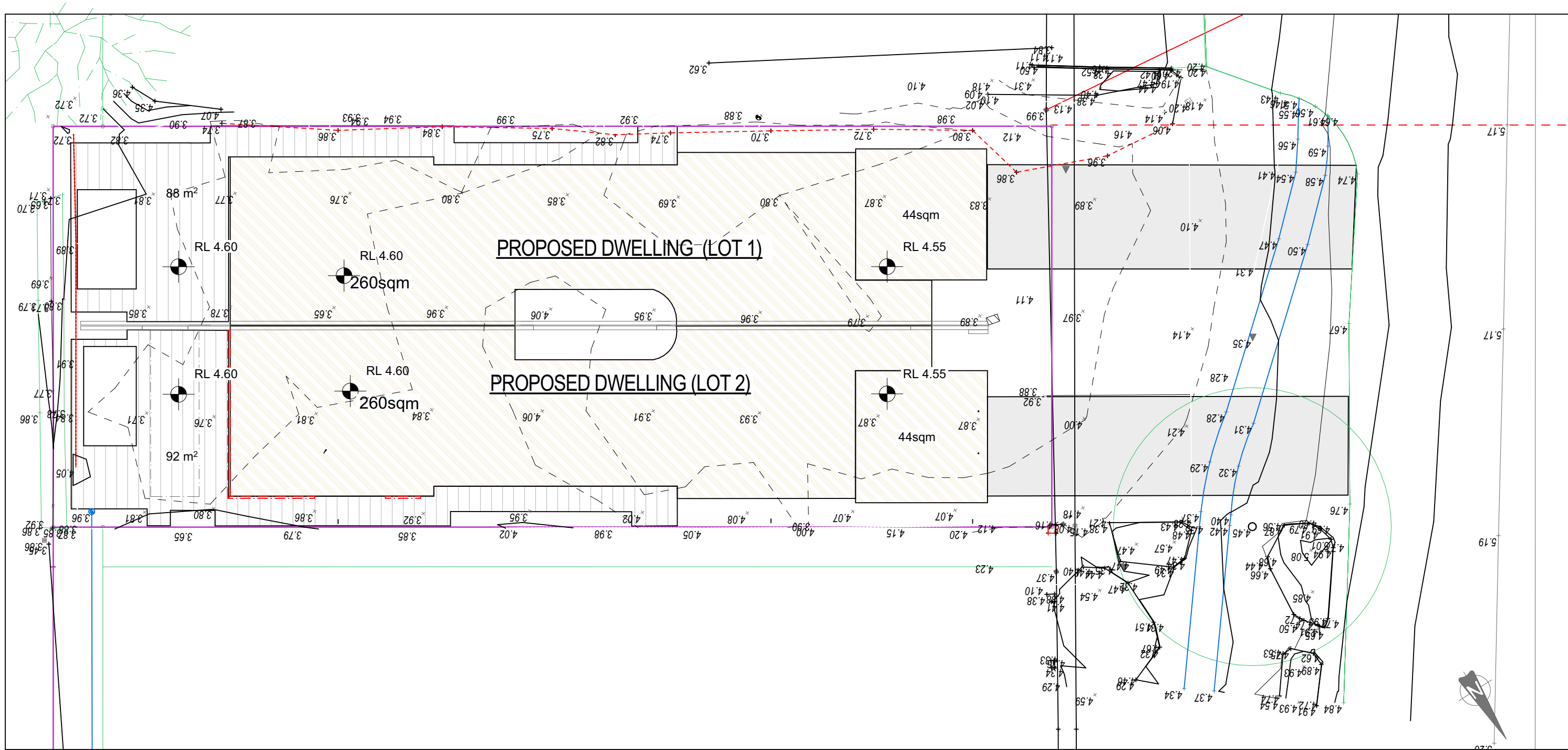
NIL

\*\* End of Current Title Search \*\*

# **Attachment 2**

## **Site, Floor and Elevation Plans**

*Prepared by RECS Pty Ltd*



2

## SURVEY PLAN

1:200

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



## CONSULTING ENGINEERS & BUILDING DESIGNERS

RPEQ No.5412  
QBCC No.1106533

RECS PTY LTD., SHOP 22 / LEVEL 2, SALTWATER BUILDING 26-30 MACROSSAN STREET, PO BOX 894, PORT DOUGLAS QLD 4877,  
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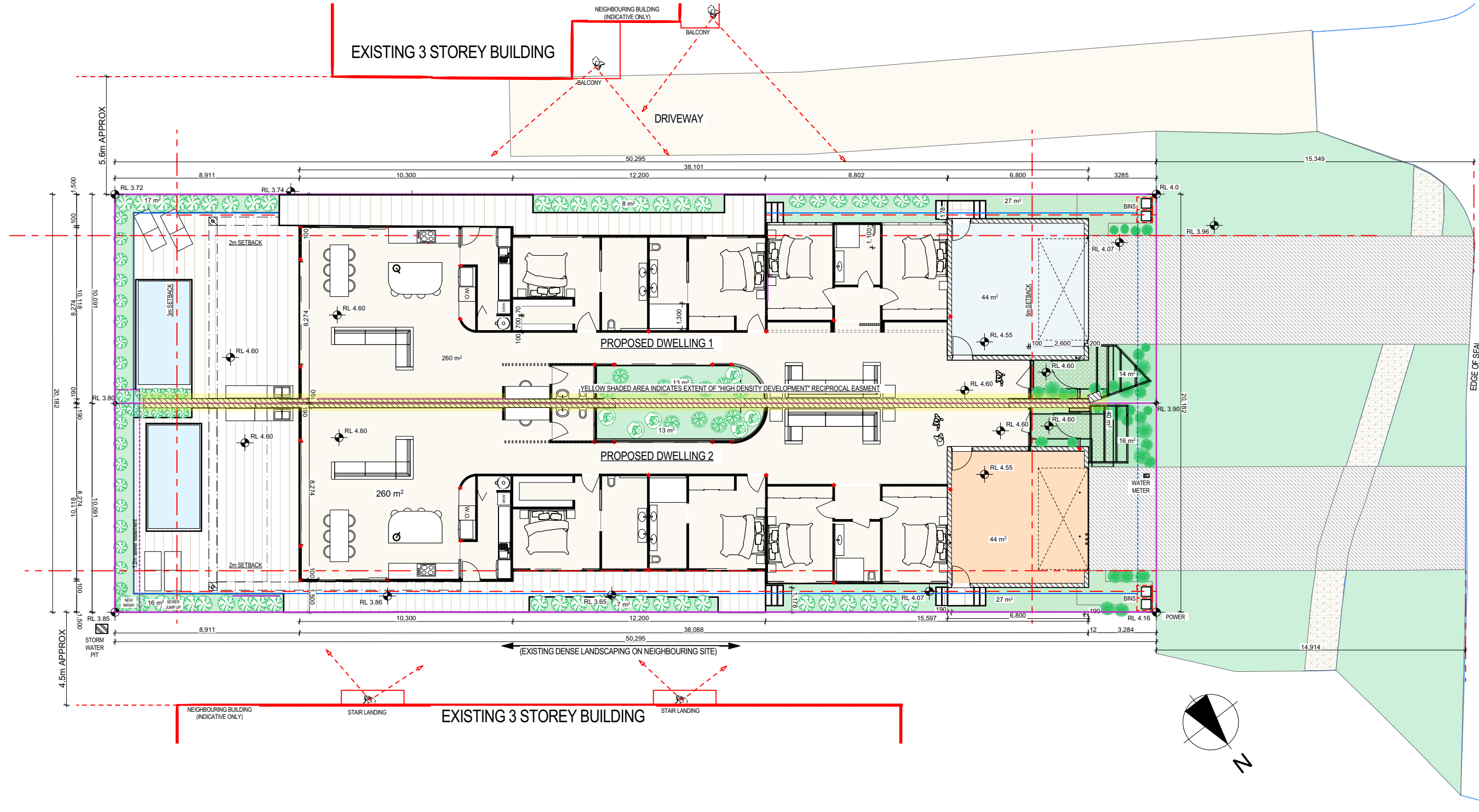
ISSUE A -  
ISSUE B -  
ISSUE C -  
ISSUED  
DRAWN MG  
DESIGN LP  
PROJECT STATUS: DEVELOPMENT APPLICATION

## SITE LEVELS

SHEET  
REVISION

DA 1  
A

SCALE @ A3 SHEET  
PRINT DATE  
3/02/2025 8:14 AM



- DWELLING AREA 260sqm
- GARAGE AREA 44sqm
- OUTDOOR RECREATION- 178sqm
- LANDSCAPING - 158sqm
- LANDSCAPING (ELEVATED) - 25sqm

TOTAL SITE AREA 1012sqm  
2 X SITES, EACH LOT SITE AREA - 506sqm  
DWELLING + GARAGE 304qm (each)  
TOTAL LANDSCAPING 183sqm  
TOTAL SITE COVERAGE 60%

12 DAVIDSON STREET  
LOT 704  
PTD2029

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024

RECS  
PTY LTD

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& BUILDING DESIGNERS

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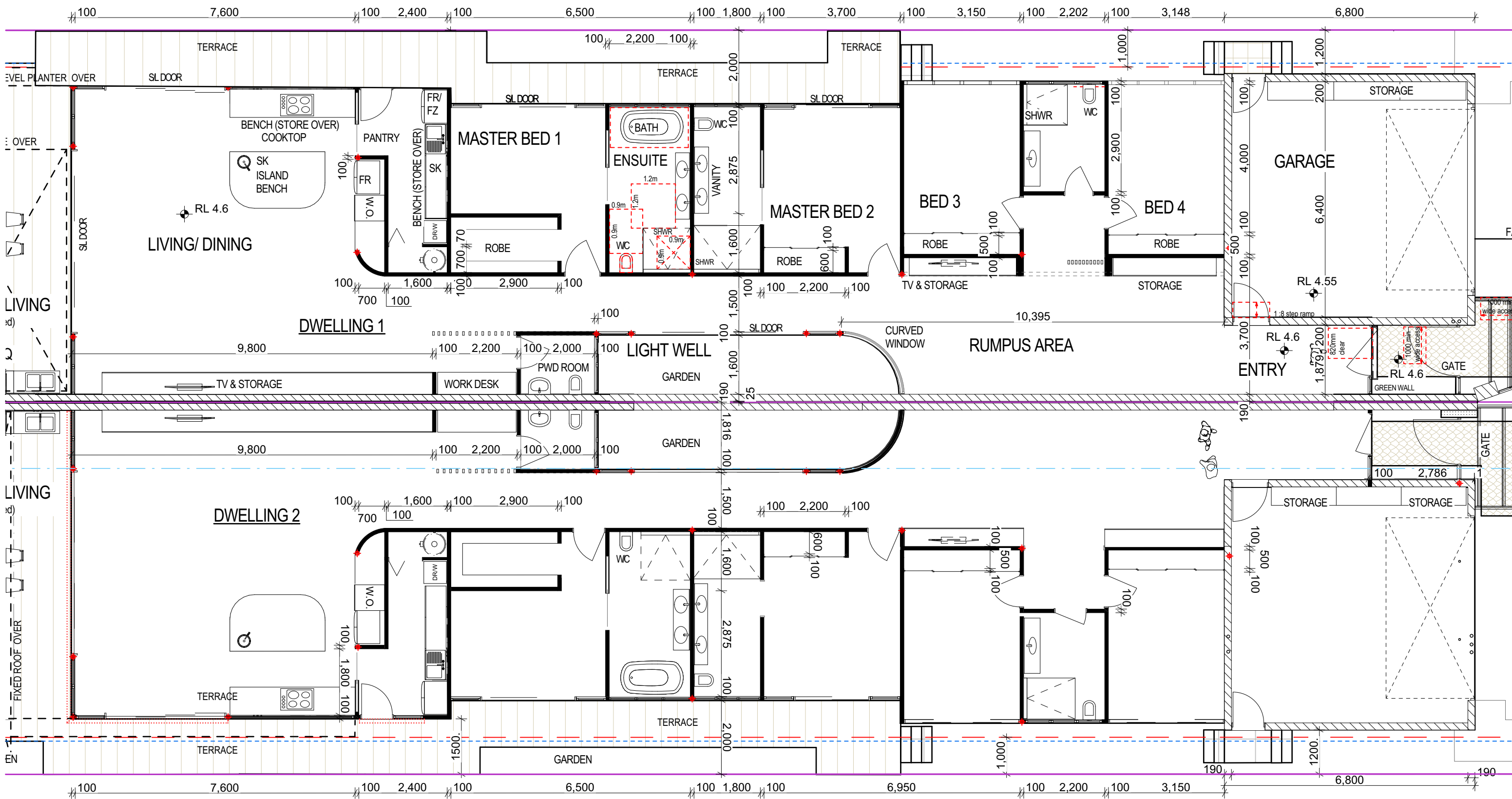
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ISSUE A -  
ISSUE B -  
ISSUE C -  
ISSUED

FOR DEVELOPMENT APPLICATION ONLY  
(NOT FOR CONSTRUCTION)  
DRAWN MG DESIGN LP PROJECT STATUS: DEVELOPMENT APPLICATION

SITE PLAN

SHEET	DA 2
REVISION	A
SCALE @ A3 SHEET	PRINT DATE
	3/02/2025 8:28 AM



2 ENS ELEV. 1  
1:100

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



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ISSUE A -  
ISSUE B -  
ISSUE C -  
ISSUED  
FOR DEVELOPMENT APPLICATION ONLY  
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DESIGN LP PROJECT STATUS: DEVELOPMENT APPLICATION

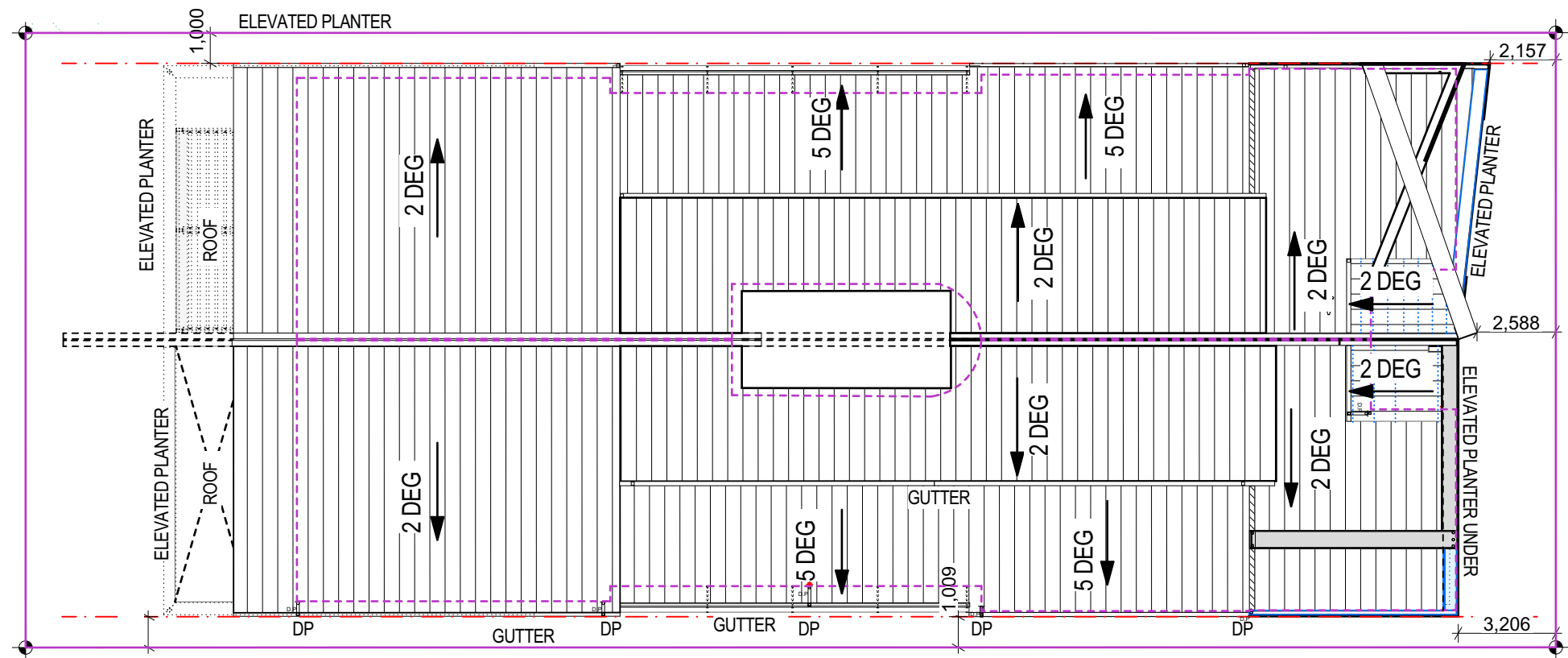
FLOOR PLAN

SHEET  
REVISION

DA 3  
A

SCALE @ A3 SHEET  
PRINT DATE  
3/02/2025 8:14 AM





1 **ROOF PLAN**  
1:200

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



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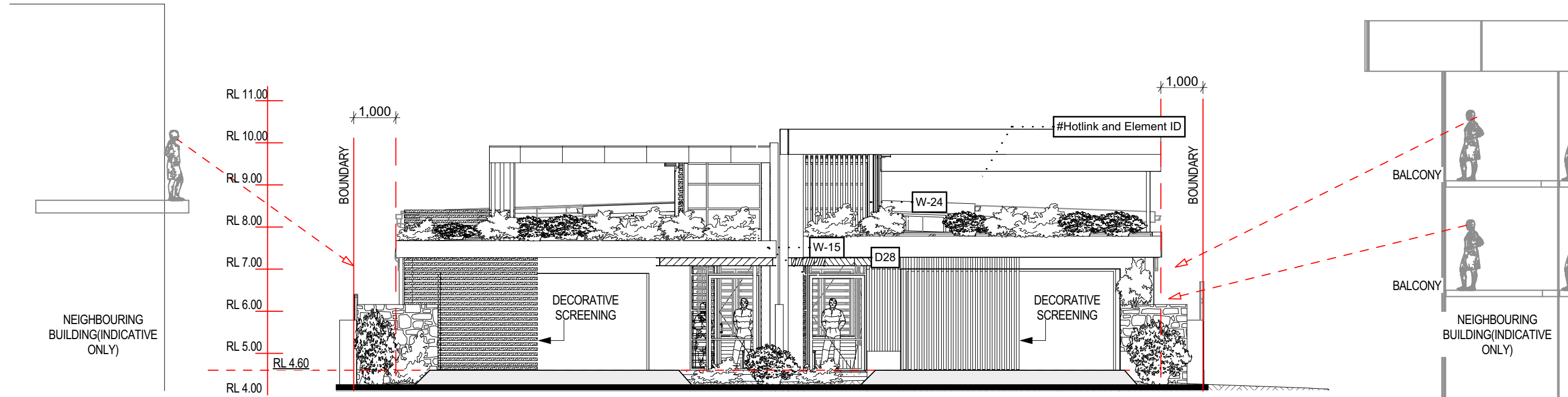
ISSUE A -  
ISSUE B -  
ISSUE C -  
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DESIGN LP PROJECT STATUS: DEVELOPMENT APPLICATION

ROOF PLAN

**SHEET  
REVISION**

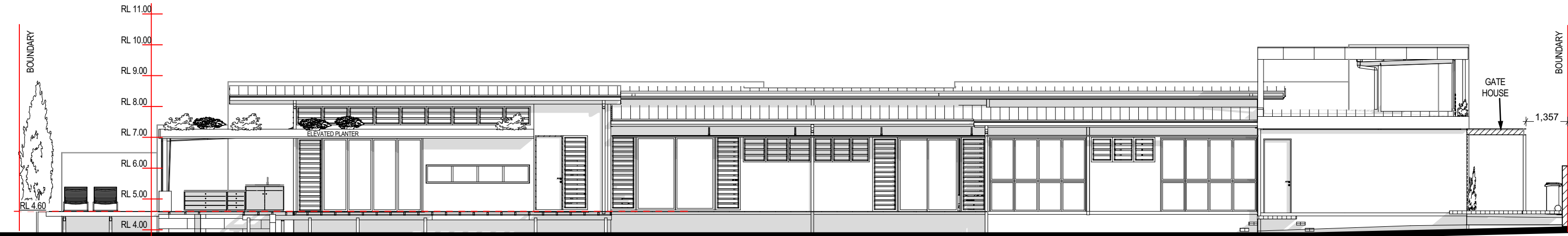
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SCALE @ A3 SHEET  
PRINT DATE  
3/02/2025 8:14 AM



AREA ACROSS SITE UP TO GARAGE DOOR LEVEL 61 sqm  
TOTAL AREA OF GARAGE DOORS 31.2sqm (51%)  
TOTAL AREA OF DECORATIVE SCREENING 19.5sqm

1 **WEST (Davidson Street)**  
1:100



2 **NORTH**  
1:125

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



**CONSULTING ENGINEERS  
& BUILDING DESIGNERS**

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ISSUE A -  
ISSUE B -  
ISSUE C -  
ISSUED

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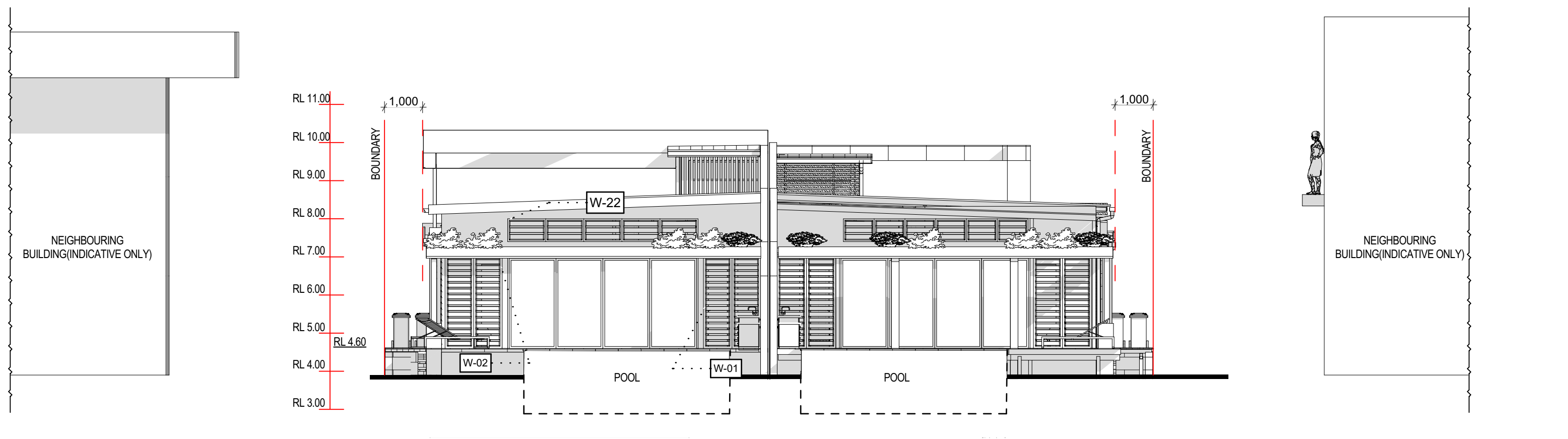
**NORTH & WEST ELEVATIONS**

**SHEET  
REVISION**

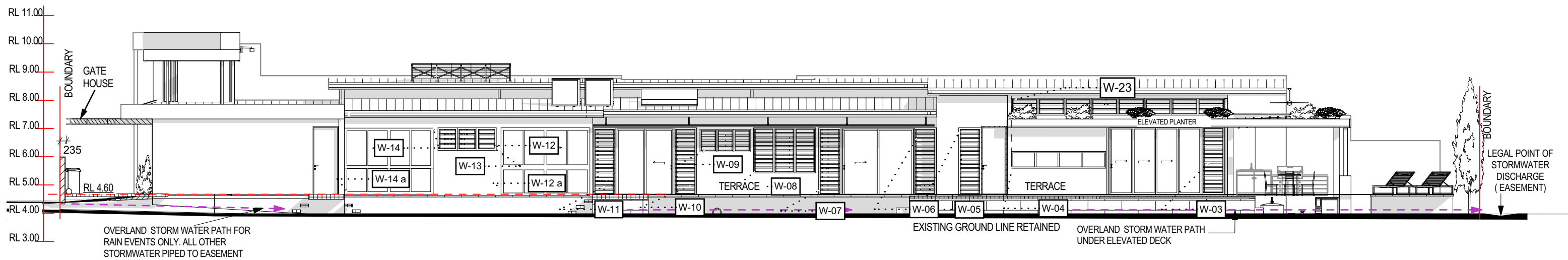
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A**

SCALE @ A3 SHEET  
PRINT DATE  
3/02/2025 8:14 AM





1 EAST ELEVATION  
1:100



2 SOUTH ELEVATION  
1:135

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



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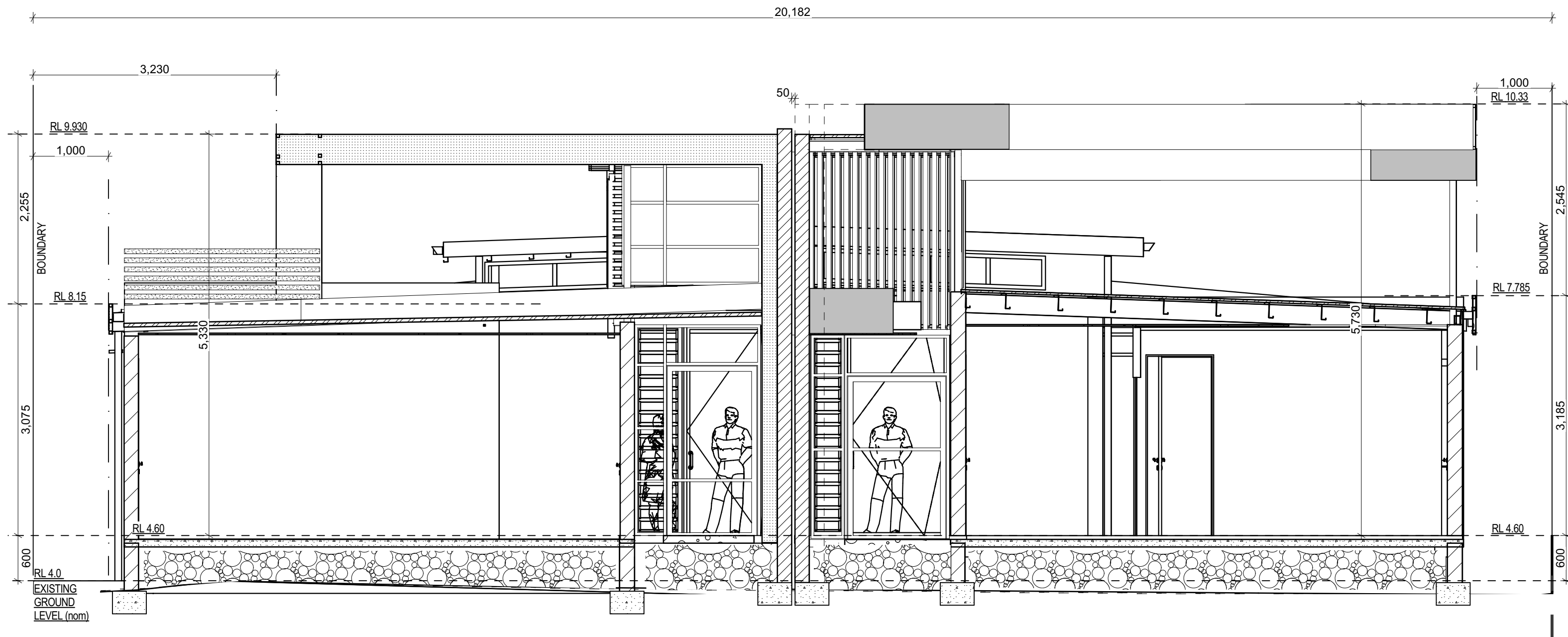
ISSUE A -  
ISSUE B -  
ISSUE C -  
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DESIGN LP PROJECT STATUS: DEVELOPMENT APPLICATION

SOUTH & EAST ELEVATIONS

SHEET  
REVISION

DA 6  
A

SCALE @ A3 SHEET  
PRINT DATE  
3/02/2025 8:14 AM



1 SECTION A-A  
1:50

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



**CONSULTING ENGINEERS  
& BUILDING DESIGNERS**

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THE WORKS AND OBTAINING ANY NECESSARY COMPLIANCE OR  
CERTIFICATION DOCUMENTATION

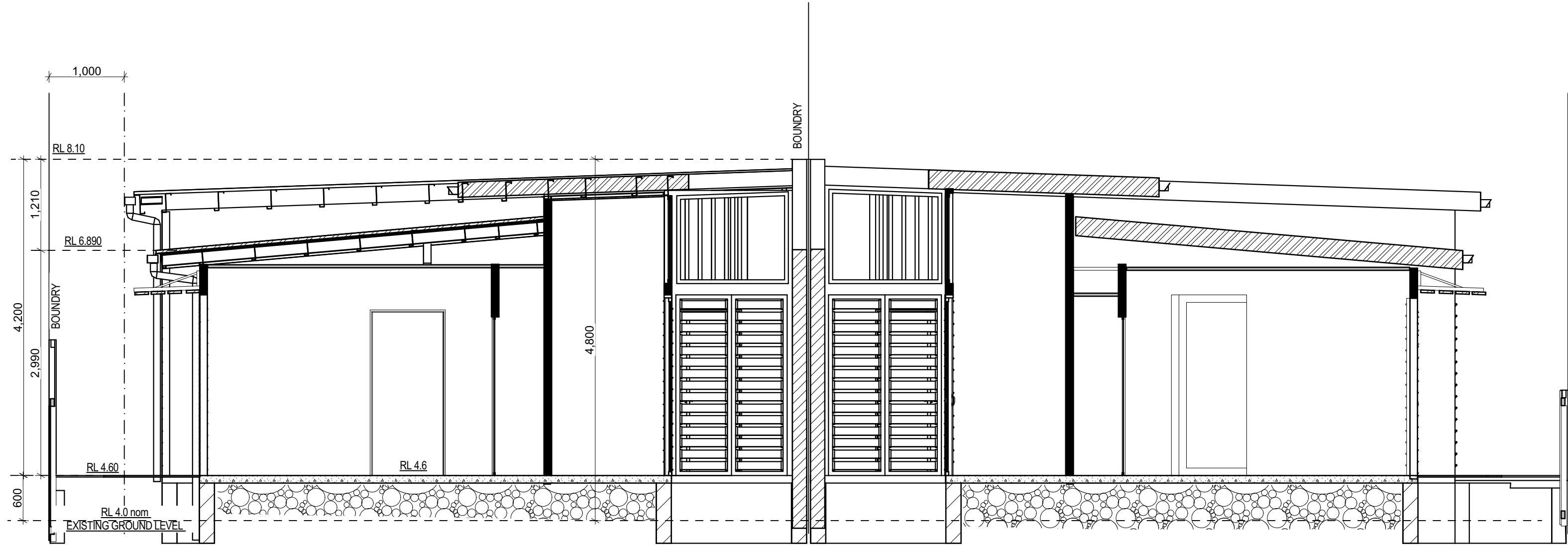
ISSUE A -  
ISSUE B -  
ISSUE C -  
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SECTION A-A

SHEET  
REVISION

DA 7  
A

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PRINT DATE  
3/02/2025 8:14 AM



**SECTION B-B**  
**1:50**

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



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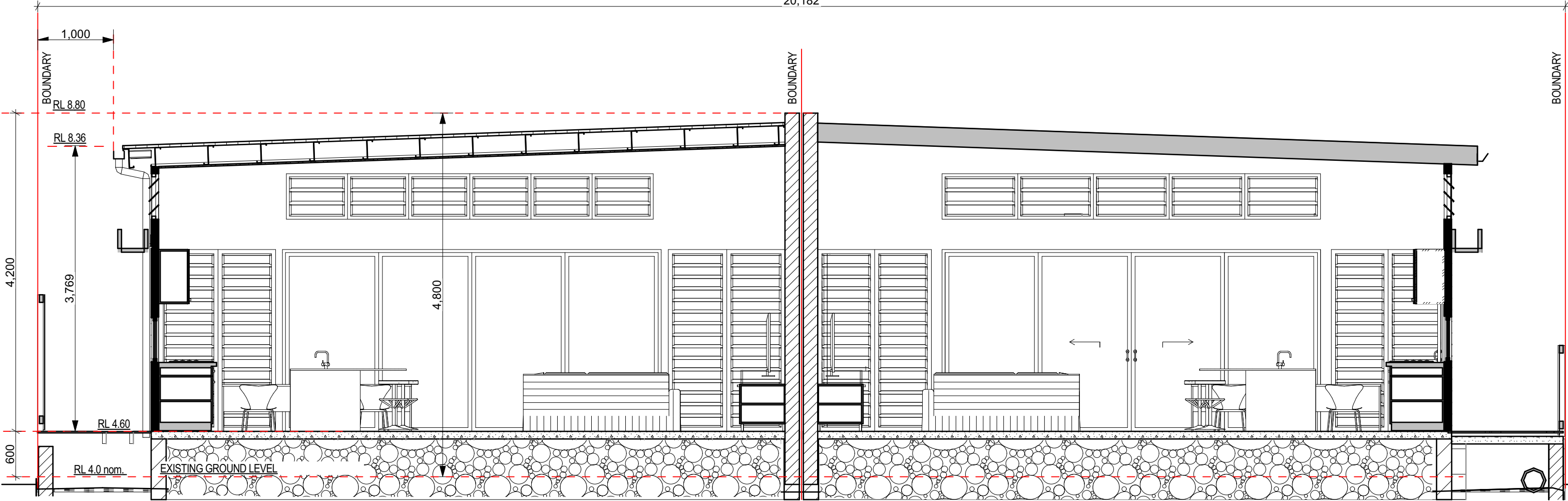
ISSUE A -  
ISSUE B -  
ISSUE C -  
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FOR DEVELOPMENT APPLICATION ONLY  
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DRAWN MG DESIGN LP PROJECT STATUS: DEVELOPMENT APPLICATION

SECTION B-B

**SHEET  
REVISION**

**DA 8  
A**

SCALE @ A3 SHEET  
PRINT DATE  
3/02/2025 8:15 AM



1 SECTION C-C  
1:50

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



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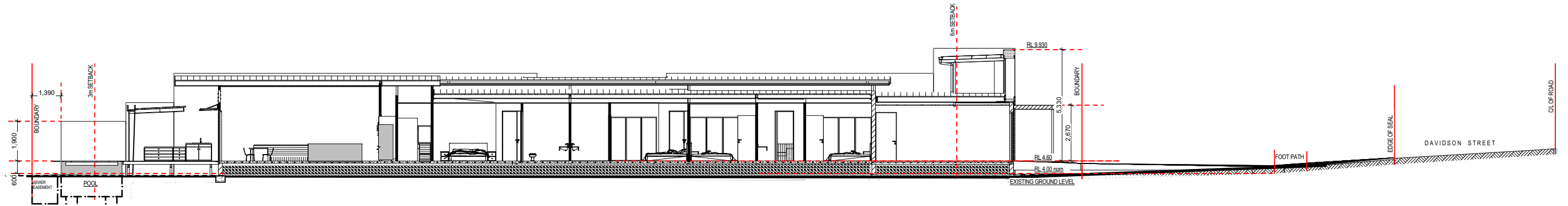
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ISSUE B -  
ISSUE C -  
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DRAWN MG DESIGN LP PROJECT STATUS: DEVELOPMENT APPLICATION

SECTION C-C

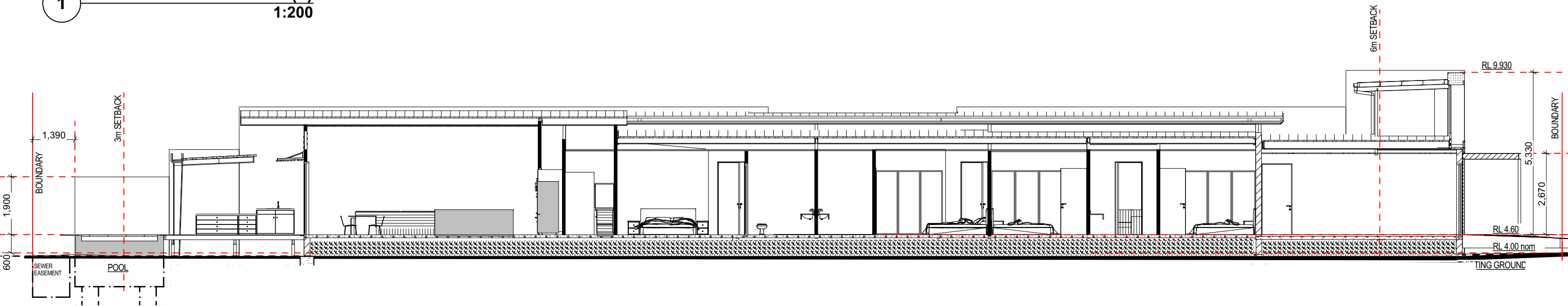
SHEET  
REVISION

DA 9  
A

SCALE @ A3 SHEET  
PRINT DATE  
3/02/2025 8:15 AM



1 SECTION C-C (a)  
1:200



2 SECTION C-C (b)  
1:125

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



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ISSUE A -  
ISSUE B -  
ISSUE C -  
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DESIGN LP

PROJECT STATUS: DEVELOPMENT APPLICATION

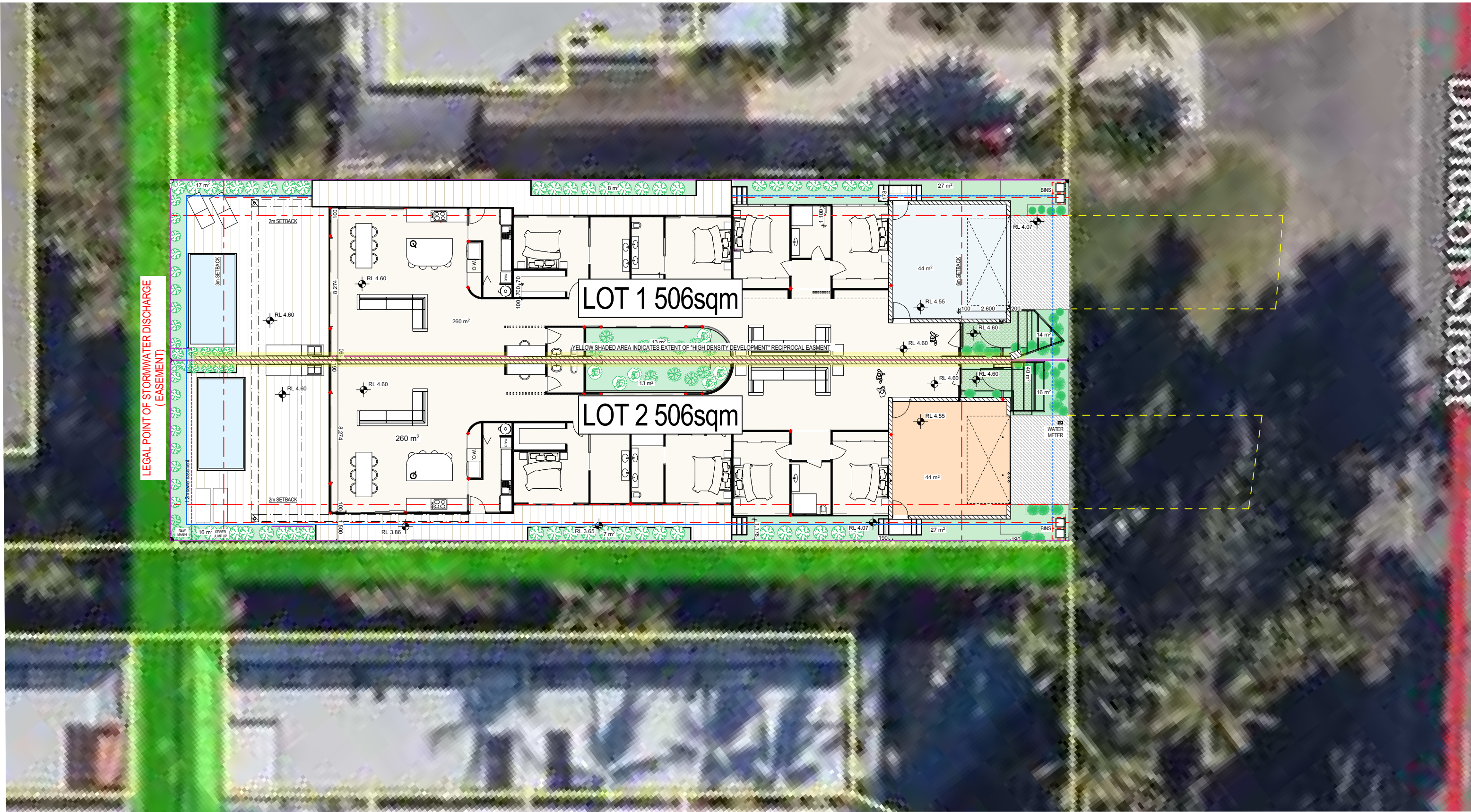
SECTION D-D

SHEET  
REVISION

DA 10  
A

SCALE @ A3 SHEET  
PRINT DATE  
3/02/2025 8:15 AM





1

**SITE OVERLAY PLAN**

1:200

Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
Client S & A COLEMAN  
PROJECT 101- 2024



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**SITE OVERLAY**

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Project NEW DWELLINGS  
Location 12 DAVIDSON STREET , PORT DOUGLAS  
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DAVIDSON STREET PERSPECTIVES


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Location	12 DAVIDSON STREET , PORT DOUGLAS				DRAWN MG	DESIGN LP	PROJECT STATUS: DEVELOPMENT APPLICATION	REVISION	A	
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# **Attachment 3**

## **Statement of Code Compliance Against the Planning Scheme**

## **6.2.14 Tourist accommodation zone code**

### **6.2.14.1 Application**

- (1) This code applies to assessing development in the Tourist accommodation zone.
- (2) When using this code, reference should be made to Part 5.

### **6.2.14.2 Purpose**

- (1) The purpose of the Tourist accommodation zone code is to provide for short-term accommodation supported by community uses and small-scale services and facilities in locations where there are tourist attractions.
- (2) The local government purpose of the code is to:
  - (a) implement the policy direction set in the Strategic Framework, in particular:
    - (i) Theme 4 : Strong communities and identity, Element 3.7.4 – Sense of place, community and identity.
    - (ii) Theme 5 – Economy. Element 3.8.2 – Economic growth and diversification, Element 3.8.2 – Tourism.
  - (b) provide for tourist accommodation development to establish in areas close to commercial and recreational services and facilities.
- (3) The purpose of the code will be achieved through the following overall outcomes:
  - (a) A range of accommodation activities, with an emphasis on short-term accommodation is established at a scale and density to service tourist needs.
  - (b) Tourist development is of an appropriate scale and achieves an attractive built form which incorporates the character and natural attributes of the site and the surrounding area as integral features of the theme and design of the development.
  - (c) Development facilitates opportunities for establishing tourist facilities and services within, or adjacent to, tourist accommodation to complement the tourist accommodation and enhance the attractiveness of tourist areas.
  - (d) Development is designed to take into account the tropical climate by incorporating appropriate architectural elements and design features.
  - (e) Landscaping of tourist development is of a high quality and contributes to the visual dominance of tropical vegetation and the local streetscape.
  - (f) Community facilities, open space and recreational areas and appropriate infrastructure to support the needs of the local community are provided.



### 6.2.14.3 Criteria for assessment

Table 6.2.14.3.a – Tourist accommodation zone code – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<b>PO1</b> The height of all buildings and structures must be in keeping with the residential character of the area.	<b>AO1</b> Buildings and structures are not more than 13.5 metres and 3 storeys in height.  Note – Height is inclusive of roof height.	<b>Complies with AO1.</b> The proposed development is single storey only, however the façade includes architectural elements which yield an overall building height of 5.73m. In context the height and scale of the building is in keeping with the character of the streetscape.
<b>Setbacks (other than for a dwelling house)</b>		
<b>PO1</b> Buildings are setback to: (a) maintain the character and amenity of the area; (b) achieve separation from neighbouring buildings and from road frontages.	<b>AO1</b> Buildings are setback: (a) a minimum of 6 metres from the main street frontage; (b) a minimum of 4 metres from any secondary street frontage; (c) 4.5 metres from a rear boundary; (d) 2 metres from a side or an average of half of the height of the building at the side setback, whichever is the greater.	<b>Not Applicable.</b> The application seeks approval for a Dwelling House, which has been designed to provide appropriate setbacks that maintain the amenity and character of the surrounding area. The proposed development includes: <ul style="list-style-type: none"> <li>• A minimum 3.284m setback from the road boundary.</li> <li>• A minimum 1.2m to 1.5m setback along the side boundaries.</li> <li>• A minimum 8.911m setback to the rear boundary.</li> </ul> <p>Given that the dwelling is single storey, the road boundary setback aligns with the established streetscape, ensuring that the building integrates seamlessly with the surrounding built form. The garage door has been deliberately designed to be discreet, reducing its visual prominence and enhancing the overall streetscape presentation. Additionally, landscaping is incorporated within the building design and setback areas, effectively softening the built form, breaking up the building's appearance, and reinforcing the tropical character of Port Douglas.</p> <p>In terms of existing street character, it is noted that the Port Douglas Motel, located on the opposite side of</p>



Davidson Street, is built to the front boundary, demonstrating a variation in setbacks within the area.

The adjacent properties on either side of the subject site are also developed, featuring multi-storey buildings with setbacks that provide an appropriate transition between structures.

- To the south, a three-storey building is setback approximately 5.6m from the common boundary. An internal driveway separates this building from the subject site, ensuring a reasonable degree of separation.
- To the north, another three-storey building is setback approximately 4.5m from the common boundary, with existing landscaping within the adjoining site providing natural screening.

The location of these neighbouring buildings has been plotted on the site plan, demonstrating that the proposed dwelling provides an acceptable level of separation and is consistent with the established development pattern in the area. The combination of appropriate setbacks, considered design, and integrated landscaping ensures that the development is compatible with its surroundings while maintaining a high standard of residential amenity and streetscape appeal.





Site coverage (other than for a dwelling house)		
<p><b>PO2</b> The site coverage of all buildings does not result in a built form that is bulky or visually obtrusive.</p>	<p><b>AO2</b> The site coverage of any building is limited to 50%</p>	<p><b>Not Applicable.</b> The application seeks approval for a Dwelling House; however, it is noted that the site coverage exceeds the typical threshold at 60%. Despite this, the scale and built form of the development are entirely in keeping with the established character of the area, where similar levels of site coverage and building intensity exist. The design ensures that the additional site coverage does not result in a bulky, overbearing, or visually obtrusive built form, as the dwelling is appropriately articulated with varying rooflines, façade treatments, and recesses to create visual interest and break up massing.</p> <p>The front façade has been carefully designed to integrate with the streetscape, incorporating architectural articulation and high-quality landscaping treatments that soften the built form and enhance visual appeal. Landscaping will be strategically incorporated at the front of the dwelling, providing a green buffer and reinforcing the tropical character of Port Douglas. Furthermore, there is an opportunity for additional planting within the 15-metre-wide road verge adjacent to the site, which would further enhance screening and integration with surrounding properties. This approach is consistent with landscaping treatments in adjoining developments, ensuring the dwelling harmonises with the surrounding streetscape while maintaining a contemporary, high-quality urban design outcome.</p>
Building proportions and scale (other than for a dwelling house)		



<p><b>PO3</b> The proportions and scale of any development are in character with the area and local streetscape.</p>	<p><b>AO3.1</b> The overall length of a building does not exceed 30 metres and the overall length of any continuous wall does not exceed 15 metres.</p> <p><b>AO3.2</b> Balconies, patios and similar spaces are not enclosed or capable of being enclosed and used as a habitable room.</p> <p><b>AO3.3</b> Balconies, patios and similar spaces are designed to be open and light weight in appearance with a maximum of 20% of the façade being fully enclosed.</p> <p><b>AO3.4</b> Roof forms, materials and colours of buildings enhance the amenity of the street and locality, including: (a) the roofs of buildings are light coloured and non-reflecting; (b) white and shining metallic finishes are avoided on external surfaces in prominent view. Note – The building incorporates building design features and architectural elements detailed in Planning scheme policy SC 6.2 – Building design and architectural elements.</p>	<p><b>Not Applicable.</b> However generally complies.</p>
<p><b>Landscaping (other than for a dwelling house)</b></p>		

**PO4**

Landscape planting is provided for the recreational amenity of residents/guests and incorporates dominant tropical vegetation which enhances the streetscape and the amenity of the area.

**AO4.1**

A minimum of 35% of the site is provided as open space and recreation area with a minimum of 30% of this total; area provided for landscape planting.

**AO4.2**

Within the frontage setback area, a minimum width of 2 metres of landscape area includes a minimum 75% dense planting.

**AO4.3**

Within the side and rear setback areas, a minimum width of 1.5 metres of landscape area includes 75% dense planting.



For assessable development		
<b>PO5</b> The establishment of uses is consistent with the outcomes sought for the Tourist accommodation zone and protects the zone from the intrusion of inconsistent uses.	<b>AO5</b> Inconsistent uses as identified in Table 6.2.14.3.b are not established in the Tourist accommodation zone.	<b>Complies with AO5.</b> Dwelling House is a consistent use.
<b>PO6</b> Development is located, designed, operated and managed to respond to the characteristics, features and constraints of the site and surrounds.  Note – Planning scheme policy – Site assessments provides guidance on identifying the characteristics, features and constraints of a site and its surrounds.	<b>AO6</b> No acceptable outcomes are prescribed.	<b>Complies with PO6.</b> A stormwater management strategy has been prepared to ensure overland flow paths are maintained, and drainage infrastructure will be designed to mitigate any potential impacts on adjoining properties.
<b>PO7</b> Development does not adversely affect the tropical, tourist and residential character and amenity of the area in terms of traffic, noise, dust, odour, lighting or other physical or environmental impacts.	<b>AO7</b> No acceptable outcomes are prescribed.	<b>Complies with PO7.</b> The development is low impact and consistent with nature of other uses in proximity to the site.
<b>PO8</b> Any loading/unloading areas, servicing areas and outdoor storage areas are screened from public view or adjacent sensitive uses.	<b>AO8</b> Outdoor loading/unloading, servicing and storage areas are sited or screened so they are: (a) not visible from any off-site public place; (b) not located adjacent to premises used for sensitive uses.	<b>Not Applicable.</b>



<p><b>PO9</b> Tourist developments include recreational and ancillary services and facilities for the enjoyment of guests.</p>	<p><b>AO9.1</b> Development which includes accommodation for tourists incorporates a mix of the following recreational and ancillary services and facilities:</p> <ul style="list-style-type: none"> <li>(a) swimming pools;</li> <li>(b) tennis courts;</li> <li>(c) barbecue areas;</li> <li>(d) outdoor lounging / recreation areas;</li> <li>(e) restaurants / bars;</li> <li>(f) tourist-focussed shopping;</li> <li>(g) tour booking office;</li> <li>(h) spa / health clubs.</li> </ul> <p><b>AO9.2</b> Any commercial services or facilities incorporated into a tourist development are small scale and predominantly service in-house guests only.</p> <p><b>AO9.3</b> Where a commercial service or facility offers services to persons over and above in-house guests, the commercial component provides on-site car parking for 50% of the floor area available for use in accordance with the relevant requirements of the Parking and access code.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO10</b> New lots contain a minimum area of 1000m<sup>2</sup>.</p>	<p><b>AO10</b> No acceptable outcomes are prescribed.</p>	<p><b>Alternative Solution.</b></p> <p>The proposed lot reconfiguration seeks to create two lots of 506m<sup>2</sup> each, which is below the minimum 1,000m<sup>2</sup> lot size. However, it is suitably demonstrated that the proposed lots are capable of accommodating an appropriate built form that meets the intent of the Douglas Shire Planning Scheme 2018 and maintains a high level of residential amenity.</p> <p>The proposed lot sizes and configurations are appropriate for the locality for the following reasons:</p> <ol style="list-style-type: none"> <li>1. Efficient Use of Urban Land &amp; Infill Development</li> </ol>



		<ul style="list-style-type: none"><li>○ The site is located in a well-served urban area with established infrastructure, road access, and proximity to local amenities.</li><li>○ The subdivision supports infill development by optimising land use efficiency while still maintaining a built form that is compatible with the surrounding area.</li><li>○ The proposal aligns with broader planning objectives that encourage consolidation within existing urban areas, reducing the demand for urban sprawl and unnecessary land consumption.</li></ul> <p>2. Capacity to Accommodate an Appropriate Built Form</p> <ul style="list-style-type: none"><li>○ The site layout and dimensions ensure that each 506m<sup>2</sup> lot is sufficiently sized to accommodate a well-designed, high-quality dwelling with appropriate setbacks, private open space, landscaping, and onsite parking.</li><li>○ The proposed dwelling houses are designed to maximise internal and external living areas, ensuring a functional and well-integrated design outcome.</li><li>○ The single-storey nature of the development ensures that site coverage remains appropriate, avoiding overdevelopment or excessive built form dominance.</li></ul> <p>3. Consistency with the Local Character and Streetscape</p> <ul style="list-style-type: none"><li>○ The area exhibits a mix of residential and tourist accommodation uses, with varying lot sizes and</li></ul>
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		<p>development typologies.</p> <ul style="list-style-type: none"><li>○ The subdivision maintains appropriate separation between buildings, ensuring that the development integrates seamlessly with the surrounding urban fabric.</li><li>○ The proposed setbacks, landscaping, and building orientation ensure that the dwellings present as individual residences, avoiding a sense of overdevelopment.</li></ul> <p>4. Compliance with Planning Scheme Intent</p> <ul style="list-style-type: none"><li>○ The proposal remains consistent with the intent of the Tourist Accommodation Zone, allowing for well-integrated residential development that can accommodate either permanent residents or short-term accommodation (subject to further approval).</li><li>○ The reduced lot sizes do not compromise streetscape appeal, amenity, or infrastructure capacity, ensuring that the development delivers a high-quality built form outcome.</li></ul>
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**PO11**

New lots have a minimum road frontage of 20 metres.

**AO11**

No acceptable outcomes are prescribed.

**Alternative Solution.**

The proposed 500m<sup>2</sup> lots will each have a frontage of approximately 10 metres, which is demonstrated to be sufficient to accommodate an appropriate built form that is both functional and consistent with the established character of the area. The proposed dwelling designs ensure that each lot can comfortably support a high-quality residential outcome, including appropriate building setbacks, private open space, on-site parking, and landscaping, all of which contribute to maintaining a high level of amenity and streetscape integration.

Lot frontages of 10 metres are increasingly common in urban infill and redevelopment scenarios, particularly in well-serviced areas where the need for sustainable land use and housing diversity is recognised. Council has previously supported similar lot configurations elsewhere, reinforcing that a narrower frontage does not compromise urban design outcomes when combined with appropriate building articulation, façade treatments, and landscaping strategies.

The proposed dwellings are architecturally designed to ensure variation in façade treatments, integration of landscaping, and appropriate garage positioning, which will prevent visual monotony along the streetscape. By positioning garages to reduce their dominance and incorporating generous front landscaping, the dwellings will present as high-quality, stand-alone residences, rather than contributing to a sense of overdevelopment or excessive density.

Furthermore, the Tourist Accommodation Zone supports flexible residential outcomes, particularly where lot sizes and frontages still enable functional and visually appealing built form integration. The



		<p>proposal ensures that each lot will contribute positively to the evolving urban character of Port Douglas, maintaining a balance between permanent residential opportunities and tourism accommodation options.</p> <p>Overall, the 10-metre frontages, combined with well-considered architectural and landscaping treatments, ensure that the proposed subdivision is entirely suitable and supportable, particularly given Council's recognition of similar lot configurations in other comparable locations. The proposal represents an efficient, logical, and high-quality residential development outcome.</p>
<b>PO12</b> New lots contain a 25 metre x 20 metre rectangle.	<b>AO12</b> No acceptable outcomes are prescribed.	<b>Alternative Solution.</b> The proposed lots are approximately 10m x 50m. Refer to supporting statements above regarding appropriate built form and consistency with other similar development approved by Council.

Table 6.2.14.3.b — Inconsistent uses within the Tourist accommodation zone

### Inconsistent uses



<ul style="list-style-type: none"> <li>• Adult store</li> <li>• Agricultural supplies store</li> <li>• Air services</li> <li>• Animal husbandry</li> <li>• Animal keeping</li> <li>• Aquaculture</li> <li>• Brothel</li> <li>• Bulk landscape supplies</li> <li>• Cropping</li> <li>• Detention facility</li> <li>• Extractive industry</li> <li>• Funeral parlour</li> <li>• Garden centre</li> <li>• Hardware and trade supplies</li> <li>• Health care services</li> <li>• High impact industry</li> <li>• Indoor sport and recreation</li> <li>• Intensive animal industry</li> </ul>	<ul style="list-style-type: none"> <li>• Intensive horticulture</li> <li>• Landing</li> <li>• Low impact industry</li> <li>• Major electricity infrastructure</li> <li>• Major sport, recreation and entertainment facility</li> <li>• Marine industry</li> <li>• Market</li> <li>• Medium impact industry</li> <li>• Motor sport facility</li> <li>• Nightclub entertainment facility</li> <li>• Office</li> <li>• Outdoor sales</li> <li>• Outdoor sport and recreation</li> <li>• Outstation</li> <li>• Park</li> <li>• Parking station</li> </ul>	<ul style="list-style-type: none"> <li>• Permanent plantation</li> <li>• Port services</li> <li>• Renewable energy facility</li> <li>• Roadside stall</li> <li>• Rural industry</li> <li>• Rural workers accommodation</li> <li>• Service station</li> <li>• Shopping centre</li> <li>• Showroom</li> <li>• Special industry</li> <li>• Substation</li> <li>• Theatre</li> <li>• Transport depot</li> <li>• Veterinary services</li> <li>• Warehouse</li> <li>• Wholesale nursery</li> <li>• Winery</li> </ul>
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Note – This table does not imply that all other uses not listed in the table are automatically consistent uses within the zone. Assessable development must still demonstrate consistency through the assessment process.

## 7.2.4 Port Douglas/Craigie local plan code

### 7.2.4.1 Application

- (1) This code applies to assessing development within the Port Douglas/Craigie local plan area as identified on the Port Douglas/Craigie local plan maps contained in Schedule 2.
- (2) When using this code, reference should be made to Part 5.

### 6.2.5.2 Context and setting

Editor's note - This section is extrinsic material under section 15 of the *Statutory Instruments Act 1992* and is intended to assist in the interpretation of the Port Douglas/Craigie local plan code.

The Port Douglas/Craigie local plan encompasses the traditional Port Douglas town centre and surrounding tourist and residential areas, including Four Mile Beach and Craigie.

Port Douglas was officially named in 1877. It was initially settled as the port of entry and supply for the Hodgkinson goldfield on the Hann Tableland which was proclaimed in 1876. It was the dominant port in Far North Queensland until a decision was made to establish Cairns as the terminus for a new railway in 1884. This ended the town's dominance, and it gradually became a small centre for local residents and fishing activities. During the 1970s and 1980s, a renewed interest in Far North Queensland as a holiday destination led to a boom in large scale tourism and residential development with Port Douglas re-emerging as a premium destination.

The Captain Cook Highway runs north-south to the west of Port Douglas through Craigie (Four Mile). Craigie caters for the permanent resident population associated with Port Douglas, as well as providing for service industries to support business in the town. The majority of urban development is confined to the eastern side of the highway. The main entrance to Port Douglas at the intersection of Port Douglas Road is accentuated by mature oil palms lining both sides of the street for almost the entire length of the corridor into the heart of Port Douglas.

Flagstaff Hill is a prominent headland on the northern side of the Port Douglas town centre providing a green tropical backdrop to the town. Island Point Road runs to the top of Flagstaff Hill and provides access to the iconic lookout overlooking the sweep of Four Mile Beach.

Macrossan Street is the main shopping area in Port Douglas running in a general east-west direction at the base of Flagstaff Hill connecting Four Mile Beach to Dickson Inlet. Tourist and commercial development is concentrated towards the western side of Macrossan Street, with marine orientated activity focussed around the inlet. The western side of the inlet provides unspoiled views across mangroves to the distinctive formations and features of the coastal range.

The street pattern in the town centre is based on the original grid pattern survey of 1878. While the town has lost many of its original buildings to cyclones and redevelopment, a number of important built features remain including the Central Hotel, the Court House Hotel, a number of relocated buildings such as St Mary's Church, the former Clink Theatre and the Court House Museum and scattered memorials such as the Carstens memorial in Macrossan Street

and the Port Douglas War memorial in Wharf Street. The Sugar Wharf on Dickson Inlet was the original terminus of the tramline to Mossman. The tramline now terminates adjacent to the Port Douglas marina and operates as the Balley Hooley passenger service on four kilometres of track between the Port Douglas Marina and St Crispins Station.

A particular characteristic of the local plan area is its high quality, lush landscaping complementing the tropical resort town atmosphere. This theme will be carried throughout the local plan area with gateways, nodes and corridor planting emphasising the role of the town as a tropical tourist destination.

#### 7.2.4.3 Purpose

- (1) The purpose of the Port Douglas/Craigie local plan code is to facilitate development outcomes consistent with community values, the local tropical built-form and protection of the natural environment within the Port Douglas/Craigie local plan area, while providing a platform for investment and prosperity.
  - (a) In addition, the purpose of the code is supported by the Port Douglas Waterfront Master Plan which provides a clear strategic direction for the incremental transformation of the Port Douglas Waterfront, including the following objectives:
    - (b) To set out a vision for revitalisation of the waterfront;
    - (c) To protect and enhance the environmental attributes; and
- (2) To provide a flexible framework, expressed through several key strategies that will assist the Council and community in managing change.
- (3) The purpose of the code will be achieved through the following overall outcomes:
  - (a) Port Douglas will continue to develop as the premium destination for international and domestic tourists in the Far North Queensland Region, while also acting for permanent residents attracted to the associated lifestyle.
  - (b) Major tourist, retail, dining and entertainment facilities will consolidate in the Town Centre and the Waterfront North sub-precincts, with improved pedestrian connections between the town centre and the waterfront.
  - (c) Craigie will develop as an integrated residential community with some low scale tourism development opportunities in appropriate locations. Craigie will also function as small scale commercial and light industry node, providing employment opportunities for the Shire's permanent resident population.
  - (d) All forms of development will complement the tropical image of the town through distinctive tropical vernacular, urban design and landscaping.
  - (e) Character will be enhanced through the identification of gateway sites, landmarks, main approach routes and pedestrian thoroughfares and view corridors;
  - (f) The Flagstaff Hill, Dickson Inlet, Four Mile Beach and other areas of scenic and environmental significance will be protected from development. Vegetation cover will dominate over built form.

- (g) Vegetation, iconic to the character of Port Douglas, including the avenues of Oil Palms, is retained and where appropriate supplemented.
  - (h) Development will be indistinguishable from view from Four Mile Beach. In addition, any development on Flagstaff Hill will be indistinguishable when viewed from vantage points in Port Douglas.
  - (i) Residential areas are designed as pleasant, functional and distinctive, in visually well-defined areas.
- (4) The purpose of the code will be further achieved through the following overall outcomes:
- (a) Precinct 1 – Port Douglas precinct
    - (i) Sub-precinct 1a – Town Centre sub-precinct
    - (ii) Sub-precinct 1b – Waterfront North sub-precinct
    - (iii) Sub-precinct 1c – Waterfront South sub-precinct
    - (iv) Sub-precinct 1d – Limited Development sub-precinct
    - (v) Sub-precinct 1e – Community and recreation sub-precinct
    - (vi) Sub-precinct 1f – Flagstaff Hill sub-precinct
  - (b) Precinct 2 – Integrated Resort precinct
  - (c) Precinct 3 – Craiglie Commercial and Light Industry precinct
  - (d) Precinct 4 – Old Port Road / Mitre Street precinct
  - (e) Precinct 5 – Very Low Density Residential/ Low Scale Recreation/Low Scale Educational/Low Scale Entertainment Uses precinct

#### **Precinct 1 – Port Douglas precinct**

- (5) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) development will contribute to the incremental transformation of the township, preserving and enhancing maritime activities and environmental areas, delivering tropical open spaces and a high quality public realm, and allowing for tourism opportunities and investment.
  - (b) development contributes to the enhancement of the Port Douglas precinct through the following development outcomes:
    - (i) access and connectivity throughout the township is enhanced through a series of improvements to circulation and mobility, including:
      - (A) access to, and connectivity along, the waterfront and foreshore areas is maintained and, where appropriate, enhanced;
      - (B) reducing reliance on the waterfront as a car parking resource.
    - (ii) the use of land in the Port Douglas precinct improves the cohesive layout of the township through:
      - (A) the establishment of distinct sub-precincts that reinforce the character and built form of the Port Douglas local plan area including:



- Port Douglas centre sub-precinct 1a – Town Centre sub-precinct;
  - Port Douglas centre sub-precinct 1b – Waterfront North sub-precinct;
  - Port Douglas centre sub-precinct 1c – Waterfront South sub-precinct;
  - Port Douglas centre sub-precinct 1d – Limited development sub-precinct;
  - Port Douglas centre sub-precinct 1e – Community and recreation precinct;
  - Port Douglas centre sub-precinct 1f – Flagstaff Hill sub-precinct;
- (B) facilitating marina facilities and supporting marine industry uses as a key part of the local economy;
- (C) reducing conflict between industry, community and commercial activities in the waterfront, without diminishing the marine industry capacity in the Port Douglas precinct;
- (iii) environment and sustainability is integrated into the township through:
- (A) preservation and enhancement of the qualities and characteristics of environmental areas of the township;
- (B) water sensitive urban design is considered as a means of water quality improvement and management of overland flow to ensure hard infrastructure solutions in Warner Street can be mitigated;
- (C) design of buildings and access way improvements prioritises walking and cycling modes of transport.
- (iv) the tropical character of the Port Douglas precinct is enhanced by ensuring development:
- (A) maintains and enhances the built form, local character, streetscapes and natural elements of the township;
- (B) is compatible with the desired character and amenity of local places and neighbourhoods;
- (C) does not exceed the height of buildings designations which contribute to the desired form of the township which contains three storey development heights in sub-precinct 1a – Town Centre sub-precinct and part of sub-precinct 1b – Waterfront North sub-precinct;
- (D) implements high quality landscaped environments around buildings and on streets;
- (E) protects the recognisable character and locally significance sites throughout the precinct.
- (v) public spaces and the streetscape are enhanced through:
- (A) an increase in the quantity and quality of public land and places throughout the precinct;
- (B) consolidating community recreation and sporting uses to create a precinct of community focussed activity between Mudlo Street and Wharf Street;
- (C) improved connections between the town centre and the waterfront marina, including an investigation of a plaza on the waterfront;

- (D) improved streetscapes with high quality landscaping, surface treatments and shaded pedestrian environments;
  - (E) the creation of a sense of place through aesthetic streetscapes and built-form character;
  - (F) managing vegetation to ensure succession of planting and the ongoing presence of significant trees.
- (vi) advertising signage is small scale, low-key and complements the tropical character of the town.

#### **Sub-precinct 1a – Town Centre sub-precinct**

- (6) In addition to other overall development outcomes, development in the Town Centre sub-precinct facilitates the following development outcomes:
- (a) tourist, retail, dining and entertainment activities are facilitated at an appropriate pedestrian scale;
  - (b) drive-through developments, bulky goods showrooms, outdoor sales, saleyards and other big-box retailing or entertainment facilities are not established;
  - (c) development contributes to a high quality public realm;
  - (d) parking (and associated infrastructure) does not undermine the relationship between buildings and street or pedestrian circulation patterns;
  - (e) consolidation of community and cultural land use activities along Mowbray Street between Wharf Street and Mudlo Street;
  - (f) active street frontages are established along Macrossan and Wharf Streets and other nearby streets as shown on the Port Douglas Centre Active Frontages and Pedestrian and Cycle Network Plan;
  - (g) Live entertainment activities are concentrated within the Live Entertainment Precinct and are subject to the recommendations of a suitably qualified acoustic engineer.

#### **Sub-precinct 1b - Waterfront North sub-precinct**

- (7) In addition to other overall development outcomes, development in the Waterfront North sub-precinct facilitates the following development outcomes:
- (a) the precinct evolves as a revitalised open space and waterside development precinct;
  - (b) development within the precinct is designed to be sympathetic to the environmentally sensitive Dickson Inlet and mitigates any adverse impacts;
  - (c) the establishment of mixed-use development is facilitated to promote activity and vitality;
  - (d) public pedestrian access is maximised along the extent of the edge of the waterfront, consisting of a boardwalk or similar structure available for 24-hour use;
  - (e) development contributes to a high quality public realm;
  - (f) built form provides an attractive point of arrival from both land and sea;
  - (g) pedestrian connectivity is safe, efficient and provides for the needs of all users of the Port Douglas waterfront;

- (h) parking (and associated infrastructure) does not undermine the relationship between buildings and street or pedestrian circulation patterns;
- (i) the importance of existing marine-based industries to the area is recognised, not diminished and protected from incompatible uses. Relocation of marine based industries to an alternative precinct does not occur until such time that agreement has been reached among all relevant stakeholders such that development does not diminish the viability of marine based industrial uses that directly serve the Port Douglas tourist and fishing operators and private boat owners;
- (j) marine infrastructure is established to service the tourism, fishing and private boating community;
- (k) Live entertainment activities are concentrated within the Live Entertainment Precinct and are subject to the recommendations of a suitably qualified acoustic engineer;

T (l) the functionality of the Balley Hooley tourist rail is retained.

#### **Sub-precinct 1c – Waterfront South sub-precinct**

- (8) In addition to all other overall development outcomes, development in the Waterfront South sub-precinct facilitates the following development outcomes:
- (a) any use of land in the precinct does not affect the environmental, habitat, conservation or scenic values of Dickson Inlet and surrounding land;
  - (b) marine-based industries are established on appropriate land having regard to site suitability, accessibility, surrounding land uses, and location of utilities and services;
  - (c) marine-based industry achieves appropriate environmental standards;
  - (d) industrial buildings have a high standard of layout and building design;
  - (e) landscaping provides an attractive streetscape and screens utility, storage and car parking from the street and other public areas;
  - (f) the precinct is protected from encroachment of incompatible land use activities.

#### **Sub-precinct 1d – Limited Development sub-precinct**

- (9) In addition to all other overall development outcomes, development in the Limited Development sub-precinct facilitates the following development outcomes:
- (a) any use of land in the precinct does not affect the environmental, habitat, conservation or scenic values of Dickson Inlet and surrounding land;
  - (b) the open nature and character of the precinct is retained maintaining view lines across the inlet;
  - (c) community and recreation land use activities are established that promote public access to the foreshore.

**Sub-precinct 1e – Community and recreation sub-precinct**

- (10) In addition to all other overall development outcomes, development in the Community and recreation sub-precinct facilitates the following development outcomes:
- (a) development for community uses, including sport and recreation is facilitated.
  - (b) sport and recreation activities predominantly involve outdoor activities;
  - (c) areas of natural vegetation are protected from further development;
  - (d) shade trees are increased, in appropriate locations, surrounding the sports fields.

**Sub-precinct 1f – Flagstaff Hill sub-precinct**

- (11) In addition to all other overall development outcomes, development in the Flagstaff Hill sub-precinct facilitates the following development outcomes:
- (a) development is not established where it results in detriment to the vegetated and scenic qualities of Flagstaff Hill;
  - (b) development minimises excavation and filling;
  - (c) buildings and other works are unobtrusive when viewed from vantage points in Port Douglas and are designed and constructed of colours and materials which complement the hill's vegetated state;
  - (d) views from public viewing points within the precinct are protected.

**Precinct 2 – Integrated Resort precinct**

- (12) In addition to the overall outcomes, development in the Integrated Resort precinct facilitates development in accordance with the *Integrated Development Resort Act, 1987*.

Editor's note – The development of land within this precinct is subject to the Integrated Development Resort Act 1987 (IDRA). Where a conflict exists between this planning scheme and the IDRA, the IDRA prevails.

**Precinct 3 – Craiglie Commercial and Light Industry precinct**

- (13) In addition to the overall outcomes, development in the Craiglie Commercial and Light Industry precinct facilitates the following overall outcomes:
- (a) development supports the tourism and marine industries in Port Douglas, along with the small-scale commercial and light industry land uses that support the local economy that would otherwise be better suited to a location outside the Port Douglas Centre Precinct unless they pose a safety issue;
  - (b) development adjacent to the Captain Cook Highway presents an attractive appearance to the highway. The rain-trees, melaleucas and eucalypt trees along the Captain Cook Highway are retained where possible, taking into account the Department of Transport and main Road's requirements;

- (c) retailing activities are generally restricted to those which are ancillary and necessarily associated with the primary service and light industry nature of the area;
- (d) adjacent residential areas are protected from industry nuisances;
- (e) lots fronting Downing Street, between Dickson Street and Beor Street, are provided with an appropriate standard of road access and infrastructure, prior to development occurring.

#### **Precinct 4 – Old Port Road / Mitre Street precinct**

- (14) In addition to the overall outcomes, development in the Old Port Road / Mitre Street precinct facilitates the following overall outcomes:
- (a) the precinct is intended to be used for outdoor recreational land use activity, primarily as a golf course;
  - (b) areas of significant vegetation are protected from development and retained;
  - (c) other forms of development will only be considered if substantial areas of open space are retained adjacent to existing residential areas to maintain the existing residential amenity of open views across open space.

#### **Precinct 5 – Very Low Density Residential/Low Scale Recreation/Low Scale Educational/Low Scale Entertainment Uses precinct**

- (15) In addition to the overall outcomes, development in the Very Low Residential Density/Low Scale Recreation/Low Scale Educational/Low Scale Entertainment Uses precinct facilitates the following overall outcomes:
- (a) residential accommodation does not exceed a maximum of 8.5 metres in building height;
  - (b) minimum lot sizes exceed 2 hectares;
  - (c) very low scale and intensity recreation/ very low scale and intensity educational/ and very low scale entertainment uses may be appropriate in areas of the precinct subject to erosion and other flooding constraints.

Note - Undeveloped lots in this precinct are located on very low-lying land. Council may consider a consolidation of existing land titles via lot reconfiguration to lot sizes less than 2 hectares, where the reconfigured lots are consolidated onto the highest terrain, to avoid a pattern of development consisting of dwelling houses located on isolated islands of raised building pads.

**Criteria for assessment****Table 7.2.4.4.a –Port Douglas / Craiglie local plan – assessable development**

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<b>Development in the Port Douglas / Craiglie local plan area generally</b>		
<b>P01</b> Pedestrians, cyclists, motorists and public transport users can easily move into and through the precinct along planned connectivity routes, identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.	<b>A01</b> A pedestrian and cycle movement network is integrated and delivered through development.	<b>Not Applicable.</b>
<b>P02</b> Development retains and enhances key landscape elements including character trees and areas of significant vegetation contributing to the character and quality of the local plan area and significant views and vistas and other landmarks important to the context of Port Douglas / Craiglie (as identified on the Port Douglas/ Craiglie Townscape Plan map contained in Schedule 2).	<b>A02.1</b> Development provides for the retention and enhancement of existing mature trees and character vegetation that contribute to the lush tropical character of the town, including: <ul style="list-style-type: none"> <li>(a) the tree covered backdrop of Flagstaff Hill;</li> <li>(b) natural vegetation along watercourses, in particular the Mowbray River, Beor Creek and Dickson Inlet;</li> <li>(c) the tidal vegetation along the foreshore;</li> <li>(d) beachfront vegetation along Four Mile Beach, including the fringe of Coconut Palms;</li> <li>(e) the oil palm avenues along the major roads;</li> <li>(f) the lush landscaping within major roundabouts at key nodes;</li> <li>(g) Macrossan Street and Warner Street;</li> <li>(h) Port Douglas waterfront.</li> </ul>	<b>Not Applicable.</b>





Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>AO2.2</b> Development protects and does not intrude into important views and vistas as identified on the Port Douglas Townscape Plan map contained in Schedule 2, in particular:</p> <ul style="list-style-type: none"> <li>(a) Flagstaff Hill;</li> <li>(b) Four Mile Beach;</li> <li>(c) Across to the ranges over Dickson Inlet;</li> <li>(d) Mowbray Valley.</li> </ul> <p><b>AO2.3</b> Important landmarks, memorials and monuments are retained.</p>	<b>Not Applicable.</b>
<p><b>PO3</b> Development contributes to the protection, reinforcement and where necessary enhancement of gateways and key intersections identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.</p>	<p><b>AO3</b> Development adjacent to the gateways and nodes as identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 incorporates architectural features and landscaping treatments and design elements that enhance the sense of arrival and way finding within the town.</p>	<b>Not Applicable.</b>
<p><b>PO4</b> Landscaping of development sites complements the existing tropical character of Port Douglas and Craiglie.</p>	<p><b>AO4</b> Landscaping incorporates the requirements of Planning scheme policy SC6.7 – Landscaping, in particular landscaping should be capable of achieving a 60% screening of development within 5 years and predominantly consists of endemic vegetation.</p>	<p><b>Will Comply.</b> Hortulus Landscapes and Design have been engaged to prepare the landscape concept plan, ensuring a high-quality and integrated landscaping outcome for the development. Planters positioned along the top of the building will enhance greenery and screening, contributing to both visual appeal and privacy. It is noted that adjoining and surrounding developments benefit from landscaping within the Davidson Street verge, reinforcing the leafy and tropical character of the area. Subject to Council approval, additional landscaping may be incorporated within the</p>



Performance outcomes	Acceptable outcomes	Applicant response
		verge area, further enhancing the streetscape and integrating the development with its surroundings.
<b>PO5</b> Development does not compromise the safety and efficiency of the State-controlled road network.	<b>AO5</b> Direct access is not provided to a State-controlled road where legal and practical access from another road is available.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>For assessable development</b>		
<b>Additional requirements in Precinct 1 – Port Douglas precinct</b>		
<b>PO6</b> The views and vistas identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 are maintained.	<b>AO6.1</b> Development does not impede continued views to scenic vistas and key streetscapes within the local plan area.  <b>AO6.2</b> Unless otherwise specified within this Local Plan, buildings are set back not less than 6 metres from the primary street frontage.	<b>Not Applicable.</b>
<b>PO7</b> Vehicle access, parking and service areas: (a) do not undermine the relationship between buildings and street or dominate the streetscape; (b) are designed to minimise pedestrian vehicle conflict; (c) are clearly identified and maintain ease of access at all times.	<b>AO7.1</b> For all buildings, parking is: (a) to the side of buildings and recessed behind the main building line; or (b) behind buildings; or (c) wrapped by the building façade, and not visible from the street.  <b>AO7.2</b> Ground level parking incorporates clearly defined pedestrian routes. <b>AO7.3</b> Any porte-cocheres, disabled and pedestrian accesses are accommodated within the boundary of new or refurbished development. <b>AO7.4</b> Where the development is an integrated mixed-use development incorporating short term accommodation or multiple dwellings and either food and drink outlet or hotel or shop or shopping centre or office, on-site parking spaces are provided as per the number prescribed in the Parking and access code with a relaxation of 30% of spaces required for the non-residential uses.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>A07.5</b> On-site car parking available for public use is clearly signed at the site frontage.</p> <p><b>A07.6</b> Boom gates, pay machines or other regulatory devices to control access to a publicly available car parking area are not constructed or installed.</p>	
<p><b>PO8</b> Precinct 1 – Port Douglas precinct is not characterised by a proliferation of advertising signs.</p>	<p><b>A08</b> No acceptable outcomes are prescribed.</p>	<b>Not Applicable.</b>
<b>Additional requirements for Sub-precinct 1a – Town Centre sub-precinct</b>		
<p><b>PO9</b> Building heights:</p> <ul style="list-style-type: none"> <li>(a) do not overwhelm or dominate the town centre;</li> <li>(b) respect the desired streetscape;</li> <li>(c) ensure a high quality appearance when viewed from both within the town centre sub-precinct and external to the town centre sub-precinct;</li> <li>(d) remain subservient to the natural environment and the backdrop of Flagstaff Hill.</li> <li>(e) do not exceed 3 storeys.</li> </ul>	<p><b>A09</b> Buildings and structures are not more than 3 storeys and 13.5 metres in height, with a roof height of not less than 3 metres.</p> <p>Note – Height is inclusive of the roof height.</p>	<b>Not Applicable.</b>
<p><b>PO10</b> Building design, the streetscape, pedestrian paths and street front spaces promote integration with the surrounding area and the rest of Precinct 1 – Port Douglas Precinct.</p>	<p><b>A010</b> No acceptable outcomes are prescribed.</p>	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO11</b> Buildings: (a) address street frontages; (b) ensure main entrances front the street or public spaces; (c) do not focus principally on internal spaces or parking areas.	<b>AO11</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO12</b> Setbacks at ground level provide for: (a) connection between pedestrian paths and public places; (b) areas for convenient movement of pedestrians; (c) changes in gradient of the street.	<b>AO12</b> Setbacks at ground level: (a) are clear of columns and other obstructions; (b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths.	<b>Not Applicable.</b>
<b>AO13</b> Buildings do not result in a reduction of views and vistas from public places to: (a) Flagstaff Hill; (b) Dickson Inlet; (c) public open space; (d) places of significance.	<b>AO13</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO14</b> Development enhances the distinctive tropical resort town and identity of Port Douglas and encourages pedestrian activity at street level including shade protection across the footpath for the length of the building.	<b>AO14</b> Development is built up to the street frontage/s at the street level and incorporates a light frame awning, a minimum of 3 metres in width for the length of the street frontage/s; or If a development includes an outdoor dining area at ground/footpath level, the dining area has a maximum setback of 3 metres and the required awning is still maintained along the length of the street frontage/s. Note – PO24 provides more detail on awning design.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO15</b> Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street-level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.	<b>AO15.1</b> Centre activities establish: at street level on active street frontages; a maximum of one level above street level.  <b>AO15.2</b> Any residential development activities or short term accommodation is located above street level of the active frontage, but not on or up to the street frontage in any development, including mixed use development.	<b>Not Applicable.</b>
<b>PO16</b> Detailed building design: (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements; (c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the lush, vegetated character of the Town Centre sub-precinct is maintained.	<b>AO16</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO17</b> Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through: (a) surface decoration; (b) wall recesses and projections; (c) a variation in wall finishes; windows, balconies, awnings and other visible structural elements.	<b>AO17</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>





Performance outcomes	Acceptable outcomes	Applicant response
(d) differentiating between the lower, middle and upper parts of the building by varying the façade and/or the shape of the built form, where comprised of more than two storeys.		
<b>P018</b> Roofs are not characterised by a cluttered display of plant and equipment, in particular: <ul style="list-style-type: none"> <li>(a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Town Centre sub-precinct;</li> <li>(b) service structures, lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view;</li> <li>(c) rooftops are not used for advertising.</li> </ul>	<b>AO18</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>P019</b> Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to: <ul style="list-style-type: none"> <li>(a) shade windows;</li> <li>(b) reduce glare;</li> <li>(c) assist in maintaining comfortable indoor temperatures;</li> <li>(d) minimising heat loads;</li> <li>(e) enrich the North Queensland tropical character of the Town Centre sub-precinct;</li> <li>(f) provide architectural interest to building façades.</li> </ul>	<b>AO19</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO20</b> Buildings are finished with high quality materials, selected for: <ul style="list-style-type: none"> <li>(a) their ability to contribute the character of Town Centre sub-precinct;</li> <li>(b) easy maintenance, durability and an ability not to readily stain, discolour or deteriorate.</li> </ul>	<b>AO20</b> No acceptable outcomes are prescribed	<b>Not Applicable.</b>
<b>PO21</b> Buildings do not incorporate any type of glass or other materials that are likely to reflect the sun's rays in a manner that may create a nuisance, discomfort or a hazard.	<b>AO21</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO22</b> Façades and elevations do not include large blank walls. Openings and setbacks are used to articulate vertical building surfaces.	<b>AO22.1</b> Development has a maximum length of unbroken building facade of 20 metres and a maximum extent of overall development in the same style/design along the street frontage/s of 40 metres.  <b>AO22.2</b> Any break in the building façade varies the alignment by a 1 metre minimum deviation.  <b>AO22.3</b> A minimum of three of the following building design features and architectural elements detailed below are incorporated to break the extended facade of a development: <ul style="list-style-type: none"> <li>(a) a change in roof profile;</li> <li>(b) a change in parapet coping;</li> <li>(c) a change in awning design;</li> <li>(d) a horizontal or vertical change in the wall plane; or</li> <li>(e) a change in the exterior finishes and exterior colours of the development.</li> </ul>	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO23</b> Building facades that face public spaces at ground level: <ul style="list-style-type: none"> <li>(a) complement the appearance of the development and surrounding streetscape;</li> <li>(b) enhance the visual amenity of the public place;</li> <li>(c) include a variety of human scale architectural elements and details;</li> <li>(d) provide an opportunity for the casual and convenient surveillance of public space from within the development.</li> </ul>	<b>AO23</b> Building facades at the ground floor of development that face public space are designed to ensure: <ul style="list-style-type: none"> <li>(a) a minimum of 70% of the façade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development;</li> <li>(b) a visually prominent main entrance that faces the principal public place;</li> <li>(c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the façade.</li> </ul>	<b>Not Applicable.</b>
<b>PO24</b> Awnings for pedestrian shelter are consistent with the character setting of the Town Centre sub-precinct and: <ul style="list-style-type: none"> <li>(a) extend and cover the footpath to provide protection from the sun and rain;</li> <li>(b) include lighting under the awning;</li> <li>(c) are continuous across the frontage of the site;</li> <li>(d) align to provide continuity with existing or future awnings on adjoining sites;</li> <li>(e) are a minimum of 3.0 metres in width and generally not more than 3.5 metres above pavement height;</li> <li>(f) do not extend past a vertical plane, 1.2 metres inside the kerb-line to enable street trees to be planted and grow;</li> <li>(g) are cantilevered from the main building with any posts within the footpath being non load-bearing.</li> </ul>	<b>AO24</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO25</b> Development integrates with the streetscape and landscaping improvements for Port Douglas.	<b>AO25</b> Development fronting Davidson Street, Macrossan Street, Wharf Street, Mowbray Street and Warner Street is designed to integrate with the on-street landscaping and design improvements as outlined within the Port Douglas landscape master plan contained within Planning scheme policy SC6.7 – Landscaping.  Note - Planning scheme policy SC6.7 - Landscaping provides guidance on meeting the Performance Outcome.	<b>Not Applicable.</b>
<b>Additional requirements for Sub-precinct 1b – Waterfront North sub-precinct</b>		
<b>PO26</b> The establishment of uses is consistent with the outcomes sought for sub-precinct 1b – Waterfront North.	<b>AO26</b> Uses identified as inconsistent uses in Table 7.2.4.b – Inconsistent uses in sub-precinct 1b Waterfront North sub precinct are not established in sub-precinct 1b - Waterfront North.	<b>Not Applicable.</b>
<b>PO27</b> The bulk and scale of buildings is consistent with surrounding development and steps down to complement the open space areas in the adjoining limited development sub-precinct.	<b>AO27</b> Buildings and structures are not more than: (a) 3 storeys and 13.5 metres in height , with a roof height of not less than 3 metres, in those parts of the precinct south of Inlet Street; (b) 2 storeys and 8.5 metres in height, with a roof height of not less than 3 metres, in those parts of the precinct north of Inlet Street. Note – Height is inclusive of roof height.	<b>Not Applicable.</b>
<b>PO28</b> Building design, streetscape, pedestrian paths and street front spaces promote integration with the surrounding area and the rest of Precinct 1 – Port Douglas Precinct	<b>AO28</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO29</b> Public pedestrian access along the water's edge is maximised.	<b>AO29.1</b> Public pedestrian access is provided along the frontage of the water's edge consisting of a boardwalk of a minimum width of 4 metres that is available of 24-hour use.  <b>AO29.2</b> A public plaza is incorporated into the design generally reflecting the requirements of the Port Douglas Waterfront Master Plan, focussing in the vicinity of the 'Duck Pond'.  <b>AO29.3</b> Built envelopes are setback a minimum of 3.0 metres from the board walk, with a shelter/shade zone between the building envelopes and the boardwalk consisting of shade structure, canopies, verandahs and the like.	<b>Not Applicable.</b>
<b>PO30</b> Buildings: (a) address street frontages; (b) ensure main entrances front the street or public spaces.	<b>AO30</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO31</b> Setbacks at ground level provide for: (a) connection between pedestrian paths and public places; (b) areas for convenient movement of pedestrians; (c) changes in gradient.	<b>AO31</b> Setbacks at ground level: (a) are clear of columns and other obstructions; (b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO32</b> Buildings do not result in a reduction of views and vistas from public places to: (a) Dickson Inlet; (b) public open space; (c) places of significance.	<b>AO32</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO33</b> Development enhances the distinctive tropical resort town and identity of Port Douglas and encourages pedestrian activity at ground level including shade protection across the footpath and open space areas.	<b>AO33</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO34</b> Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street-level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.	<b>AO34.1</b> Centre activities establish: (a) at street level on active street frontages; (b) a maximum of one level above street level. <b>AO34.2</b> Residential development activities or short term accommodation is located above street /ground floor level of the active frontage, but not on or up to the street / public frontage in any development, including mixed use development.	<b>Not Applicable.</b>
<b>PO35</b> Detailed building design: (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements; (c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the lush, vegetated character of the Waterfront North sub-precinct is maintained.	<b>AO35</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>





Performance outcomes	Acceptable outcomes	Applicant response
<b>PO36</b> Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through: <ul style="list-style-type: none"> <li>(a) surface decoration;</li> <li>(b) wall recesses and projections;</li> <li>(c) a variation in wall finishes; windows, balconies, awnings and other visible structural elements.</li> <li>(d) differentiating between the lower, middle and upper parts of the building by varying the façade and/or the shape of the built form, where comprised of more than two storeys.</li> </ul>	<b>AO36</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO37</b> Roofs are not characterised by a cluttered display of plant and equipment, in particular: <ul style="list-style-type: none"> <li>(a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Waterfront North sub-precinct;</li> <li>(b) service structures, lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view;</li> <li>(c) rooftops are not used for advertising.</li> </ul>	<b>AO37</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO38</b> Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to: <ul style="list-style-type: none"> <li>(a) shade windows;</li> <li>(b) reduce glare;</li> <li>(c) assist in maintaining comfortable indoor temperatures;</li> <li>(d) minimising heat loads;</li> </ul>	<b>AO38</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
(e) enriching the North Queensland tropical character of the Waterfront North sub-precinct; (f) architectural interest to building façades.		
<b>PO39</b> Buildings are finished with high quality materials, selected for: (a) their ability to contribute the character of Waterfront North sub-precinct; (b) easy maintenance, durability and an ability not to readily stain, discolour or deteriorate.	<b>AO39</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO40</b> Buildings do not incorporate any type of glass or other materials that are likely to reflect the sun's rays in a manner that may create a nuisance, discomfort or a hazard.	<b>AO40</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO41</b> Façades and elevations do not include large blank walls and openings and setbacks are used to articulate vertical building surfaces.	<b>AO41.1</b> Development has a maximum length of unbroken building facade of 20 metres and a maximum extent of overall development in the same style/design along the street frontage/s of 40 metres.  <b>AO41.2</b> Any break in the building façade varies the alignment by a 1 metre minimum deviation.  <b>AO41.3</b> A minimum of three of the following building design features and architectural elements detailed below are incorporated to break the extended facade of a development: (a) a change in roof profile; (b) a change in parapet coping; (c) a change in awning design;	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
	(d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development	
<b>PO42</b> Building facades that face public spaces at ground level: (a) complement the appearance of the development and surrounding streetscape; (b) enhance the visual amenity of the public place; (c) include a variety of human scale architectural elements and details; (d) provide an opportunity for the casual and convenient surveillance of public space from within the development.	<b>AO42</b> Building facades at the ground floor of development that face public space are designed to ensure: (a) a minimum of 70% of the façade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development; (b) a visually prominent main entrance that faces the principal public place; (c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the façade.	<b>Not Applicable.</b>
<b>PO43</b> Awnings for pedestrian shelter are consistent with the character setting of the Waterfront North sub-precinct and: (a) extend and cover the footpath to provide protection from the sun and rain; (b) include lighting under the awning; (c) are continuous across pedestrian circulation areas; (d) align to provide continuity with existing or future awnings on adjoining sites; (e) are a minimum of 3 metres in width and generally not more than 3.5 metres above pavement height; (f) do not extend past a vertical plane, 1.2 metres inside the street kerb-line to enable street trees to be planted and grow;	<b>AO43</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
(g) are cantilevered from the main building with any posts within the footpath being non load-bearing.		
<b>PO44</b> The Balley Hooley rail line and turn-table is retained and incorporated into development and maintains its functionality.	<b>AO44.1</b> Bally Hooley rail line and turn-table is retained and incorporated into development to maintain its functionality.  <b>AO44.2</b> Where development provides floor area for the Bally Hooley rail station, the gross floor area of the rail line and station does not generate a requirement for additional vehicle parking.	<b>Not Applicable.</b>
<b>PO45</b> Development recognises the importance of and relationship between the marina, commercial and residential development in the Waterfront North sub-precinct, and includes measures to mitigate the impact of: (a) noise; (b) odour; (c) hazardous materials; (d) waste and recyclable material storage.	<b>AO45</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO46</b> Formalised public spaces and pedestrian paths/areas on freehold land are made accessible to the public.	<b>AO46</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO47</b> Buildings, civic spaces, roads and pedestrian links are enhanced by: <ul style="list-style-type: none"> <li>(a) appropriate landscape design and planting;</li> <li>(b) themed planting that defines entry points, and creates strong 'entry corridors' into the waterfront;</li> <li>(c) lighting and well-considered discrete signage that complements building and landscape design;</li> <li>(d) public artwork and other similar features that reflect the heritage and character of the Port Douglas Waterfront.</li> </ul>	<b>AO47</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO48</b> Buildings are designed and sited to provide vistas along shared pedestrian/open space and movement areas in suitable locations.	<b>AO48</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO49</b> Development does not diminish the viability of marine-based industrial uses that directly serve the Port Douglas tourist and fishing operators and private boat owners, particularly with respect to the slipway operation.	<b>AO49</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO50</b> Marine infrastructure to service the tourism, fishing and private boating community is provided.	<b>AO50</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO51</b> Changes to the Port Douglas Waterfront quay-line do not cause adverse impacts to the environmentally sensitive Dickson Inlet.	<b>AO51</b> Development that results in changes to the Port Douglas Waterfront quay-line is only established where an Ecological assessment report provides support to the changes.  Note - Planning scheme policy SC6.8 – Natural environment provides guidance on preparing an ecological assessment report.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>Additional requirements for Sub-precinct 1c – Waterfront South sub-precinct</b>		
<b>PO52</b> The establishment of uses is consistent with the outcomes sought for Precinct 1c – Waterfront South.	<b>AO52</b> Uses identified as inconsistent uses in Table 7.2.4.4.c are not established in Precinct 1c – Waterfront South.	<b>Not Applicable.</b>
<b>PO53</b> Development does not adversely impact on the natural environment, natural vegetation or watercourses.	<b>AO53.1</b> An Ecological assessment report is prepared identifying the environmental qualities of the surrounding natural and built features which are to be managed.  Note - Planning scheme policy SC6.8 – Natural environment provides guidance on preparing an ecological assessment report.  <b>AO53.2</b> An Environmental Management Plan is prepared to manage potential impacts of the operation of the development on surrounding natural areas.  Note - Planning scheme policy SC6.4 – Environmental management plans contains information to demonstrate compliance and guidance on preparing an Environmental Management Plan.	<b>Not Applicable.</b>
<b>PO54</b> Development of land at the end of Port Street adjacent to Dickson Inlet incorporates a slipway, or an alternative functioning facility, with capacity to service the Port Douglas marine and tourism industry.	<b>AO54</b> A master plan for the development is provided and implemented to demonstrate the integration of the slipway, or an alternative functioning facility, with other supporting service industry activities that service the marine and tourism industry of Port Douglas.	<b>Not Applicable.</b>
<b>PO55</b> Buildings and structures are of a height, and are set back from side boundaries and other sensitive areas to ensure the scenic amenity and environmental qualities of the adjacent area are not adversely affected.	<b>AO55.1</b> Development has a height of not more than 10 metres.  <b>AO55.2</b> Development is setback from all property boundaries not less than 3 metres.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO56</b> The site coverage of all buildings and structures ensures development: <ul style="list-style-type: none"> <li>(a) is sited in an existing cleared area or in an area approved for clearing;</li> <li>(b) has sufficient area for the provision of services;</li> <li>(c) development does not have an adverse effect on the environmental, habitat, conservation or landscape values of the on-site and surrounding sensitive areas.</li> </ul>	<b>AO56</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO57</b> Premises include adequate provision for service vehicles, to cater for generated demand. Loading areas for service vehicles are designed to: <ul style="list-style-type: none"> <li>(a) be accommodated on-site;</li> <li>(b) maximise safety and efficiency of loading;</li> <li>(c) protect the visual and acoustic amenity of sensitive land use activities;</li> <li>(d) minimise adverse impacts on natural characteristics of adjacent areas.</li> </ul>	<b>AO57.1</b> Sufficient manoeuvring area is provided on-site to allow a Medium Rigid Vehicle to enter and leave the site in a forward gear.  <b>AO57.2</b> Development is designed to ensure all service vehicles are contained within the site when being loaded/unloaded.  <b>AO57.3</b> Driveways, parking and manoeuvring areas are constructed and maintained to: <ul style="list-style-type: none"> <li>(a) minimise erosion from storm water runoff;</li> <li>(b) retain all existing vegetation.</li> </ul>	<b>Not Applicable.</b>
<b>PO58</b> Development ensures adverse impacts from service vehicles on the road network, external to the site, are minimised.	<b>AO58</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>





Performance outcomes	Acceptable outcomes	Applicant response
<b>PO59</b> Entry to the site is landscaped to enhance the amenity of the area and provide a pleasant working environment.	<b>AO59</b> Areas used for loading and unloading, storage, utilities and car parking are screened from public view: (a) by a combination of landscaping and screen fencing; (b) dense planting along any road frontage is a minimum width of 3 metres.	<b>Not Applicable.</b>
<b>PO60</b> Landscaping is informal in character and complementary to the existing natural environment, provides screening and enhances the visual appearance of the development.	<b>AO60</b> For any development landscaping is in accordance with the Plant species schedule in Planning scheme policy SC6.7– Landscaping.	<b>Not Applicable.</b>
<b>Additional requirements for Sub-precinct 1d – Limited Development sub-precinct</b>		
<b>PO61</b> The height of buildings and structures contributes to the desired form and outcomes for the sub-precinct and are limited to a single storey.	<b>AO61</b> Buildings and structures are not more than one storey and 4 metres in height.  Note - Height is inclusive of the roof height.	<b>Not Applicable.</b>
<b>Additional requirements for Sub-precinct 1e – Community and recreation sub-precinct</b>		
<b>PO62</b> The precinct is developed for organised sporting activities and other community uses.	<b>AO62</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>Additional requirements for Sub-precinct 1f – Flagstaff Hill sub-precinct</b>		
<b>PO63</b> Flagstaff Hill is protected from inappropriate development to protect the hill as an important natural landmark feature of Port Douglas and as a vegetated backdrop to the Town centre.	<b>AO63</b> No acceptable outcomes are prescribed	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>PO64</b> All development on Flagstaff Hill is designed to minimise the visibility of the development and to ensure development is subservient to the natural landscape and topography of the site, including through: <ul style="list-style-type: none"> <li>(a) building design which minimises excavation and filling;</li> <li>(b) buildings being designed to step down the site and incorporate foundations and footings on piers or poles;</li> <li>(c) buildings being visually unobtrusive and incorporating exterior finishes and muted colours which are non-reflective and complement the colours of the surrounding vegetation and view-shed;</li> <li>(d) protection of the views from public viewing points in the Port Douglas precinct.</li> </ul>	<b>AO64</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>Additional requirements for Precinct 3 – Craiglie Commercial and Light Industry precinct</b>		
<b>PO65</b> Development supports the tourism and marine industries in Port Douglas, along with the small-scale commercial and light industry land uses that support the local economy that would otherwise be better suited to a location outside the Port Douglas Town Centre Precinct.	<b>AO65</b> Development consists of service and light industries and associated small scale commercial activities.	<b>Not Applicable.</b>
<b>PO66</b> Development on lots adjacent to the Captain Cook Highway is sited, designed and landscaped to provide an attractive visual approach to Port Douglas with all buildings, structures and car parking areas setback a sufficient distance from the frontage to enable landscaping to soften or screen the appearance of the development.	<b>AO66.1</b> Buildings and structures are setback 8 metres from the Captain Cook Highway frontage, or no closer to the Captain Cook Highway frontage than buildings and structures on adjoining sites (averaged), whichever is the greater.	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>AO66.2</b> The setback area to the Captain Cook Highway frontage is landscaped with advanced dense planting including tree species (100 litre bag stock), which will, at maturity, exceed the height of the building(s) on the site.</p> <p><b>AO66.3</b> Advertising signs are discreet in appearance with no large advertising signs, including tenancy signs, located on or near the Captain Cook Highway frontage, or within any landscaped setback area.</p> <p><b>AO66.4</b> Car parking areas, loading and other service areas are designed to be screened from the Captain Cook Highway and are located so as to not be visually prominent from the Captain Cook Highway.</p>	
<b>Additional requirements for Precinct 6 – Very Low Residential Density / Low Scale Recreation / Low Scale Educational / Low Scale Entertainment Uses precinct</b>		
<p><b>PO67</b> No additional lots are created within the precinct.</p>	<p><b>AO67</b> No acceptable outcomes are prescribed.</p>	<b>Not Applicable.</b>
<p><b>PO68</b> Reconfigured lots have a minimum lot size of 2 hectares, unless the lot reconfiguration transfers lots to the higher parts of the land, to avoid the need to fill existing lots to accommodate dwelling houses.</p>	<p><b>AO68</b> No acceptable outcomes are prescribed.</p>	<b>Not Applicable.</b>

Table 7.2.4.4.b – Inconsistent uses in sub-precinct 1b – Waterfront North sub-precinct

Inconsistent uses		
<ul style="list-style-type: none"> <li>• Agricultural supplies store</li> <li>• Air services</li> <li>• Animal husbandry</li> <li>• Animal keeping</li> <li>• Aquaculture</li> <li>• Brothel</li> <li>• Bulk landscape supplies</li> <li>• Car wash</li> <li>• Cemetery</li> <li>• Crematorium</li> <li>• Cropping</li> <li>• Detention facility</li> <li>• Dual occupancy</li> <li>• Dwelling house</li> </ul>	<ul style="list-style-type: none"> <li>• Extractive industry</li> <li>• Funeral parlour</li> <li>• High impact industry</li> <li>• Intensive animal industry</li> <li>• Intensive horticulture</li> <li>• Major electricity infrastructure</li> <li>• Major sport, recreation and entertainment facility</li> <li>• Medium impact industry</li> <li>• Motor sport facility,</li> <li>• Outstation</li> <li>• Permanent plantation</li> </ul>	<ul style="list-style-type: none"> <li>• Relocatable home park</li> <li>• Roadside stall</li> <li>• Rural industry</li> <li>• Rural workers accommodation</li> <li>• Service station</li> <li>• Showroom</li> <li>• Special industry</li> <li>• Tourist park</li> <li>• Transport depot</li> <li>• Veterinary services</li> <li>• Warehouse</li> <li>• Wholesale nursery</li> <li>• Winery</li> </ul>

Table 7.2.4.4.c – Inconsistent uses in sub-precinct 1c – Waterfront South sub-precinct

Inconsistent uses		
<ul style="list-style-type: none"> <li>• Adult store</li> <li>• Agricultural supplies store</li> <li>• Air services</li> <li>• Animal husbandry</li> <li>• Animal keeping</li> <li>• Brothel</li> <li>• Bulk landscape supplies</li> <li>• Car wash</li> <li>• Cemetery</li> <li>• Child care centre</li> <li>• Community care centre</li> <li>• Community residence</li> <li>• Community use</li> <li>• Crematorium</li> <li>• Cropping</li> <li>• Detention facility</li> <li>• Dual occupancy</li> <li>• Dwelling house</li> <li>• Dwelling unit</li> <li>• Extractive industry</li> <li>• Function facility</li> <li>• Funeral parlour</li> <li>• Garden centre</li> </ul>	<ul style="list-style-type: none"> <li>• Hardware and trade supplies</li> <li>• Health care services</li> <li>• Home based business</li> <li>• Hospital</li> <li>• Hotel</li> <li>• Indoor sport and recreation</li> <li>• Intensive animal industry</li> <li>• Intensive horticulture</li> <li>• Major electricity infrastructure</li> <li>• Major sport, recreation and entertainment facility</li> <li>• Market</li> <li>• Motor sport facility</li> <li>• Multiple dwelling</li> <li>• Nature-based tourism</li> <li>• Nightclub entertainment facility</li> <li>• Outdoor sales</li> <li>• Outdoor sport and recreation</li> <li>• Outstation</li> </ul>	<ul style="list-style-type: none"> <li>• Permanent plantation</li> <li>• Place of worship</li> <li>• Relocatable home park</li> <li>• Residential care facility</li> <li>• Resort complex</li> <li>• Retirement facility</li> <li>• Roadside stall</li> <li>• Rooming accommodation</li> <li>• Rural industry</li> <li>• Rural workers accommodation</li> <li>• Sales office</li> <li>• Shopping centre</li> <li>• Short-term accommodation</li> <li>• Showroom</li> <li>• Special industry</li> <li>• Theatre</li> <li>• Tourist attraction</li> <li>• Tourist park</li> <li>• Transport depot</li> <li>• Veterinary services</li> <li>• Warehouse</li> <li>• Wholesale nursery</li> <li>• Winery</li> </ul>

Note –

Table 7.2.4.4.b – Inconsistent uses in sub-precinct 1b – Waterfront North sub-precinct or

**Table 7.2.4.4.c – Inconsistent uses in sub-precinct 1c – Waterfront South sub-precinct** do not imply that all other uses not listed in the table are automatically consistent uses within the zone. Assessable development must still demonstrate consistency through the assessment process.



## 8.2.1 Acid sulfate soils overlay code

### 8.2.1.1 Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Acid sulfate soils overlay, if:
  - (a) self-assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
  - (b) impact assessable development.
- (2) Land in the Acid sulphate soils overlay is identified on the Acid sulfate soils overlay map in Schedule 2 and includes the following sub-categories:
  - (a) Land at or below the 5m AHD sub-category;
  - (b) Land above the 5m AHD and below the 20m AHD sub-category.
- (3) When using this code, reference should be made to Part 5.

### 8.2.1.2 Purpose

- (1) The purpose of the acid sulfate soils overlay code is to:
  - (a) implement the policy direction in the Strategic Framework, in particular:
    - (i) Theme 2: Environment and landscape values, Element 3.5.4 Coastal zones.
    - (ii) Theme 3: Natural resource management, Element 3.6.2 land and catchment management, Element 3.6.3 Primary production, forestry and fisheries.
- (2) enable an assessment of whether development is suitable on land within the Acid sulfate soils overlay sub-categories.
- (3) The purpose of the code will be achieved through the following overall outcomes:
  - (a) Development ensures that the release of any acid and associated metal contaminant is avoided by not disturbing acid sulfate soils when excavating, removing soil or extracting ground water or filling land;
  - (b) Development ensures that disturbed acid sulfate soils, or drainage waters, are treated and, if required, on-going management practices are adopted that minimise the potential for environmental harm from acid sulfate soil and protect corrodible assets from acid sulfate soil.

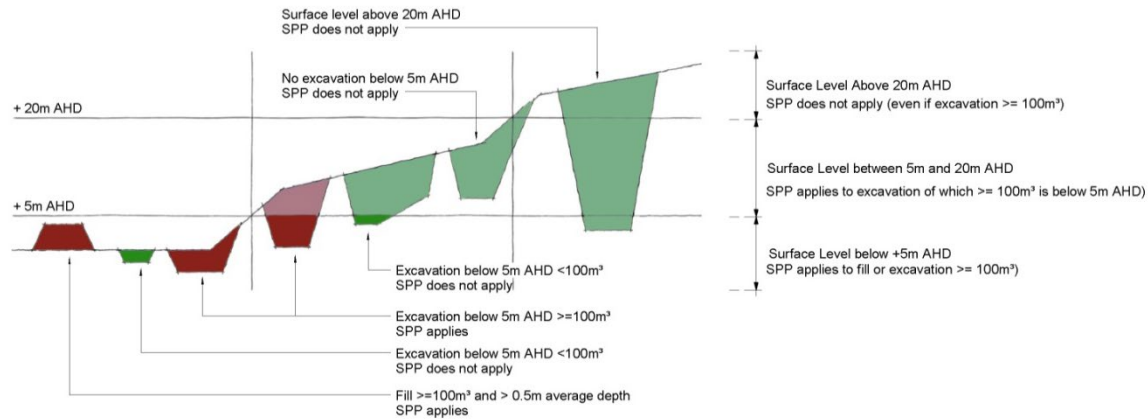
**Criteria for assessment****Table 8.2.1.3.a – Acid sulfate soils overlay code – assessable development**

Performance outcomes	Acceptable outcomes	Applicant response
<b>For assessable development</b>		
<b>PO1</b> The extent and location of potential or actual acid sulfate soils is accurately identified.	<b>AO1.1</b> No excavation or filling occurs on the site.  or  <b>AO1.2</b> An acid sulfate soils investigation is undertaken.  Note - Planning scheme policy SC 6.12– Potential and actual acid sulfate soils provides guidance on preparing an acid sulfate soils investigation.	<b>Complies with PO1.</b> Only minimal excavation and filling is proposed to construct the building pads. It is proposed to manage this issue during the construction stage.
<b>PO2</b> Development avoids disturbing potential acid sulfate soils or actual acid sulfate soils, or is managed to avoid or minimise the release of acid and metal contaminants.	<b>AO2.1</b> The disturbance of potential acid sulfate soils or actual acid sulfate soils is avoided by: (a) not excavating, or otherwise removing, soil or sediment identified as containing potential or actual acid sulfate soils; (b) not permanently or temporarily extracting groundwater that results in the aeration of previously saturated acid sulfate soils; (c) not undertaking filling that results in: (i) actual acid sulfate soils being moved below the water table; (ii) previously saturated acid sulfate soils being aerated.  or	<b>Complies with PO2.</b> Refer to comment above.



Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>AO2.2</b></p> <p>The disturbance of potential acid sulfate soils or actual acid sulfate soils is undertaken in accordance with an acid sulfate soils management plan and avoids the release of metal contaminants by:</p> <ul style="list-style-type: none"> <li>(a) neutralising existing acidity and preventing the generation of acid and metal contaminants;</li> <li>(b) preventing the release of surface or groundwater flows containing acid and metal contaminants into the environment;</li> <li>(c) preventing the in situ oxidisation of potential acid sulfate soils and actual acid sulfate soils through ground water level management;</li> <li>(d) appropriately treating acid sulfate soils before disposal occurs on or off site;</li> <li>(e) documenting strategies and reporting requirements in an acid sulfate soils environmental management plan.</li> </ul> <p>Note - Planning scheme policy SC 6.12 – Acid sulfate soils provides guidance on preparing an acid sulfate soils management plan.</p>	
<p><b>PO3</b></p> <p>No environmental harm is caused as a result of exposure to potential acid sulfate soils or actual acid sulfate soils.</p>	<p><b>AO3</b></p> <p>No acceptable outcomes are prescribed.</p>	<p><b>May be Conditioned to Comply.</b></p>

Figure 8.2.1.3.a – Acid sulfate soils (SPP triggers)



## 8.2.10 Transport network overlay code

### 8.2.10.1 Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Transport network overlay; if:
  - (a) self-assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
  - (b) impact assessable development.
- (2) Land within the Transport network overlay is identified on the Transport network (Road Hierarchy) overlay map and the Transport network (Pedestrian and Cycle) overlay map in Schedule 2 and includes the following sub-categories:
  - (a) Transport network (Road Hierarchy) overlay sub-categories:
    - (i) State controlled road sub-category;
    - (ii) Sub-arterial road sub-category;
    - (iii) Collector road sub-category;
    - (iv) Access road sub-category;
    - (v) Industrial road sub-category;
    - (vi) Major rural road sub-category;
    - (vii) Minor rural road sub-category;
    - (viii) Unformed road sub-category;
    - (ix) Major transport corridor buffer area sub-category.
  - (b) Transport network (Pedestrian and Cycle) overlay sub-categories:
    - (i) Principal route;
    - (ii) Future principal route;
    - (iii) District route;
    - (iv) Neighbourhood route;
    - (v) Strategic investigation route.

**8.2.10.2 Purpose**

- (1) The purpose of the Transport network overlay code is to:
  - (a) implement the policy direction of the Strategic Framework, in particular:
    - (i) Theme 1: Settlement pattern Element 3.4.2 Urban settlement, Element 3.4.3 Activity centres;
    - (ii) Theme 6: Infrastructure and transport Element 3.9.4 Transport;
  - (b) enable an assessment of whether development is suitable on land within the Transport network overlay.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) development provides for transport infrastructure (including active transport infrastructure);
  - (b) development contributes to a safe and efficient transport network;
  - (c) development supports the existing and future role and function of the transport network;
  - (d) development does not compromise the safety and efficiency of major transport infrastructure and facilities.

**Criteria for assessment****Table 8.2.10.3 a – Transport network overlay code – assessable development**

Performance outcomes	Acceptable outcomes	Applicant response
<b>For assessable development</b>		
<b>PO1</b> Development supports the road hierarchy for the region.  Note -A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.	<b>AO1.1</b>  Development is compatible with the intended role and function of the transport network as identified on the Transport network overlay maps contained in Schedule 2.  <b>AO1.2</b>  Development does not compromise the safety and efficiency of the transport network.	<b>Complies with AO1.1.</b> The proposed development low scale and will not compromise the role and function of the transport network.  <b>Complies with AO1.2.</b> The proposed development has been designed to provide access of an existing sealed road. The proposed development is





Performance outcomes	Acceptable outcomes	Applicant response
		not of a scale which would impact safety and efficiency.
	<b>AO1.3</b> Development is designed to provide access via the lowest order road, where legal and practicable access can be provided to that road.	<b>Complies with AO1.3.</b> The site has single road frontage to Davidson Street.
<b>P02</b> Transport infrastructure is provided in an integrated and timely manner.  Note - A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.	<b>AO2</b> Development provides infrastructure (including improvements to existing infrastructure) in accordance with: (a) the Transport network overlay maps contained in Schedule 2; (b) any relevant Local Plan.  Note – The Translink Public Transport Infrastructure Manual provides guidance on the design of public transport facilities.	<b>Not Applicable.</b> No new infrastructure upgrades are proposed.
<b>P03</b> Development involving sensitive land uses within a major transport corridor buffer area is located, designed and maintained to avoid or mitigate adverse impacts on amenity for the sensitive land use.	<b>AO3</b> No acceptable outcomes are prescribed.  Note – Part 4.4 of the Queensland Development Code provides requirements for residential building design in a designated transport noise corridor.	<b>Not Applicable.</b>
<b>P04</b> Development does not compromise the intended role and function or safety and efficiency of major transport corridors.  Note - A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.	<b>AO4.1</b> Development is compatible with the role and function (including the future role and function) of major transport corridors.  <b>AO4.2</b> Direct access is not provided to a major transport corridor where legal and practical access from another road is available.	<b>Complies with P04.</b>



Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>AO4.3</b> Intersection and access points associated with major transport corridors are located in accordance with: (a) the Transport network overlay maps contained in Schedule 2; and (b) any relevant Local Plan.</p> <p><b>AO4.4</b> The layout of development and the design of the associated access is compatible with existing and future boundaries of the major transport corridor or major transport facility.</p>	
<p><b>PO5</b> Development retains and enhances existing vegetation between a development and a major transport corridor, so as to provide screening to potential noise, dust, odour and visual impacts emanating from the corridor.</p>	<p><b>AO5</b> No acceptable outcomes are prescribed.</p>	<p><b>Complies with PO5.</b> The development incorporates landscaping within the front setback areas. Additional landscaping may be provided to Davidson Street road verge consistent with other existing adjoining and surrounding developments.</p>
<b>Pedestrian and cycle network</b>		
<p><b>PO6</b> Lot reconfiguration assists in the implementation of the pedestrian and cycle movement network to achieve safe, attractive and efficient pedestrian and cycle networks</p>	<p><b>AO6.1</b> Where a lot is subject to, or adjacent to an element of the pedestrian and cycle Movement network (identified on the Transport network overlay maps contained in Schedule 2) the specific location of this element of the pedestrian and cycle network is incorporated in the design of the lot layout.</p> <p><b>AO6.2</b> The element of the pedestrian and cycle network is constructed in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme</p>	<p><b>Not Applicable.</b> A footpath currently exists adjacent the site.</p> <p><b>Not Applicable.</b></p>



Performance outcomes	Acceptable outcomes	Applicant response
	policy SC6.5 – FNQROC Regional Development Manual.	

## 9.4 Other development codes

### 9.4.1 Access, parking and servicing code

#### 9.4.1.1 Application

- (1) This code applies to assessing:
  - (a) operational work which requires a compliance assessment as a condition of a development permit; or
  - (b) a material change of use or reconfiguring a lot if:
    - (i) self-assessable or assessable development where this code is identified in the assessment criteria column of the table of assessment;
    - (ii) impact assessable development, to the extent relevant.
- (2) When using this code, reference should be made to Part 5.

#### 9.4.1.2 Purpose

- (1) The purpose of the Access, parking and servicing code is to assess the suitability of access, parking and associated servicing aspects of a development.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) sufficient vehicle parking is provided on-site to cater for all types of vehicular traffic accessing and parking on-site, including staff, guests, patrons, residents and short term delivery vehicles;
  - (b) sufficient bicycle parking and end of trip facilities are provided on-site to cater for customer and service staff;
  - (c) on-site parking is provided so as to be accessible and convenient, particularly for any short term uses;
  - (d) development provides walking and cycle routes through the site which link the development to the external walking and cycling network;
  - (e) the provision of on-site parking, loading / unloading facilities and the provision of access to the site do not impact on the efficient function of street network or on the area in which the development is located;
  - (f) new vehicular access points are safely located and are not in conflict with the preferred ultimate streetscape character and local character and do not unduly disrupt any current or future on-street parking arrangements.

### 9.4.1.3 Criteria for assessment

Table 9.4.1.3.a – Access, parking and servicing code – assessable development

Performance outcomes	Acceptable outcomes	Applicant Response
<b>For self-assessable and assessable development</b>		
<p><b>PO1</b> Sufficient on-site car parking is provided to cater for the amount and type of vehicle traffic expected to be generated by the use or uses of the site, having particular regard to:</p> <ul style="list-style-type: none"> <li>(a) the desired character of the area;</li> <li>(b) the nature of the particular use and its specific characteristics and scale;</li> <li>(c) the number of employees and the likely number of visitors to the site;</li> <li>(d) the level of local accessibility;</li> <li>(e) the nature and frequency of any public transport serving the area;</li> <li>(f) whether or not the use involves the retention of an existing building and the previous requirements for car parking for the building</li> <li>(g) whether or not the use involves a heritage building or place of local significance;</li> <li>(h) whether or not the proposed use involves the retention of significant vegetation.</li> </ul>	<p><b>AO1.1</b> The minimum number of on-site vehicle parking spaces is not less than the number prescribed in Table 9.4.1.3.b for that particular use or uses.</p> <p>Note - Where the number of spaces calculated from the table is not a whole number, the number of spaces provided is the next highest whole number.</p> <p><b>AO1.2</b> Car parking spaces are freely available for the parking of vehicles at all times and are not used for external storage purposes, the display of products or rented/sub-leased.</p> <p><b>AO1.3</b> Parking for motorcycles is substituted for ordinary vehicle parking to a maximum level of 2% of total ordinary vehicle parking.</p> <p><b>AO1.4</b> For parking areas exceeding 50 spaces parking, is provided for recreational vehicles as a substitute for ordinary vehicle parking to a maximum of 5% of total ordinary vehicle parking rate.</p>	<p><b>Complies with AO1.1.</b> Two enclosed onsite parking spaces are provided for each Dwelling House.</p> <p><b>Complies with AO1.2.</b> Parking spaces are dedicated to each Dwelling House and freely available to residents.</p> <p><b>Alternative Solution.</b> No formalised motorcycle parking is proposed.</p> <p><b>Not Applicable.</b></p>
<p><b>PO2</b> Vehicle parking areas are designed and constructed in accordance with relevant standards.</p>	<p><b>AO2</b> Vehicle parking areas are designed and constructed in accordance with Australian Standard:</p> <ul style="list-style-type: none"> <li>(a) AS2890.1;</li> <li>(b) AS2890.3;</li> <li>(c) AS2890.6.</li> </ul>	<p><b>Complies with AO2.</b> Appropriate conditions may be imposed to ensure compliance is achieved.</p>



<p><b>PO3</b> Access points are designed and constructed:</p> <ul style="list-style-type: none"> <li>(a) to operate safely and efficiently;</li> <li>(b) to accommodate the anticipated type and volume of vehicles</li> <li>(c) to provide for shared vehicle (including cyclists) and pedestrian use, where appropriate;</li> <li>(d) so that they do not impede traffic or pedestrian movement on the adjacent road area;</li> <li>(e) so that they do not adversely impact upon existing intersections or future road or intersection improvements;</li> <li>(f) so that they do not adversely impact current and future on-street parking arrangements;</li> <li>(g) so that they do not adversely impact on existing services within the road reserve adjacent to the site;</li> <li>(h) so that they do not involve ramping, cutting of the adjoining road reserve or any built structures (other than what may be necessary to cross over a stormwater channel).</li> </ul>	<p><b>AO3.1</b> Access is limited to one access cross over per site and is an access point located, designed and constructed in accordance with:</p> <ul style="list-style-type: none"> <li>(a) Australian Standard AS2890.1;</li> <li>(b) Planning scheme policy SC6.5 – FNQROC Regional Development Manual - access crossovers.</li> </ul> <p><b>AO3.2</b> Access, including driveways or access crossovers:</p> <ul style="list-style-type: none"> <li>(a) are not placed over an existing: <ul style="list-style-type: none"> <li>(i) telecommunications pit;</li> <li>(ii) stormwater kerb inlet;</li> <li>(iii) sewer utility hole;</li> <li>(iv) water valve or hydrant.</li> </ul> </li> <li>(b) are designed to accommodate any adjacent footpath;</li> <li>(c) adhere to minimum sight distance requirements in accordance with AS2980.1.</li> </ul> <p><b>AO3.3</b> Driveways are:</p> <ul style="list-style-type: none"> <li>(a) designed to follow as closely as possible to the existing contours, but are no steeper than the gradients outlined in Planning scheme policy SC6.5 – FNQROC Regional Development Manual;</li> <li>(b) constructed such that where there is a grade shift to 1 in 4 (25%), there is an area with a grade of no more than 1 in 6 (16.6%) prior to this area, for a distance of at least 5 metres;</li> <li>(c) on gradients greater than 1 in 6 (16.6%) driveways are constructed to ensure the cross-fall of the driveway is one way and directed into the hill, for vehicle safety and drainage purposes;</li> </ul>	<p><b>Complies with PO3.</b> Individual crossovers are proposed to each Dwelling House. The outcomes is appropriate given the scale and nature of the proposed development.</p> <p>There are no obstructions to the proposed crossover and driveway location.</p> <p>The driveway and parking will be imperviously sealed, likely being concrete construction.</p>
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	<p>(d) constructed such that the transitional change in grade from the road to the lot is fully contained within the lot and not within the road reserve;</p> <p>(e) designed to include all necessary associated drainage that intercepts and directs storm water runoff to the storm water drainage system.</p> <p><b>AO3.4</b> Surface construction materials are consistent with the current or intended future streetscape or character of the area and contrast with the surface construction materials of any adjacent footpath.</p>	
<p><b>P04</b> Sufficient on-site wheel chair accessible car parking spaces are provided and are identified and reserved for such purposes.</p>	<p><b>AO4</b> The number of on-site wheel chair accessible car parking spaces complies with the rates specified in AS2890 Parking Facilities.</p>	<b>Not Applicable.</b>
<p><b>P05</b> Access for people with disabilities is provided to the building from the parking area and from the street.</p>	<p><b>AO5</b> Access for people with disabilities is provided in accordance with the relevant Australian Standard.</p>	<b>Not Applicable.</b>
<p><b>P06</b> Sufficient on-site bicycle parking is provided to cater for the anticipated demand generated by the development.</p>	<p><b>AO6</b> The number of on-site bicycle parking spaces complies with the rates specified in Table 9.4.1.3.b.</p>	<b>Not Applicable.</b>



<p><b>P07</b> Development provides secure and convenient bicycle parking which:</p> <ul style="list-style-type: none"> <li>(a) for visitors is obvious and located close to the building's main entrance;</li> <li>(b) for employees is conveniently located to provide secure and convenient access between the bicycle storage area, end-of-trip facilities and the main area of the building;</li> <li>(c) is easily and safely accessible from outside the site.</li> </ul>	<p><b>AO7.1</b> Development provides bicycle parking spaces for employees which are co-located with end-of-trip facilities (shower cubicles and lockers);</p> <p><b>AO7.2</b> Development ensures that the location of visitor bicycle parking is discernible either by direct view or using signs from the street.</p> <p><b>AO7.3</b> Development provides visitor bicycle parking which does not impede pedestrian movement.</p>	<p><b>Not Applicable.</b></p>
<p><b>P08</b> Development provides walking and cycle routes through the site which:</p> <ul style="list-style-type: none"> <li>(a) link to the external network and pedestrian and cyclist destinations such as schools, shopping centres, open space, public transport stations, shops and local activity centres along the safest, most direct and convenient routes;</li> <li>(b) encourage walking and cycling;</li> <li>(c) ensure pedestrian and cyclist safety.</li> </ul>	<p><b>AO8</b> Development provides walking and cycle routes which are constructed on the carriageway or through the site to:</p> <ul style="list-style-type: none"> <li>(a) create a walking or cycle route along the full frontage of the site;</li> <li>(b) connect to public transport and existing cycle and walking routes at the frontage or boundary of the site.</li> </ul>	<p><b>Not Applicable.</b></p>
<p><b>P09</b> Access, internal circulation and on-site parking for service vehicles are designed and constructed:</p> <ul style="list-style-type: none"> <li>(a) in accordance with relevant standards;</li> </ul>	<p><b>AO9.1</b> Access driveways, vehicle manoeuvring and on-site parking for service vehicles are designed and constructed in accordance with AS2890.1 and</p>	<p><b>Not Applicable.</b> The development does not require service and loading areas.</p>

<p>(b) so that they do not interfere with the amenity of the surrounding area;</p> <p>(c) so that they allow for the safe and convenient movement of pedestrians, cyclists and other vehicles.</p>	<p>AS2890.2.</p> <p><b>AO9.2</b> Service and loading areas are contained fully within the site.</p> <p><b>AO9.3</b> The movement of service vehicles and service operations are designed so they:</p> <ul style="list-style-type: none"> <li>(a) do not impede access to parking spaces;</li> <li>(b) do not impede vehicle or pedestrian traffic movement.</li> </ul>	
<p><b>PO10</b> Sufficient queuing and set down areas are provided to accommodate the demand generated by the development.</p>	<p><b>AO10.1</b> Development provides adequate area on-site for vehicle queuing to accommodate the demand generated by the development where drive through facilities or drop-off/pick-up services are proposed as part of the use, including, but not limited to, the following land uses:</p> <ul style="list-style-type: none"> <li>(a) car wash;</li> <li>(b) child care centre;</li> <li>(c) educational establishment where for a school;</li> <li>(d) food and drink outlet, where including a drive-through facility;</li> <li>(e) hardware and trade supplies, where including a drive-through facility;</li> <li>(f) hotel, where including a drive-through facility;</li> <li>(g) service station.</li> </ul> <p><b>AO10.2</b> Queuing and set-down areas are designed and constructed in accordance with AS2890.1.</p>	<p><b>Not Applicable.</b></p>

**Table 9.4.1.3.b – Access, parking and servicing requirements**

Note – Where the number of spaces is not a whole number, the number of spaces to be provided is the next highest whole number.

Note – Where the proposed development involves one or more land use, the minimum number of spaces for the proposed development will be calculated using the minimum number of spaces specified for each land use component.

## **9.4.3 Environmental performance code**

### **9.4.3.1 Application**

- (1) This code applies to assessing:
  - (a) building work for outdoor lighting;
  - (b) a material change of use or reconfiguring a lot if:
    - (i) assessable development where the code is identified in the assessment criteria column of a table of assessment; or
    - (ii) impact assessable development, to the extent relevant.

Note – Where for the purpose of lighting a tennis court in a Residential zone, a compliance statement prepared by a suitably qualified person must be submitted to Council with the development application for building work.

- (2) When using this code, reference should be made to Part 5.

### **9.4.3.2 Purpose**

- (1) The purpose of the Environmental performance code is to ensure development is designed and operated to avoid or mitigate impacts on sensitive receiving environments.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) activities that have potential to cause an adverse impact on amenity of adjacent and surrounding land, or environmental harm is avoided through location, design and operation of the development;
  - (b) sensitive land uses are protected from amenity related impacts of lighting, odour, airborne particles and noise, through design and operation of the development;
  - (c) stormwater flowing over, captured or discharged from development sites is of a quality adequate to enter receiving waters and downstream environments;
  - (d) development contributes to the removal and ongoing management of weed species.



### 9.4.3.3 Criteria for assessment

Table 9.4.3.3.a – Environmental performance code – assessable development

Performance outcomes	Acceptable outcomes	Applicant Response
<b>Lighting</b>		
<b>PO1</b> Lighting incorporated within development does not cause an adverse impact on the amenity of adjacent uses and nearby sensitive land uses.	<b>AO1.1</b> Technical parameters, design, installation, operation and maintenance of outdoor lighting comply with the requirements of Australian standard AS4282-1997 Control of the obtrusive effects of outdoor lighting.  <b>AO1.2</b> Development that involves flood lighting is restricted to a type that gives no upward component of light where mounted horizontally.  <b>AO1.3</b> Access, car parking and manoeuvring areas are designed to shield nearby residential premises from impacts of vehicle headlights.	<b>Not Applicable.</b> Only domestic external lighting is proposed.
<b>Noise</b>		
<b>PO2</b> Potential noise generated from the development is avoided through design, location and operation of the activity. Note – Planning Scheme Policy SC6.4 – Environmental	<b>AO2.1</b> Development does not involve activities that would cause noise related environmental harm or nuisance; or	<b>Complies with AO2.1.</b> The proposed development is for residential accommodation. The proposed development is not of a nature that will cause noise related environmental harm.



<p>management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p><b>AO2.2</b> Development ensures noise does not emanate from the site through the use of materials, structures and architectural features to not cause an adverse noise impact on adjacent uses.</p> <p><b>AO2.3</b> The design and layout of development ensures car parking areas avoid noise impacting directly on adjacent sensitive land uses through one or more of the following:</p> <ul style="list-style-type: none"> <li>(a) car parking is located away from adjacent sensitive land uses;</li> <li>(b) car parking is enclosed within a building;</li> <li>(c) a noise ameliorating fence or structure is established adjacent to car parking areas where the fence or structure will not have a visual amenity impact on the adjoining premises;</li> <li>(d) buffered with dense landscaping.</li> </ul> <p>Editor's note - The <i>Environmental Protection (Noise) Policy 2008</i>, Schedule 1 provides guidance on acoustic quality objectives to ensure environmental harm (including nuisance) is avoided.</p>	<p><b>Will Comply with AO2.2.</b> Appropriate materials will be used in the construction of the development.</p> <p><b>Complies with AO2.3.</b> Noise from vehicle movements will be minimised through the design of parking spaces at the front of the property. Furthermore, the scale of development and traffic generation is consistent with other existing development in the locality.</p>
<b>Airborne particles and other emissions</b>		
<p><b>PO3</b> Potential airborne particles and emissions generated from the development are avoided through design, location and operation of the activity.</p> <p>Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p><b>AO3.1</b> Development does not involve activities that will result in airborne particles or emissions being generated;</p> <p>or</p> <p><b>AO3.2</b> The design, layout and operation of the development activity ensures that no airborne particles or emissions cause environmental harm or nuisance.</p>	<p><b>Not applicable</b> The proposed use does not involve activities that result in airborne emissions.</p>





	<p>Note - examples of activities which generally cause airborne particles include spray painting, abrasive blasting, manufacturing activities and car wash facilities.</p> <p>Examples of emissions include exhaust ventilation from basement or enclosed parking structures, air conditioning/refrigeration ventilation and exhaustion.</p> <p>The <i>Environmental Protection (Air) Policy 2008</i>, Schedule 1 provides guidance on air quality objectives to ensure environmental harm (including nuisance) is avoided.</p>	
<b>Odours</b>		
<p><b>PO4</b> Potential odour causing activities associated with the development are avoided through design, location and operation of the activity.</p> <p>Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p><b>A04.1</b> The development does not involve activities that create odorous emissions;</p> <p>or</p> <p><b>A04.2</b> The use does not result in odour that causes environmental harm or nuisance with respect to surrounding land uses.</p>	<p><b>Not applicable</b> The proposed use does not involve activities that result in odour emissions.</p>
<b>Waste and recyclable material storage</b>		

**PO5**

Waste and recyclable material storage facilities are located and maintained to not cause adverse impacts on adjacent uses.

Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.

**AO5.1**

The use ensures that all putrescent waste is stored in a manner that prevents odour nuisance and is disposed of at regular intervals.

**AO5.2**

Waste and recyclable material storage facilities are located, designed and maintained to not cause an adverse impact on users of the premises and adjacent uses through consideration of:

- (a) the location of the waste and recyclable material storage areas in relation to the noise and odour generated;
- (b) the number of receptacles provided in relation to the collection, maintenance and use of the receptacles;
- (c) the durability of the receptacles, sheltering and potential impacts of local climatic conditions;
- (d) the ability to mitigate spillage, seepage or leakage from receptacles into adjacent areas and sensitive receiving waters and environments.

Editor's note - the *Environmental Protection (Waste Management) Policy 2008* provides guidance on the design of waste containers (receptacles) to ensure environmental harm (including nuisance) is avoided.

**Complies with AO5.1-AO5.2.**

Waste will be stored appropriately onsite so not to impact adjoining land uses. Bin storage is illustrated on the proposal plans located adjacent the site frontage. This area will be screened. Appropriate development conditions may be imposed to ensure compliance.

**Sensitive land use activities**



<p><b>PO6</b> Sensitive land use activities are not established in areas which will receive potentially incompatible impacts on amenity from surrounding, existing development activities and land uses.</p>	<p><b>AO6.1</b> Sensitive land use activities are not established in areas that will be adversely impacted upon by existing land uses, activities and potential development possible in an area;</p> <p>or</p> <p><b>AO6.2</b> Sensitive land activities are located in areas where potential adverse amenity impacts mitigate all potential impacts through layout, design, operation and maintenance.</p>	<p><b>Not Applicable.</b></p>
<p><b>Stormwater quality</b></p>		
<p><b>PO7</b> The quality of stormwater flowing over, through or being discharged from development activities into watercourses and drainage lines is of adequate quality for downstream environments, with respect to:</p> <ul style="list-style-type: none"> <li>(a) the amount and type of pollutants borne from the activity;</li> <li>(c) maintaining natural stream flows;</li> <li>(d) the amount and type of site disturbance;</li> <li>(e) site management and control measures.</li> </ul>	<p><b>AO7.1</b> Development activities are designed to ensure stormwater over roofed and hard stand areas is directed to a lawful point of discharge.</p> <p><b>AO7.2</b> Development ensures movement of stormwater over the site is not impeded or directed through potentially polluting activities.</p> <p><b>AO7.3</b> Soil and water control measures are incorporated into the activity's design and operation to control sediment and erosion potentially entering watercourses, drainage lines and downstream receiving waters.</p> <p>Note - Planning scheme policy - FNQROC Regional Development Manual provides guidance on soil and water control measures to meet the requirements of the <i>Environmental Protection Act 1994</i>.</p> <p>During construction phases of development, contractors and builders are to have consideration in their work methods and site preparation for their environmental duty to protect stormwater quality.</p>	<p><b>Complies with AO7.1-AO7.3.</b> Stormwater flows overland onto the site from Davidson Street. The proposed stormwater strategy seeks to capture stormwater via a shallow swale in the road verge. This will be directed either side of the Dwelling Houses to the rear of the site. This will be primarily directed by pits and pipes, however there will be provision for secondary overland flows. At the rear stormwater will discharge via Council's existing drainage network.</p>



Pest plants (for material change of use on vacant land over 1,000m <sup>2</sup> )		
<p><b>PO8</b> Development activities and sites provide for the removal of all pest plants and implement ongoing measures to ensure that pest plants do not reinfest the site or nearby sites.</p> <p>Editor's note - This does not remove or replace all land owner's obligations or responsibilities under the <i>Land Protection (Pest and Stock Route Management) Act 2002</i>.</p>	<p><b>AO8.1</b> The land is free of declared pest plants before development establishes new buildings, structures and practices;</p> <p>or</p> <p><b>AO8.2</b> Pest plants detected on a development site are removed in accordance with a management plan prepared by an appropriately qualified person prior to construction of buildings and structures or earthworks.</p> <p>Note - A declaration from an appropriately qualified person validates the land being free from pest plants.</p> <p>Declared pest plants include locally declared and State declared pest plants.</p>	<p><b>Not Applicable.</b></p>

## **9.4.4 Filling and excavation code**

### **9.4.4.1 Application**

- (1) This code applies to assessing:
  - (a) operational work for filling or excavation which is self-assessable or code assessable development if this code is an applicable code identified in the assessment criteria column of a table of assessment; or
  - (b) a material change of use or reconfiguring a lot if:
    - (i) assessable development where this code is identified as a prescribed secondary code in the assessment criteria column of a table of assessment; or
    - (ii) impact assessable development, to the extent relevant.

Note—This code does not apply to building work that is regulated under the Building Code of Australia.

- (2) When using this code, reference should be made to Part 5.

### **9.4.4.2 Purpose**

- (1) The purpose of the Filling and excavation code is to assess the suitability of development for filling or excavation.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) filling or excavation does not impact on the character or amenity of the site and surrounding areas;
  - (b) filling and excavation does not adversely impact on the environment;
  - (c) filling and excavation does not impact on water quality or drainage of upstream, downstream or adjoining properties;
  - (d) filling and excavation is designed to be fit for purpose and does not create land stability issues;
  - (e) filling and excavation works do not involve complex engineering solutions.

#### 9.4.4.3 Criteria for assessment

Table 9.4.4.3.a – Filling and excavation code – for self-assessable and assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<b>Filling and excavation - General</b>		
<p><b>PO1</b> All filling and excavation work does not create a detrimental impact on the slope stability, erosion potential or visual amenity of the site or the surrounding area.</p>	<p><b>AO1.1</b> The height of cut and/or fill, whether retained or not, does not exceed 2 metres in height.</p> <p>and</p> <p>Cuts in excess of those stated in A1.1 above are separated by benches/ terraces with a minimum width of 1.2 metres that incorporate drainage provisions and screen planting.</p> <p><b>AO1.2</b> Cuts are supported by batters, retaining or rock walls and associated benches/terraces are capable of supporting mature vegetation.</p> <p><b>AO1.3</b> Cuts are screened from view by the siting of the building/structure, wherever possible.</p>	<p><b>Complies with AO1.1.</b> Only minor excavation and filling is required to construct the building pads and pools. No cut and batters created.</p> <p><b>Not Applicable.</b></p> <p><b>Not Applicable.</b></p>



	<p><b>AO1.4</b> Topsoil from the site is retained from cuttings and reused on benches/terraces.</p> <p><b>AO1.5</b> No crest of any cut or toe of any fill, or any part of any retaining wall or structure is closer than 600mm to any boundary of the property, unless the prior written approval of the adjoining landowner has been obtained.</p> <p><b>AO1.6</b> Non-retained cut and/or fill on slopes are stabilised and protected against scour and erosion by suitable measures, such as grassing, landscaping or other protective/aesthetic measures.</p>	<p><b>Will Comply.</b></p> <p><b>Not Applicable.</b></p> <p><b>Not Applicable.</b></p>
<b>Visual Impact and Site Stability</b>		
<p><b>PO2</b> Filling and excavation are carried out in such a manner that the visual/scenic amenity of the area and the privacy and stability of adjoining properties is not compromised.</p>	<p><b>AO2.1</b> The extent of filling and excavation does not exceed 40% of the site area, or 500m<sup>2</sup> whichever is the lesser,  except that AO2.1 does not apply to reconfiguration of 5 lots or more.</p> <p><b>AO2.2</b> Filling and excavation does not occur within 2 metres of the site boundary.</p>	<p><b>Complies with AO2.1-AO2.2.</b> The site is generally level. Minor works only are required to prepare the site for Building Works.</p>
<b>Flooding and drainage</b>		





<p><b>PO3</b> Filling and excavation does not result in a change to the run off characteristics of a site which then have a detrimental impact on the site or nearby land or adjacent road reserves.</p>	<p><b>AO3.1</b> Filling and excavation does not result in the ponding of water on a site or adjacent land or road reserves.</p> <p><b>AO3.2</b> Filling and excavation does not result in an increase in the flow of water across a site or any other land or road reserves.</p> <p><b>AO3.3</b> Filling and excavation does not result in an increase in the volume of water or concentration of water in a watercourse and overland flow paths.</p> <p><b>AO3.4</b> Filling and excavation complies with the specifications set out in Planning Scheme Policy No SC5 – FNQROC Development Manual.</p>	<p><b>AO3.1-AO3.4.</b> The proposed stormwater strategy includes capture of overland flow from Davidson Street road reserve via a shallow swale drain in the road reserve. Stormwater will be primarily channeled via pits and pipes down the side of the Dwelling Houses and discharge to the existing Council stormwater drainage network. The design includes secondary overland flow alongside the Dwelling Houses. It is expected that Council will impose conditions around stormwater drainage.</p>
<b>Water quality</b>		
<p><b>PO4</b> Filling and excavation does not result in a reduction of the water quality of receiving waters.</p>	<p><b>AO4</b> Water quality is maintained to comply with the specifications set out in Planning Scheme Policy No SC5 – FNQROC Development Manual.</p>	<p><b>Will Comply.</b> Any concerns in this regard may be conditioned under a Development Permit.</p>
<b>Infrastructure</b>		
<p><b>PO5</b> Excavation and filling does not impact on Public Utilities.</p>	<p><b>AO5</b> Excavation and filling is clear of the zone of influence of public utilities.</p>	<p><b>Not Applicable.</b></p>

## **9.4.5 Infrastructure works code**

### **9.4.5.1 Application**

- (1) This code applies to assessing:
  - (a) operational work which requires an assessment as a condition of a development permit or is assessable development if this code is identified in the assessment criteria column of a table of assessment;
  - (b) a material change of use or reconfiguring a lot if:
    - (i) assessable development where this code is identified in the assessment criteria column of the table of assessment;
    - (ii) impact assessable development, to the extent relevant.

Note – The Filling and excavation code applies to operational work for filling and excavation.

- (2) When using this code, reference should be made to Part 5.

### **9.4.5.2 Purpose**

- (1) The purpose of the Infrastructure works code is to ensure that development is safely and efficiently serviced by, and connected to, infrastructure.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) the standards of water supply, waste water treatment and disposal, stormwater drainage, local electricity supply, telecommunications, footpaths and road construction meet the needs of development and are safe and efficient;
  - (b) development maintains high environmental standards;
  - (c) development is located, designed, constructed and managed to avoid or minimise impacts arising from altered stormwater quality or flow, wastewater discharge, and the creation of non-tidal artificial waterways;
  - (d) the integrity of existing infrastructure is maintained;
  - (e) development does not detract from environmental values or the desired character and amenity of an area.

Performance outcomes		Acceptable outcomes	Applicant response
For self-assessable and assessable development			
Works on a local government road			
<b>PO1</b> Works on a local government road do not adversely impact on footpaths or existing infrastructure within the road verge and maintain the flow, safety and efficiency of pedestrians, cyclists and vehicles.	<b>AO1.1</b> Footpaths/pathways are located in the road verge and are provided for the hierarchy of the road and located and designed and constructed in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual.  <b>AO1.2</b> Kerb ramp crossovers are constructed in accordance with Planning scheme policy SC 5 – FNQROC Regional Development Manual.  <b>AO1.3</b> New pipes, cables, conduits or other similar infrastructure required to cross existing footpaths: (a) are installed via trenchless methods; or (b) where footpath infrastructure is removed to install infrastructure, the new section of footpath is installed to the standard detailed	<b>Not Applicable.</b>  <b>Will Comply with AO1.2.</b> It is anticipated that this matter will be appropriately conditioned.  <b>Not Applicable.</b>	



	<p>in the Planning scheme policy SC5 – FNQROC Regional Development Manual, and is not less than a 1.2 metre section.</p> <p><b>AO1.4</b> Where existing footpaths are damaged as a result of development, footpaths are reinstated ensuring:</p> <ul style="list-style-type: none"> <li>(a) similar surface finishes are used;</li> <li>(b) there is no change in level at joins of new and existing sections;</li> <li>(c) new sections are matched to existing in terms of dimension and reinforcement.</li> </ul> <p>Note – Figure 9.4.5.3.a provides guidance on meeting the outcomes.</p> <p><b>AO1.5</b> Decks, verandahs, stairs, posts and other structures located in the road reserve do not restrict or impede pedestrian movement on footpaths or change the level of the road verges.</p>	<p><b>Will Comply with AO1.4.</b> It is anticipated that this matter will be appropriately conditioned.</p> <p><b>Not Applicable.</b></p>
<b>Accessibility structures</b>		
<p><b>PO2</b> Development is designed to ensure it is accessible for people of all abilities and accessibility features do not impact on the efficient and safe use of footpaths.</p> <p>Note – Accessibility features are those features required to ensure access to premises is provided for people of all abilities and include ramps and lifts.</p>	<p><b>AO2.1</b> Accessibility structures are not located within the road reserve.</p> <p><b>AO2.2</b> Accessibility structures are designed in accordance with AS1428.3.</p> <p><b>AO2.3</b> When retrofitting accessibility features in existing buildings, all structures and changes in grade are contained within the boundaries of the lot and not within the road reserve.</p>	<p><b>Not Applicable.</b></p>
<b>Water supply</b>		

**PO3**

An adequate, safe and reliable supply of potable, fire fighting and general use water is provided.

**AO3.1**

The premises is connected to Council's reticulated water supply system in accordance with the Design Guidelines set out in Section D6 of the Planning scheme policy SC5 – FNQROC Regional Development Manual;

or

**AO3.2**

Where a reticulated water supply system is not available to the premises, on site water storage tank/s with a minimum capacity of 10,000 litres of stored water, with a minimum 7,500 litre tank, with the balance from other sources (e.g. accessible swimming pool, dam etc.) and access to the tank/s for fire trucks is provided for each new house or other development. Tank/s are to be fitted with a 50mm ball valve with a camlock fitting and installed and connected prior to occupation of the house and sited to be visually unobtrusive.

**Complies**

The subject site is connected to Council's reticulated water supply.



Treatment and disposal of effluent		
<p><b>PO4</b> Provision is made for the treatment and disposal of effluent to ensure that there are no adverse impacts on water quality and no adverse ecological impacts as a result of the system or as a result of increasing the cumulative effect of systems in the locality.</p>	<p><b>AO4.1</b> The site is connected to Council's sewerage system and the extension of or connection to the sewerage system is designed and constructed in accordance with the Design Guidelines set out in Section D7 of the Planning scheme policy SC5 – FNQROC Regional Development Manual;</p> <p>or</p> <p><b>AO4.2</b> Where not in a sewerage scheme area, the proposed disposal system meets the requirements of Section 33 of the <i>Environmental Protection Policy (Water) 1997</i> and the proposed on site effluent disposal system is designed in accordance with the <i>Plumbing and Drainage Act (2002)</i>.</p>	<p><b>Complies.</b> The sites will be connected to Council's sewerage system.</p>
Stormwater quality		
<p><b>PO5</b> Development is planned, designed, constructed and operated to avoid or minimise adverse impacts on stormwater quality in natural and developed catchments by:</p> <ul style="list-style-type: none"> <li>(a) achieving stormwater quality objectives;</li> <li>(b) protecting water environmental values;</li> <li>(c) maintaining waterway hydrology.</li> </ul>	<p><b>AO5.1</b> A connection is provided from the premises to Council's drainage system;</p> <p>or</p> <p><b>AO5.2</b> An underground drainage system is constructed to convey stormwater from the premises to Council's drainage system in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p>	<p><b>Complies.</b> Stormwater will be directed to the existing Council drainage infrastructure at the rear of the site.</p>

**AO5.3**

A stormwater quality management plan is prepared, and provides for achievable stormwater quality treatment measures meeting design objectives listed in Table 9.4.5.3.b and Table 9.4.5.3.c, reflecting land use constraints, such as:

- (a) erosive, dispersive and/or saline soil types;
- (b) landscape features (including landform);
- (c) acid sulfate soil and management of nutrients of concern;
- (d) rainfall erosivity.

**AO5.4**

Erosion and sediment control practices are designed, installed, constructed, monitored, maintained, and carried out in accordance with an erosion and sediment control plan.

**AO5.5**

Development incorporates stormwater flow control measures to achieve the design objectives set out in Table 9.4.5.3.b and Table 9.4.5.3.c, including management of frequent flows, peak flows, and construction phase hydrological impacts.

Note – Planning scheme policy SC5 – FNQROC Regional Development Manual provides guidance on soil and water control measures to meet the requirements of the *Environmental Protection Act 1994*.

Note – During construction phases of development, contractors and builders are to have





	consideration in their work methods and site preparation for their environmental duty to protect stormwater quality.	
<b>Non-tidal artificial waterways</b>		
<b>PO6</b> Development involving non-tidal artificial waterways is planned, designed, constructed and operated to: <ul style="list-style-type: none"> <li>(a) protect water environmental values;</li> <li>(b) be compatible with the land use constraints for the site for protecting water environmental values;</li> <li>(c) be compatible with existing tidal and non-tidal waterways;</li> <li>(d) perform a function in addition to stormwater management;</li> <li>(e) achieve water quality objectives.</li> </ul>	<b>AO6.1</b> Development involving non-tidal artificial waterways ensures: <ul style="list-style-type: none"> <li>(a) environmental values in downstream waterways are protected;</li> <li>(b) any ground water recharge areas are not affected;</li> <li>(c) the location of the waterway incorporates low lying areas of the catchment connected to an existing waterway;</li> <li>(d) existing areas of ponded water are included.</li> </ul> <b>AO6.2</b> Non-tidal artificial waterways are located: <ul style="list-style-type: none"> <li>(a) outside natural wetlands and any associated buffer areas;</li> <li>(b) to minimise disturbing soils or sediments;</li> <li>(c) to avoid altering the natural hydrologic regime in acid sulfate soil and nutrient hazardous areas.</li> </ul> <b>AO6.3</b> Non-tidal artificial waterways located adjacent to, or connected to a tidal waterway by means of a weir, lock, pumping system or similar ensures: <ul style="list-style-type: none"> <li>(a) there is sufficient flushing or a tidal range of &gt;0.3 m; or</li> <li>(b) any tidal flow alteration does not adversely impact on the tidal waterway; or</li> </ul>	<b>Not Applicable.</b>



	<p>(c) there is no introduction of salt water into freshwater environments.</p> <p><b>AO6.4</b> Non-tidal artificial waterways are designed and managed for any of the following end-use purposes:</p> <ul style="list-style-type: none"> <li>(a) amenity (including aesthetics), landscaping or recreation; or</li> <li>(b) flood management, in accordance with a drainage catchment management plan; or</li> <li>(c) stormwater harvesting plan as part of an integrated water cycle management plan; or aquatic habitat.</li> </ul> <p><b>AO6.5</b> The end-use purpose of the non-tidal artificial waterway is designed and operated in a way that protects water environmental values.</p> <p><b>AO6.6</b> Monitoring and maintenance programs adaptively manage water quality to achieve relevant water quality objectives downstream of the waterway.</p> <p><b>AO6.7</b> (d) Aquatic weeds are managed to achieve a low percentage of coverage of the water surface area, and pests and vectors are managed through design and maintenance.</p>	
<b>Wastewater discharge</b>		



<p><b>P07</b> Discharge of wastewater to waterways, or off site:</p> <ul style="list-style-type: none"> <li>(a) meets best practice environmental management;</li> <li>(b) is treated to: <ul style="list-style-type: none"> <li>(i) meet water quality objectives for its receiving waters;</li> <li>(ii) avoid adverse impact on ecosystem health or waterway health;</li> <li>(iii) maintain ecological processes, riparian vegetation and waterway integrity;</li> <li>(iv) offset impacts on high ecological value waters.</li> </ul> </li> </ul>	<p><b>AO7.1</b> A wastewater management plan is prepared and addresses:</p> <ul style="list-style-type: none"> <li>(a) wastewater type;</li> <li>(b) climatic conditions;</li> <li>(c) water quality objectives;</li> <li>(d) best practice environmental management.</li> </ul> <p><b>AO7.2</b> The waste water management plan is managed in accordance with a waste management hierarchy that:</p> <ul style="list-style-type: none"> <li>(a) avoids wastewater discharge to waterways; or</li> <li>(b) if wastewater discharge cannot practicably be avoided, minimises wastewater discharge to waterways by re-use, recycling, recovery and treatment for disposal to sewer, surface water and ground water.</li> </ul> <p><b>AO7.3</b> Wastewater discharge is managed to avoid or minimise the release of nutrients of concern so as to minimise the occurrence, frequency and intensity of algal blooms.</p> <p><b>AO7.4</b> Development in coastal catchments avoids or minimises and appropriately manages soil disturbance or altering natural hydrology and:</p> <ul style="list-style-type: none"> <li>(a) avoids lowering ground water levels where potential or actual acid sulfate soils are present;</li> <li>(b) manages wastewater so that: <ul style="list-style-type: none"> <li>(i) the pH of any wastewater</li> </ul> </li> </ul>	<p><b>Not Applicable.</b></p>
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	<p>discharges is maintained between 6.5 and 8.5 to avoid mobilisation of acid, iron, aluminium and other metals;</p> <p>(ii) holding times of neutralised wastewater ensures the flocculation and removal of any dissolved iron prior to release;</p> <p>visible iron floc is not present in any discharge;</p> <p>(iv) precipitated iron floc is contained and disposed of;</p> <p>(iii) wastewater and precipitates that cannot be contained and treated for discharge on site are removed and disposed of through trade waste or another lawful method.</p>	
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Electricity supply		
<b>PO8</b> Development is provided with a source of power that will meet its energy needs.	<b>AO8.1</b> A connection is provided from the premises to the electricity distribution network;  or  <b>AO8.2</b> The premises is connected to the electricity distribution network in accordance with the Design Guidelines set out in Section D8 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.  Note - Areas north of the Daintree River have a different standard.	<b>Will Comply.</b> The site is connected to electrical supply. Augmentation of this supply will be taken with Ergon.
<b>PO9</b> Development incorporating pad-mount electricity infrastructure does not cause an adverse impact on amenity.	<b>AO9.1</b> Pad-mount electricity infrastructure is: (a) not located in land for open space or sport and recreation purposes; (b) screened from view by landscaping or fencing; (c) accessible for maintenance.  <b>AO9.2</b> Pad-mount electricity infrastructure within a building, in a Town Centre is designed and located to enable an active street frontage.  Note – Pad-mounts in buildings in activity centres should not be located on the street frontage.	<b>Not Applicable.</b>
Telecommunications		
<b>PO10</b> Development is connected to a telecommunications service approved by the relevant telecommunication regulatory authority.	<b>AO10</b> The development is connected to telecommunications infrastructure in accordance with the standards of the relevant regulatory authority.	<b>Will Comply.</b> The site will be connected to telecommunication.



<b>PO11</b> Provision is made for future telecommunications services (e.g. fibre optic cable).	<b>AO11</b> Conduits are provided in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual.	
<b>Road construction</b>		
<b>PO12</b> The road to the frontage of the premises is constructed to provide for the safe and efficient movement of: <ul style="list-style-type: none"> <li>(a) pedestrians and cyclists to and from the site;</li> <li>(b) pedestrians and cyclists adjacent to the site;</li> <li>(c) vehicles on the road adjacent to the site;</li> <li>(d) vehicles to and from the site;</li> <li>(e) emergency vehicles.</li> </ul>	<b>AO12.1</b> The road to the frontage of the site is constructed in accordance with the Design Guidelines set out in Sections D1 and D3 of the Planning scheme policy SC5 – FNQROC Regional Development Manual, for the particular class of road, as identified in the road hierarchy.  <b>AO12.2</b> There is existing road, kerb and channel for the full road frontage of the site.  <b>AO12.3</b> Road access minimum clearances of 3.5 metres wide and 4.8 metres high are provided for the safe passage of emergency vehicles.	<b>Complies.</b> The road frontages are constructed. There is currently no kerb and channel to Davidson Street.
<b>Alterations and repairs to public utility services</b>		
<b>PO13</b> Infrastructure is integrated with, and efficiently extends, existing networks.	<b>AO13</b> Development is designed to allow for efficient connection to existing infrastructure networks.	<b>Not Applicable.</b>



<p><b>PO14</b> Development and works do not affect the efficient functioning of public utility mains, services or installations.</p>	<p><b>AO14.1</b> Public utility mains, services and installations are not required to be altered or repaired as a result of the development;</p> <p>or</p> <p><b>AO14.2</b> Public utility mains, services and installations are altered or repaired in association with the works so that they continue to function and satisfy the relevant Design Guidelines set out in Section D8 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p>	<p><b>Not Applicable.</b></p>
<p><b>Construction management</b></p>		
<p><b>PO15</b> Work is undertaken in a manner which minimises adverse impacts on vegetation that is to be retained.</p>	<p><b>AO15</b> Works include, at a minimum:</p> <ul style="list-style-type: none"> <li>(a) installation of protective fencing around retained vegetation during construction;</li> <li>(b) erection of advisory signage;</li> <li>(c) no disturbance, due to earthworks or storage of plant, materials and equipment, of ground level and soils below the canopy of any retained vegetation;</li> <li>(d) removal from the site of all declared noxious weeds.</li> </ul>	<p><b>Not Applicable.</b> The site does not comprise any vegetation.</p>
<p><b>PO16</b> Existing infrastructure is not damaged by construction activities.</p>	<p><b>AO16</b> Construction, alterations and any repairs to infrastructure is undertaken in accordance with the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p> <p>Note - Construction, alterations and any repairs to State-controlled roads and rail corridors are undertaken in accordance with the Transport Infrastructure Act 1994.</p>	<p><b>May be Conditioned to Comply.</b></p>





Performance outcomes	Acceptable outcomes	Applicant response
<b>For assessable development</b>		
<b>High speed telecommunication infrastructure</b>		
<b>PO17</b> Development provides infrastructure to facilitate the roll out of high speed telecommunications infrastructure.	<b>AO17</b> No acceptable outcomes are prescribed.	<b>May be Conditioned if Applicable.</b>
<b>Trade waste</b>		
<b>PO18</b> Where relevant, the development is capable of providing for the storage, collection treatment and disposal of trade waste such that: (a) off-site releases of contaminants do not occur; (b) the health and safety of people and the environment are protected; (c) the performance of the wastewater system is not put at risk.	<b>AO18</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>Fire services in developments accessed by common private title</b>		
<b>PO19</b> Hydrants are located in positions that will enable fire services to access water safely, effectively and efficiently.	<b>AO19.1</b> Residential streets and common access ways within a common private title places hydrants at intervals of no more than 120 metres and at each intersection. Hydrants may have a single outlet and be situated above or below ground.  <b>AO19.2</b> Commercial and industrial streets and access ways within a common private title serving commercial properties such as factories and warehouses and offices are provided with above or below ground fire hydrants located at not more than 90 metre intervals and at each intersection. Above ground fire hydrants have dual-valved outlets.	<b>Not Applicable.</b>



<p><b>PO20</b> Hydrants are suitable identified so that fire services can locate them at all hours.</p> <p>Note – Hydrants are identified as specified in the Department of Transport and Main Roads Technical Note: 'Identification of street hydrants for fire fighting purposes' available under 'Publications'.</p>	<p><b>AO20</b> No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
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**Table 9.4.5.3.b – Stormwater management design objectives (Construction phase).**

Issue	Design objectives
<p><b>Drainage control</b> (Temporary drainage works)</p>	<ul style="list-style-type: none"> <li>(a) Design life and design storm for temporary drainage works:               <ul style="list-style-type: none"> <li>(i) Disturbed open area for &lt;12 months – 1 in 2 year ARI event;</li> <li>(ii) Disturbed open area for 12-24 months – 1 in 5 year ARI event;</li> <li>(iii) Disturbed open area for &gt;24 months – 1 in 10 year ARI event.</li> </ul> </li> <li>(b) Design capacity excludes minimum 150mm freeboard.</li> <li>(c) Temporary culvert crossing – minimum of 1 in 1-year ARI hydraulic capacity.</li> </ul>
<p><b>Erosion control</b> (Erosion control measures)</p>	<ul style="list-style-type: none"> <li>(a) Minimise exposure of disturbed soils at any time.</li> <li>(b) Divert water run-off from undisturbed areas around disturbed areas.</li> <li>(c) Determine erosion risk rating using local rainfall erosivity, rainfall depth, soil loss rate or other acceptable methods.</li> <li>(d) Implement erosion control methods corresponding to identified erosion risk rating.</li> </ul>
<p><b>Sediment control measures</b> (sediment control measures, design storm for sediment control basins, Sediment basin dewatering)</p>	<ul style="list-style-type: none"> <li>(a) Determine appropriate sediment control measures using:               <ul style="list-style-type: none"> <li>(i) potential soil loss rate; or</li> <li>(ii) monthly erosivity; or</li> <li>(iii) average monthly rainfall.</li> </ul> </li> <li>(b) Collect and drain stormwater from disturbed soils to sediment basin for design storm event:               <ul style="list-style-type: none"> <li>(i) design storm for sediment basin sizing is 80th% five-day event or similar.</li> </ul> </li> <li>(c) Site discharge during sediment basin dewatering:               <ul style="list-style-type: none"> <li>(i) TSS &lt; 50mg/L TSS;</li> <li>(ii) Turbidity not &gt; 10% receiving water's turbidity;</li> <li>(iii) pH 6.5-8.5.</li> </ul> </li> </ul>



<b>Water quality</b> (Litter and other waste, hydrocarbons and other contaminants)	(a) Avoid wind-blown litter; remove grass pollutants. (b) Ensure there is no visible oil or grease sheen on released waters. (c) Dispose of waste containing contaminants at authorised facilities.
<b>Waterway stability and flood flow management</b> (Changes to the natural hydraulics and hydrology)	(a) For peak flow for the 100% AEP event and 1% AEP event, use constructed sediment basins to attenuate the discharge rate of stormwater from the site.

**Table 9.4.5.3.c – Stormwater management design objectives (post-construction phase)**

Design objectives				Application
Minimum reductions in mean annual load from unmitigated development (%)				
Total suspended solids (TSS)	Total phosphorus (TP)	Total nitrogen (TN)	Gross pollutants >5mm	
80	60	40	90	Development for urban purposes  Excludes development that is less than 25% pervious.  In lieu of modelling, the default bio-retention treatment area to comply with load reduction targets of 1.5% of contributing catchment area.

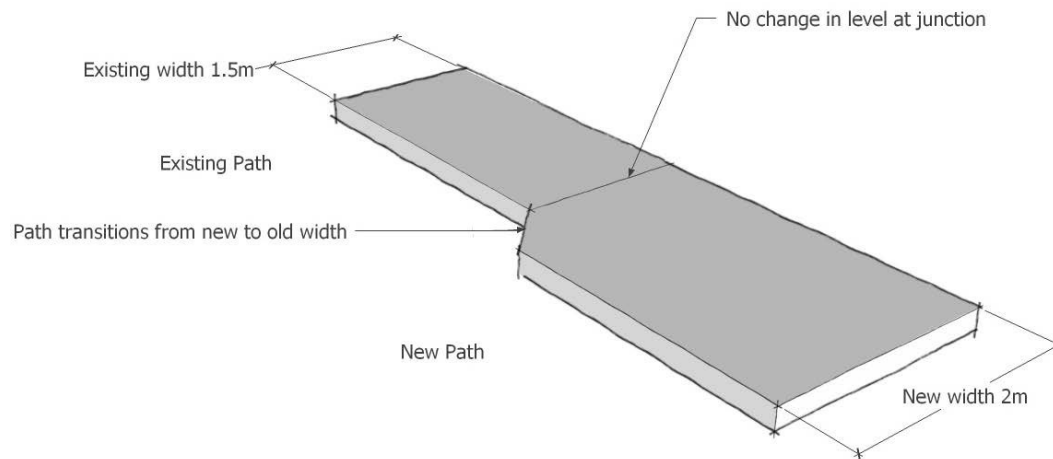
#### Water stability management

(a) Limit peak 100% AEP event discharge within the receiving waterway to the pre-development peak 100% AEP event discharge.

Catchments contributing to un-lined receiving waterway. Degraded waterways may seek alternative discharge management objectives to achieve waterway stability.

For peak flow for the 100% AEP event, use co-located storages to attenuate site discharge rate of stormwater.

**Figure 9.4.5.3.a – New footpath sections**



## 9.4.6 Landscaping code

### 9.4.6.1 Application

- (1) This code applies to assessing:
  - (a) operational work which requires a compliance assessment as a condition of a development permit; or
  - (b) a material change of use or reconfiguring a lot if:
    - (i) assessable development where this code is identified in the assessment criteria column of the table of assessment;
    - (ii) impact assessable development, to the extent relevant.
- (2) When using this code, reference should be made to Part 5.

### 9.4.6.2 Purpose

- (1) The purpose of the Landscaping code is to assess the landscaping aspects of a development.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) The tropical, lush landscape character of the region is retained, promoted and enhanced through high quality landscape works;
  - (b) The natural environment of the region is enhanced;
  - (c) The visual quality, amenity and identity of the region is enhanced;
  - (d) Attractive streetscapes and public places are created through landscape design;
  - (e) As far as practical, existing vegetation on site is retained, and protected during works and integrated with the built environment;
  - (f) Landscaping is provided to enhance the tropical landscape character of development and the region;
  - (g) Landscaping is functional, durable, contributes to passive energy conservation and provides for the efficient use of water and ease of ongoing maintenance;
  - (h) Landscaping takes into account utility service protection;
  - (i) Weed species and invasive species are eliminated from development sites;
  - (j) Landscape design enhances personal safety and incorporates CPTED principles.

### 9.4.6.3 Criteria for assessment

**Table 9.4.6.3.a – Landscaping code –assessable development**

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<b>Landscape design</b>		



<p><b>PO1</b> Development provides landscaping that contributes to and creates a high quality landscape character for the site, street and local areas of the Shire by:</p> <ul style="list-style-type: none"> <li>(a) promoting the Shire's character as a tropical environment;</li> <li>(b) softening the built form of development;</li> <li>(c) enhancing the appearance of the development from within and outside the development and makes a positive contribution to the streetscape;</li> <li>(d) screening the view of buildings, structures, open storage areas, service equipment, machinery plant and the like from public places, residences and other sensitive development;</li> <li>(e) where necessary, ensuring the privacy of habitable rooms and private outdoor recreation areas;</li> <li>(f) contributing to a comfortable living environment and improved energy efficiency, by providing shade to reduce glare and heat absorption and re-radiation from buildings, parking areas and other hard surfaces;</li> <li>(g) ensuring private outdoor recreation space is useable;</li> <li>(h) providing long term soil erosion protection;</li> <li>(i) providing a safe environment;</li> <li>(j) integrating existing vegetation and other natural features of the premises into the development;</li> <li>(k) not adversely affecting vehicular and pedestrian sightlines and road safety.</li> </ul>	<p><b>AO1</b> Development provides landscaping:</p> <ul style="list-style-type: none"> <li>(a) in accordance with the minimum area, dimensions and other requirements of applicable development codes;</li> <li>(b) that is designed and planned in a way that meets the guidelines for landscaping outlined in Planning Scheme Policy SC6.7 – Landscaping;</li> <li>(c) that is carried out and maintained in accordance with a landscaping plan that meets the guidelines for landscaping outlined in Planning Scheme Policy SC6.7 – Landscaping.</li> </ul> <p>Note - Planning scheme policy SC6.7 – Landscaping provides guidance on meeting the outcomes of this code. A landscape plan submitted for approval in accordance with the Planning policy is one way to achieve this outcome.</p>	<p><b>May be Conditioned to Comply.</b> The proposal plans illustrate the proposed landscaping concepts. A detailed landscaping design has not yet been developed for the subject site, however Hortulus Landscape and Design have been engaged to prepare such detailed design plans. This matter may be conditioned to ensure compliance.</p>
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For assessable development		
<p><b>PO2</b> Landscaping contributes to a sense of place, is functional to the surroundings and enhances the streetscape and visual appearance of the development.</p>	<p><b>AO2.1</b> No acceptable outcomes are specified.</p> <p>Note - Landscaping is in accordance with the requirements specified in Planning scheme policy SC6.7 – Landscaping.</p> <p><b>AO2.2</b> Tropical urbanism is incorporated into building design.</p> <p>Note – ‘Tropical urbanism’ includes many things such as green walls, green roofs, podium planting and vegetation incorporated into the design of a building.</p>	<p><b>May be Conditioned to Comply.</b> The proposal plans illustrate the proposed landscaping concepts. A detailed landscaping design has not yet been developed for the subject site, however Hortulus Landscape and Design have been engaged to prepare such detailed design plans. This matter may be conditioned to ensure compliance.</p>
<p><b>PO3</b> Development provides landscaping that is , as far as practical, consistent with the existing desirable landscape character of the area and protects trees, vegetation and other features of ecological, recreational, aesthetic and cultural value.</p>	<p><b>AO3.1</b> Existing vegetation on site is retained and incorporated into the site design, wherever possible, utilising the methodologies and principles outline in AS4970-2009 Protection of Trees on Development Sites.</p> <p><b>AO3.2</b> Mature vegetation on the site that is removed or damaged during development is replaced with advanced species.</p> <p><b>AO3.3</b> Where there is an existing landscape character in a street or locality which results from existing vegetation, similar species are incorporated into new development.</p> <p><b>AO3.4</b> Street trees are species which enhance the landscape character of the streetscape, with species chosen from the Planning scheme policy SC6.7 – Landscaping.</p>	<p><b>Not Applicable.</b> There is no vegetation present onsite.</p>





<p><b>PO4</b> Plant species are selected with consideration to the scale and form of development, screening, buffering, streetscape, shading and the locality of the area.</p>	<p><b>AO4</b> Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.</p>	<p><b>May be Conditioned to Comply.</b> As commented above.</p>
<p><b>PO5</b> Shade planting is provided in car parking areas where uncovered or open, and adjacent to driveways and internal roadways.</p>	<p><b>AO5</b> Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.</p>	<p><b>May be Conditioned to Comply.</b> As commented above.</p>
<p><b>PO6</b> Landscaped areas are designed in order to allow for efficient maintenance.</p>	<p><b>AO6.1</b> A maintenance program is undertaken in accordance with Planning scheme policy SC6.7 – Landscaping.</p> <p><b>AO6.2</b> Tree maintenance is to have regard to the 'Safe Useful Life Expectancy of Trees (SULE).</p> <p>Note – It may be more appropriate to replace trees with a SULE of less than 20 years (as an example), and replant with younger healthy species.</p>	<p><b>May be Conditioned to Comply.</b> As commented above.</p>
<p><b>PO7</b> Podium planting is provided with appropriate species for long term survival and ease of maintenance, with beds capable of proper drainage.</p>	<p><b>AO7.1</b> Podium planting beds are provided with irrigation and are connected to stormwater infrastructure to permit flush out.</p> <p><b>AO7.2</b> Species of plants are selected for long term performance designed to suit the degree of access to podiums and roof tops for maintenance.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO8</b> Development provides for the removal of all weed and invasive species and implement on-going measures to ensure that weeds and invasive species do not reinfest the site and nearby premises.</p>	<p><b>AO8</b> Weed and invasive species detected on a development site are removed in accordance with a management plan prepared by an appropriately qualified person.</p>	<p><b>Not Applicable.</b></p>



<b>PO9</b> The landscape design enhances personal safety and reduces the potential for crime and vandalism.	<b>AO9</b> No acceptable outcomes are specified.  Note - Planning scheme policy SC6.3 – Crime prevention through environmental design (CPTED) provides guidance on meeting this outcome.	<b>May be Conditioned to Comply.</b> As commented above.
<b>PO10</b> The location and type of plant species does not adversely affect the function and accessibility of services and facilities and service areas.	<b>AO10</b> Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.	<b>May be Conditioned to Comply.</b> As commented above.

## 9.4.7 Reconfiguring a lot code

### 9.4.7.1 Application

- (1) This code applies to assessing reconfiguring a lot if:
  - (a) assessable development where the code is an applicable code identified in the assessment criteria column of a table of assessment;
  - (b) impact assessable development, to the extent relevant.
- (2) When using this code, reference should be made to Part 5.

### 9.4.7.2 Purpose

- (1) The purpose of the Reconfiguring a lot code is to regulate development for reconfiguring a lot.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) development results in a well-designed pattern of streets supporting walkable communities;
  - (b) lots have sufficient areas, dimensions and shapes to be suitable for their intended use taking into account environmental features and site constraints;
  - (c) road networks provide connectivity that is integrated with adjoining existing or planned development while also catering for the safe and efficient access for pedestrians, cyclists and for public transport;
  - (d) lots are arranged to front all streets and parkland such that development enhances personal safety, traffic safety, property safety and security; and contributes to streetscape and open space quality;
  - (e) development does not diminish environmental and scenic values, and where relevant, maintains and enhances public access and use of natural areas, rivers, dams, creeks and the foreshore, in a way that protects natural resources;
  - (f) people and property are not placed at risk from natural hazards;
  - (g) a range of functional parkland, including local and district parks, major areas of parkland with a region-wide focus and open space links are available for the use and enjoyment of residents and visitors to the region;
  - (h) the appropriate standard of infrastructure is provided.

### 9.4.7.3 Criteria for assessment

Table 9.4.7.3.a – Reconfiguring a lot code – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>General lot design standards</b>		
<b>PO1</b> Lots comply with the lot reconfiguration outcomes of the applicable Zone code in Part 5.	<b>AO1</b> No acceptable outcomes are prescribed.	<b>Complies with PO1.</b> Refer to commentary under P10-P12 of the Tourist Accommodation Zone Code.



<b>PO2</b> New lots are generally rectangular in shape with functional areas for land uses intended by the zone.	<b>AO2</b> Boundary angles are not less than 45 degrees.	<b>Complies with AO2.</b> The proposed lots are regular in shape and plans demonstrate that sufficient functional areas are provided onsite.
<b>PO3</b> Lots have legal and practical access to a public road.	<b>AO3</b> Each lot is provided with: (a) direct access to a gazetted road reserve; or (b) access to a gazetted road via a formal access arrangement registered on the title.	<b>Complies with AO3.</b> Each lot is provided direct access to Davidson Street.
<b>PO4</b> Development responds appropriately to its local context, natural systems and site features.	<b>AO4</b> Existing site features such as: (a) significant vegetation and trees; (b) waterways and drainage paths; (c) vistas and vantage points are retained and/or are incorporated into open space, road reserves, near to lot boundaries or as common property.	<b>Not Applicable.</b>
<b>PO5</b> New lots which have the capability of being further reconfigured into smaller lots at a later date are designed to not compromise ultimate development outcomes permitted in the relevant zone.	<b>AO5</b> The ability to further reconfigure land at a later date is demonstrated by submitting a concept plan that meets the planning scheme requirements for the applicable Zone.	<b>Not Applicable.</b> It is not intended to further reconfigure the resulting lots.
<b>PO6</b> Where existing buildings or structures are to be retained, development results in: (a) boundaries that offer regular lot shapes and usable spaces; (b) existing improvements complying with current building and amenity standards in relation to boundary setbacks.  Note - This may require buildings or structures to be modified, relocated or demolished to meet setback standards, resolve encroachments and the like.	<b>AO6</b> Development ensures setbacks between existing buildings or structures and proposed boundaries satisfy relevant building standards or zone code requirements, whichever is the greater.	<b>Not Applicable.</b> There are no existing buildings onsite.



<p><b>PO7</b> Where rear lots are proposed, development:</p> <ul style="list-style-type: none"> <li>(a) provides a high standard of amenity for residents and other users of the site and adjoining properties;</li> <li>(b) positively contributes to the character of adjoining properties and the area;</li> <li>(c) does not adversely affect the safety and efficiency of the road from which access is gained.</li> </ul>	<p><b>A07.1</b> Where rear lots are to be established:</p> <ul style="list-style-type: none"> <li>(a) the rear lot is generally rectangular in shape, avoiding contrived sharp boundary angles;</li> <li>(b) no more than 6 lots directly adjoin the rear lot;</li> <li>(c) no more than one rear lot occurs behind the road frontage lot;</li> <li>(d) no more than two access strips to rear lots directly adjoin each other;</li> <li>(e) access strips are located only on one side of the road frontage lot.</li> </ul> <p><b>A07.2</b> Access strips to the rear lot have a minimum width dimension of:</p> <ul style="list-style-type: none"> <li>(a) 4.0 metres in Residential Zones.</li> <li>(b) 8.0 metres in Industrial Zones category.</li> <li>(c) 5.0 metres in all other Zones.</li> </ul> <p>Note - Rear lots are generally not appropriate in non-Residential or non-Rural zones.</p> <p><b>A07.3</b> Access strips are provided with a sealed pavement of sufficient width to cater for the intended traffic, but no less than:</p> <ul style="list-style-type: none"> <li>(a) 3.0 metres in Residential Zone.</li> <li>(b) 6.0 metres in an Industrial Zone.</li> <li>(c) 3.5 metres in any other Zone.</li> </ul>	<p><b>Not Applicable.</b></p>
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Performance outcomes		Acceptable outcomes
<b>Structure plans</b>		
<p>Additional requirements for:</p> <p>(a) a site which is more than 5,000m<sup>2</sup> in any of the Residential zones; or</p> <p>within these zones, and</p> <p>(b) creates 10 or more lots; or</p> <p>(c) involves the creation of new roads and/or public use land.</p> <p>or</p> <p>(d) For a material change of use involving:</p> <p>(i) preliminary approval to vary the effect of the planning scheme;</p> <p>(ii) establishing alternative Zones to the planning scheme.</p> <p>Note - This part is to be read in conjunction with the other parts of the code</p>		
<p><b>PO8</b></p> <p>A structure plan is prepared to ensure that neighbourhood design, block and lot layout, street network and the location and provision on any open space recognises previous planning for the area and its surroundings, and integrates appropriately into its surroundings.</p>	<p><b>AO8.1</b></p> <p>Neighbourhood design, lot and street layout, and open space provides for, and integrates with, any:</p> <p>(a) approved structure plan;</p> <p>(b) the surrounding pattern of existing or approved subdivision.</p> <p>Note - Planning scheme policy SC14– Structure planning provides guidance on meeting the performance outcomes.</p> <p><b>AO8.2</b></p> <p>Neighbourhood design, lot and street layouts enable future connection and integration with adjoining undeveloped land.</p>	<b>Not Applicable.</b>



<p><b>PO9</b> Neighbourhood design results in a connected network of walkable streets providing an easy choice of routes within and surrounding the neighbourhood.</p>	<p><b>AO9.1</b> Development does not establish cul-de-sac streets unless:</p> <ul style="list-style-type: none"> <li>(a) cul-de-sacs are a feature of the existing pattern of development in the area;</li> <li>(b) there is a physical feature or incompatible zone change that dictates the need to use a cul-de-sac streets.</li> </ul> <p><b>AO9.2</b> Where a cul-de-sac street is used, it:</p> <ul style="list-style-type: none"> <li>(a) is designed to be no longer than 150 metres in length;</li> <li>(b) is designed so that the end of the cul-de-sac is visible from its entrance;</li> <li>(c) provides connections from the top of the cul-de-sac to other streets for pedestrians and cyclists, where appropriate.</li> </ul> <p><b>AO9.3</b> No more than 6 lots have access to the turning circle or turning-tee at the end of a cul-de-sac street.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO10</b> Neighbourhood design supports diverse housing choices through block sizes and lot design. In developing areas, significant changes in lot size and frontage occur at the rear of lots rather than on opposite sides of a street.</p>	<p><b>PO10</b> No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO11</b> Provision of physical and social infrastructure in developing residential neighbourhoods is facilitated through the orderly and sequential development of land.</p> <p>Note - Part 4 – Local government infrastructure plan may identify specific levels of infrastructure to be provided within development sites.</p>	<p><b>AO11.1</b> New development adjoins adjacent existing or approved urban development.</p> <p><b>AO11.2</b> New development is not established beyond the identified Local government infrastructure plan area.</p>	<p><b>Not Applicable.</b></p>
<p><b>Urban parkland and environmental open space</b></p>		





<b>PO12</b> Where appropriate development maintains and enhances public access and use of natural areas, rivers, dams, creeks and the foreshore.	<b>AO12</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>
<b>PO13</b> Development provides land to: (a) meet the recreation needs of the community; (b) provide an amenity commensurate with the structure of neighbourhoods and land uses in the vicinity; and adjacent to open space areas; (c) provide for green corridors and linkages.	<b>AO13</b> No acceptable outcomes are prescribed.  Note - Part 4 – Priority infrastructure plan and Planning scheme policy SC14 – Structure Plans provides guidance in providing open space and recreation land.	<b>Not Applicable.</b>

## AO14

Lot size, dimensions, frontage and orientation permits buildings to be established that will facilitate casual surveillance to urban parkland and environmental open space.

### AO14.1

Urban parkland is regular in shape.

### AO14.2

At least 75% of the urban parkland's frontage is provided as road.

### AO14.3

Urban parkland and environmental open space areas are positioned to be capable of being overlooked by surrounding development.

### AO14.4

Surrounding lots are orientated so that facades will front and overlook the urban parkland and environmental open space.

### AO14.5


The number of lots that back onto, or are side-orientated to the urban parkland and environmental open space is minimised.



Inconsistent design solution - low total number of lots complying with the acceptable outcomes.

Not Applicable.



 Lots orientated to front and overlook park to provide casual surveillance.  
Consistent design solution - high total number of lots complying with the acceptable outcomes.

#### Private subdivisions (gated communities)

##### PO15

Private subdivisions (gated communities) do not compromise the establishment of connected and integrated infrastructure and open space networks.

##### PO15

No acceptable outcomes are prescribed.

**Not Applicable.**

#### Additional requirements for reconfiguration involving the creation of public streets or roads

##### PO16

The function of new roads is clearly identified and legible and provides integration, safety and convenience for all users.

##### AO16

No acceptable outcomes are prescribed.

Note - The design and construction standards are set out in Planning scheme policy SC5 – FNQROC Regional Development Manual, with reference to the specifications set out in Sections D1 and D3.

**Not Applicable.**

##### PO17

Street design supports an urban form that creates walkable neighbourhoods. Street design:

- (a) is appropriate to the function(s) of the street;
- (b) meets the needs of users and gives priority to the needs of vulnerable users.

##### AO17

No acceptable outcomes are prescribed.

**Not Applicable.**

#### Public transport network



<p><b>PO18</b> Development provides a street pattern that caters for the extension of public transport routes and infrastructure including safe pedestrian pick-up and set-down up facilities.</p>	<p><b>AO18</b> No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
<p><b>Pest plants</b></p>		
<p><b>PO19</b> Development activities and sites provide for the removal of all pest plants and implement ongoing measures to ensure that pest plants do not reinfest the site or nearby sites.</p> <p>Editor's note - This does not remove or replace all land owner's obligations or responsibilities under the Land Protection (Pest and Stock Route Management) Act 2002.</p>	<p><b>AO19</b> Pest plants detected on a development site are removed in accordance with a management plan prepared by an appropriately qualified person prior to earthworks commencing.</p> <p>Note - A declaration from an appropriately qualified person validates the land being free from pest plants. Declared pest plants include locally declared and State declared pest plants.</p>	<p><b>Not Applicable.</b></p>

# **Attachment 4**

## **Statement of Code Compliance Against the State Development Assessment Code**

# State code 1: Development in a state-controlled road environment

State Development Assessment Provisions guideline - State Code 1: Development in a state-controlled road environment. This guideline provides direction on how to address State Code 1.

**Table 1.1 Development in general**

Performance outcomes	Acceptable outcomes	Response
<b>Buildings, structures, infrastructure, services and utilities</b>		
<b>PO1</b> The location of the development does not create a safety hazard for users of the <b>state-controlled road</b> .	<b>AO1.1</b> Development is not located in a <b>state-controlled road</b> .  AND  <b>AO1.2</b> Development can be maintained without requiring access to a <b>state-controlled road</b> .	<b>Complies with AO1.1-AO1.2.</b> The proposed development is situated entirely within private property; however, an opportunity has been presented to Council to incorporate landscaping within the Davidson Street road reserve. This initiative aims to enhance streetscape continuity by aligning with the existing landscaping character of the area, ensuring a cohesive and visually appealing urban environment.
<b>PO2</b> The design and construction of the development does not adversely impact the <b>structural integrity</b> or physical condition of the <b>state-controlled road</b> or <b>road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Complies with PO2.</b> The development is low scale and does not involve works which would impact the integrity and condition of the road.
<b>PO3</b> The location of the development does not obstruct <b>road transport infrastructure</b> or adversely impact the operating performance of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies with PO3.</b> The development does not involve the obstruction of the road and will not affect operating performance.
<b>PO4</b> The location, placement, design and operation of advertising devices, visible from the <b>state-controlled road</b> , do not create a	No acceptable outcome is prescribed.	<b>Not Applicable.</b> No advertising devices are proposed.

Performance outcomes	Acceptable outcomes	Response
safety hazard for users of the <b>state-controlled road</b> .		
<b>PO5</b> The design and construction of buildings and <b>structures</b> does not create a safety hazard by distracting users of the <b>state-controlled road</b> .	<p><b>AO5.1</b> Facades of buildings and <b>structures</b> fronting the <b>state-controlled road</b> are made of non-reflective materials.</p> <p>AND</p> <p><b>AO5.2</b> Facades of buildings and <b>structures</b> do not direct or reflect point light sources into the face of oncoming traffic on the <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO5.3</b> External lighting of buildings and <b>structures</b> is not directed into the face of oncoming traffic on the <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO5.4</b> External lighting of buildings and <b>structures</b> does not involve flashing or laser lights.</p>	<p><b>Complies with AO5.1-AO5.4</b></p> <p><b>The proposed development is consistent with the existing streetscape and not expected to result in distraction or safety hazards.</b></p>
<b>PO6</b> Road, pedestrian and bikeway bridges over a <b>state-controlled road</b> are designed and constructed to prevent projectiles from being thrown onto the <b>state-controlled road</b> .	<b>AO6.1</b> Road, pedestrian and bikeway bridges over the <b>state-controlled road</b> include throw protection screens in accordance with section 4.11 of the Design Criteria for Bridges and Other Structures Manual, Department of Transport and Main Roads, 2020.	<b>Not Applicable.</b>
<b>Landscaping</b>		
<b>PO7</b> The location of landscaping does not create a safety hazard for users of the <b>state-controlled road</b> .	<p><b>AO7.1</b> Landscaping is not located in a <b>state-controlled road</b>.</p> <p>AND</p>	<p><b>May be Conditioned to Comply with AO7.1-AO7.3.</b></p> <p><b>An opportunity has been presented to Council to incorporate landscaping within the Davidson Street road reserve. This initiative aims to enhance streetscape continuity by</b></p>



Performance outcomes	Acceptable outcomes	Response
	<p><b>AO7.2</b> Landscaping can be maintained without requiring access to a <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO7.3</b> Landscaping does not block or obscure the sight lines for vehicular access to a <b>state-controlled road</b>.</p>	<p><b>aligning with the existing landscaping character of the area, ensuring a cohesive and visually appealing urban environment. This is a unique situation, Davidson Street is not the typical State Controlled Road.</b></p>
<b>Stormwater and overland flow</b>		
<b>PO8</b> Stormwater run-off or overland flow from the development site does not create or exacerbate a safety hazard for users of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies with PO8</b> <b>Stormwater will be directed to a lawful point of discharge and existing stormwater infrastructure at the rear of the subject site.</b>
<b>PO9</b> Stormwater run-off or overland flow from the development site does not result in a material worsening of the operating performance of the <b>state-controlled road</b> or <b>road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Will Comply with PO9</b> <b>Stormwater will be directed to a lawful point of discharge and existing stormwater infrastructure at the rear of the subject site.</b>
<b>PO10</b> Stormwater run-off or overland flow from the development site does not adversely impact the <b>structural integrity</b> or physical condition of the <b>state-controlled road</b> or <b>road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Will Comply with PO10</b> <b>Stormwater will be directed to a lawful point of discharge and existing stormwater infrastructure at the rear of the subject site.</b>
<b>PO11</b> Development ensures that stormwater is lawfully discharged.	<p><b>AO11.1</b> Development does not create any new points of discharge to a <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO11.2</b> Development does not concentrate flows to a <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO11.3</b> Stormwater run-off is discharged to a <b>lawful point of discharge</b>.</p>	<b>Complies</b>

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State code 1: Development in a state-controlled road environment

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Performance outcomes	Acceptable outcomes	Response
	<p>AND</p> <p><b>AO11.4</b> Development does not worsen the condition of an existing <b>lawful point of discharge</b> to the <b>state-controlled road</b>.</p>	
<b>Flooding</b>		
<b>PO12</b> Development does not result in a material worsening of flooding impacts within a <b>state-controlled road</b> .	<p><b>AO12.1</b> For all flood events up to 1% <b>annual exceedance probability</b>, development results in negligible impacts (within +/- 10mm) to existing flood levels within a <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO12.2</b> For all flood events up to 1% <b>annual exceedance probability</b>, development results in negligible impacts (up to a 10% increase) to existing peak velocities within a <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO12.3</b> For all flood events up to 1% <b>annual exceedance probability</b>, development results in negligible impacts (up to a 10% increase) to existing time of submergence of a <b>state-controlled road</b>.</p>	<p><b>Not Applicable.</b></p> <p><b>The subject site is located outside of the mapped Q100 Flood Levels.</b></p>
<b>Drainage Infrastructure</b>		
<b>PO13</b> Drainage infrastructure does not create a safety hazard for users in the <b>state-controlled road</b> .	<p><b>AO13.1</b> Drainage infrastructure is wholly contained within the development site, except at the <b>lawful point of discharge</b>.</p> <p>AND</p>	<p><b>Complies with AO13.1-13.2</b></p> <p><b>Stormwater will be directed to a lawful point of discharge and existing stormwater infrastructure at the rear of the subject site.</b></p>

Performance outcomes	Acceptable outcomes	Response
	<b>AO13.2</b> Drainage infrastructure can be maintained without requiring access to a <b>state-controlled road</b> .	
<b>PO14</b> Drainage infrastructure associated with, or within, a <b>state-controlled road</b> is constructed, and designed to ensure the <b>structural integrity</b> and physical condition of existing drainage infrastructure and the surrounding drainage network.	No acceptable outcome is prescribed.	<b>Not Applicable.</b> <b>No drainage infrastructure is proposed within the State Controlled Road.</b>

**Table 1.2 Vehicular access, road layout and local roads**

Performance outcomes	Acceptable outcomes	Response
<b>Vehicular access to a state-controlled road or within 100 metres of a state-controlled road intersection</b>		
<b>PO15</b> The location, design and operation of a <b>new or changed access</b> to a <b>state-controlled road</b> does not compromise the safety of users of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies with PO15.</b> The development involves 2 new access to Davidson Street which is a 60km/hr zone. The proposed development is not a high traffic generator and is not expected to raise safety concerns.
<b>PO16</b> The location, design and operation of a <b>new or changed access</b> does not adversely impact the <b>functional requirements</b> of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies with PO16.</b> The proposed access arrangement is not expected to raise safety concerns. Any concerns may be addressed through conditions.
<b>PO17</b> The location, design and operation of a <b>new or changed access</b> is consistent with the <b>future intent</b> of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies with PO17.</b> The proposed access arrangement is not expected to raise safety concerns. Any concerns may be addressed through conditions.
<b>PO18</b> <b>New or changed access</b> is consistent with the access for the relevant <b>limited access road policy</b> : 1. <b>LAR 1</b> where direct access is prohibited; or 2. <b>LAR 2</b> where access may be permitted, subject to assessment.	No acceptable outcome is prescribed.	<b>Not Applicable.</b>

Performance outcomes	Acceptable outcomes	Response
<b>PO19</b> New or changed access to a <b>local road</b> within 100 metres of an intersection with a <b>state-controlled road</b> does not compromise the safety of users of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies with PO19.</b> The proposed access arrangement is not expected to raise safety concerns. Any concerns may be addressed through conditions.
<b>PO20</b> New or changed access to a <b>local road</b> within 100 metres of an intersection with a <b>state-controlled road</b> does not adversely impact on the operating performance of the intersection.	No acceptable outcome is prescribed.	<b>Complies with PO20.</b> The proposed access arrangement is not expected to raise safety concerns. Any concerns may be addressed through conditions.
<b>Public passenger transport and active transport</b>		
<b>PO21</b> Development does not compromise the safety of users of <b>public passenger transport infrastructure, public passenger services and active transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO22</b> Development maintains the ability for people to access <b>public passenger transport infrastructure, public passenger services and active transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO23</b> Development does not adversely impact the operating performance of <b>public passenger transport infrastructure, public passenger services and active transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO24</b> Development does not adversely impact the <b>structural integrity</b> or physical condition of <b>public passenger transport infrastructure and active transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>

**Table 1.3 Network impacts**

Performance outcomes	Acceptable outcomes	Response
<b>PO25</b> Development does not compromise the safety of users of the <b>state-controlled road network</b> .	No acceptable outcome is prescribed.	<b>Complies with PO25.</b> The proposed access arrangement is not expected to raise safety concerns. Any concerns may be addressed through conditions.

Performance outcomes	Acceptable outcomes	Response
<b>PO26</b> Development ensures <b>no net worsening</b> of the operating performance of the <b>state-controlled road</b> network.	No acceptable outcome is prescribed.	<b>Complies with PO26.</b> <b>The proposed access arrangement is not expected to raise operational performance concerns. Any concerns may be addressed through conditions.</b>
<b>PO27</b> Traffic movements are not directed onto a <b>state-controlled road</b> where they can be accommodated on the <b>local road</b> network.	No acceptable outcome is prescribed.	<b>Complies with PO27.</b> <b>Traffic cannot be directed elsewhere.</b>
<b>PO28</b> Development involving haulage exceeding 10,000 tonnes per year does not adversely impact the pavement of a <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO29</b> Development does not impede delivery of <b>planned upgrades</b> of <b>state-controlled roads</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b> <b>No planned upgrades are known.</b>
<b>PO30</b> Development does not impede delivery of <b>corridor improvements</b> located entirely within the <b>state-controlled road corridor</b> .	No acceptable outcome is prescribed.	<b>Complies with PO30.</b> <b>Development of the site would not compromise corridor improvements.</b>

**Table 1.4 Filling, excavation, building foundations and retaining structures**

Performance outcomes	Acceptable outcomes	Response
<b>PO31</b> Development does not create a safety hazard for users of the <b>state-controlled road</b> or <b>road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO32</b> Development does not adversely impact the operating performance of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO33</b> Development does not undermine, damage or cause subsidence of a <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO34</b> Development does not cause ground water disturbance in a <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO35</b> Excavation, boring, piling, blasting and fill compaction do not adversely impact the physical condition or <b>structural integrity</b> of a <b>state-</b>	No acceptable outcome is prescribed.	<b>Not Applicable.</b>

Performance outcomes	Acceptable outcomes	Response
<b>controlled road or road transport infrastructure.</b>		
<b>PO36</b> Filling and excavation associated with the construction of <b>new or changed access</b> do not compromise the operation or capacity of existing drainage infrastructure for a <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>

## Table 1.5 Environmental emissions

Statutory note: Where a **state-controlled road** is co-located in the same transport corridor as a railway, the development should instead comply with Environmental emissions in State code 2: Development in a railway environment.

Performance outcomes	Acceptable outcomes	Response
<b>Reconfiguring a lot</b>		
<b>Involving the creation of 5 or fewer new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<b>PO37</b> Development minimises free field noise intrusion from a <b>state-controlled road</b> .	<b>AO37.1</b> Development provides a noise barrier or earth mound which is designed, sited and constructed: <ol style="list-style-type: none"> <li>1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.1);</li> <li>2. in accordance with: <ol style="list-style-type: none"> <li>a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</li> <li>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</li> <li>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.</li> </ol> </li> </ol> <p>OR</p>	<b>Not Applicable.</b>

Performance outcomes	Acceptable outcomes	Response
	<p><b>AO37.2</b> Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.</p> <p>OR</p> <p><b>AO37.3</b> Development provides a <b>solid gap-free fence</b> or other <b>solid gap-free structure</b> along the full extent of the boundary closest to the <b>state-controlled road</b>.</p>	
<b>Involving the creation of 6 or more new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<b>PO38</b> Reconfiguring a lot minimises free field noise intrusion from a <b>state-controlled road</b> .	<p><b>AO38.1</b> Development provides noise barrier or earth mound which is designed, sited and constructed:</p> <ol style="list-style-type: none"> <li>1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.1);</li> <li>2. in accordance with: <ol style="list-style-type: none"> <li>a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</li> <li>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</li> <li>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.</li> </ol> </li> </ol> <p>OR</p> <p><b>AO38.2</b> Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by <b>alternative noise attenuation measures</b></p>	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Response
	where it is not practical to provide a noise barrier or earth mound.	
<b>Material change of use (accommodation activity)</b>		
<b>Ground floor level requirements adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<b>PO39</b> Development minimises noise intrusion from a <b>state-controlled road</b> in <b>private open space</b> .	<p><b>AO39.1</b> Development provides a noise barrier or earth mound which is designed, sited and constructed:</p> <ol style="list-style-type: none"> <li>1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.2) for <b>private open space</b> at the ground floor level;</li> <li>2. in accordance with: <ol style="list-style-type: none"> <li>a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</li> <li>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</li> <li>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.</li> </ol> </li> </ol> <p>OR</p> <p><b>AO39.2</b> Development achieves the maximum free field acoustic level in reference table 2 (item 2.2) for <b>private open space</b> by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.</p>	<b>May be Conditioned to Comply.</b> <b>This matter will be further assessed at the time of Building Approval.</b>
<b>PO40</b> Development (excluding a <b>relevant residential building</b> or <b>relocated building</b> ) minimises noise intrusion from a <b>state-controlled road</b> in <b>habitable rooms</b> at the facade.	<b>AO40.1</b> Development (excluding a <b>relevant residential building</b> or <b>relocated building</b> ) provides a noise barrier or earth mound which is designed, sited and constructed:	<b>May be Conditioned to Comply.</b> <b>This matter will be further assessed at the time of Building Approval.</b>

Performance outcomes	Acceptable outcomes	Response
	<ol style="list-style-type: none"> <li>1. to achieve the maximum building façade acoustic level in reference table 1 (item 1.1) for <b>habitable rooms</b>;</li> <li>2. in accordance with: <ol style="list-style-type: none"> <li>a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</li> <li>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</li> <li>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.</li> </ol> </li> </ol> <p>OR</p> <p><b>AO40.2</b> Development (excluding a <b>relevant residential building</b> or <b>relocated building</b>) achieves the maximum building façade acoustic level in reference table 1 (item 1.1) for <b>habitable rooms</b> by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.</p>	
<b>PO41 Habitable rooms</b> (excluding a <b>relevant residential building</b> or <b>relocated building</b> ) are designed and constructed using materials to achieve the maximum internal acoustic level in reference table 3 (item 3.1).	No acceptable outcome is provided.	<b>May be Conditioned to Comply.</b> <b>This matter will be further assessed at the time of Building Approval.</b>
<b>Above ground floor level requirements (accommodation activity) adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<b>PO42</b> Balconies, podiums, and roof decks include: <ol style="list-style-type: none"> <li>1. a continuous <b>solid gap-free structure</b> or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia);</li> </ol>	No acceptable outcome is provided.	<b>Not Applicable.</b>

Performance outcomes	Acceptable outcomes	Response
2. highly acoustically absorbent material treatment for the total area of the soffit above balconies, podiums, and roof decks.		
<b>PO43 Habitable rooms</b> (excluding a <b>relevant residential building</b> or <b>relocated building</b> ) are designed and constructed using materials to achieve the maximum internal acoustic level in reference table 3 (item 3.1).	No acceptable outcome is provided.	<b>Not Applicable.</b>
<b>Material change of use (other uses)</b>		
<b>Ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<b>PO44 Development:</b> 1. provides a noise barrier or earth mound that is designed, sited and constructed: <ul style="list-style-type: none"> <li>a. to achieve the maximum free field acoustic level in reference table 2 (item 2.3) for all <b>outdoor education areas</b> and <b>outdoor play areas</b>;</li> <li>b. in accordance with: <ul style="list-style-type: none"> <li>i. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</li> <li>ii. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</li> <li>iii. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or</li> </ul> </li> </ul> 2. achieves the maximum free field acoustic level in reference table 2 (item 2.3) for all <b>outdoor education areas</b> and <b>outdoor play areas</b> by <b>alternative noise attenuation measures</b> where it is not	No acceptable outcome is provided.	<b>Not Applicable.</b>

Performance outcomes	Acceptable outcomes	Response
practical to provide a noise barrier or earth mound.		
<b>PO45</b> Development involving a <b>childcare centre</b> or <b>educational establishment</b> : <ol style="list-style-type: none"> <li>provides a noise barrier or earth mound that is designed, sited and constructed:</li> <li>to achieve the maximum building facade acoustic level in reference table 1 (item 1.2);</li> <li>in accordance with:               <ol style="list-style-type: none"> <li>Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</li> <li>Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</li> <li>Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or</li> </ol> </li> <li>achieves the maximum building facade acoustic level in reference table 1 (item 1.2) by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.</li> </ol>	No acceptable outcome is provided.	<b>Not Applicable.</b>
<b>PO46</b> Development involving: <ol style="list-style-type: none"> <li><b>indoor education areas</b> and <b>indoor play areas</b>; or</li> <li>sleeping rooms in a <b>childcare centre</b>; or</li> <li><b>patient care areas</b> in a <b>hospital</b> achieves the maximum internal acoustic level in reference table 3 (items 3.2-3.4).</li> </ol>	No acceptable outcome is provided.	<b>Not Applicable.</b>
<b>Above ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor</b>		

Performance outcomes	Acceptable outcomes	Response
<b>PO47</b> Development involving a <b>childcare centre</b> or <b>educational establishment</b> which have balconies, podiums or elevated <b>outdoor play areas</b> predicted to exceed the maximum free field acoustic level in reference table 2 (item 2.3) due to noise from a <b>state-controlled road</b> are provided with: <ol style="list-style-type: none"> <li>1. a continuous <b>solid gap-free structure</b> or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia);</li> <li>2. highly acoustically absorbent material treatment for the total area of the soffit above balconies or elevated <b>outdoor play areas</b>.</li> </ol>	No acceptable outcome is provided.	<b>Not Applicable.</b>
<b>PO48</b> Development including: <ol style="list-style-type: none"> <li>1. <b>indoor education areas</b> and <b>indoor play areas</b> in a <b>childcare centre</b> or <b>educational establishment</b>; or</li> <li>2. sleeping rooms in a <b>childcare centre</b>; or</li> <li>3. <b>patient care areas</b> in a <b>hospital</b> located above ground level, is designed and constructed to achieve the maximum internal acoustic level in reference table 3 (items 3.2-3.4).</li> </ol>	No acceptable outcome is provided.	<b>Not Applicable.</b>
<b>Air, light and vibration</b>		

Performance outcomes	Acceptable outcomes	Response
<b>PO49 Private open space, outdoor education areas and outdoor play areas</b> are protected from air quality impacts from a <b>state-controlled road</b> .	<b>AO49.1</b> Each dwelling or unit has access to a <b>private open space</b> which is shielded from a <b>state-controlled road</b> by a building, <b>solid gap-free fence</b> , or other <b>solid gap-free structure</b> .  OR  <b>AO49.2</b> Each <b>outdoor education area</b> and <b>outdoor play area</b> is shielded from a <b>state-controlled road</b> by a building, <b>solid gap-free fence</b> , or other <b>solid gap-free structure</b> .	<b>Not Applicable.</b>
<b>PO50 Patient care areas</b> within <b>hospitals</b> are protected from vibration impacts from a <b>state-controlled road</b> or <b>type 1 multi-modal corridor</b> .	<b>AO50.1 Hospitals</b> are designed and constructed to ensure vibration in the patient treatment area does not exceed a vibration dose value of $0.1\text{m/s}^{1.75}$ .  AND  <b>AO50.2 Hospitals</b> are designed and constructed to ensure vibration in the ward of a <b>patient care area</b> does not exceed a vibration dose value of $0.4\text{m/s}^{1.75}$ .	<b>Not Applicable.</b>
<b>PO51</b> Development is designed and sited to ensure light from infrastructure within, and from users of, a <b>state-controlled road</b> or <b>type 1 multi-modal corridor</b> , does not: <ol style="list-style-type: none"> <li>1. intrude into buildings during night hours (10pm to 6am);</li> <li>2. create unreasonable disturbance during evening hours (6pm to 10pm).</li> </ol>	No acceptable outcomes are prescribed.	<b>Not Applicable.</b>

**Table 1.6: Development in a future state-controlled road environment**

Performance outcomes	Acceptable outcomes	Response
<b>PO52</b> Development does not impede delivery of a <b>future state-controlled road</b> .	<p><b>AO52.1</b> Development is not located in a <b>future state-controlled road</b>.</p> <p>OR ALL OF THE FOLLOWING APPLY:</p> <p><b>AO52.2</b> Development does not involve filling and excavation of, or material changes to, a <b>future state-controlled road</b>.</p> <p>AND</p> <p><b>AO52.3</b> The intensification of lots does not occur within a <b>future state-controlled road</b>.</p> <p>AND</p> <p><b>AO52.4</b> Development does not result in the landlocking of parcels once a <b>future state-controlled road</b> is delivered.</p>	<b>Not Applicable.</b>
<b>PO53</b> The location and design of <b>new or changed access</b> does not create a safety hazard for users of a <b>future state-controlled road</b> .	<b>AO53.1</b> Development does not include <b>new or changed access</b> to a <b>future state-controlled road</b> .	<b>Not Applicable.</b>
<b>PO54</b> Filling, excavation, building foundations and <b>retaining structures</b> do not undermine, damage or cause subsidence of a <b>future state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO55</b> Development does not result in a material worsening of stormwater, flooding, overland flow or drainage impacts in a <b>future state-controlled road</b> or <b>road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Not Applicable.</b>
<b>PO56</b> Development ensures that stormwater is lawfully discharged.	<b>AO56.1</b> Development does not create any new points of discharge to a <b>future state-controlled road</b> .	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Response
	<p>AND</p> <p><b>AO56.2</b> Development does not concentrate flows to a <b>future state-controlled road</b>.</p> <p>AND</p> <p><b>AO56.3</b> Stormwater run-off is discharged to a <b>lawful point of discharge</b>.</p> <p>AND</p> <p><b>AO56.4</b> Development does not worsen the condition of an existing <b>lawful point of discharge</b> to the <b>future state-controlled road</b>.</p>	