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17 June 2026

**Our Ref:** 2026-001 Propel Funeral Partners Ltd

Chief Executive Officer  
Douglas Shire Council  
PO Box 723  
MOSSMAN Qld 4873

To: [enquiries@douglas.qld.gov.au](mailto:enquiries@douglas.qld.gov.au) (By Email)

Dear Neil Beck (Team Leader Planning),

**Development Application seeking a Development Permit for a Material Change of Use (Funeral Parlor) at 15 Theresa Drive, Mossman (Lot 17 on RP895020)**

Kelly Reaston Development and Property Services has been engaged by FPT Pty Ltd (the Applicant and Landowner) to prepare the attached Development Application seeking approval for a Material Change of Use for a Funeral Parlour over the subject land at 15 Theresa Drive, Mossman (Lot 17 on RP895020).

The Applicant owns and operates Mossman and Port Douglas Community Funerals and currently conducts its business from premises located at 14 Sawmill Street, Mossman, immediately adjoining the subject site.

The proposal seeks to consolidate the primary funeral operations at the Theresa Drive property, creating a purpose designed facility capable of accommodating the full range of funeral services provided by the business. The Theresa Drive premises will accommodate client consultations, funeral arrangements, meetings with families, administrative functions, preparation of deceased persons, mortuary facilities, body storage and the coordination and management of funeral services.

As part of the proposal, the existing mortuary and body storage functions currently undertaken at the Sawmill Street premises will be relocated to the Theresa Drive site. The new facility will become the principal operational base for Mossman and Port Douglas Community Funerals and provide an improved level of service, privacy and functionality for clients and staff.



The existing Sawmill Street property will continue to operate in conjunction with the funeral business, however, its role will be focused on cremation-related activities, including the operation of the existing crematorium and pet cremation business. The site will also continue to accommodate the storage and maintenance of company vehicles, including hearses, transfer vehicles and other operational fleet vehicles.

The proposed arrangement will provide a clear separation between funeral administration and family-facing services on one site and cremation-related activities on the adjoining property, resulting in a more efficient and functional operational model while maintaining the established funeral services offered to the Mossman and Port Douglas communities.

In support of the application, please find attached the following documents:

1. Completed DA Form 1 - **Attachment 1**
2. Title Search - **Attachment 2**
3. Plans of Development (Architectural Plans) - **Attachment 3**
4. Planning Report - **Attachment 4**
5. Assessment against the applicable development codes Douglas Shire Planning Scheme 2018 v1.0 - **Attachment 5**

In accordance with Council's Schedule of Fees for the 2025/26 Financial Year the application fee is calculated to be **\$2,357.00** and will be paid on lodgement by the Applicant. A full breakdown of the fee components is provided in the attached Planning Report.

Should you require any further information, please do not hesitate to contact Kelly Reaston on 0400 974 688 or at [kelly@kellyreaston.com.au](mailto:kelly@kellyreaston.com.au).

Kind regards



**Kelly Reaston** | Director



# Attachment 1

## Completed DA Form 1



DEVELOPMENT & PROPERTY SERVICES

# DA Form 1 – Development application details

Approved form (version 1.6 effective 2 August 2024) made under section 282 of the Planning Act 2016.

This form **must** be used to make a development application **involving code assessment or impact assessment**, except when applying for development involving only building work.

For a development application involving **building work only**, use *DA Form 2 – Building work details*.

For a development application involving **building work associated with any other type of assessable development (i.e. material change of use, operational work or reconfiguring a lot)**, use this form (*DA Form 1*) and parts 4 to 6 of *DA Form 2 – Building work details*.

Unless stated otherwise, all parts of this form **must** be completed in full and all required supporting information **must** accompany the development application.

One or more additional pages may be attached as a schedule to this development application if there is insufficient space on the form to include all the necessary information.

This form and any other form relevant to the development application must be used to make a development application relating to strategic port land and Brisbane core port land under the *Transport Infrastructure Act 1994*, and airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*. For the purpose of assessing a development application relating to strategic port land and Brisbane core port land, any reference to a planning scheme is taken to mean a land use plan for the strategic port land, Brisbane port land use plan for Brisbane core port land, or a land use plan for airport land.

**Note:** All terms used in this form have the meaning given under the *Planning Act 2016*, the *Planning Regulation 2017*, or the *Development Assessment Rules (DA Rules)*.

## PART 1 – APPLICANT DETAILS

### 1) Applicant details

Applicant name(s) (individual or company full name)	FPT Pty Ltd
Contact name (only applicable for companies)	c/- Daniel Favier Kelly Reaston Development and Property Services
Postal address (P.O. Box or street address)	2/51 Sheridan Street
Suburb	Cairns
State	QLD
Postcode	4870
Country	Australia
Contact number	0418 826 560 or 0400 974 688
Email address (non-mandatory)	<a href="mailto:daniel@kellyreaston.com.au">daniel@kellyreaston.com.au</a>
Mobile number (non-mandatory)	0418 826 560 or 0400 974 688
Fax number (non-mandatory)	
Applicant's reference number(s) (if applicable)	2026-001 Propel Funeral Partners Ltd

### 1.1) Home-based business

Personal details to remain private in accordance with section 264(6) of *Planning Act 2016*

### 2) Owner's consent

2.1) Is written consent of the owner required for this development application?

- Yes – the written consent of the owner(s) is attached to this development application  
 No – proceed to 3)

## PART 2 – LOCATION DETAILS

### 3) Location of the premises (complete 3.1) or 3.2, and 3.3) as applicable)

**Note:** Provide details below and attach a site plan for any or all premises part of the development application. For further information, see DA Forms Guide: Relevant plans.

#### 3.1) Street address and lot on plan

- Street address **AND** lot on plan (all lots must be listed), **or**  
 Street address **AND** lot on plan for an adjoining or adjacent property of the premises (appropriate for development in water but adjoining or adjacent to land e.g. jetty, pontoon. All lots must be listed).

a)	Unit No.	Street No.	Street Name and Type	Suburb
		15	Theresa Drive	Mossman
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)
	4873	17	RP895020	Douglas Shire
b)	Unit No.	Street No.	Street Name and Type	Suburb
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)

#### 3.2) Coordinates of premises (appropriate for development in remote areas, over part of a lot or in water not adjoining or adjacent to land e.g. channel dredging in Moreton Bay)

**Note:** Place each set of coordinates in a separate row.

Coordinates of premises by longitude and latitude

Longitude(s)	Latitude(s)	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

Coordinates of premises by easting and northing

Easting(s)	Northing(s)	Zone Ref.	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> 54 <input type="checkbox"/> 55 <input type="checkbox"/> 56	<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

#### 3.3) Additional premises

- Additional premises are relevant to this development application and the details of these premises have been attached in a schedule to this development application  
 Not required

#### 4) Identify any of the following that apply to the premises and provide any relevant details

In or adjacent to a water body or watercourse or in or above an aquifer  
 Name of water body, watercourse or aquifer: \_\_\_\_\_

On strategic port land under the *Transport Infrastructure Act 1994*  
 Lot on plan description of strategic port land: \_\_\_\_\_  
 Name of port authority for the lot: \_\_\_\_\_

In a tidal area  
 Name of local government for the tidal area (if applicable): \_\_\_\_\_  
 Name of port authority for tidal area (if applicable): \_\_\_\_\_

<input type="checkbox"/> On airport land under the <i>Airport Assets (Restructuring and Disposal) Act 2008</i>
Name of airport: <input type="text"/>
<input type="checkbox"/> Listed on the Environmental Management Register (EMR) under the <i>Environmental Protection Act 1994</i>
EMR site identification: <input type="text"/>
<input type="checkbox"/> Listed on the Contaminated Land Register (CLR) under the <i>Environmental Protection Act 1994</i>
CLR site identification: <input type="text"/>

**5) Are there any existing easements over the premises?**

*Note: Easement uses vary throughout Queensland and are to be identified correctly and accurately. For further information on easements and how they may affect the proposed development, see [DA Forms Guide](#).*

- Yes – All easement locations, types and dimensions are included in plans submitted with this development application
- No

## PART 3 – DEVELOPMENT DETAILS

### Section 1 – Aspects of development

**6.1) Provide details about the first development aspect**

a) What is the type of development? *(tick only one box)*

- Material change of use     Reconfiguring a lot     Operational work     Building work

b) What is the approval type? *(tick only one box)*

- Development permit     Preliminary approval     Preliminary approval that includes a variation approval

c) What is the level of assessment?

- Code assessment     Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

Funeral Parlour

e) Relevant plans

*Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms guide: Relevant plans](#).*

- Relevant plans of the proposed development are attached to the development application

**6.2) Provide details about the second development aspect**

a) What is the type of development? *(tick only one box)*

- Material change of use     Reconfiguring a lot     Operational work     Building work

b) What is the approval type? *(tick only one box)*

- Development permit     Preliminary approval     Preliminary approval that includes a variation approval

c) What is the level of assessment?

- Code assessment     Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

e) Relevant plans

*Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).*

- Relevant plans of the proposed development are attached to the development application

**6.3) Additional aspects of development**

- Additional aspects of development are relevant to this development application and the details for these aspects that would be required under Part 3 Section 1 of this form have been attached to this development application
- Not required

**6.4) Is the application for State facilitated development?**

- Yes - Has a notice of declaration been given by the Minister?
- No

**Section 2 – Further development details****7) Does the proposed development application involve any of the following?**

Material change of use	<input checked="" type="checkbox"/> Yes – complete division 1 if assessable against a local planning instrument
Reconfiguring a lot	<input type="checkbox"/> Yes – complete division 2
Operational work	<input type="checkbox"/> Yes – complete division 3
Building work	<input type="checkbox"/> Yes – complete <i>DA Form 2 – Building work details</i>

**Division 1 – Material change of use**

**Note:** This division is only required to be completed if any part of the development application involves a material change of use assessable against a local planning instrument.

**8.1) Describe the proposed material change of use**

Provide a general description of the proposed use	Provide the planning scheme definition <i>(include each definition in a new row)</i>	Number of dwelling units <i>(if applicable)</i>	Gross floor area (m <sup>2</sup> ) <i>(if applicable)</i>
The proposal seeks to consolidate the primary funeral operations at the Theresa Drive property, creating a purpose designed facility capable of accommodating the full range of funeral services provided by the business. The Theresa Drive premises will accommodate client consultations, funeral arrangements, meetings with families, administrative functions, preparation of deceased persons, mortuary facilities, body storage and the coordination and management of funeral services.	Funeral Parlour	na	162m <sup>2</sup>

**8.2) Does the proposed use involve the use of existing buildings on the premises?**

- Yes
- No

**8.3) Does the proposed development relate to temporary accepted development under the Planning Regulation?**

- Yes – provide details below or include details in a schedule to this development application
- No

Provide a general description of the temporary accepted development	Specify the stated period dates under the Planning Regulation

## Division 2 – Reconfiguring a lot

**Note:** This division is only required to be completed if any part of the development application involves reconfiguring a lot.

9.1) What is the total number of existing lots making up the premises?

9.2) What is the nature of the lot reconfiguration? (tick all applicable boxes)

<input type="checkbox"/> Subdivision (complete 10)	<input type="checkbox"/> Dividing land into parts by agreement (complete 11)
<input type="checkbox"/> Boundary realignment (complete 12)	<input type="checkbox"/> Creating or changing an easement giving access to a lot from a constructed road (complete 13)

### 10) Subdivision

10.1) For this development, how many lots are being created and what is the intended use of those lots:

Intended use of lots created	Residential	Commercial	Industrial	Other, please specify:
Number of lots created				

10.2) Will the subdivision be staged?

Yes – provide additional details below  
 No

How many stages will the works include?

What stage(s) will this development application apply to?

11) Dividing land into parts by agreement – how many parts are being created and what is the intended use of the parts?

Intended use of parts created	Residential	Commercial	Industrial	Other, please specify:
Number of parts created				

### 12) Boundary realignment

12.1) What are the current and proposed areas for each lot comprising the premises?

Current lot		Proposed lot	
Lot on plan description	Area (m <sup>2</sup> )	Lot on plan description	Area (m <sup>2</sup> )

12.2) What is the reason for the boundary realignment?

13) What are the dimensions and nature of any existing easements being changed and/or any proposed easement? (attach schedule if there are more than two easements)

Existing or proposed?	Width (m)	Length (m)	Purpose of the easement? (e.g. pedestrian access)	Identify the land/lot(s) benefitted by the easement

## Division 3 – Operational work

**Note:** This division is only required to be completed if any part of the development application involves operational work.

14.1) What is the nature of the operational work?

<input type="checkbox"/> Road work	<input type="checkbox"/> Stormwater	<input type="checkbox"/> Water infrastructure
<input type="checkbox"/> Drainage work	<input type="checkbox"/> Earthworks	<input type="checkbox"/> Sewage infrastructure
<input type="checkbox"/> Landscaping	<input type="checkbox"/> Signage	<input type="checkbox"/> Clearing vegetation
<input type="checkbox"/> Other – please specify: <input type="text"/>		
<b>14.2) Is the operational work necessary to facilitate the creation of new lots? (e.g. subdivision)</b>		
<input type="checkbox"/> Yes – specify number of new lots: <input type="text"/>		
<input type="checkbox"/> No		
<b>14.3) What is the monetary value of the proposed operational work? (include GST, materials and labour)</b>		
\$ <input type="text"/>		

## PART 4 – ASSESSMENT MANAGER DETAILS

<b>15) Identify the assessment manager(s) who will be assessing this development application</b>
Douglas Shire Council
<b>16) Has the local government agreed to apply a superseded planning scheme for this development application?</b>
<input type="checkbox"/> Yes – a copy of the decision notice is attached to this development application <input type="checkbox"/> The local government is taken to have agreed to the superseded planning scheme request – relevant documents attached <input checked="" type="checkbox"/> No

## PART 5 – REFERRAL DETAILS

<b>17) Does this development application include any aspects that have any referral requirements?</b>
<b>Note:</b> A development application will require referral if prescribed by the Planning Regulation 2017.
<input checked="" type="checkbox"/> No, there are no referral requirements relevant to any development aspects identified in this development application – proceed to Part 6
<b>Matters requiring referral to the Chief Executive of the Planning Act 2016:</b>
<input type="checkbox"/> Clearing native vegetation <input type="checkbox"/> Contaminated land ( <i>unexploded ordnance</i> ) <input type="checkbox"/> Environmentally relevant activities (ERA) ( <i>only if the ERA has not been devolved to a local government</i> ) <input type="checkbox"/> Fisheries – aquaculture <input type="checkbox"/> Fisheries – declared fish habitat area <input type="checkbox"/> Fisheries – marine plants <input type="checkbox"/> Fisheries – waterway barrier works <input type="checkbox"/> Hazardous chemical facilities <input type="checkbox"/> Heritage places – Queensland heritage place ( <i>on or near a Queensland heritage place</i> ) <input type="checkbox"/> Infrastructure-related referrals – designated premises <input type="checkbox"/> Infrastructure-related referrals – state transport infrastructure <input type="checkbox"/> Infrastructure-related referrals – State transport corridor and future State transport corridor <input type="checkbox"/> Infrastructure-related referrals – State-controlled transport tunnels and future state-controlled transport tunnels <input type="checkbox"/> Infrastructure-related referrals – near a state-controlled road intersection <input type="checkbox"/> Koala habitat in SEQ region – interfering with koala habitat in koala habitat areas outside koala priority areas <input type="checkbox"/> Koala habitat in SEQ region – key resource areas <input type="checkbox"/> Ports – Brisbane core port land – near a State transport corridor or future State transport corridor <input type="checkbox"/> Ports – Brisbane core port land – environmentally relevant activity (ERA) <input type="checkbox"/> Ports – Brisbane core port land – tidal works or work in a coastal management district <input type="checkbox"/> Ports – Brisbane core port land – hazardous chemical facility <input type="checkbox"/> Ports – Brisbane core port land – taking or interfering with water <input type="checkbox"/> Ports – Brisbane core port land – referable dams <input type="checkbox"/> Ports – Brisbane core port land – fisheries

- Ports – Land within Port of Brisbane’s port limits (*below high-water mark*)
- SEQ development area
- SEQ regional landscape and rural production area or SEQ rural living area – tourist activity or sport and recreation activity
- SEQ regional landscape and rural production area or SEQ rural living area – community activity
- SEQ regional landscape and rural production area or SEQ rural living area – indoor recreation
- SEQ regional landscape and rural production area or SEQ rural living area – urban activity
- SEQ regional landscape and rural production area or SEQ rural living area – combined use
- SEQ northern inter-urban break – tourist activity or sport and recreation activity
- SEQ northern inter-urban break – community activity
- SEQ northern inter-urban break – indoor recreation
- SEQ northern inter-urban break – urban activity
- SEQ northern inter-urban break – combined use
- Tidal works or works in a coastal management district
- Reconfiguring a lot in a coastal management district or for a canal
- Erosion prone area in a coastal management district
- Urban design
- Water-related development – taking or interfering with water
- Water-related development – removing quarry material (*from a watercourse or lake*)
- Water-related development – referable dams
- Water-related development –levees (*category 3 levees only*)
- Wetland protection area

**Matters requiring referral to the local government:**

- Airport land
- Environmentally relevant activities (ERA) (*only if the ERA has been devolved to local government*)
- Heritage places – Local heritage places

**Matters requiring referral to the Chief Executive of the distribution entity or transmission entity:**

- Infrastructure-related referrals – Electricity infrastructure

**Matters requiring referral to:**

- The **Chief Executive of the holder of the licence**, if not an individual
- The **holder of the licence**, if the holder of the licence is an individual
- Infrastructure-related referrals – Oil and gas infrastructure

**Matters requiring referral to the Brisbane City Council:**

- Ports – Brisbane core port land

**Matters requiring referral to the Minister responsible for administering the Transport Infrastructure Act 1994:**

- Ports – Brisbane core port land (*where inconsistent with the Brisbane port LUP for transport reasons*)
- Ports – Strategic port land

**Matters requiring referral to the relevant port operator, if applicant is not port operator:**

- Ports – Land within Port of Brisbane’s port limits (*below high-water mark*)

**Matters requiring referral to the Chief Executive of the relevant port authority:**

- Ports – Land within limits of another port (*below high-water mark*)

**Matters requiring referral to the Gold Coast Waterways Authority:**

- Tidal works or work in a coastal management district (*in Gold Coast waters*)

**Matters requiring referral to the Queensland Fire and Emergency Service:**

- Tidal works or work in a coastal management district (*involving a marina (more than six vessel berths)*)

**18) Has any referral agency provided a referral response for this development application?**

- Yes – referral response(s) received and listed below are attached to this development application

<input type="checkbox"/> No		
Referral requirement	Referral agency	Date of referral response
Identify and describe any changes made to the proposed development application that was the subject of the referral response and this development application, or include details in a schedule to this development application (if applicable).		

## PART 6 – INFORMATION REQUEST

<b>19) Information request under the DA Rules</b>
<input checked="" type="checkbox"/> I agree to receive an information request if determined necessary for this development application <input type="checkbox"/> I do not agree to accept an information request for this development application <b>Note:</b> By not agreeing to accept an information request I, the applicant, acknowledge: <ul style="list-style-type: none"> <li>that this development application will be assessed and decided based on the information provided when making this development application and the assessment manager and any referral agencies relevant to the development application are not obligated under the DA Rules to accept any additional information provided by the applicant for the development application unless agreed to by the relevant parties</li> <li>Part 3 under Chapter 1 of the DA Rules will still apply if the application is an application listed under section 11.3 of the DA Rules or</li> <li>Part 2 under Chapter 2 of the DA Rules will still apply if the application is for state facilitated development</li> </ul> Further advice about information requests is contained in the <a href="#">DA Forms Guide</a> .

## PART 7 – FURTHER DETAILS

<b>20) Are there any associated development applications or current approvals? (e.g. a preliminary approval)</b>			
<input type="checkbox"/> Yes – provide details below or include details in a schedule to this development application <input checked="" type="checkbox"/> No			
List of approval/development application references	Reference number	Date	Assessment manager
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			

<b>21) Has the portable long service leave levy been paid? (only applicable to development applications involving building work or operational work)</b>		
<input type="checkbox"/> Yes – a copy of the receipted QLeave form is attached to this development application <input type="checkbox"/> No – I, the applicant will provide evidence that the portable long service leave levy has been paid before the assessment manager decides the development application. I acknowledge that the assessment manager may give a development approval only if I provide evidence that the portable long service leave levy has been paid <input checked="" type="checkbox"/> Not applicable (e.g. building and construction work is less than \$150,000 excluding GST)		
Amount paid	Date paid (dd/mm/yy)	QLeave levy number (A, B or E)
\$		

<b>22) Is this development application in response to a show cause notice or required as a result of an enforcement notice?</b>
<input type="checkbox"/> Yes – show cause or enforcement notice is attached <input checked="" type="checkbox"/> No

### 23) Further legislative requirements

#### **Environmentally relevant activities**

23.1) Is this development application also taken to be an application for an environmental authority for an **Environmentally Relevant Activity (ERA)** under section 115 of the *Environmental Protection Act 1994*?

- Yes – the required attachment (form ESR/2015/1791) for an application for an environmental authority accompanies this development application, and details are provided in the table below
- No

**Note:** Application for an environmental authority can be found by searching "ESR/2015/1791" as a search term at [www.qld.gov.au](http://www.qld.gov.au). An ERA requires an environmental authority to operate. See [www.business.qld.gov.au](http://www.business.qld.gov.au) for further information.

Proposed ERA number:		Proposed ERA threshold:	
Proposed ERA name:			

- Multiple ERAs are applicable to this development application and the details have been attached in a schedule to this development application.

#### **Hazardous chemical facilities**

23.2) Is this development application for a **hazardous chemical facility**?

- Yes – *Form 536: Notification of a facility exceeding 10% of schedule 15 threshold* is attached to this development application
- No

**Note:** See [www.business.qld.gov.au](http://www.business.qld.gov.au) for further information about hazardous chemical notifications.

#### **Clearing native vegetation**

23.3) Does this development application involve **clearing native vegetation** that requires written confirmation that the chief executive of the *Vegetation Management Act 1999* is satisfied the clearing is for a relevant purpose under section 22A of the *Vegetation Management Act 1999*?

- Yes – this development application includes written confirmation from the chief executive of the *Vegetation Management Act 1999* (s22A determination)
- No

**Note:** 1. Where a development application for operational work or material change of use requires a s22A determination and this is not included, the development application is prohibited development.  
2. See <https://www.qld.gov.au/environment/land/vegetation/applying> for further information on how to obtain a s22A determination.

#### **Environmental offsets**

23.4) Is this development application taken to be a prescribed activity that may have a significant residual impact on a **prescribed environmental matter** under the *Environmental Offsets Act 2014*?

- Yes – I acknowledge that an environmental offset must be provided for any prescribed activity assessed as having a significant residual impact on a prescribed environmental matter
- No

**Note:** The environmental offset section of the Queensland Government's website can be accessed at [www.qld.gov.au](http://www.qld.gov.au) for further information on environmental offsets.

#### **Koala habitat in SEQ Region**

23.5) Does this development application involve a material change of use, reconfiguring a lot or operational work which is assessable development under Schedule 10, Part 10 of the Planning Regulation 2017?

- Yes – the development application involves premises in the koala habitat area in the koala priority area
- Yes – the development application involves premises in the koala habitat area outside the koala priority area
- No

**Note:** If a koala habitat area determination has been obtained for this premises and is current over the land, it should be provided as part of this development application. See koala habitat area guidance materials at [www.desi.qld.gov.au](http://www.desi.qld.gov.au) for further information.

### Water resources

23.6) Does this development application involve **taking or interfering with underground water through an artesian or subartesian bore, taking or interfering with water in a watercourse, lake or spring, or taking overland flow water under the *Water Act 2000***?

Yes – the relevant template is completed and attached to this development application and I acknowledge that a relevant authorisation or licence under the *Water Act 2000* may be required prior to commencing development

No

**Note:** Contact the Department of Resources at [www.resources.qld.gov.au](http://www.resources.qld.gov.au) for further information.

DA templates are available from [planning.statedevelopment.qld.gov.au](http://planning.statedevelopment.qld.gov.au). If the development application involves:

- Taking or interfering with underground water through an artesian or subartesian bore: complete DA Form 1 Template 1
- Taking or interfering with water in a watercourse, lake or spring: complete DA Form 1 Template 2
- Taking overland flow water: complete DA Form 1 Template 3.

### Waterway barrier works

23.7) Does this application involve **waterway barrier works**?

Yes – the relevant template is completed and attached to this development application

No

DA templates are available from [planning.statedevelopment.qld.gov.au](http://planning.statedevelopment.qld.gov.au). For a development application involving waterway barrier works, complete DA Form 1 Template 4.

### Marine activities

23.8) Does this development application involve **aquaculture, works within a declared fish habitat area or removal, disturbance or destruction of marine plants**?

Yes – an associated resource allocation authority is attached to this development application, if required under the *Fisheries Act 1994*

No

**Note:** See guidance materials at [www.daf.qld.gov.au](http://www.daf.qld.gov.au) for further information.

### Quarry materials from a watercourse or lake

23.9) Does this development application involve the **removal of quarry materials from a watercourse or lake under the *Water Act 2000***?

Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development

No

**Note:** Contact the Department of Resources at [www.resources.qld.gov.au](http://www.resources.qld.gov.au) and [www.business.qld.gov.au](http://www.business.qld.gov.au) for further information.

### Quarry materials from land under tidal waters

23.10) Does this development application involve the **removal of quarry materials from land under tidal water under the *Coastal Protection and Management Act 1995***?

Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development

No

**Note:** Contact the Department of Environment, Science and Innovation at [www.desi.qld.gov.au](http://www.desi.qld.gov.au) for further information.

### Referable dams

23.11) Does this development application involve a **referable dam** required to be failure impact assessed under section 343 of the *Water Supply (Safety and Reliability) Act 2008* (the *Water Supply Act*)?

Yes – the 'Notice Accepting a Failure Impact Assessment' from the chief executive administering the *Water Supply Act* is attached to this development application

No

**Note:** See guidance materials at [www.resources.qld.gov.au](http://www.resources.qld.gov.au) for further information.

### **Tidal work or development within a coastal management district**

23.12) Does this development application involve **tidal work or development in a coastal management district**?

- Yes – the following is included with this development application:
- Evidence the proposal meets the code for assessable development that is prescribed tidal work (*only required if application involves prescribed tidal work*)
  - A certificate of title

No

**Note:** See guidance materials at [www.desi.qld.gov.au](http://www.desi.qld.gov.au) for further information.

### **Queensland and local heritage places**

23.13) Does this development application propose development on or adjoining a place entered in the **Queensland heritage register** or on a place entered in a local government's **Local Heritage Register**?

Yes – details of the heritage place are provided in the table below

No

**Note:** See guidance materials at [www.desi.qld.gov.au](http://www.desi.qld.gov.au) for information requirements regarding development of Queensland heritage places. For a heritage place that has cultural heritage significance as a local heritage place and a Queensland heritage place, provisions are in place under the Planning Act 2016 that limit a local categorising instrument from including an assessment benchmark about the effect or impact of, development on the stated cultural heritage significance of that place. See guidance materials at [www.planning.statedevelopment.qld.gov.au](http://www.planning.statedevelopment.qld.gov.au) for information regarding assessment of Queensland heritage places.

Name of the heritage place:

Place ID:

### **Decision under section 62 of the Transport Infrastructure Act 1994**

23.14) Does this development application involve new or changed access to a state-controlled road?

Yes – this application will be taken to be an application for a decision under section 62 of the *Transport Infrastructure Act 1994* (subject to the conditions in section 75 of the *Transport Infrastructure Act 1994* being satisfied)

No

### **Walkable neighbourhoods assessment benchmarks under Schedule 12A of the Planning Regulation**

23.15) Does this development application involve reconfiguring a lot into 2 or more lots in certain residential zones (except rural residential zones), where at least one road is created or extended?

Yes – Schedule 12A is applicable to the development application and the assessment benchmarks contained in schedule 12A have been considered

No

**Note:** See guidance materials at [www.planning.statedevelopment.qld.gov.au](http://www.planning.statedevelopment.qld.gov.au) for further information.

## PART 8 – CHECKLIST AND APPLICANT DECLARATION

### 24) Development application checklist

I have identified the assessment manager in question 15 and all relevant referral requirement(s) in question 17

Yes

**Note:** See the *Planning Regulation 2017* for referral requirements

If building work is associated with the proposed development, Parts 4 to 6 of [DA Form 2 – Building work details](#) have been completed and attached to this development application

Yes

Not applicable

Supporting information addressing any applicable assessment benchmarks is with the development application

**Note:** This is a mandatory requirement and includes any relevant templates under question 23, a planning report and any technical reports required by the relevant categorising instruments (e.g. local government planning schemes, State Planning Policy, State Development Assessment Provisions). For further information, see [DA Forms Guide: Planning Report Template](#).

Yes

Relevant plans of the development are attached to this development application

**Note:** Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).

Yes

The portable long service leave levy for QLeave has been paid, or will be paid before a development permit is issued (see 21)

Yes

Not applicable



**25) Applicant declaration**

- By making this development application, I declare that all information in this development application is true and correct
- Where an email address is provided in Part 1 of this form, I consent to receive future electronic communications from the assessment manager and any referral agency for the development application where written information is required or permitted pursuant to sections 11 and 12 of the *Electronic Transactions Act 2001*

*Note: It is unlawful to intentionally provide false or misleading information.*

**Privacy** – Personal information collected in this form will be used by the assessment manager and/or chosen assessment manager, any relevant referral agency and/or building certifier (including any professional advisers which may be engaged by those entities) while processing, assessing and deciding the development application. All information relating to this development application may be available for inspection and purchase, and/or published on the assessment manager’s and/or referral agency’s website.

Personal information will not be disclosed for a purpose unrelated to the *Planning Act 2016*, Planning Regulation 2017 and the DA Rules except where:

- such disclosure is in accordance with the provisions about public access to documents contained in the *Planning Act 2016* and the Planning Regulation 2017, and the access rules made under the *Planning Act 2016* and Planning Regulation 2017; or
- required by other legislation (including the *Right to Information Act 2009*); or
- otherwise required by law.

This information may be stored in relevant databases. The information collected will be retained as required by the *Public Records Act 2002*.

**PART 9 – FOR COMPLETION OF THE ASSESSMENT MANAGER – FOR OFFICE USE ONLY**

Date received:  Reference number(s):

**Notification of engagement of alternative assessment manager**

Prescribed assessment manager	
Name of chosen assessment manager	
Date chosen assessment manager engaged	
Contact number of chosen assessment manager	
Relevant licence number(s) of chosen assessment manager	

**QLeave notification and payment**

*Note: For completion by assessment manager if applicable*

Description of the work	
QLeave project number	
Amount paid (\$)	Date paid (dd/mm/yy)
Date receipted form sighted by assessment manager	
Name of officer who sighted the form	

## Attachment 2

### Title Search



DEVELOPMENT & PROPERTY SERVICES

Queensland Titles Registry Pty Ltd  
ABN 23 648 568 101

<b>Title Reference:</b> 50108844	<b>Search Date:</b> 05/05/2026 10:26
<b>Date Title Created:</b> 15/01/1996	<b>Request No:</b> 56005131
<b>Previous Title:</b> 21464177	

#### ESTATE AND LAND

Estate in Fee Simple

LOT 17 REGISTERED PLAN 895020  
Local Government: DOUGLAS

#### REGISTERED OWNER

Dealing No: 724753852 12/03/2026  
FPT PTY LIMITED A.C.N. 165 196 814

#### EASEMENTS, ENCUMBRANCES AND INTERESTS

1. Rights and interests reserved to the Crown by  
Deed of Grant No. 20122206 (POR 135)  
Deed of Grant No. 20122207 (POR 135)

#### ADMINISTRATIVE ADVICES

NIL

#### UNREGISTERED DEALINGS

NIL

\*\* End of Current Title Search \*\*

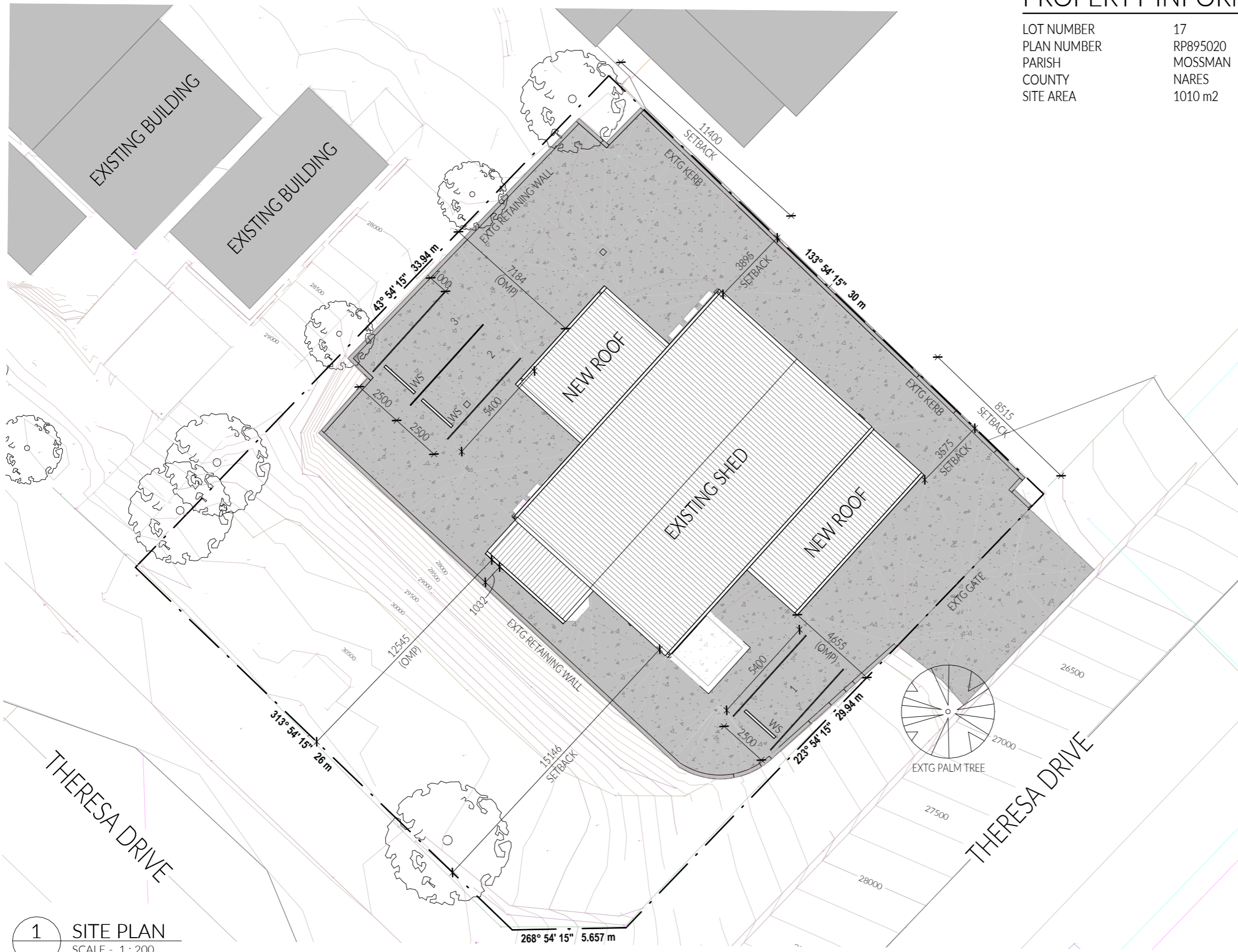
## **Attachment 3**

### **Plans of Development (Architectural Plans)**

*Prepared by Informed Architects*



DEVELOPMENT & PROPERTY SERVICES



### PROPERTY INFORMATION

LOT NUMBER 17  
 PLAN NUMBER RP895020  
 PARISH MOSSMAN  
 COUNTY NARES  
 SITE AREA 1010 m<sup>2</sup>

### LEGEND - PHASES

- EXISTING
- DEMOLISHED
- PROPOSED

LEGEND	
(OMP)	SETBACK - OUTER MOST PROJECTION

TOTAL SITE AREA	
SITE AREA	1010 m <sup>2</sup>

GFA SCHEDULE		
BUILDING AREA	162 m <sup>2</sup>	16%
FREE AREA	848 m <sup>2</sup>	84%
Grand total	1010 m <sup>2</sup>	100%

LANDSCAPE AREA		
LANDSCAPE AREA	360 m <sup>2</sup>	36%
NON-LANDSCAPE AREA	650 m <sup>2</sup>	64%
Grand total	1010 m <sup>2</sup>	100%

PLOTTED ON : 3/06/2026 10:23:56 AM

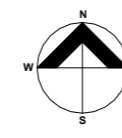
**1** SITE PLAN  
 SCALE - 1 : 200



251 Kamerunga Road FRESHWATER  
 CAIRNS QLD 4870  
 0437 735 693  
 admin@informedarchitects.com.au  
 www.informedarchitects.com.au

AMENDMENTS:	DATE:
P2 ISSUE TO CONSULTANT	13/04/26
P3 SURVEY ADDED	12/05/26
P4 CAR PARKING	01/06/26

DO NOT SCALE DRAWINGS.  
 USE FIGURED DIMENSIONS ONLY.  
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 VERIFY ALL DIMENSIONS AND SITE CONDITIONS PRIOR TO COMMENCING WORK. THIS DOCUMENT IS AND SHALL REMAIN THE PROPERTY OF INFORMED ARCHITECTS. UNAUTHORISED USE OF THIS DOCUMENT IN ANY WAY IS PROHIBITED.



DRAWN: RG APPROVED: DI DATE: JUN 2026 SCALE: As indicated SIZE: A3



PROJECT: 15 THERESA DR, MOSSMAN QLD 4873  
 MOSSMAN COMMUNITY FUNERALS




FOR: PROPEL FUNERAL PARTNERS

DWG: SITE PLAN

DWG No. 0071-DD - A-001 : **PRELIMINARY**

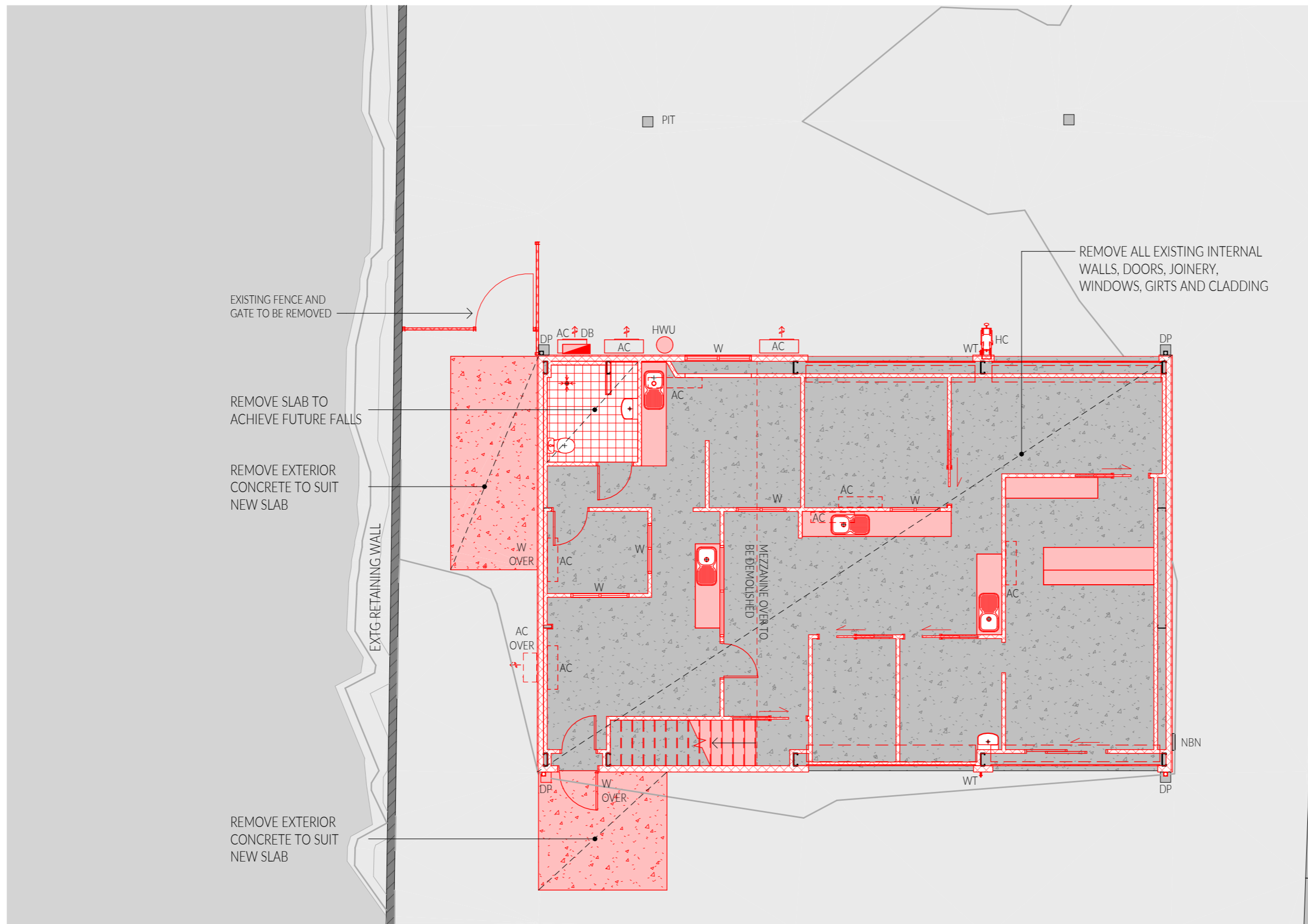
ISSUE: P4

LEGEND - PHASES

	EXISTING
	DEMOLISHED
	PROPOSED

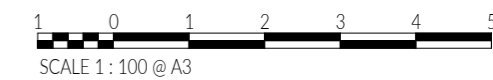
LEGEND - ABBREV

AC	AIR CONDITIONER
CF	CONCRETE FINISH
DB	DISTRIBUTION BOARD
DP	DOWNPIPE
FT	FENCE TYPE
FW	FLOOR WASTE-AS SPEC.
HC	HOSE COCK
HWU	HOT WATER UNIT
NBN	NETWORK
SK	SINK
TL	TILE
W	WINDOW
WC	TOILET
WT	WALL TYPE



PLOTTED ON : 3/06/2026 10:23:56 AM

**1 FLOOR PLAN**  
A-701 SCALE - 1 : 100



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AMENDMENTS:	DATE:
P1 SITE MEASURE	17/03/26
P2 ISSUE TO CONSULTANT	13/04/26
P3 SURVEY ADDED	12/05/26

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PROJECT: 15 THERESA DR, MOSSMAN QLD 4873  
MOSSMAN COMMUNITY FUNERALS

FOR: PROPEL FUNERAL PARTNERS

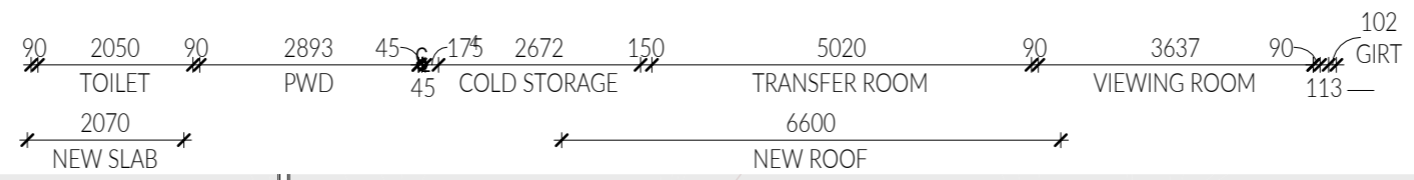
DWG: FLOOR PLAN - EXISTING & DEMOLITION

DWG No. 0071-DD - A-101 : **PRELIMINARY**

ISSUE: P3

DRAWN: RG APPROVED: DI DATE: JUN 2026 SCALE: 1 : 100 SIZE: A3





**LEGEND - PHASES**

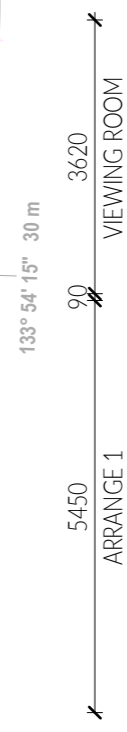
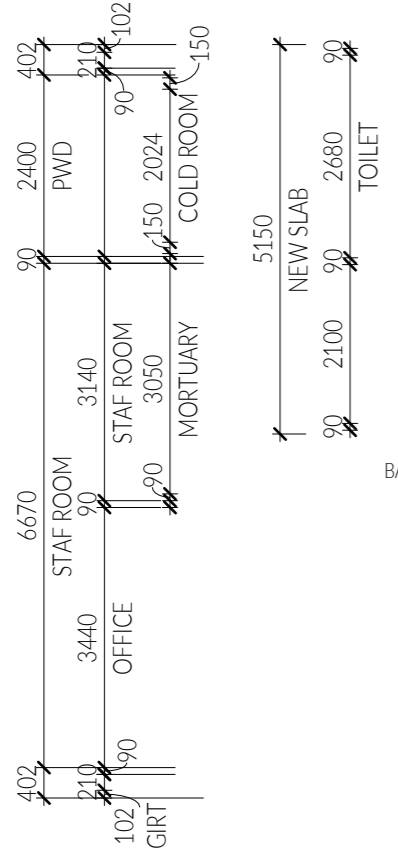
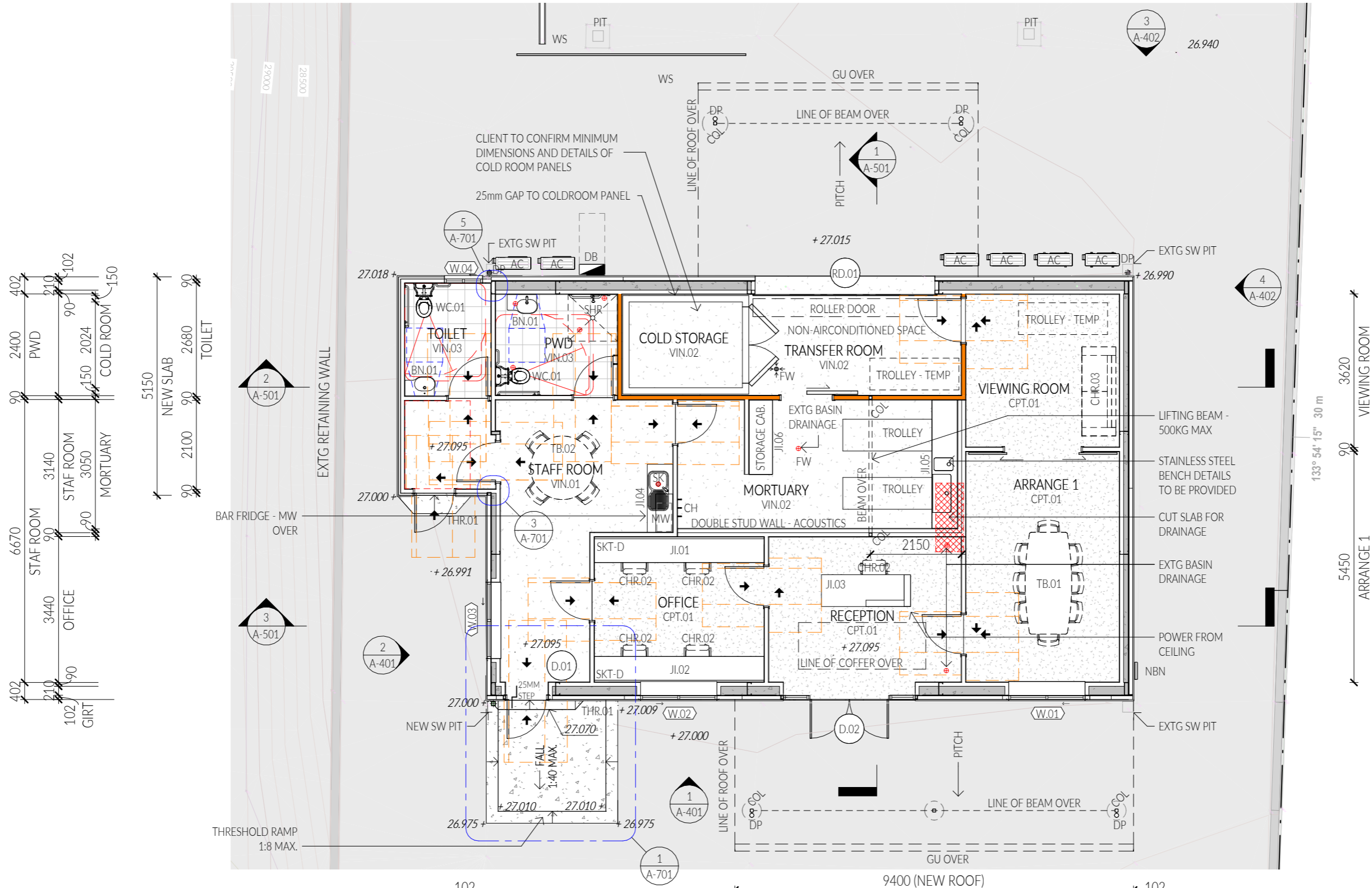
- EXISTING
- DEMOLISHED
- PROPOSED

- LEGEND**
- + DRAINAGE POINT
  - CONSIDER TAKING WALL TO UNDERSIDE OF ROOF - TO MITIGATE HUMID AIR FROM CONDITIONED SPACE

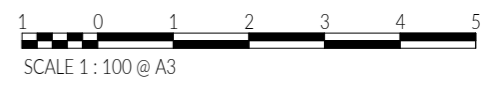
**LEGEND - ABBREV**

AC	AIR CONDITIONER
BN	BASIN
CH	COAT HOOK
CHR	CHAIR
CL	CEILING LINING
CLD	CLADDING
COL	COLUMN
CPT	CARPET
D	DOOR
DB	DISTRIBUTION BOARD
DP	DOWNSPIPE
FW	FLOOR WASTE-AS SPEC.
GU	GUTTER
JI	JOINERY
MW	MICROWAVE
NBN	NETWORK
PBD	PLASTERBOARD
RSH	ROOF SHEETING
SHR	SHOWER
SK	SINK
SKT-D	SKIRTING DUCK
TB	TABLE
THR	THRESHOLD
VIN	VINYL
WC	TOILET
WS	WHEELSTOP
WT	WALL TYPE

NB  
 -CONFIGURATION OF PWD TO BE CONFIRMED IN DETAIL DESING.  
 -TUNDISH: ALL AC'S TO BE POSITIONED TO COORDINATE WITH EXISTING DRAINAGE (WHERE PRACTICABLE)



**1 FLOOR PLAN - PROPOSED**  
 SCALE - 1 : 100



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 admin@informedarchitects.com.au  
 www.informedarchitects.com.au

AMENDMENTS:	DATE:
P3 SURVEY ADDED	12/05/26
P4 PWD	19/05/26
P5 CLIENT ISSUE	03/06/26

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PROJECT: 15 THERESA DR, MOSSMAN QLD 4873  
 MOSSMAN COMMUNITY FUNERALS  
 FOR: PROPEL FUNERAL PARTNERS  
 DWG: FLOOR PLAN - PROPOSED  
 DWG No. 0071-DD - A-103 : **PRELIMINARY** ISSUE: P5

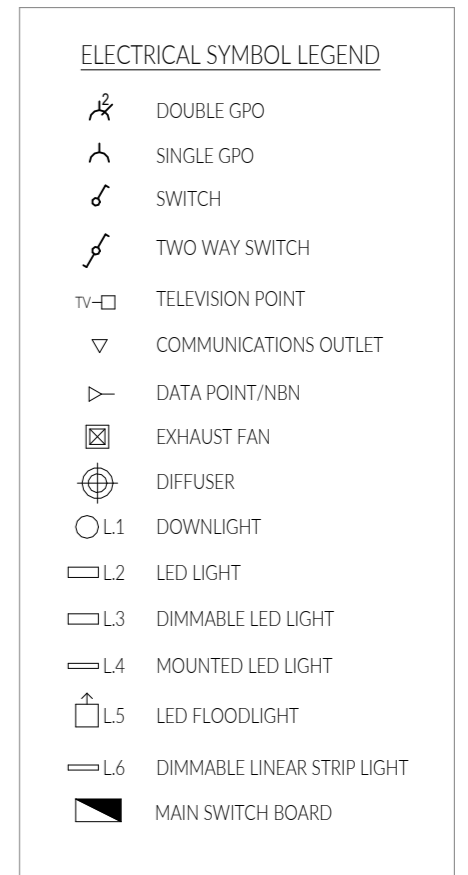
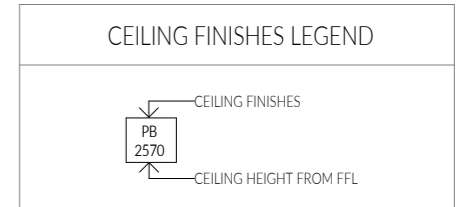
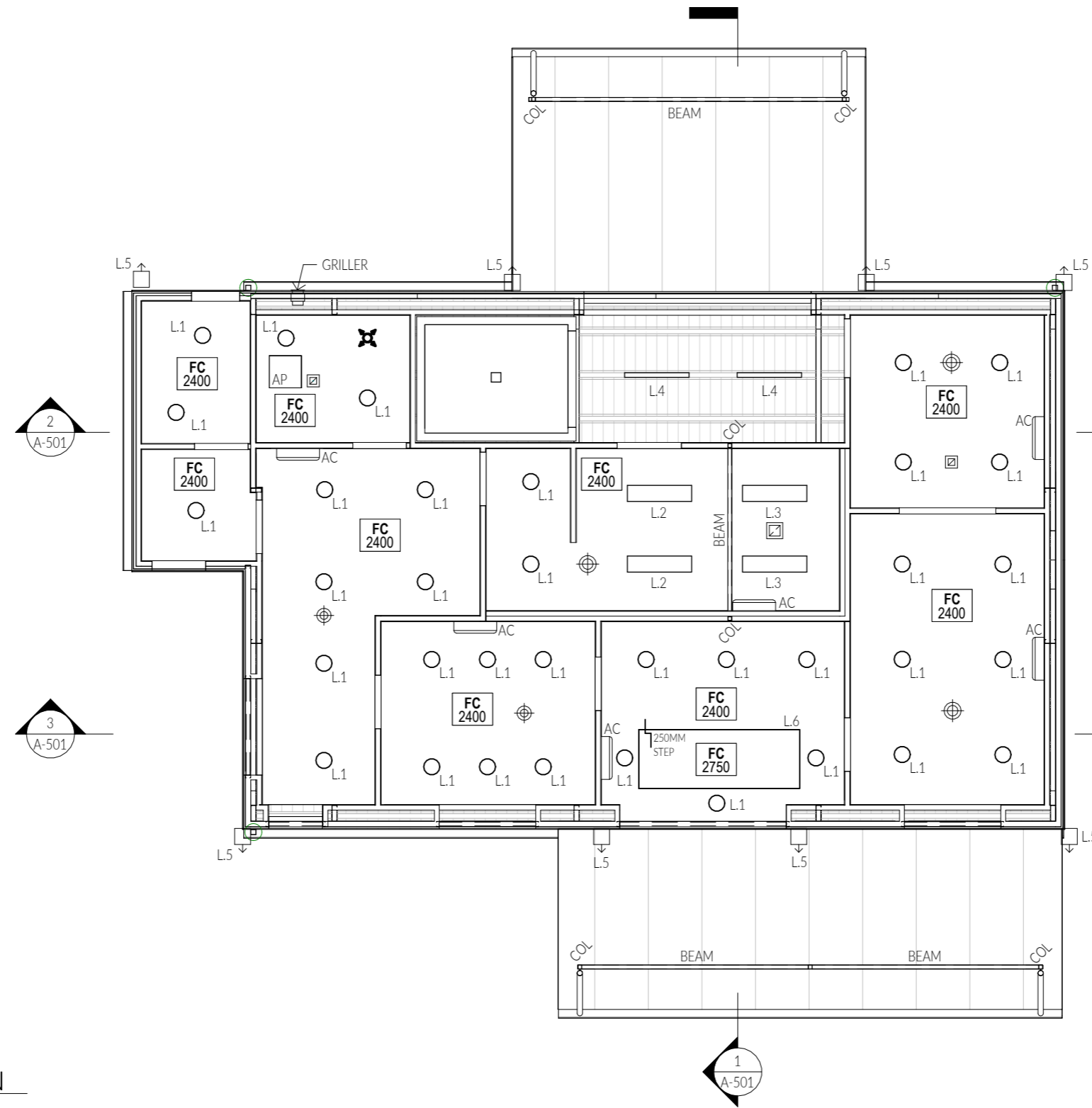
PLOTTED ON : 3/06/2026 10:24:01 AM

LEGEND - ABBREV	
AP	ACCESS PANEL
CL	CEILING LINING
CLD	CLADDING
COL	COLUMN
DP	DOWNPIPE

LEGEND - ABBREV	
GU	GUTTER
PBD	PLASTERBOARD
RSH	ROOF SHEETING
WT	WALL TYPE

LEGEND - PHASES

-  EXISTING
-  DEMOLISHED
-  PROPOSED



NOTE: REFER TO ELECTRICAL AND MECHANICAL DOCUMENTATION FOR COMPLETE SCOPE

**1** REFLECTED CEILING PLAN  
A-701 SCALE - 1 : 100



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P1 CLIENT ISSUE	03/06/26

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PROJECT: 15 THERESA DR, MOSSMAN QLD 4873  
MOSSMAN COMMUNITY FUNERALS

FOR: PROPEL FUNERAL PARTNERS

DWG: REFLECTED CEILING PLAN

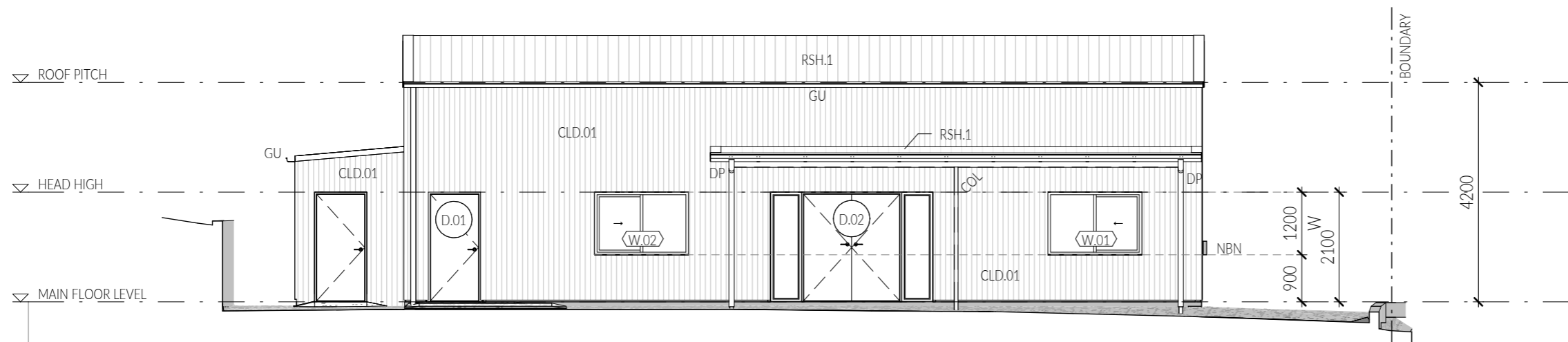
DWG No. 0071-DD - A-300 : **PRELIMINARY**

ISSUE: P1

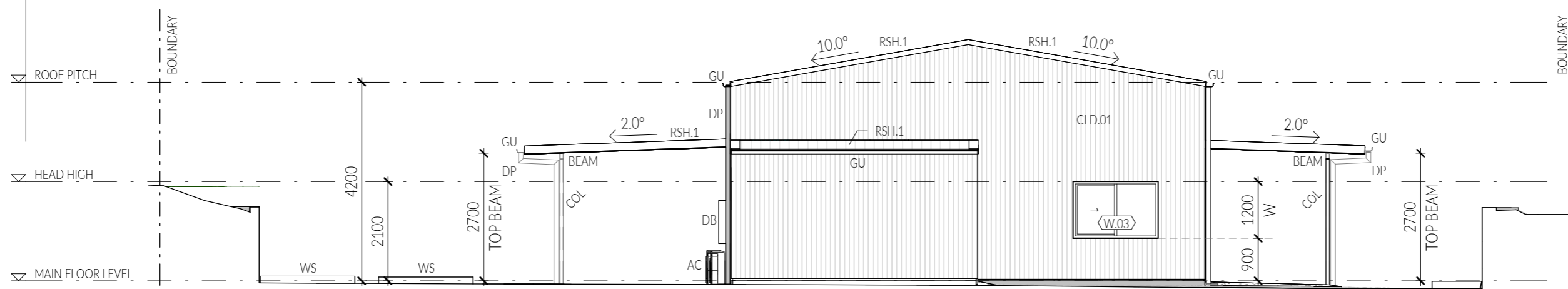
LEGEND - PHASES

- EXISTING
- DEMOLISHED
- PROPOSED

LEGEND - ABBREV	
AC	AIR CONDITIONER
AP	ACCESS PANEL
BN	BASIN
CH	COAT HOOK
CHR	CHAIR
CL	CEILING LINING
CLD	CLADDING
COL	COLUMN
CPT	CARPET
D	DOOR
DB	DISTRIBUTION BOARD
DP	DOWNPIPE
FW	FLOOR WASTE-AS SPEC.
GU	GUTTER
JI	JOINERY
MW	MICROWAVE
NBN	NETWORK
PBD	PLASTERBOARD
RSH	ROOF SHEETING
SHR	SHOWER
SK	SINK
SKT-D	SKIRTING DUCK
TB	TABLE
THR	THRESHOLD
VIN	VINYL
WC	TOILET
WS	WHEELSTOP
WT	WALL TYPE



**1** ELEVATION  
A-103 SCALE - 1 : 100



**2** ELEVATION  
A-103 SCALE - 1 : 100

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P1 ISSUE TO CONSULTANT	13/04/26
P2 CLIENT ISSUE	03/06/26

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MOSSMAN COMMUNITY FUNERALS  
FOR: PROPEL FUNERAL PARTNERS




DWG: ELEVATIONS

DWG No. 0071-DD - A-401 : **PRELIMINARY**

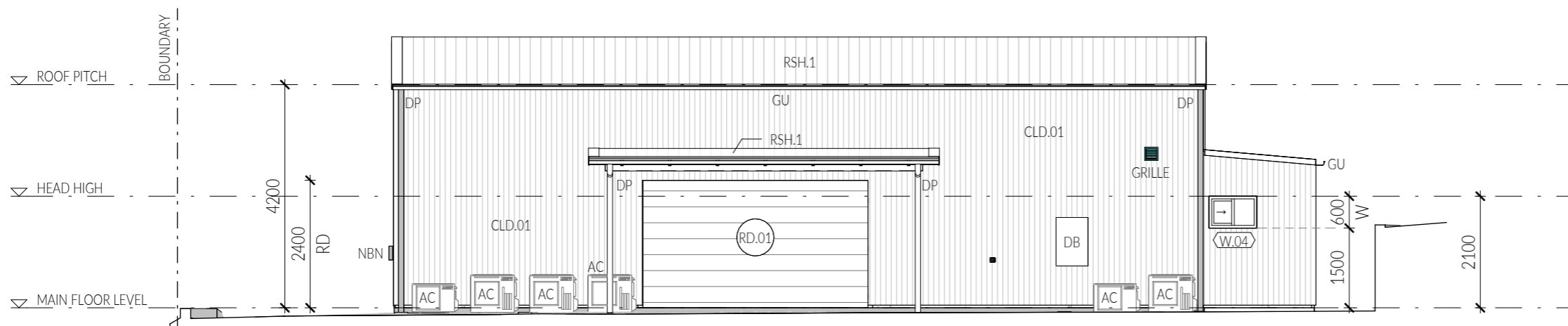
ISSUE: P2

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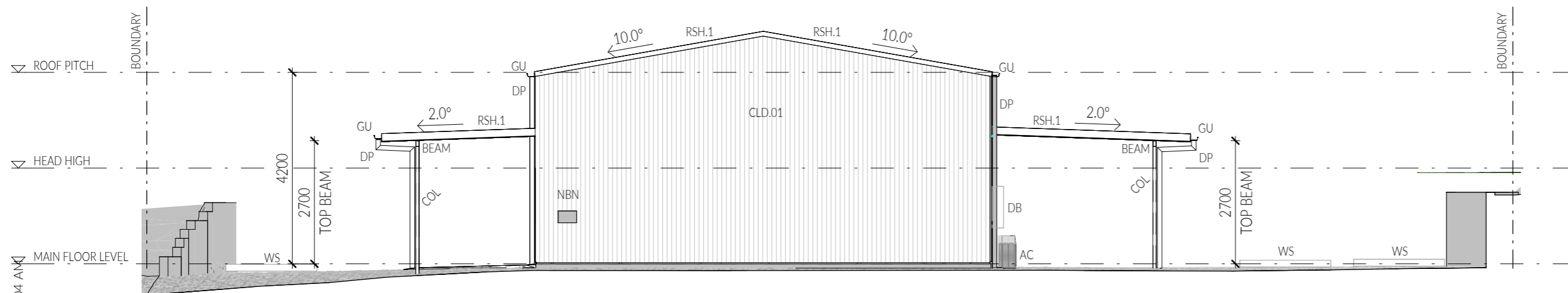
### LEGEND - PHASES

	EXISTING
	DEMOLISHED
	PROPOSED

LEGEND - ABBREV	
AC	AIR CONDITIONER
AP	ACCESS PANEL
BN	BASIN
CH	COAT HOOK
CHR	CHAIR
CL	CEILING LINING
CLD	CLADDING
COL	COLUMN
CPT	CARPET
D	DOOR
DB	DISTRIBUTION BOARD
DP	DOWNPIPE
FW	FLOOR WASTE-AS SPEC.
GU	GUTTER
JI	JOINERY
MW	MICROWAVE
NBN	NETWORK
PBD	PLASTERBOARD
RSH	ROOF SHEETING
SHR	SHOWER
SK	SINK
SKT-D	SKIRTING DUCK
TB	TABLE
THR	THRESHOLD
VIN	VINYL
WC	TOILET
WS	WHEELSTOP
WT	WALL TYPE



**3** ELEVATION  
A-103 SCALE - 1 : 100



**4** ELEVATION  
A-103 SCALE - 1 : 100

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P1 ISSUE TO CONSULTANT	13/04/26
P2 CLIENT ISSUE	03/06/26

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PROJECT: 15 THERESA DR, MOSSMAN QLD 4873  
MOSSMAN COMMUNITY FUNERALS

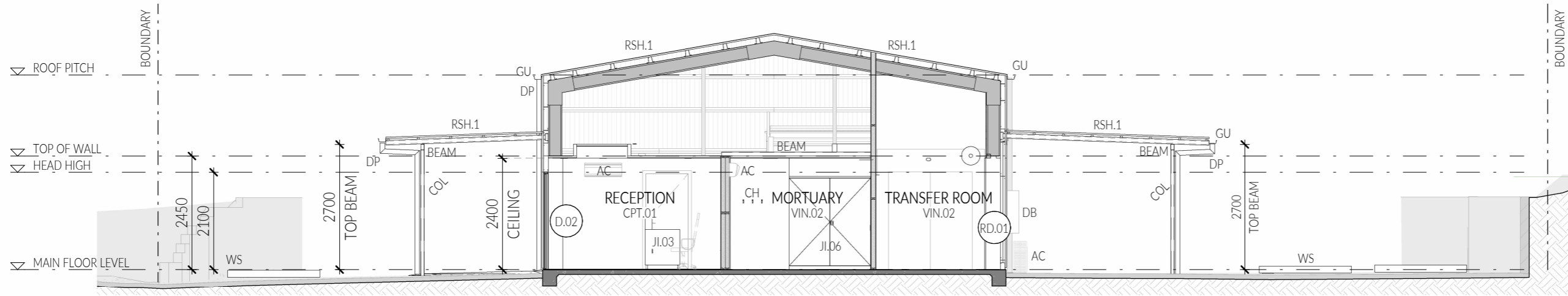
FOR: PROPEL FUNERAL PARTNERS

DWG: ELEVATIONS

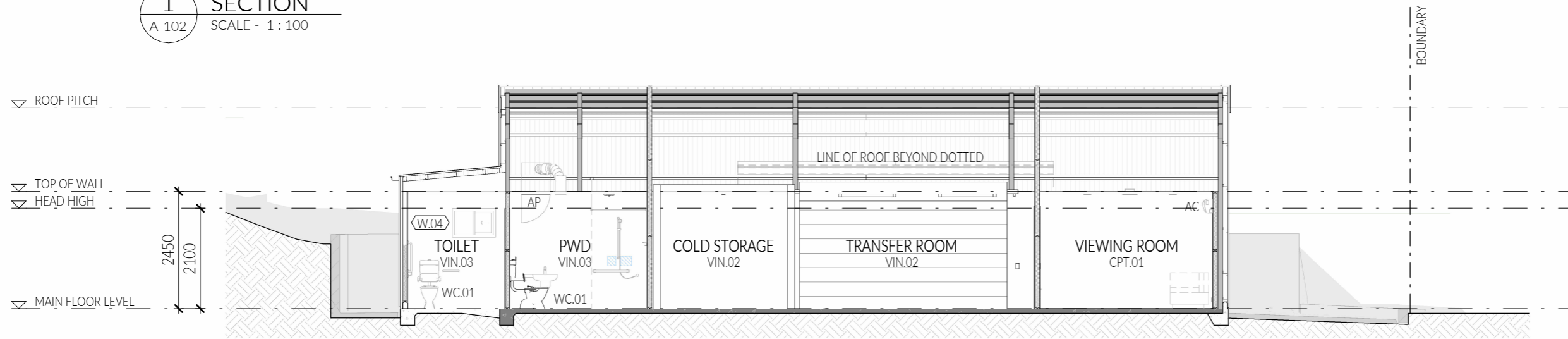
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ISSUE: P2

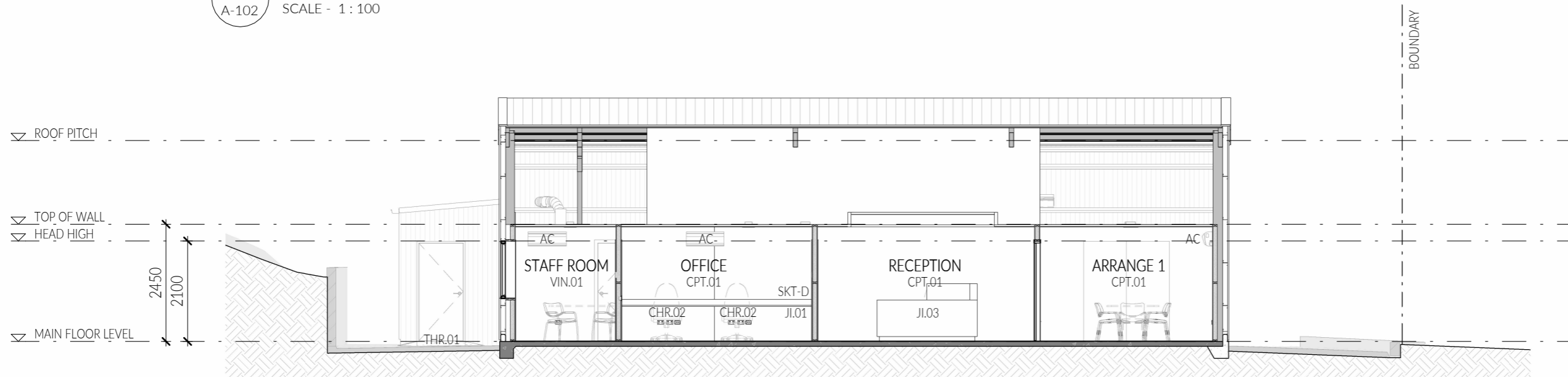
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1 SECTION  
A-102 SCALE - 1 : 100



2 SECTION  
A-102 SCALE - 1 : 100



3 SECTION  
A-102 SCALE - 1 : 100

LEGEND - PHASES

- EXISTING
- DEMOLISHED
- PROPOSED

LEGEND - ABBREV	
AC	AIR CONDITIONER
AP	ACCESS PANEL
BN	BASIN
CH	COAT HOOK
CHR	CHAIR
CL	CEILING LINING
CLD	CLADDING
COL	COLUMN
CPT	CARPET
D	DOOR
DB	DISTRIBUTION BOARD
DP	DOWNPIPE
FW	FLOOR WASTE-AS SPEC.
GU	GUTTER
JI	JOINERY
MW	MICROWAVE
PBD	PLASTERBOARD
RSH	ROOF SHEETING
SHR	SHOWER
SK	SINK
SKT-D	SKIRTING DUCK
TB	TABLE
THR	THRESHOLD
VIN	VINYL
WC	TOILET
WS	WHEELSTOP
WT	WALL TYPE

PLOTTED ON : 3/06/2026 10:52:33 AM



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AMENDMENTS:	DATE:
P1 ISSUE TO CONSULTANT	13/04/26
P2 ISSUE TO CONSULTANT	03/06/26

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PROJECT: 15 THERESA DR, MOSSMAN QLD 4873  
MOSSMAN COMMUNITY FUNERALS

FOR: PROPEL FUNERAL PARTNERS

DWG: SECTIONS

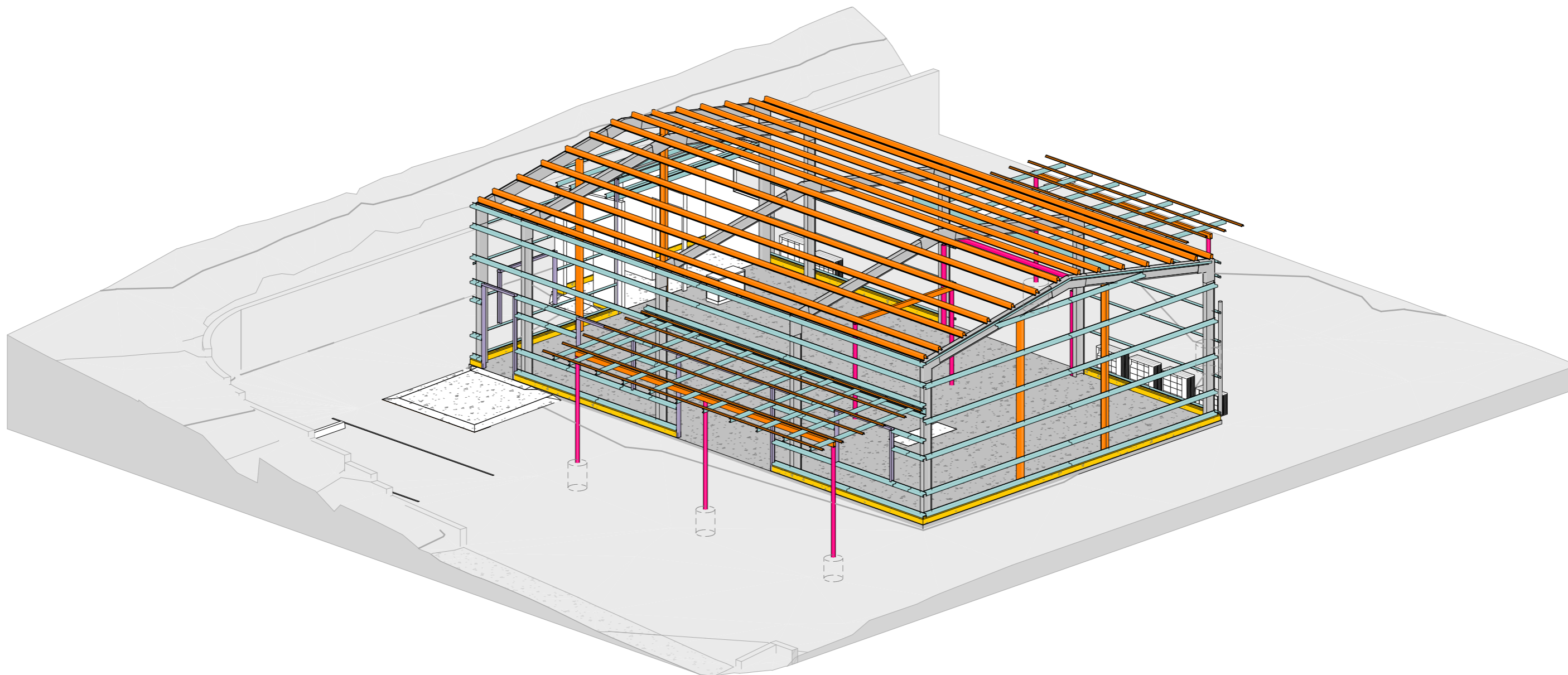
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ISSUE: P2

DRAWN: RG APPROVED: DI DATE: JUN 2026 SCALE: 1 : 100 SIZE: A3

LEGEND - PHASES

- EXISTING
- DEMOLISHED
- PROPOSED



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**1** 3D BUILDING STRUCTURE  
SCALE -



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AMENDMENTS:	DATE:
P1 CLIENT ISSUE	03/06/26

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MOSSMAN COMMUNITY FUNERALS

FOR: PROPEL FUNERAL PARTNERS

DWG: 3D - STRUCTURE

DWG No. 0071-DD - A-901 : **PRELIMINARY**

ISSUE: P1

DRAWN: RG APPROVED: DI DATE: JUN 2026 SCALE: SIZE: A3

**Attachment 4**

**Planning Report**

*Prepared by Kelly Reaston Development and Property Services*

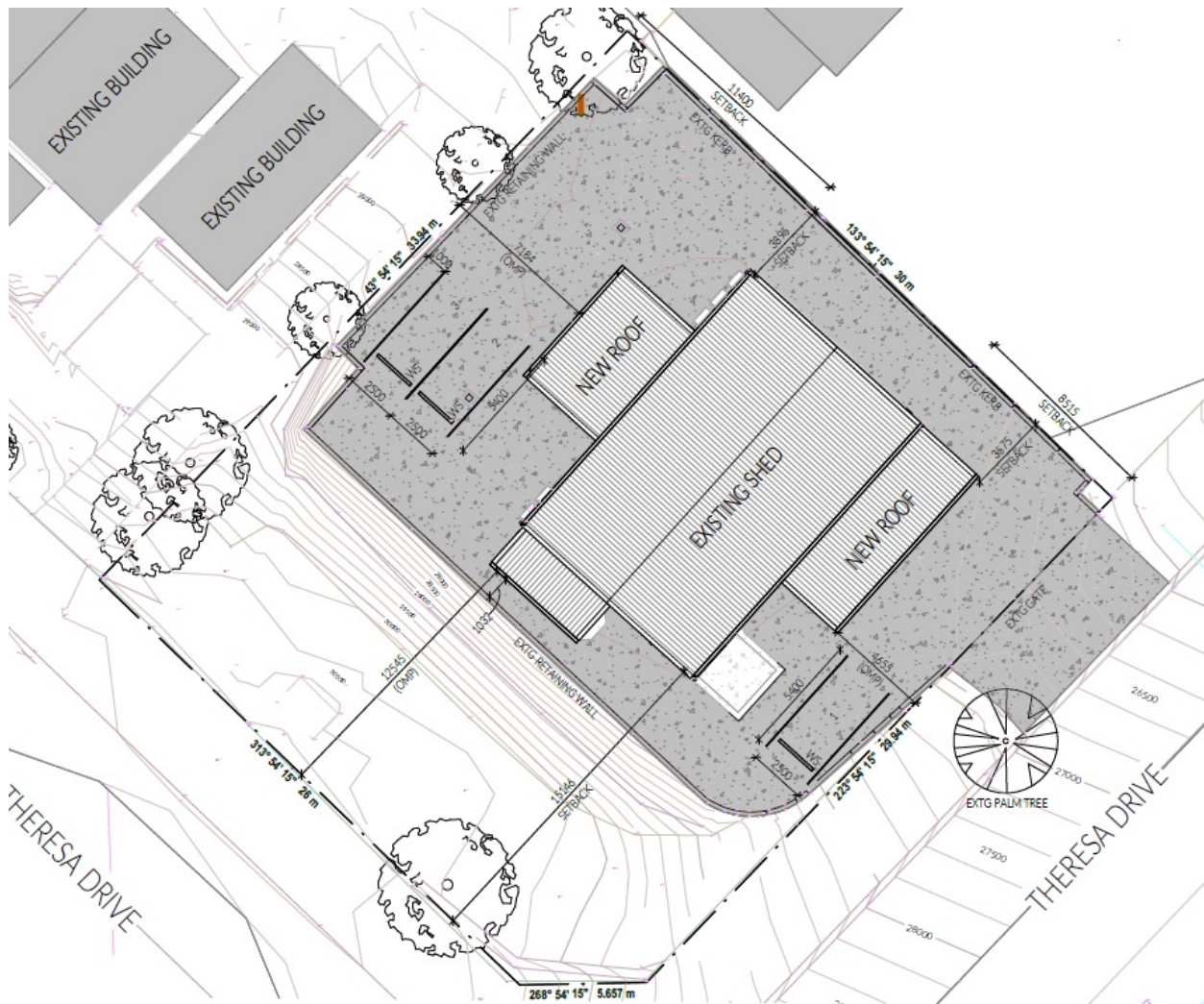


DEVELOPMENT & PROPERTY SERVICES

*KELLY REASTON DEVELOPMENT  
& PROPERTY SERVICES*

# PLANNING REPORT

JUNE 2026



Development Application for a  
Development Permit for a Material  
Change of Use for a Funeral Parlour

*PREPARED FOR  
FPT PTY LTD*

15 Theresa Drive, Mossman

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This report has been prepared relying on information that was current at the time of preparation. The material within this report has been prepared for our client and is for the purpose of statutory assessment by the relevant Local Authority.

The material should not be relied upon by any third parties or for any other purpose outside the intended scope without consulting the authors.

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1	June 2026	Kelly Reaston	Kelly Reaston
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## 1. EXECUTIVE SUMMARY

Kelly Reaston Development and Property Services has been engaged to prepare a Development Application seeking a Development Permit for a Material Change of Use for a Funeral Parlour over land located at 15 Theresa Drive, Mossman.

The subject site is included within the Industry Zone under the Douglas Shire Planning Scheme 2018 v1 and the proposed Funeral Parlour is identified as an Impact Assessable land use. As such, the application is required to be assessed against the Strategic Framework, Industry Zone Code and all other relevant provisions of the Planning Scheme, with Council afforded the opportunity to consider the broader planning merits and community benefits of the proposal.

The application is lodged on behalf of FPT Pty Ltd, the owner and operator of Mossman and Port Douglas Community Funerals. The business has been operating within Mossman for many years and provides an essential community service to residents throughout Mossman, Port Douglas, the Daintree and surrounding districts.

The proposal seeks to establish a purpose designed funeral facility at 15 Theresa Drive and relocate the primary funeral operations currently undertaken at the adjoining Sawmill Street premises. The facility will accommodate funeral bookings, consultations with families, administration functions, preparation of deceased persons, mortuary facilities, body storage and the coordination and management of funeral services.

The proposal represents a logical expansion of an existing and established local business. Importantly, the development does not introduce an unfamiliar or incompatible use into the locality. Rather, it consolidates and modernises an existing funeral service operation on land that adjoins the Applicant's existing premises and is located within an established industrial area where amenity impacts can be effectively managed.

The existing Sawmill Street property will continue to accommodate cremation activities, including the operation of the existing crematorium and pet cremation business, together with vehicle storage and associated operational functions. The proposed arrangement creates a clear separation between funeral administration and family facing services at the Theresa Drive property and cremation related activities at the Sawmill Street site, resulting in a more efficient and functional business operation.

The site is well suited to the proposed use. It benefits from established road access, proximity to the Applicant's existing operations, adequate separation from sensitive residential land uses and sufficient area to accommodate the proposed buildings, parking, manoeuvring areas and landscaping. The industrial setting also ensures that the development can operate without adversely impacting the amenity of surrounding properties.

The proposal will deliver a number of community and economic benefits, including:

- The provision of a modern and purpose-designed funeral facility servicing the Douglas Shire;
- Improved facilities and privacy for grieving families;
- Consolidation of an existing local business within Mossman;
- Continued employment opportunities within the local community;
- Efficient use of existing infrastructure and services;
- Investment in the local economy; and
- The ongoing provision of an essential community service within the Shire.

In summary, the proposed Funeral Parlour represents an appropriate and well-considered form of development that supports the continued operation and growth of an established local business whilst providing an important service to the Douglas Shire community. The proposal is compatible with the surrounding industrial land uses, delivers clear community benefits and is capable of operating without giving rise to unacceptable impacts on surrounding properties.

Given its strategic location, functional relationship with the existing business operations and the essential nature of the service provided, the proposal warrants Council's favourable consideration, subject to reasonable and relevant conditions.

## 2. APPLICATION DETAILS

### 2.1 Application Summary

**Table 1: Application Summary**

Approval Sought	Development Permit for a Material Change of Use for a "Funeral Parlour"
Applicant	FPT Pty Ltd
<b>Assessment Details</b>	
Assessment Manager	Douglas Shire Council
Development Category	Assessable development
Assessment level	Impact Assessable
Public Notification	Yes. Notification will be coordinated by Kelly Reaston Development and Property Services subject to the DA Rules v3.
<b>Relevant State Planning Instruments</b>	
Legislation	<i>Planning Act 2016</i> (Qld)
Planning Policy	State Planning Policy (July 2017)
<b>Relevant Local Planning Instruments</b>	
Planning Scheme	Douglas Shire Planning Scheme 2018 (V1)
Local Plan	Mossman Local Plan
Local Plan Precinct	Precinct 8 – Mossman South Industry Precinct
Zone	Industry
Zone Precinct	N/A
Zone Code	6.2.5 - Industry zone code
Local Plan Code	7.2.3 - Mossman local plan code
Overlay Codes	8.2.2 - Bushfire hazard overlay code 8.2.6 - Landscape values overlay code 8.2.10 - Transport network overlay code
Development Codes	9.4.1 - Access, parking and servicing code 9.4.3 - Environmental performance code 9.4.4 - Filling and excavation code 9.4.5 - Infrastructure works code 9.4.6 - Landscaping code 9.4.9 - Vegetation management code
Referral Triggers	Nil

## 2.2 Application Fee

In accordance with Council's Schedule of Fees for the 2025/26 Financial Year, the application fees have been calculated for a Development Application for a Material Change of Use for "Funeral Parlour" based on 162m<sup>2</sup> GFA, comprising

- Base fee for up to 100m<sup>2</sup> = \$1,949.00
- Plus additional fee of \$408.00 per 100m<sup>2</sup>, or part thereof, up to 2,000m<sup>2</sup> = \$408.00
- Total Fee **\$2,357.00**

## 2.3 Supporting Documentation

**Table 2: Supporting Documentation**

Document	Company	Reference	Issue	Date
Planning Report	KRDPS	2026-001 Propel Funeral Partners Ltd	1	June 2026
Architectural Plans	Informed Architects	Mossman Community Funerals	-	June 2026

Plans of Development are provided as **Attachment 3**.

## 3. SITE AND SURROUNDS

### 3.1 Site Description

**Table 3: Site Description**

Registered Landowners	FPT Pty Ltd
Site Location	15 Theresa Drive, Mossman
Real Property Description	Lot 17 on RP895020
Site Area	1,010m <sup>2</sup>
Street Frontage (Approx.)	Corner site – approximate total frontage of 61.6m to Theresa Drive
Tenure	Freehold
Easements/Encumbrances	Nil
Local Government Authority	Douglas Shire Council



Figure 1a - Site Location (Source: Queensland Globe, June 2026)

### 3.2 Site Analysis

**Table 4: Site Analysis**

Current Uses	The site was formally used as a veterinary clinic which has ceased operating. The site contains an existing Shed.
Topography	The site is gently elevated relative to surrounding land and has been subject to previous earthworks, resulting in a series of benched building platforms. The existing Shed is located within the lower benched portion of the site, taking advantage of an existing level area and minimising the need for additional earthworks or disturbance.
Waterways	Nil.
Vegetation	No native vegetation is present. The site contains some landscaping attributed to the former use.
Landslip	The site is not subject to the Potential landslide hazard overlay code.
EMR/CLR	To the best of knowledge, the site is not registered on either the EMR or CLR.
Heritage Places	The site is not on, or adjacent to, a local or State heritage place.

### 3.4 Infrastructure and Services

Given the historical use of the site, it is understood that the property is appropriately serviced by existing reticulated infrastructure networks, including water, sewer and electricity services. The site has previously accommodated development of a comparable or greater intensity and, as such, existing infrastructure has been established to support development activities on the land.

Whilst a detailed civil engineering assessment has not been undertaken as part of this application, the nature and scale of the proposed development are such that it is not anticipated to place an unreasonable demand on existing infrastructure networks. Having regard to the former use of the site and the modest servicing requirements associated with the proposal, it is expected that sufficient capacity exists within the relevant networks to accommodate the development without adverse impacts on surrounding land or external infrastructure systems.

Accordingly, the proposal is not expected to result in the need for significant upgrades to existing reticulated infrastructure and can be adequately serviced by the infrastructure available to the site.

**Table 5: Infrastructure and Services**

Access and Parking	<p>Vehicular access to the site is gained via the existing crossover within Theresa Drive, adjacent the north-eastern side boundary. No changes are proposed.</p> <p>Pedestrian access is also provided from this crossover.</p> <p>On-site parking is provided at the front and rear of the site. With considerable road frontage, onstreet parking is available adjacent the site.</p>
Water Supply	<p>The site is located within an established urban area serviced by Council's reticulated water network, with existing water mains located within Theresa Drive. This provides a readily accessible and reliable supply for the proposed development.</p>
Wastewater	<p>The site is connected to Council's reticulated sewerage network.</p>
Stormwater	<p>Stormwater will be directed to existing stormwater infrastructure within Theresa Drive.</p>
Earthworks and Site Levels	<p>The site contains existing benching. No additional earthworks are required.</p>
Electricity and Telecommunications	<p>The site is serviced by existing underground electricity infrastructure within Theresa Drive, with an existing connection already in place. It is understood that the site is also connected to telecommunications.</p>

## 5. DEVELOPMENT PROPOSAL

### 5.1 General Description

The application seeks approval for a Development Permit for a Material Change of Use for a Funeral Parlour over land located at 15 Theresa Drive, Mossman.

The proposal involves the adaptive reuse and refurbishment of an existing industrial shed located on the site to facilitate the operation of Mossman and Port Douglas Community Funerals. The development represents the relocation of the existing Community Funerals business from the adjoining premises at 14 Sawmill Road, to a purpose designed facility capable of accommodating funeral administration, family consultation, mortuary and body preparation functions.

The subject site has an area of approximately 1,010m<sup>2</sup> and is improved by an existing shed structure. The proposal retains the existing building footprint and repurposes the internal layout to provide facilities associated with the operation of a modern funeral parlour. Works include internal refurbishment, installation of new roofed awnings, provision of on-site parking, landscaping and associated site improvements. The development will provide a gross floor area of approximately 162m<sup>2</sup>.

The proposed internal layout comprises:

- Reception and waiting area;
- Office and administration facilities;
- Staff room and amenities;
- Accessible and standard toilet facilities;
- Mortuary and body preparation room;
- Cold storage facilities;
- Transfer room;
- Two private viewing rooms for families; and
- Associated storage and service areas.

The proposed Funeral Parlour will become the principal operational base for the Community Funerals business. Activities undertaken from the site will include funeral bookings, meetings with families, funeral planning and administration, preparation of deceased persons, mortuary operations, body storage and coordination of funeral services.

Importantly, the proposal does not seek to expand the scale of the existing business. Rather, it seeks to separate funeral administration and family facing activities from the cremation related operations currently undertaken at the Sawmill Road property. This separation will improve operational efficiency and provide a more appropriate environment for grieving families whilst reducing the concentration of business activities currently occurring at the Sawmill Road site.

Under the proposed arrangement, the existing Sawmill Road premises will continue to operate as the location for cremation activities, including the existing crematorium and pet cremation business. The Sawmill Road property will also continue to accommodate the business vehicle fleet, including the hearse, transfer van and associated operational vehicles.

Operationally, on the day of a funeral service, the hearse will travel from the Sawmill Road premises to the Theresa Drive facility where the coffin will be loaded prior to proceeding to the funeral service location, such as a church, chapel or cemetery. Following completion of the service, where cremation is required, the deceased will be transported to the Sawmill Road premises for cremation.

The Funeral Parlour is proposed to operate with a maximum of three to four staff members. Standard operating hours will be:

- Monday to Friday: 8:30am to 5:00pm; and
- Saturday: By appointment only.

The nature of the business is such that visitor numbers will remain low. It is anticipated that one to two families may visit the premises per day, typically involving no more than six persons at any one time. Funeral services themselves will not be conducted from the site, with services occurring at churches, cemeteries, chapels or other approved locations.

The business currently undertakes approximately two to three funerals per week. Including transfers associated with mortuary operations, it is anticipated that the site will generate approximately eight to ten vehicle movements per week associated with funeral activities. Vehicle movements are therefore expected to be low and consistent with the capacity of the transport network.

No regular deliveries are anticipated to occur at the Theresa Drive site. Coffin deliveries and bulk storage will remain at the Sawmill Drive premises, which contains existing storage facilities suitable for that purpose.

The proposal provides three onsite car parking spaces, together with pedestrian access and landscaped areas. The development maintains a substantial proportion of the site as open landscaped area and provides a professional and discreet presentation to Theresa Drive.

Overall, the proposal represents the relocation and consolidation of existing Funeral Parlour functions into a dedicated facility without increasing the overall scale or intensity of the business. The development will enable an improved level of service to be provided to families while reducing the concentration of customer visits, staff activity and operational functions currently occurring at the Sawmill Drive premises.

The Architectural Plans are provided within **Attachment 3**.

## 5.2 Proposal Details

The key details of the proposal are summarised by Table 6.

**Table 6: Proposal Details**

<b>Building Height</b>	The building is 4.2m to the gutter and approximately 5m to the apex.
<b>Site Area</b>	1,010m <sup>2</sup>
<b>Gross Floor Area</b>	162m <sup>2</sup>
<b>Car parking</b>	The design provides 3 onsite parking spaces.  Additional on street parking is available within the road reserve adjacent the site.
<b>Landscaping</b>	The Architectural Plans demonstrate that approximately 360m <sup>2</sup> (or 36%) of the site is retained and provides for landscaped area. Detailed Landscape Plans have not been developed at this stage.
<b>Setbacks</b>	A detailed setback plan is provided at Attachment 3. Based on this plan, the proposed development provides varying setbacks to all boundaries, with the minimum setbacks generally as follows: <ul style="list-style-type: none"> <li>• Northern Rear Boundary: Varies between 7.184m to the new awning and 1.4m to the main building</li> <li>• Eastern Side Boundary: Varies between 3.575m to 3.895m.</li> <li>• Southern Road Boundary: A minimum setback of approximately 4.65m to the new awning and 8.515m to the main building.</li> <li>• Western Road Boundary: Varies between 12.545m to 15.146m.</li> </ul>

	Overall, the setbacks demonstrate that appropriate separation is maintained from all boundaries. This approach assists in mitigating visual bulk, preserving privacy, retaining vegetation buffers and achieving a low-intensity, outcome consistent with the site context.
<b>Access Locations</b>	The proposal relies on the existing crossover and driveway arrangement. No changes are proposed.
<b>Infrastructure</b>	Given the former use of the site, it is understood that the site is connected to (or has access to) to all critical infrastructure services including water, wastewater, telecommunications, electricity, and stormwater. All stormwater will be directed to a lawful point of discharge, being Theresa Drive. There are no known infrastructure capacity constraints.

#### 5.4 Infrastructure Charges

In accordance with Council's Charges Resolution (No. 2 of 2021), it is anticipated that infrastructure charges will be calculated and levied against the development at the time of approval. Further discussions with Council Officers following lodgement will be required to confirm the applicable chargeable demand and to determine the availability and quantum of any infrastructure credits associated with the site, having regard to the former use of the land as a Veterinary Clinic.

## 6. LEGISLATIVE REQUIREMENTS

### 6.1 Planning Act 2016

#### 6.1.1 Prohibited Development

The proposed development is not prohibited. This has been established by considering all relevant instruments, which can provide prohibitions under the *Planning Act 2016* (The Act), including:

- Schedule 10 of the *Planning Regulation 2017*; and
- Relevant categorising instruments.

#### 6.1.2 Assessable Development

Section 44(3) of the Act identifies that Assessable Development is development for which a Development Approval is required. As such, the development proposed by this application is made assessable under the Douglas Shire Planning Scheme 2018 (V1) in accordance with Section 43(1) of the Act.

#### 6.1.3 Assessment Manager

The Assessment Manager for this development application is the Douglas Shire Council as determined by Schedule 8 of the *Planning Regulations 2017*.

## 7. STATE PLANNING INSTRUMENTS

### 7.1 FNQ Regional Plan 2026

The subject site is located within the Urban Footprint identified under the Far North Queensland Regional Plan 2026.

Whilst the Far North Queensland Regional Plan 2026 has not yet been fully integrated into the Douglas Shire Planning Scheme 2018, the strategic policy direction established under the former Far North Queensland Regional Plan 2009–2031 remains substantially consistent with the current Regional Plan insofar as it relates to the proposed development.

The proposal comprises the reuse of an existing building within an established urban and serviced industrial area and does not involve development that would expand the urban footprint, result in the fragmentation of rural land, adversely affect matters of regional environmental significance, or otherwise give rise to issues of regional planning significance. Rather, the proposal supports the continued use and efficient utilisation of land and infrastructure within an existing urban area and is consistent with the principles of consolidation and orderly development promoted by both regional plans.

Given the limited scale and localised nature of the proposal, together with the consistency between the strategic intent of the former and current Regional Plans, it is considered that a detailed assessment against the provisions of the Far North Queensland Regional Plan 2026 is not warranted. The proposal does not raise any matters that would conflict with the regional planning framework and is considered to be generally consistent with the regional policy direction applicable to the site.

## 7.2 State Planning Policy 2017

The Douglas Shire Planning Scheme 2018 (V1) has been endorsed by the Minister as appropriately reflecting the April 2016 version of the State Planning Policy (SPP). An assessment against the 2017 version of the SPP has been undertaken in the event that any amended elements are not appropriately captured by the current Planning Scheme. The relevant assessment benchmarks of the current SPP are assessed as being appropriately captured by the Planning Scheme.

## 7.3 Referrals and State Development Assessment Provisions (SDAP)

Review of the *Planning Regulation 2017*, Schedule 10, confirms that the proposed development does not trigger referral.

# 8. PLANNING SCHEME

## 8.1 Douglas Shire Planning Scheme 2018 (V1)

### 8.1.1 Definitions

In accordance with Schedule 1 of the Planning Scheme and Schedule 24 of the *Planning Regulation 2017*, the following land use definitions are relevant to the proposal:

Funeral Parlour means—

*“Premises used to arrange and conduct funerals, memorial services and the like, but do not include burial or cremation. The use includes a mortuary and the storage and preparation of bodies for burial or cremation.”*

### 8.1.2 Applicable Codes

A detailed assessment of the proposal has been conducted against the relevant codes and is provided as **Attachment 5**. Based on that assessment, it is concluded that the development substantially

complies with the applicable assessment benchmarks. Table 7 provides a summary of the key issues considered by the assessment.

**Table 7: Code Compliance Summary**

Applicable Code	Compliance comments
<b>Industry zone code</b>	The proposed Funeral Parlour is consistent with the purpose and overall outcomes of the Industry Zone Code. The development utilises an existing building within the established Industry Zone and represents an appropriate service-based activity that supports the ongoing operation of industrial area without compromising its long-term industrial function. The proposal is modest in scale, compatible with surrounding industrial uses, appropriately serviced by existing infrastructure and designed to avoid adverse impacts on adjoining properties. Whilst a minor variation to the front setback is proposed through the introduction of an entry awning, the development maintains the streetscape character and achieves the intent of the relevant performance outcomes. Overall, the proposal complies with the Industry Zone Code.
<b>Mossman local plan code</b>	The proposed development is consistent with the intent of the Mossman Local Plan Code. The proposal supports Mossman's role as the primary administrative, commercial and industrial centre of the northern Douglas Shire by facilitating the continued operation and investment of an established local business. The development makes productive use of existing industrial land, contributes to local employment and service provision, and does not detract from the character, function or identity of Mossman. The proposal is therefore considered to comply with the outcomes sought by the Mossman Local Plan Code.
<b>Bushfire hazard overlay code</b>	The proposed development involves the adaptive reuse of an existing building and does not intensify the use of the land in a manner that would materially increase bushfire risk. The site benefits from an established access arrangement, existing cleared areas surrounding the building and established infrastructure. Any future building works will be required to comply with applicable building and fire safety requirements. Having regard to the nature and scale of the proposal, the development does not compromise public safety or emergency response capability and is consistent with the intent of the Bushfire Hazard Overlay Code.
<b>Landscape values overlay code</b>	The proposal will have no adverse impact on the landscape values of the locality. The development utilises an existing building within an established industrial area and does not involve prominent built form, significant vegetation removal or substantial earthworks. The scale, appearance and operation of the development are consistent with the existing character of the locality and do not diminish views, scenic amenity or identified landscape features. The proposal therefore complies with the purpose and outcomes of the Landscape Values Overlay Code.

Applicable Code	Compliance comments
<b>Transport network overlay code</b>	The proposed development is compatible with the intended role and function of the surrounding transport network. Traffic generation associated with the Funeral Parlour will be low, with limited staff numbers, appointment-based family consultations and no funeral services conducted on-site. The site benefits from an established access arrangement and sufficient capacity exists within the surrounding road network to accommodate the anticipated vehicle movements. The proposal will not adversely impact the safety or efficiency of the transport network and therefore complies with the Transport Network Overlay Code.
<b>Access, parking and servicing code</b>	The proposal generally complies with the Access, Parking and Servicing Code. Whilst an alternative solution is proposed in relation to the numerical parking requirement, the Applicant has demonstrated that the prescribed parking rate significantly exceeds the operational needs of the use. The proposed three on-site parking spaces, supplemented by substantial on-street parking opportunities along Theresa Drive, are considered sufficient to cater for anticipated staff and visitor demand. Safe access, manoeuvring and servicing arrangements are provided and the development will not adversely impact the surrounding road network. Accordingly, the proposal achieves the intent of the code and complies with the relevant performance outcomes.
<b>Environmental performance code</b>	The proposed Funeral Parlour is a low-intensity land use that is not expected to generate unacceptable impacts relating to noise, lighting, emissions, odour or other environmental nuisance. Activities will be undertaken predominantly within an enclosed building during normal business hours and visitor numbers will remain limited. The proposal incorporates appropriate design and operational measures to protect the amenity of surrounding properties and is therefore consistent with the purpose and overall outcomes of the Environmental Performance Code.
<b>Filling and excavation code</b>	The proposal does not rely on significant filling or excavation works and will utilise the existing benched building pad that has been previously established on the site. No substantial changes to landform, drainage patterns or site levels are proposed. Accordingly, the development will not adversely impact slope stability, visual amenity, drainage or adjoining properties and is considered to comply with the intent and outcomes of the Filling and Excavation Code.
<b>Infrastructure works code</b>	The development is capable of being serviced by existing infrastructure and does not require significant upgrades or extensions to public infrastructure networks. The proposal utilises an existing developed site and established building and is expected to generate only a modest demand on water, sewer, stormwater, electricity and transport infrastructure. The development will not adversely impact

Applicable Code	Compliance comments
	existing infrastructure assets and therefore complies with the purpose and outcomes of the Infrastructure Works Code.
<b>Landscaping code</b>	The site contains sufficient area to accommodate landscaping appropriate to the scale and nature of the proposed development. Whilst detailed landscape plans have not been prepared at this stage, there are no site constraints preventing the provision of landscaping that will soften the built form, enhance the streetscape and contribute positively to the appearance of the development. A detailed Landscape Plan can be readily conditioned by Council if required. Accordingly, the proposal is capable of achieving the outcomes sought by the Landscaping Code and is considered compliant.
<b>Vegetation management code</b>	No vegetation removal is proposed as part of the current application. The proposal involves the adaptive reuse of an existing developed site and will not adversely impact vegetation, biodiversity values, landscape character or ecological functions. Should minor vegetation management be required in the future, this can be addressed through detailed landscape design and approval conditions. The proposal is therefore consistent with the intent and outcomes of the Vegetation Management Code.

#### 8.1.4 Strategic Framework

The Strategic Framework sets the policy direction for the planning scheme and forms the basis for ensuring appropriate development occurs within the Shire for the life of the planning scheme.

The proposed development is Impact Assessable and an assessment against the Strategic Framework has therefore been undertaken. When assessed holistically, the proposed Funeral Parlour is consistent with the intent and outcomes of the Douglas Shire Strategic Framework.

The proposal facilitates the continued operation and growth of an established local business that provides an essential community service to residents throughout Mossman, Port Douglas, the Daintree and surrounding districts. The development involves the adaptive reuse of an existing building within an established industrial area, making efficient use of existing infrastructure and services while maintaining compatibility with surrounding land uses.

The proposal does not seek to intensify or expand the overall scale of the existing funeral business. Rather, it relocates funeral administration, family consultation, mortuary and body preparation activities from the adjoining Sawmill Street premises into a purpose designed facility that is better suited to these functions. This separation of activities will improve operational efficiency, provide enhanced privacy and amenity for grieving families and reduce the concentration of business activities currently occurring on the Sawmill Street site.

The development supports local employment, represents continued investment within Mossman and contributes to the ongoing provision of essential community services within the Shire. Furthermore,

the proposal is of a scale and nature that can be accommodated without adverse impacts on surrounding properties, infrastructure networks or the character of the locality.

Table 8 provides an assessment of the proposal against the six themes of the Douglas Shire Strategic Framework and demonstrates that the development is consistent with the broader strategic intent of the Planning Scheme.

**Table 8: Strategic Framework Compliance Assessment**

Strategic Framework	Compliance comments
<p><b>3.4 Theme 1: Settlement Pattern</b></p>	<p>Complies.</p> <p>The Strategic Framework seeks to reinforce the Shire's established settlement pattern by directing development to appropriate locations within existing urban areas and ensuring land uses are compatible with their surrounding context.</p> <p>The proposed Funeral Parlour is located within the Industry Zone on land that forms part of the established urban area of Mossman. The proposal does not involve the expansion of the urban footprint, fragmentation of rural land or development in an environmentally sensitive location. Rather, it represents the adaptive reuse of an existing building within a serviced industrial precinct.</p> <p>The development supports the continued operation of an established local business and consolidates funeral related activities within an existing urban area where infrastructure and services are readily available. The proposal therefore promotes the efficient use of land and infrastructure while maintaining the integrity of the Shire's settlement hierarchy.</p> <p>Accordingly, the proposal is consistent with the Strategic Framework's settlement pattern objectives.</p>
<p><b>3.5 Theme 2 – Environmental and Landscape Values</b></p>	<p>Complies.</p> <p>The proposal involves the reuse of an existing industrial building and does not require significant site disturbance, vegetation clearing or alteration to natural landforms.</p> <p>The site is located within an established industrial area and is not affected by significant environmental values that would be adversely impacted by the development. The proposal is confined to an already developed site and utilises existing building infrastructure, thereby minimising the environmental footprint of the development.</p> <p>It is anticipated that Council will condition the requirement for landscaping improvements and maintain a visually tidy presentation to Theresa Drive. The scale and appearance of the development remain consistent with the character of the</p>

Strategic Framework	Compliance comments
	<p>surrounding industrial area and will not adversely impact the landscape or scenic values of the locality.</p> <p>The proposal therefore aligns with the Strategic Framework's objective of protecting environmental and landscape values while encouraging the efficient reuse of developed land.</p>
<b>3.6 Theme 3 – Natural Resource Management</b>	<p>Complies.</p> <p>The Strategic Framework promotes the efficient use of land, infrastructure and natural resources.</p> <p>The proposed Funeral Parlour is to be established within an existing building and will utilise existing reticulated infrastructure services. The development is modest in scale, employing only three to four staff and generating relatively low visitor numbers. As such, demand on water, sewer, electricity and transport infrastructure is expected to be limited.</p> <p>The proposal does not involve the consumption or extraction of significant natural resources and does not place unreasonable demands on public infrastructure networks. By adapting an existing building rather than constructing a new facility elsewhere, the development represents an efficient and sustainable use of existing resources and infrastructure.</p> <p>Accordingly, the proposal is consistent with the natural resource management outcomes of the Strategic Framework.</p>
<b>3.7 Theme 4 – Strong Communities</b>	<p>Complies.</p> <p>The Strategic Framework recognises the importance of providing services and facilities that support the health, wellbeing and social needs of the Douglas Shire community.</p> <p>Funeral services are an essential community service that provide support to residents during significant life events. The proposal facilitates the continued operation and improvement of an established local funeral business that services Mossman, Port Douglas, the Daintree and surrounding communities.</p> <p>The development will provide improved facilities for grieving families, including dedicated consultation rooms, private viewing rooms and a purpose designed environment that offers greater comfort, privacy and dignity than the existing arrangements.</p> <p>Importantly, the proposal does not introduce activities likely to adversely impact community amenity. Visitor numbers are expected to remain low, with only one to two family appointments typically occurring per day. Funeral services themselves will not be conducted from the site.</p>

Strategic Framework	Compliance comments
	<p>The proposal therefore contributes positively to the social infrastructure of the Shire and supports the creation of strong and resilient communities.</p>
<p><b>3.8 Theme 5 – Economy</b></p>	<p>Complies.</p> <p>The Strategic Framework seeks to support a diverse and sustainable local economy by encouraging investment, business growth and employment opportunities.</p> <p>The proposal represents investment in a local business that provides an essential service to the Douglas Shire community. The relocation of funeral administration and mortuary functions to the Theresa Drive site will improve business efficiency, service delivery and operational functionality without increasing the overall scale of the business.</p> <p>The development supports local employment and contributes to the economic sustainability of Mossman by retaining and strengthening an established service based business within the township.</p> <p>Furthermore, the proposal makes productive use of an existing industrial property and contributes to the ongoing vitality and utilisation of industrial land within Mossman.</p> <p>Accordingly, the proposal supports the economic objectives of the Strategic Framework.</p>
<p><b>3.9 Theme 6 – Infrastructure and Transport</b></p>	<p>Complies.</p> <p>The Strategic Framework seeks to ensure that development is appropriately serviced and does not place an unreasonable burden on infrastructure or transport networks.</p> <p>The proposed Funeral Parlour will operate with three to four staff and is expected to generate only modest traffic volumes. The Applicant anticipates one to two family appointments per day and approximately two to three funerals per week. Consequently, vehicle movements associated with the development are expected to be low and well within the capacity of the surrounding road network.</p> <p>The site is serviced by existing infrastructure and benefits from approximately 60 metres of frontage to Theresa Drive, providing opportunities for on-street parking in addition to the three on-site parking spaces proposed. Given the operational characteristics of the use, parking demand is expected to remain low and manageable.</p>

Strategic Framework	Compliance comments
	<p>The proposal does not require significant infrastructure upgrades and is capable of being accommodated by the existing transport and servicing networks.</p> <p>Accordingly, the development is consistent with the Strategic Framework's infrastructure and transport objectives.</p>

## 9. CONCLUSION

This Development Application seeks approval for a Development Permit for a Material Change of Use for a Funeral Parlour over land located at 15 Theresa Drive, Mossman. The proposal represents the adaptive reuse and refurbishment of an existing building to accommodate the relocation of Mossman and Port Douglas Community Funerals from the adjoining Sawmill Road premises.

The proposed development has been carefully designed to provide a purpose built facility for funeral administration, family consultations, mortuary operations, body storage and associated funeral services. The proposal does not seek to expand the scale or intensity of the existing business, but rather to improve operational efficiency and service delivery through the separation of funeral parlour functions from the cremation and pet cremation activities that will continue to operate from the Sawmill Road property.

A comprehensive assessment has been undertaken against the relevant provisions of the *Planning Act 2016*, State Planning Policy and Douglas Shire Planning Scheme 2018 (V1). The assessment demonstrates that the proposal is appropriately located within the Industry Zone, where it can operate without giving rise to unacceptable impacts on surrounding land uses, and that it substantially complies with the relevant assessment benchmarks, either directly or subject to reasonable and relevant conditions.

The proposal utilises an existing developed site and existing building, thereby minimising the need for extensive construction works, vegetation clearing or infrastructure upgrades. The development is modest in scale, employing only three to four staff members and generating relatively low levels of traffic and visitor activity. Family consultations are expected to occur on an appointment basis, with only one to two family groups typically attending the premises each day. Funeral services themselves will not be conducted from the site, further limiting operational impacts.

Potential planning considerations including traffic generation, parking demand, amenity impacts, infrastructure capacity and compatibility with surrounding land uses have been carefully considered. The proposal is not expected to place an unreasonable burden on the local road network or existing infrastructure services. Whilst only three on-site parking spaces are proposed, the operational characteristics of the use, combined with the substantial frontage to Theresa Drive and availability of on-street parking, ensure that parking demands can be adequately accommodated.

Importantly, the proposal supports the continued operation and growth of an established local business that provides an essential community service to residents throughout Mossman, Port Douglas, the Daintree and surrounding districts. The development will provide improved facilities for grieving families, including dedicated consultation rooms, private viewing rooms and purpose-designed mortuary facilities, resulting in a higher level of service and improved customer experience.

When assessed against the Strategic Framework, the proposal is consistent with each of the six Strategic Framework themes. It supports the efficient use of land and infrastructure within an established urban area, maintains compatibility with the surrounding industrial locality, contributes to the local economy, provides important social infrastructure and operates within the capacity of existing infrastructure networks.

Overall, the proposed Funeral Parlour represents an appropriate, orderly and well considered form of development that delivers clear community and economic benefits while maintaining the amenity of surrounding properties. The proposal facilitates the continued provision of an essential service within Douglas Shire and can be approved without unacceptable impacts on the environment, infrastructure or surrounding land uses.

For these reasons, it is concluded that the proposed development warrants Council's favourable consideration and approval, subject to reasonable and relevant conditions.

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## **Attachment 5**

**Assessment against the applicable development codes Douglas  
Shire Planning Scheme 2018 v1.0**

***Prepared by Kelly Reaston Development and Property Services***



DEVELOPMENT & PROPERTY SERVICES

## 6.2.5 Industry zone code

### 6.2.5.1 Application

- (1) This code applies to assessing development in the Industry zone.
- (2) When using this code, reference should be made to Part 5.

### 6.2.5.2 Purpose

- (1) The purpose of the Industry zone code is to provide for a range of service, low or medium impact industrial uses. It may include non-industrial and business uses that support the industrial activities where they do not compromise the long-term use of the land for industrial purposes
- (2) The local government purpose of the code is to:
  - (a) implement the policy direction set in the Strategic Framework, in particular:
    - (i) Theme 1 : Settlement pattern, Element 3.4.4 – Industry areas and activities.
    - (ii) Theme 2 : Environment and landscape values, Element 3.5.6 – Air and acoustic protection and hazardous materials.
    - (iii) Theme 5 : Economy, Element 5.8.2 – Economic growth and diversification, Element 5.8.5 – Innovation and technology.
  - (b) provide and protect land that is accessible and serviced for the location of industry;
  - (c) manage development to maintain an industrial amenity and provide adequate separation to sensitive land use activities.
  - (d) ensure the long term dominance of the Mossman Mill as an industrial activity on Industry zoned land in Mossman will continue to contribute to the development and prosperity of the town.
  - (e) recognise the opportunity to consolidate further industrial development around the Mosman Mill site to create a low /medium impact industry precinct in Mossman.
- (3) The purpose of the code will be achieved through the following overall outcomes:
  - (a) Uses and works for industrial purposes are located, designed and managed to maintain safety to people, avoid significant adverse effects on the natural environment and minimise impacts on adjacent non-industrial land.
  - (b) The scale, character and built form of development contributes to a high standard of amenity.
  - (c) Development has access to development infrastructure and essential services.
  - (d) The viability of both existing and future industrial activities is protected from the intrusion of incompatible uses.
  - (e) Industrial uses are adequately separated from sensitive land uses to minimise the likelihood of environmental harm or environmental nuisance occurring.

### 6.2.5.3 Criteria for assessment

Table 6.2.5.3.a – Industry zone – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<p><b>PO1</b> The height of buildings and structures is consistent with those of nearby buildings.</p>	<p><b>AO1</b> Buildings and structures are not more than 10 metres in height.</p>	<p><b>Complies with AO1.</b></p> <p><b>The proposal utilises an existing building with a building height of 4.2m to the gutter and approximately 5m to the apex.</b></p>
<p><b>PO2</b> Buildings and structures are setback to contribute to an attractive and consistent streetscape appearance and to protect the amenity of other land uses.</p>	<p><b>AO2.1</b> Buildings, structures, display and storage areas are set back a minimum of: (a) 8 metres to a State-controlled road (b) 6 metres from any other road frontage(s).</p>	<p><b>Does not strictly comply with AO2.1 Complies with PO2.</b></p> <p><b>The existing building is setback 8.515m from Teresa Drive. However a new awning is proposed at the front in order to provide weather protection adjacent the building entry. The awning 4.655m from the Teresa Drive boundary. The awning is an open structure and is positioned lower than the gutter line. The structure is not considered to contribute to the building bulk or detract from the streetscape appearance.</b></p>
	<p><b>AO2.2</b> Where a site has a common boundary with land in an Industry zone, the buildings are setback either: (a) 0 metres from the side and rear boundaries; or (b) 2.5 metres or ¼ of the height of the building, whichever is the greater; and (c) not any distance between 0 metres and 2.5 metres. Note – Building Code requirements must be satisfied.</p> <p><b>AO2.3</b> Where a site has a common boundary with land not in an Industry zone, the buildings, structures, display areas and storage are setback 2.5 metres or ¼ of the height of the</p>	<p><b>Complies with AO2.2.</b></p> <p><b>Refer to the building setback plan, which demonstrates that the proposal achieves compliance with the minimum side and rear boundary setbacks.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
	building, whichever is the greater from the common boundary. Note – Building Code requirements must be satisfied.	
<b>PO3</b> The site coverage of buildings ensures that there is sufficient space available to cater for services, landscaping and the on-site parking and manoeuvring of vehicles.	<b>AO3</b> The site coverage of buildings does not exceed 60%.	<b>Complies with AO3.</b>
<b>PO4</b> Development provides a quality workplace.	<b>AO4.1</b> Pedestrian entrances to buildings are: (a) easy to identify from the street and on-site car parking areas; (b) provided with sun and rain protection consisting of a minimum width of 900mm and positioned immediately above the entry way.  <b>AO4.2</b> Any office or sales spaces are orientated toward the street and are provided with human scale elements (including, but not limited to, windows, doors, shading devices and variations in construction materials, colours etc.).  <b>AO4.3</b> Customer parking is located at the front of the building between the building and the street or to the side of the building with clear visibility to the street.  <b>AO4.4</b> Any gates are sliding, or alternatively, open inward to the site so that the adjoining footpath reserve is not blocked when gates are open.  <b>AO4.5</b> Car parking surfaces are constructed or coated with glare-reducing materials.	<b>Complies with AO4.1-AO4.5.</b>  <b>The main reception addresses the street frontage. Customer parking is located between the building and road boundary and is clearly visible. Parking is limited at the front due to the existing site configuration. The parking area is imperviously sealed (concrete). Existing gates open inward and such operation may be further conditioned.</b>
<b>PO5</b> The appearance and amenity of development is enhanced through landscaping works.	<b>AO5.1</b> A minimum of 20% of the site is provided with space available for landscape planting.	<b>Complies with PO5.</b>

Performance outcomes	Acceptable outcomes	Applicant response
<p>Note – Planning scheme policy – Landscaping provides further guidance on meeting the performance outcome.</p>	<p><b>AO5.2</b> A 2 metre landscape planting strip for dense planting is provided along the road frontage(s), except that a 3 metre strip is provided along any frontage to the Captain Cook Highway.</p> <p><b>AO5.3</b> Landscape planting beds adjacent to parking and manoeuvring areas are protected from vehicle encroachment by a 150mm high vertical kerb edge or similar durable obstruction.</p> <p><b>AO5.4</b> Landscape planting consists of hardy tropical species suited to Douglas Shire’s climatic conditions.</p>	<p><b>Approximately 36% of the site is available for landscaping. Whilst a formal landscaping plan has not been prepared for the purpose of this Development Application. It is expected that this will form a condition of approval.</b></p>
<p><b>PO6</b> The movement of traffic on roads is not compromised by the loading and unloading of goods.</p>	<p><b>AO6</b> All delivery/pick up vehicles are situated entirely within the site when being loaded and/or unloaded with goods.</p>	<p><b>Not Applicable.</b></p> <p><b>Given the nature of the development deliveries are not required.</b></p>
<p><b>PO7</b> Industrial areas are not characterised by a proliferation of advertising signs and/or the use of large advertising signs.</p>	<p><b>AO7</b> No wall signs or painted advertising are located on the walls of industrial buildings facing, or visible to, the Captain Cook Highway.</p>	<p><b>Not Applicable.</b></p> <p><b>The development does not face the Captain Cook Highway.</b></p>
<p><b>PO7</b> The movement of traffic on roads is not compromised by access and egress to the site.</p>	<p><b>AO7.1</b> Site access for vehicles is limited to one point per road frontage. or</p> <p><b>AO7.2</b> If needed, two access points separated by a minimum of 10 metres to facilitate on-site vehicular manoeuvring for large vehicles.</p>	<p><b>Complies with AO7.1</b></p> <p><b>The development utilises the existing driveway and crossover.</b></p> <p><b>Not Applicable.</b></p> <p><b>A second crossover is not required.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>A07.3</b> Sufficient space is available for vehicles to manoeuvre within the site so as to enter and leave the site in forward gear.</p>	<p><b>Complies with A07.3.</b></p> <p><b>Vehicles are able to access the site and exit in forward gear.</b></p>
<p><b>PO8</b> Development collects and disposes of waste materials and caters for spillages in a manner that prevents contamination of land or water.</p>	<p><b>A08.1</b> Sources of potential contaminants are roofed and sealed with impervious surfaces and provided with 110% storage capacity bund for spillage containment.</p> <p><b>A08.2</b> Roof and storm water are directed away from areas of potential contamination.</p> <p><b>A08.3</b> Contaminating materials are stored at levels above the defined flood / storm tide event, whichever is the highest.</p>	<p><b>Not Applicable.</b></p> <p><b>The development does not generate or store sources of potential contaminants.</b></p>
<b>For assessable development</b>		
<p><b>PO9</b> The establishment of uses is consistent with the outcomes sought for the Industry zone and protects the zone from the intrusion of inconsistent uses.</p>	<p><b>A09</b> Uses identified in Table 6.2.5.3.b are not established in the Industry zone.</p>	<p><b>Complies with A09.</b></p> <p><b>The use is not listed within Table 6.2.5.3.b.</b></p>
<p><b>PO10</b> Development does not lower the standards of amenity in terms of air, noise, odour, electrical interference and vibrations at any land use associated with the:</p> <p>(a) the Accommodation activity group, located outside the Industry zone;</p> <p>(b) the Sensitive land use activity group, located outside the Industry zone.</p>	<p><b>A010</b> No acceptable outcomes are prescribed.</p>	<p><b>Complies with PO10.</b></p> <p><b>The development is of a modest scale and does not give rise to matters concerning air, noise, odour, electrical interference and vibrations.</b></p>
<p><b>PO11</b> New lots contain a minimum area of 1000m<sup>2</sup>.</p>	<p><b>A011</b> No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO12</b> New lots have a minimum road frontage of 20 metres.</p>	<p><b>A012</b> No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<p><b>PO13</b> New lots contain a 20 metre x 40 metre rectangle.</p>	<p><b>AO13</b> No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>

Table 6.2.5.3.b – Inconsistent uses within the Industry Zone

Inconsistent uses		
<ul style="list-style-type: none"> <li>• Air services</li> <li>• Animal husbandry</li> <li>• Bar</li> <li>• Cemetery</li> <li>• Child care centre</li> <li>• Club</li> <li>• Community care centre</li> <li>• Community residence</li> <li>• Cropping</li> <li>• Detention facility</li> <li>• Dual occupancy</li> <li>• Dwelling house</li> <li>• Environment facility</li> <li>• Extractive industry</li> <li>• Function facility</li> <li>• Health care services</li> <li>• Home based business</li> <li>• Hospital</li> </ul>	<ul style="list-style-type: none"> <li>• Hotel</li> <li>• Intensive animal industry</li> <li>• Intensive horticulture</li> <li>• Major sport and entertainment facility</li> <li>• Motor sport facility</li> <li>• Multiple dwelling</li> <li>• Nature based tourism</li> <li>• Nightclub entertainment facility</li> <li>• Non-resident workforce accommodation</li> <li>• Outdoor sport and recreation</li> <li>• Outstation</li> <li>• Permanent plantation</li> </ul>	<ul style="list-style-type: none"> <li>• Relocatable home park</li> <li>• Renewable energy facility, being a wind farm</li> <li>• Residential care facility</li> <li>• Resort complex</li> <li>• Retirement facility</li> <li>• Roadside stall</li> <li>• Rooming accommodation</li> <li>• Rural workers accommodation</li> <li>• Shopping centre</li> <li>• Short-term accommodation</li> <li>• Theatre</li> <li>• Tourist attraction</li> <li>• Tourist park</li> <li>• Wholesale nursery</li> </ul>

Note – This table does not imply that all other uses not listed in the table are automatically consistent uses within the zone. Assessable development must still demonstrate consistency through the assessment process.

## 7.2.3 Mossman local plan code

### 7.2.3.1 Application

- (1) This code applies to development within the Mossman local plan area as identified on the Mossman local plan maps contained in Schedule
- (2) When using this code, reference should be made to Part 5.

### 7.2.3.2 Context and setting

Editor's note - This section is extrinsic material under section 15 of the Statutory Instruments Act 1992 and is intended to assist in the interpretation of the Mossman local plan code.

The Mossman local plan area is located at the northern end of the Captain Cook Highway where it continues on to the Daintree township as the Mossman-Daintree Road. The local plan area contains the rural and local administrative centre of Mossman.

The town of Mossman is located on a flat plain framed by the southern portion of the Daintree National Park to the west and Mount Beaufort to the east. The lush Daintree National Park dominates the town with striking views across to Mt Demi (Manjal Dimbi) and the spectacular Mossman Bluff above the Mossman Gorge. South Mossman River and North Mossman river provide significant natural entry and exit gateways to the town supported by Marrs Creek to the west. Parker Creek divides the town midway separating the commercial township from the more predominately residential areas in the southern half of the town.

Mossman developed as a strong sugar producing region at the end of the nineteenth century and quickly developed into a prosperous small town. From the mid-1930s the main commercial street was Mill Street. However Front Street, being the main access from Port Douglas and the Daintree also saw a concentration of commercial development from the 1930s onward. The town focusses on "the Triangle" and central grassed area at the five way junction at the northern end of the town providing a focal community hub that contributes significantly to the town's central setting.

The Mossman Sugar Mill in the north eastern part of the town is the northern most sugar mill in Queensland with its building and chimney stack dominating the town centre while forming part of an important vista along Mill Street looking toward Mount Beaufort. The mill is also the central focus of the cane rail network that radiates outward through the town adding an important character element that contributes to the appeal of the sugar town. Part of the cane rail network runs east-west through the Triangle occasionally delaying north-south vehicular traffic during cane harvesting months.

Mossman is a discrete linear township surrounded by sugar cane cultivation. The established business centre serves much of the northern part of the Shire with generally lower order goods and services. Service industries are concentrated at the southern end of the town providing for the general needs of the community. A limited area of expansion is available if the need arises.

Much of the township's character is derived from its picturesque rural setting and heritage character. Significant stands of mature vegetation (rain-trees and fig trees) dominate parts of the streetscape providing a much valued and identifiable feature to the town.

At the southern end of town centre, Johnston Road heads west from Front Street providing access to the Mossman Gorge, a popular tourist attraction and small indigenous community in the Daintree National Park to the west of the town. This intersection is another important focal point, particularly for tourists finding their way to the gorge. Johnston Road also provides access to the hospital and showgrounds on the western side of the town and the developing residential area off Daintree Horizon Drive: currently the main expanding residential estate in the township.

A more contemporary shopping facility is available at the southern end of Front Street on the western side of the road. Expansion of this shopping precinct has recently been completed. It is not intended that this precinct be expanded any further, and any form of redevelopment of the older retail component, will occur as an integrated development.

On the south western side of the town, off Coral Sea Drive, is a rural residential subdivision providing a green backdrop to the town. It is not intended that further lot reconfiguration occur in this area in order to protect the appeal of the hillside on the western flank of the township.

The indigenous Mossman Gorge community is located approximately three kilometres to the west of Mossman township where improved infrastructure, housing and economic opportunities are to be facilitated.

The Mossman North community located approximately two kilometres north of the Mossman township will remain as a residential community.

### 7.2.3.3 Purpose

- (1) The purpose of the Mossman local plan code is to facilitate development which creates a vibrant and independent community which supports the needs of the local community and surrounding rural areas, while protecting and enhancing the unique local and historic character of the town.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) Mossman will continue to develop as the major administrative, commercial and industrial centre in Douglas Shire.
  - (b) The key built form and main street character of the town centre is to be retained and reinforced.
  - (c) Mossman's identity as Queensland's northern-most sugar mill town is strengthened through the development of a distinct, ordered and attractive streetscape which responds to the tropical climate and the special features of the town's setting and layout.
  - (d) Mossman's distinct character is enhanced through appropriate building design and landscaping.
  - (e) The significant avenues of rain-trees and fig trees and other such vegetation that contribute significantly to township identity are protected.
  - (f) Residential development is encouraged within designated areas to consolidate Mossman's character as a permanent residential settlement.
  - (g) Residential areas are pleasant, functional, distinctive and well-defined and residential amenity is maintained and enhanced with all residential areas having good access to services and facilities, while minimising any land use conflicts associated with different urban activities or nearby rural activities.

- (h) Development in the Low-medium density residential zone provides a range of housing options and contributes to a high standard of residential amenity, scale and design consistent with the character of Mossman.
  - (i) Opportunities for a limited range of tourist accommodation and services are facilitated to cater for the requirements of tourists passing through Mossman or visiting the Mossman Gorge.
  - (j) To provide the opportunity for an alternative truck route to by-pass the town centre for safer and less disruptive access between the sugar mill and Cairns (subject to further investigation as a local initiative).
  - (k) Improved local land use planning, housing and infrastructure arrangements enable private home ownership, economic development and municipal service delivery for the Mossman Gorge community.
  - (l) Mossman North will remain as a residential land use community only, with no further outward expansion intended.
  - (m) Conflicts between alternative land uses are minimised.
  - (n) Mossman's role as an industrial service centre is enhanced by facilitating the expansion of industrial development adjacent to existing industrial areas and protecting industrial areas from encroachment of incompatible land use activities.
  - (o) Remnant vegetation areas, riverine corridors and natural features are protected by ensuring any adjacent development is low key and sensitive to its surroundings.
- (3) The purpose of the code will be further achieved through the following overall outcomes:
- (a) Precinct 1 – Mossman North precinct;
  - (b) Precinct 2 – Foxtan Avenue precinct;
  - (c) Precinct 3 – Junction Road residential precinct;
  - (d) Precinct 4 – Junction Road industry precinct;
  - (e) Precinct 5 – Town Centre precinct;
  - (f) Precinct 6 – Front Street precinct;
  - (g) Precinct 7 – Emerging community precinct;
  - (h) Precinct 8 – Mossman South industry precinct;
  - (i) Precinct 9 – Mossman Gorge community precinct

**Precinct 1 – Mossman North precinct**

- (4) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) development is restricted to low density residential uses only.
  - (b) development reliant on exposure to the Mossman-Daintree Road does not occur.

**Precinct 2 – Foxtan Avenue precinct**

- (5) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) development occurs that is compatible with the establishment of a botanical garden, including a range of ancillary tourist facilities (not tourist accommodation), educational facilities and research facilities.
  - (b) Development takes into account physical constraints with particular attention paid to flooding and vegetation.
  - (c) development is adequately separated from, and protects, the existing cane railway track along the south boundary of the land;
  - (d) development does not impact on the environmental values of Marrs Creek.

**Precinct 3 - Junction Road residential precinct**

- (6) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) land within the Junction Road residential precinct is developed taking into account the opportunities and constraints with particular attention paid to flooding and vegetation. Any form of urban development is to be free from flood inundation and will not impact on current drainage regimes;
  - (b) development in the form of lot reconfiguration consists of lot sizes and shapes that match the character and configuration of surrounding lots;
  - (c) development on the site does not impact on the environmental values of the North Mossman River.

**Precinct 4 - Junction Road industry precinct**

- (7) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) the Mossman Mill is located within Precinct 4 and is the catalyst for encouraging and accommodating further industrial development.
  - (b) low and medium impact industry uses are located within the Junction Road industry precinct to service the needs of the sugar mill and to consolidate allied industrial uses;
  - (c) residential areas on the western side of Junction Street are protected from any industrial use, including industrial lot reconfiguration, by a dense screen of vegetation.

**Precinct 5 - Town Centre precinct**

- (8) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) key elements which contribute to the character and integrity of the town centre are retained;
  - (b) the sense of place which characterises the main town intersection of Foxtan Avenue, Mill Street and Junction Road is reinforced with new development or redevelopment contributing to the existing continuity of built form by being built up to the street frontage;
  - (c) the cane tram line which runs along Mill Street, the vista down Mill Street to Mount Beaufort and the sugar mill chimney are retained as unique features of the town and its sugar town heritage;
  - (d) views from Front Street of the mountains (from various vantage points) are maintained;
  - (e) avenue planting within the town centre along the centre median in Front Street is maintained and extended to reinforce the character of the town centre.

**Precinct 6 - Front Street precinct**

- (9) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that any expansion of the development is integrated with the existing shopping facilities incorporating the following design parameters:
- (a) vehicular access is limited to:
  - (b) the existing access from Front Street opposite the Harper Street intersection;
  - (c) the existing access at the southern boundary of the precinct limited to commercial vehicles and staff only.
  - (d) any expansion complements the existing development in scale, height, roof alignment and colour;
  - (e) any expansion is integrated with existing development such that the final development functions as one shopping/commercial development;
  - (f) any expansion takes into account adjacent (existing and future) residential development and incorporates service areas, car parking and other utilities which are visually and acoustically screened to protect the residential amenity of the area.

**Precinct 7 – Emerging community precinct**

- (10) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure:
- (a) development takes into account the provision of road links, cycle links, pedestrian connections and parkland allocation, generally in accordance with the local plan, to ensure that each land subdivision does not compromise the future development of adjoining land. Open space is provided with extensive road frontage for visibility / utility.

**Precinct 8 - Mossman south industry precinct**

- (11) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) low impact industry uses are the predominant form of industry within the Mossman South industry precinct;
  - (b) no uses that compete with the commercial and retail primacy of the town centre are established;
  - (c) development protects the amenity of adjacent and nearby residential land uses.

**Precinct 9 – Mossman Gorge community precinct**

- (12) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) existing commercial, community and residential uses are recognised within the discrete area contained by the Mossman Gorge community;
  - (b) a flexible approach to land use planning is advanced through the adoption of a structure plan for the community;
  - (c) a flexible approach to lot reconfiguration is permitted to advance home ownership aspirations for the community;
  - (d) infrastructure upgrading is undertaken and transitioned to Council for future maintenance.

**Criteria for assessment**

Table 7.2.3.4.a – Mossman local plan – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<b>PO1</b> Building and structures complement the height of surrounding development	<b>AO1</b> Buildings and structures are not more than 8.5 metres in height, except where included in the Industry zone where buildings and structures are not more than 10 metres in height.	<b>Complies with AO1.</b>  <b>Building height is approximately maximum 5m.</b>



Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>AO1.3</b> Important landmarks, memorials and monuments are retained, including, but not limited to:</p> <ul style="list-style-type: none"> <li>(a) the cane tram line running east west through the town at Mill Street;</li> <li>(b) the general configuration of the 'Triangle' at the intersection of Front Street, Mill Street, Foxton Avenue and Junction Road</li> </ul>	<b>Not Applicable.</b>
<p><b>PO2</b> Development contributes to the protection, reinforcement and where necessary enhancement of gateways and key intersections identified on the Mossman local plan maps contained in Schedule 2.</p>	<p><b>AO2</b> Development adjacent to the gateways and key intersections as identified on the Mossman local plan maps contained in Schedule 2 incorporates architectural features and landscaping treatments and design elements that enhance the sense of arrival and way finding within the town.</p>	<b>Not Applicable.</b>
<p><b>PO3</b> Landscaping of development sites complements the existing tropical character of Mossman.</p>	<p><b>AO3</b> Landscaping incorporates the requirements of Planning scheme policy SC 6.2- Landscaping.</p>	<p><b>May be conditioned to Comply.</b></p> <p><b>A development specific Landscape Plan has not been prepared at this stage. It is anticipated that this will be required as a condition of approval.</b></p>
<p><b>P04</b> Development does not compromise the safety and efficiency of the State-controlled road network.</p>	<p><b>AO4</b> Direct access is not provided to a State-controlled road where legal and practical access from another road is available.</p>	<b>Not Applicable.</b>



Performance outcomes	Acceptable outcomes	Applicant response
<b>For assessable development</b>		
<b>Additional requirements for Precinct 2 – Foxtton Avenue precinct</b>		
<b>PO5</b> Development takes into account the opportunities and constraints with particular attention paid to flooding and vegetation.	<b>AO5</b> Buildings and structures are located outside areas subject to flooding. Development is undertaken in accordance with the recommendations of a Drainage/Flood Study which outlines the necessary improvements to be undertaken on the site to make it suitable for development and avoid impacts on adjoining land.	<b>Not Applicable.</b>
<b>PO6</b> Development is adequately separated from and protects the existing cane railway track along the southern boundary of the land.	<b>PO6.1</b> Buildings and structures are setback a minimum of 10 metres from the cane railway.  <b>PO6.2</b> Pedestrian access to the cane railway is restricted.	<b>Not Applicable.</b>
<b>Additional requirements for Precinct 3 – Junction Road residential precinct</b>		
<b>PO7</b> Land within the Junction Road residential precinct is developed taking into account of the opportunities and constraints with particular attention paid to flooding and vegetation. Any form of urban development is to be free from flood inundation and will not impact on current drainage regimes.	<b>AO7</b> Development is undertaken in accordance with the recommendations of a Drainage/Flood Study which outlines the necessary improvements to be undertaken on the site to make it suitable for residential development and avoid impacts on adjoining land.	<b>Not Applicable.</b>
<b>PO8</b> Development in the form of lot reconfiguration consists of lot sizes and shapes that match the character and configuration of surrounding lots.	<b>AO8.1</b> Lots have a minimum area of 800m <sup>2</sup> .  <b>AO8.2</b> Lots have a minimum frontage of 20m.	<b>Not Applicable.</b>

Performance outcomes	Acceptable outcomes	Applicant response
<p><b>PO9</b> Development on the site does not impact on the environmental values of the North Mossman River, with any land dedication along the creek provided with access to, at minimum, a partial esplanade road frontage.</p>	<p><b>AO9.1</b> Subject to any greater width requirement as a consequence of the studies required to satisfy AO8, a minimum riparian width of 30 metres is dedicated as open space along the frontage to the Mossman River.</p> <p><b>AO9.2</b> Practical road access is available to the minimum riparian width of 30 metres along the frontage to the Mossman River.</p>	<p><b>Not Applicable.</b></p>
<b>Additional requirements for Precinct 4 – Junction Road industry precinct</b>		
<p><b>PO10</b> Residential areas on the western side of Junction Road are protected from any industrial use, including industrial lot reconfiguration, by a dense screen of vegetation.</p>	<p><b>AO10.1</b> A dense screen of vegetation of at least 10 metres depth separates any industrial use, including any lot reconfiguration, along the full frontage of Junction Road except where road access is required.</p> <p><b>AO10.2</b> No individual lots will have direct access to Junction Road across the 10 metre dense screen of vegetation.</p>	<p><b>Not Applicable.</b></p>
<b>Additional requirements for Precinct 5 – Town Centre precinct</b>		
<p><b>PO11</b> Buildings in the precinct are designed and sited to complement the existing distinctive and cohesive character of the retail and business area, including:</p> <ul style="list-style-type: none"> <li>(a) buildings built to the frontage to reinforce the existing built-form character;</li> <li>(b) buildings that address the street;</li> <li>(c) development that incorporates awnings and verandahs providing weather protection for pedestrians.</li> </ul>	<p><b>AO11</b> With respect to Front Street, Foxtan Avenue, Mill Street and Johnston Road, development incorporates buildings that front the street designed with non-transparent awnings that:</p> <ul style="list-style-type: none"> <li>(a) provide for pedestrian shelter that are consistent with the character and setting of the town centre;</li> <li>(b) are a minimum of 3.2 metres and a maximum of 3.5 metres above the finished footpath level;</li> <li>(c) extend and cover the adjoining footpath with a 1.5 metre setback to the kerb;</li> <li>(d) are continuous across the frontage of the site;</li> </ul>	<p><b>Not Applicable.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>(e) are cantilevered from the main building and where posts are used, posts are non-load bearing;</p> <p>(f) include under awning lighting</p>	
<p><b>PO12</b> Development in the precinct contributes positively to the character of the town and is complementary in scale to surrounding development.</p>	<p><b>AO12</b> Development incorporates the following design features:</p> <p>(a) built up to the front' alignment addressing the street frontage and continuing the scale of the existing built form and where necessary providing car parking spaces at the rear of the site;*</p> <p>(b) appropriate built form and roofing material;</p> <p>(c) appropriate fenestration in combination with roof form;</p> <p>(d) appropriate window openings, screens or eaves shading 80% of window openings;</p> <p>(e) minimum of 700mm eaves;</p> <p>(f) orientation of the building to address the street/s;</p> <p>(g) sheltered pedestrian access by enclosed covered common area walkway of 1.5 metres in width from the car park area/s to the development;</p> <p>(h) ground level façades facing streets consist of windows, wall openings or shop fronts;</p> <p>(i) vertical architectural elements a minimum of 3 metres along the length of the ground level façade;</p> <p>(j) inclusion of windows and balconies on the upper levels facing the street façade;</p> <p>(j) provision of lattice, battens or privacy screens;</p> <p>(k) the overall length of a building does not exceed 30 metres and the overall length of any continuous wall does not exceed 15 metres;</p>	<p><b>Not Applicable.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>(l) Any air conditioning plant is screened from the street frontage and public view by use of architectural features.</p> <p>*Note - access to car parking must not adversely impact on 'built up to the front' alignment continuity.</p>	
<p><b>PO13</b> Site coverage of all buildings:</p> <p>(a) does not result in a built form that is bulky or visually intrusive to the streetscape;</p> <p>(b) respects the individual character of the town centre.</p>	<p><b>AO13</b> Site cover does not exceed 60%.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO14</b> Side and rear setbacks:</p> <p>(a) are appropriate for the scale of the development and the character of the town centre;</p> <p>(b) provide adequate daylight for habitable rooms on adjoining sites;</p> <p>(c) adequate separation between residential and non-residential uses.</p>	<p><b>AO14.1</b> For side boundary setbacks, no acceptable measures are specified.</p> <p><b>AO14.2</b> Buildings are setback a minimum of 6 metres from rear boundaries.</p> <p>Note: Building code requirements must be satisfied.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO15</b> Development in the precinct is predominantly retail or office based in nature or has a service delivery function.</p>	<p><b>AO15</b> Development at street level is limited to retail, office or restaurant/cafe based activities or personal services, with residential development limited to minor ancillary residential uses or to tourist accommodation located above ground level, or to the rear of the site at ground level.</p>	<p><b>Not Applicable.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<b>Additional requirements for Precinct 6 – Front Street precinct</b>		
<p><b>PO16</b>            Vehicular access is limited to:            (a) the existing access from Front Street opposite the Harper Street intersection;            (b) the existing access at the southern boundary of the precinct limited to commercial vehicles and staff only.</p>	<p><b>AO16</b>            No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO17</b>            Any expansion complements the existing development in scale, height, roof alignment and colour</p>	<p><b>AO17</b>            No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO18</b>            Any expansion is integrated with existing development such that the final development functions as one shopping/commercial development.</p>	<p><b>AO18</b>            No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO19</b>            Any expansion takes into account adjacent residential development and incorporates service areas, car parking and other utilities which are visually and acoustically screened to protect the residential amenity of the area.</p>	<p><b>AO19</b>            No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
<b>Additional requirements for Precinct 7 – Emerging Community precinct</b>		
<p><b>PO20</b>            Development provides road connections, pedestrian and cycling links and open space to establish integrated, connected communities with adjoining land.</p>	<p><b>AO20</b>            No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<b>Additional requirements for Precinct 8 – Mossman South industry</b>		
<p><b>PO21</b> Low impact industry uses are the predominant form of industry.</p>	<p><b>AO21</b> Development for industrial purposes consists of service industry or low impact industry uses.</p>	<p><b>Alternative Solution</b></p> <p><b>The development is proposed due to its close proximity to the Applicant’s existing crematorium operations located on the adjoining property at Sawmill Road and the desire to collocate complementary funeral industry activities.</b></p> <p><b>The proposal will enable funeral administration, family consultation, mortuary and body preparation functions to be undertaken from a dedicated facility while maintaining a direct operational relationship with the existing crematorium. This arrangement provides significant efficiencies in the day-to-day management of the business by reducing travel distances between facilities, streamlining the transfer of deceased persons and improving coordination between funeral and cremation services.</b></p> <p><b>The collocation of these complementary uses also allows the Applicant to continue utilising existing infrastructure, operational systems and management resources while improving the overall level of service provided to clients. Importantly, the proposal establishes a clear separation between family facing activities and cremation operations, creating a more appropriate and dignified environment for grieving families while</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
		<p>retaining the functional benefits associated with the proximity of the crematorium.</p> <p>From a planning perspective, the proposal represents a logical and efficient land use outcome. The development consolidates related business activities within an established industrial area, maximises the use of existing infrastructure and services, and avoids the need to establish duplicate facilities elsewhere within the Shire. The proximity of the two sites ensures the ongoing operation of the business can occur in a coordinated manner while minimising unnecessary vehicle movements and operational inefficiencies. The use does not impact on the other surrounding industrial activities.</p>
<p><b>PO22</b> No uses that compete with the commercial and retail primacy of the Mossman town centre are established.</p>	<p><b>AO22</b> Office or retail uses: (a) are ancillary to an industrial use; or (b) directly service the needs of the surrounding industrial precinct; (c) do not rely on passing trade from Alchera Drive.</p>	<p><b>Complies with PO22.</b></p> <p><b>The proposed development does not compete with the commercial and retail primacy of Mossman town centre.</b></p>
<p><b>PO23</b> Development protects the amenity of adjacent and nearby residential land uses.</p>	<p><b>AO23</b> No acceptable outcomes are prescribed.</p>	<p><b>Complies with PO23.</b></p> <p><b>The development is modest in scale and customer visits per day/week. Given the nature of the business this will not impact on nearby residential uses.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<b>Additional requirements for Precinct 9 – Mossman Gorge Community</b>		
<b>PO24</b> No uses that compete with commercial and retail activities in Mossman town centre are established.	<b>AO24</b> No acceptable outcomes are prescribed.	<b>Not Applicable.</b>

## 8.2.2 Bushfire hazard overlay code

Note - Land shown on the bushfire hazard overlay map is designated as the bushfire prone area for the purposes of section 12 of the Building Regulations 2006. The bushfire hazard area (bushfire prone area) includes land covered by the high and medium hazard areas as well as the buffer area category on the overlay map.

### 8.2.2.1 Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational works or building work in the Bushfire hazard overlay, if:
  - (a) self-assessable or assessable where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
  - (b) impact assessable development.
- (2) Land in the Bushfire hazard overlay is identified on the Bushfire hazard overlay map in Schedule 2 and includes the following sub-categories:
  - (a) Medium bushfire risk sub-category;
  - (b) High bushfire risk sub-category;
  - (c) Very high bushfire risk sub-category;
  - (d) Potential impact buffer sub-category.
- (3) When using this code, reference should be made to Part 5.

### 8.2.2.2 Purpose

- (1) The purpose of the Bushfire overlay code is to:
  - (a) implement the policy direction in the Strategic Framework, in particular:
    - (i) Theme 1 Settlement pattern: Element 3.4.7 Mitigation of hazards;
    - (ii) Theme 6 Infrastructure and transport: Element 3.9.2 Energy.
  - (b) enable an assessment of whether development is suitable on land within the Bushfire risk overlay sub-categories.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) development avoids the establishment or intensification of vulnerable activities within or near areas that are subject to bushfire hazard;
  - (b) development is designed and located to minimise risks to people and property from bushfires;
  - (c) bushfire risk mitigation treatments are accommodated in a manner that avoids or minimises impacts on the natural environment and ecological processes;

- (d) development involving the manufacture or storage of hazardous materials does not increase the risk to public safety or the environment in a bushfire event;
- (e) development contributes to effective and efficient disaster management response and recovery capabilities.

Note - A site based assessment may ground-truth the extent of hazardous vegetation and extent and nature of the bushfire hazard area (bushfire prone area). Such assessments should be undertaken using the methodology set out in Planning scheme policy SC6.9 - Natural Hazards.

### Criteria for assessment

Table 8.2.2.3.a – Bushfire hazard overlay code –assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<b>Compatible development</b>		
<p><b>PO1</b> A vulnerable use is not established or materially intensified within a bushfire hazard area (bushfire prone area) unless there is an overriding need or other exceptional circumstances.</p> <p>Note - See the end of this code for examples of vulnerable uses.</p>	<p><b>AO1</b> Vulnerable uses are not established or expanded.</p> <p>Note – Where, following site inspection and consultation with Council, it is clear that the mapping is in error in identifying a premises as being subject to a medium, high, very high bushfire hazard or potential impact buffer sub-category, Council may supply a letter exempting the need for a Bushfire Management Plan. Note – Where the assessment manager has not previously approved a Bushfire Management Plan (either by condition of a previous development approval), the development proponent will be expected to prepare such a plan. Note – Planning scheme policy SC6.9 - Natural hazards, provides a guide to the preparation of a Bushfire Management Plan.</p>	<p><b>Not Applicable.</b></p> <p><b>The proposal does not involve a vulnerable use.</b></p>
<p><b>PO2</b> Emergency services and uses providing community support services are able to function effectively during and immediately after a bushfire hazard event.</p>	<p><b>AO2</b> Emergency Services and uses providing community support services are not located in a bushfire hazard sub-category and have direct access to low hazard evacuation routes.</p>	<p><b>Not Applicable.</b></p> <p><b>The proposal is not an Emergency Service or use providing community support services.</b></p>
<p><b>PO3</b> Development involving hazardous materials manufactured or stored in bulk is not located in bushfire hazard sub-category.</p>	<p><b>AO3</b> The manufacture or storage of hazardous material in bulk does not occur within bushfire hazard sub-category.</p>	<p><b>Not Applicable.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
		<p><b>The proposal does not involve the manufacture or storage of hazardous material.</b></p>
<p><b>Development design and separation from bushfire hazard – reconfiguration of lots</b></p>		
<p><b>PO4.1</b> Where reconfiguration is undertaken in an urban area or is for urban purposes or smaller scale rural residential purposes, a separation distance from hazardous vegetation is provided to achieve a radiant heat flux level of 29kW/m<sup>2</sup> at the edge of the proposed lot(s).</p> <p>Note - “Urban purposes” and “urban area” are defined in the <i>Sustainable Planning Regulations 2009</i>. Reconfiguration will be taken to be for rural residential purposes where proposed lots are between 2000m<sup>2</sup> and 2ha in area. “Smaller scale” rural residential purposes will be taken to be where the average proposed lot size is 6000m<sup>2</sup> or less.</p> <p>Note - The radiant heat levels and separation distances are to be established in accordance with method 2 set out in AS3959-2009.</p> <p><b>PO4.2</b> Where reconfiguration is undertaken for other purposes, a building envelope of reasonable dimensions is provided on each lot which achieves radiant heat flux level of 29kW/m<sup>2</sup> at any point.</p>	<p><b>AO4.1</b> No new lots are created within a bushfire hazard sub-category.</p> <p>or</p> <p><b>AO4.2</b> Lots are separated from hazardous vegetation by a distance that:</p> <p>(a) achieves radiant heat flux level of 29kW/m<sup>2</sup> at all boundaries; and</p> <p>(b) is contained wholly within the development site.</p> <p>Note - Where a separation distance is proposed to be achieved by utilising existing cleared developed areas external to the site, certainty must be established (through tenure or other means) that the land will remain cleared of hazardous vegetation. For staged developments, temporary separation distances, perimeter roads or fire trails may be absorbed as part of subsequent stages.</p> <p>Note - The achievement of a cleared separation distance may not be achievable where other provisions within the planning scheme require protection of certain ecological, slope, visual or character features or functions.</p>	<p><b>Not Applicable.</b></p> <p><b>The proposal is not for reconfiguration of a lot.</b></p>
<p><b>PO5</b> Where reconfiguration is undertaken in an urban area or is for urban purposes, a constructed perimeter road with reticulated water supply is established between the lots and the hazardous vegetation and is readily accessible at all times for urban fire fighting vehicles.</p>	<p><b>AO5.1</b> Lot boundaries are separated from hazardous vegetation by a public road which:</p> <p>(a) has a two lane sealed carriageway;</p> <p>(b) contains a reticulated water supply;</p> <p>(c) is connected to other public roads at both ends and at intervals of no more than 500m;</p>	<p><b>Not Applicable.</b></p> <p><b>The proposal is not for reconfiguration of a lot.</b></p>



Performance outcomes	Acceptable outcomes	Applicant response
<p>The access is available for both fire fighting and maintenance/defensive works.</p>	<p>(d) accommodates geometry and turning radii in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines;</p> <p>(e) has a minimum of 4.8m vertical clearance above the road;</p> <p>(f) is designed to ensure hydrants and water access points are not located within parking bay allocations; and</p> <p>(g) incorporates roll-over kerbing.</p> <p><b>AO5.2</b> Fire hydrants are designed and installed in accordance with AS2419.1 2005, unless otherwise specified by the relevant water entity.</p> <p>Note - Applicants should have regard to the relevant standards set out in the reconfiguration of a lot code and works codes in this planning scheme.</p>	
<p><b>PO6</b> Where reconfiguration is undertaken for smaller scale rural residential purposes, either a constructed perimeter road or a formed, all weather fire trail is established between the lots and the hazardous vegetation and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>The access is available for both fire fighting and maintenance/hazard reduction works.</p>	<p><b>AO6</b> Lot boundaries are separated from hazardous vegetation by a public road or fire trail which has:</p> <p>(a) a reserve or easement width of at least 20m;</p> <p>(b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15 tonne vehicle and which is at least 6m clear of vegetation;</p> <p>(c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path;</p> <p>(d) a minimum of 4.8m vertical clearance;</p> <p>(e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines;</p> <p>(f) a maximum gradient of 12.5%;</p>	<p><b>Not Applicable.</b></p> <p><b>The proposal is not for reconfiguration of a lot.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
	<ul style="list-style-type: none"> <li>(g) a cross fall of no greater than 10 degrees;</li> <li>(h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy;</li> <li>(i) vehicular access at each end which is connected to the public road network at intervals of no more than 500m;</li> <li>(j) designated fire trail signage;</li> <li>(k) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and</li> <li>(l) if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services.</li> </ul>	
<p><b>PO7</b> Where reconfiguration is undertaken for other purposes, a formed, all weather fire trail is provided between the hazardous vegetation and either the lot boundary or building envelope, and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>However, a fire trail will not be required where it would not serve a practical fire management purpose.</p>	<p><b>AO7</b> Lot boundaries are separated from hazardous vegetation by a public road or fire trail which has:</p> <ul style="list-style-type: none"> <li>(a) a reserve or easement width of at least 20m;</li> <li>(b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15 tonne vehicle and which is at least 6m clear of vegetation;</li> <li>(c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path;</li> <li>(d) a minimum of 4.8m vertical clearance;</li> <li>(e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines;</li> <li>(f) a maximum gradient of 12.5%;</li> <li>(g) a cross fall of no greater than 10 degrees;</li> <li>(h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy;</li> </ul>	<p><b>Not Applicable.</b></p> <p><b>The proposal is not for reconfiguration of a lot.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
	<ul style="list-style-type: none"> <li>(i) vehicular access at each end which is connected to the public road network;</li> <li>(j) designated fire trail signage;</li> <li>(k) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and</li> <li>(l) if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services.</li> </ul>	
<p><b>PO8</b> The development design responds to the potential threat of bushfire and establishes clear evacuation routes which demonstrate an acceptable or tolerable risk to people.</p>	<p><b>AO8</b> The lot layout:</p> <ul style="list-style-type: none"> <li>(a ) minimises the length of the development perimeter exposed to, or adjoining hazardous vegetation;</li> <li>(b) avoids the creation of potential bottle-neck points in the movement network;</li> <li>(c) establishes direct access to a safe assembly /evacuation area in the event of an approaching bushfire; and</li> <li>(d) ensures roads likely to be used in the event of a fire are designed to minimise traffic congestion.</li> </ul> <p>Note - For example, developments should avoid finger-like or hour-glass subdivision patterns or substantive vegetated corridors between lots. In order to demonstrate compliance with the performance outcome, a bushfire management plan prepared by a suitably qualified person may be required. The bushfire management plan should be developed in accordance with the Public Safety Business Agency (PSBA) guideline entitled "Undertaking a Bushfire Protection Plan. Advice from the Queensland Fire and Emergency Services (QFES) should be sought as appropriate</p>	<p><b>Not Applicable.</b></p> <p><b>The proposal is not for reconfiguration of a lot.</b></p>
<p><b>PO9</b> Critical infrastructure does not increase the potential bushfire hazard.</p>	<p><b>AO9</b> Critical or potentially hazardous infrastructure such as water supply, electricity, gas and telecommunications are placed underground.</p>	<p><b>Not Applicable.</b></p> <p><b>The proposal is not for reconfiguration of a lot.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<b>Development design and separation from bushfire hazard – material change of use</b>		
<p><b>PO10</b> Development is located and designed to ensure proposed buildings or building envelopes achieve a radiant heat flux level at any point on the building or envelope respectively, of:</p> <ul style="list-style-type: none"> <li>(a) 10kW/m<sup>2</sup> where involving a vulnerable use; or</li> <li>(b) 29kW/m<sup>2</sup> otherwise.</li> </ul> <p>The radiant heat flux level is achieved by separation unless this is not practically achievable.</p> <p>Note - The radiant heat levels and separation distances are to be established in accordance with method 2 set out in AS3959-2009.</p>	<p><b>AO10</b> Buildings or building envelopes are separated from hazardous vegetation by a distance that:</p> <ul style="list-style-type: none"> <li>(a) achieves a radiant heat flux level of at any point on the building or envelope respectively, of 10kW/m<sup>2</sup> for a vulnerable use or 29kW/m<sup>2</sup> otherwise; and</li> <li>(b) is contained wholly within the development site.</li> </ul> <p>Note - Where a separation distance is proposed to be achieved by utilising existing cleared developed areas external to the site, certainty must be established (through tenure or other means) that the land will remain cleared of hazardous vegetation.</p> <p>For staged developments, temporary separation distances, perimeter roads or fire trails may be absorbed as part of subsequent stages.</p> <p>Note - The achievement of a cleared separation distance may not be achievable where other provisions within the planning scheme require protection of certain ecological, slope, visual or character features or functions.</p>	<p><b>Will comply with AO10.</b></p> <p><b>This is a matter for detailed design, however at this stage may be conditioned by Council.</b></p>
<p><b>PO11</b> A formed, all weather fire trail is provided between the hazardous vegetation and the site boundary or building envelope, and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>However, a fire trail will not be required where it would not serve a practical fire management purpose.</p>	<p><b>AO11</b> Development sites are separated from hazardous vegetation by a public road or fire trail which has:</p> <ul style="list-style-type: none"> <li>(a) a reserve or easement width of at least 20m;</li> <li>(b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15 tonne vehicle and which is at least 6m clear of vegetation;</li> <li>(c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path;</li> <li>(d) a minimum of 4.8m vertical clearance;</li> </ul>	<p><b>Complies with AO11.</b></p> <p><b>The site is separated from the mapped hazards by public road.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<p>Note - Fire trails are unlikely to be required where a development site involves less than 2.5ha</p>	<ul style="list-style-type: none"> <li>(e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines;</li> <li>(f) a maximum gradient of 12.5%;</li> <li>(g) a cross fall of no greater than 10 degrees;</li> <li>(h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy;</li> <li>(i) vehicular access at each end which is connected to the public road network which is connected to the public road network at intervals of no more than 500m;</li> <li>(j) designated fire trail signage;</li> <li>(k) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and</li> <li>(l) if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services.</li> </ul>	
<b>All development</b>		
<p><b>PO12</b> All premises are provided with vehicular access that enables safe evacuation for occupants and easy access by fire fighting appliances.</p>	<p><b>AO12</b> Private driveways:</p> <ul style="list-style-type: none"> <li>(a) do not exceed a length of 60m from the street to the building;</li> <li>(b) do not exceed a gradient of 12.5%;</li> <li>(c) have a minimum width of 3.5m;</li> <li>(d) have a minimum of 4.8m vertical clearance;</li> <li>(e) accommodate turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; and</li> <li>(f) serve no more than 3 dwellings or buildings.</li> </ul>	<p><b>Complies with PO12.</b></p>



Performance outcomes	Acceptable outcomes	Applicant response
<p><b>PO13</b> Development outside reticulated water supply areas includes a dedicated static supply that is available solely for fire fighting purposes and can be accessed by fire fighting appliances.</p>	<p><b>AO13</b> A water tank is provided within 10m of each building (other than a class 10 building) which:</p> <ul style="list-style-type: none"> <li>(a) is either below ground level or of non-flammable construction;</li> <li>(b) has a take off connection at a level that allows the following dedicated, static water supply to be left available for access by fire fighters: <ul style="list-style-type: none"> <li>(i) 10,000l for residential buildings</li> </ul> </li> </ul> <p>Note – A minimum of 7,500l is required in a tank and the extra 2,500l may be in the form of accessible swimming pools or dams.</p> <ul style="list-style-type: none"> <li>(ii) 45,000l for industrial buildings; and</li> <li>(iii) 20,000l for other buildings;</li> <li>(c) includes shielding of tanks and pumps in accordance with the relevant standards;</li> <li>(d) includes a hardstand area allowing medium rigid vehicle (15 tonne fire appliance) access within 6m of the tank;</li> <li>(e) is provided with fire brigade tank fittings – 50mm ball valve and male camlock coupling and, if underground, an access hole of 200mm (minimum) to accommodate suction lines; and</li> <li>(f) is clearly identified by directional signage provided at the street frontage.</li> </ul>	<p><b>Not Applicable.</b></p> <p><b>The site is connected to reticulated supply.</b></p>
<p><b>PO14</b> Landscaping does not increase the potential bushfire risk.</p>	<p><b>AO14</b> Landscaping uses species that are less likely to exacerbate a bushfire event, and does not increase fuel loads within separation areas.</p>	<p><b>Will comply and may be further conditioned. The landscaping layout is subject to further detailed design.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<p><b>PO15</b> The risk of bushfire and the need to mitigate that risk is balanced against other factors (such as but not limited to, biodiversity or scenic amenity).</p>	<p><b>AO15</b> Bushfire risk mitigation treatments do not have a significant impact on the natural environment or landscape character of the locality where this has value.</p>	<p><b>Not Applicable.</b></p>

Note – ‘Vulnerable activities’ are those involving:

- (1) the accommodation or congregation of vulnerable sectors of the community such as child care centres, community care centre, educational establishments, detention facilities, hospitals, rooming accommodation, retirement facilities or residential care facilities; or
- (2) the provision of essential services including community uses, emergency services, utility installation, telecommunications facility, substations and major electricity infrastructure.

## 8.2.6 Landscape values overlay code

### 8.2.6.1 Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Landscape values overlay, if:
  - (a) self-assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
  - (b) impact assessable development.
- (2) Land in the Landscape values overlay is identified on the Landscape values overlay map in Schedule 2 and includes in following sub-categories:
  - (a) High landscape value sub-category;
  - (b) Medium landscape value sub-category;
  - (c) Scenic route buffer / view corridor area sub-category;
  - (d) Coastal scenery area sub-category.
- (3) When using this code, reference should be made to Part 5.

### 8.2.6.2 Purpose

- (1) The purpose of the Landscape values overlay code is to:
  - (a) implement the policy direction of the Strategic Framework, in particular:
    - (i) Theme 2: Environment and landscape values Element 3.5.5 Scenic amenity;
    - (ii) Theme 3: Natural resource management Element 3.6.4 – Resource extraction.
  - (b) enable an assessment of whether development is suitable on land within the Landscape values overlay sub-categories.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) areas of High landscape value are protected, retained and enhanced;
  - (b) areas of Medium landscape value are managed to integrate and limit the visual impact of development;
  - (c) the landscape values of the Coastal scenery area are managed to integrate and limit the visual impact of development;
  - (d) development maintains and enhances the significant landscape elements and features which contribute to the distinctive character and identity of Douglas Shire;
  - (e) ridges and vegetated hillslopes are not developed in a way that adversely impacts on landscape values;

- (f) watercourses, forested mountains and coastal landscape character types remain predominantly natural in appearance in order to maintain the region’s diverse character and distinctive tropical image, in particular:
  - (i) areas in the coastal landscape character type which are predominantly natural and undeveloped in appearance retain this natural landscape character;
  - (ii) watercourses which are predominantly natural and undeveloped in appearance retain this natural landscape character;
  - (iii) the rural character of cane fields and lowlands landscape character types which are predominantly rural or natural in appearance are maintained;
  - (iv) landscape values are maintained when viewed from lookouts, scenic routes, gateways and public places.
- (g) views towards High landscape value areas and the Coral Sea are not diminished;
- (h) development is consistent with the prevailing landscape character of its setting, and is neither visually dominant nor visually intrusive;
- (i) advertising devices do not detract from the landscape values, character types or amenity of an area.

**Criteria for assessment**

**Table 8.2.6.3.z – Landscape values overlay code – assessable development**

Performance outcomes	Acceptable outcomes	Applicant response
<b>For assessable development</b>		
<b>Development in a High landscape value area</b>		
<p><b>PO1</b> Development within High landscape value areas identified on the Landscape values overlay maps contained in Schedule 2:</p> <p>(a) avoids detrimental impacts on the landscape values of forested skylines, visible hillslopes, ridgelines, the coastal foreshore or the shoreline of other water bodies through the loss of vegetation;</p> <p>(b) is effectively screened from view from a road, lookout or other public place by an existing natural landform or native vegetation, or will be effectively screened by native vegetation within 3 years of construction;</p>	<p><b>AO1.1</b> Buildings and structures are not more than 8.5 metres and two storeys in height.</p> <p>Note - Height is inclusive of roof height.</p> <p><b>AO1.2</b> Buildings and structures are setback not less than 50 metres from ridgelines or peaks.</p> <p><b>AO1.3</b> Development is screened from view from roads or other public places by an existing natural landform or an existing native vegetation buffer.</p>	<p><b>Complies with AO1.1-AO1.8.</b></p> <p><b>Building height is less than 8.5m. The site is not located close to a ridgeline or peak. The development is consistent with the surrounding development in terms of built form and does not raise any issues of impact on landscape values.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<p>(c) retains existing vegetation and incorporates new landscaping to enhance existing vegetation and visually soften built form elements;</p> <p>(d) incorporates development of a scale, design, height, position on site, construction materials and external finishes that are compatible with the landscape values of the locality;</p> <p>(e) avoids detrimental impacts on landscape values and excessive changes to the natural landform as a result of the location, position on site, scale, design, extent and alignment of earthworks, roads, driveways, retaining walls and other on-ground or in-ground infrastructure;</p> <p>(f) avoids detrimental impacts on landscape values and views as a result of the location, position on site, scale, design and alignment of telecommunications facilities, electricity towers, poles and lines and other tall infrastructure;</p> <p>(g) extractive industry operations are avoided.</p> <p>Note - A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in order to satisfy performance outcomes.</p>	<p><b>AO1.4</b> Where development on land steeper than 1 in 6 (16.6%) cannot be avoided: (a) development follows the natural; contours of the site; buildings are split level or suspended floor construction, or a combination of the two; lightweight materials are used to areas with suspended floors.</p> <p>Note - Examples of suitable lightweight materials include timber or fibre cement boards or sheeting for walls and factory treated metal sheeting for walls and roofs.</p> <p><b>AO1.5</b> The external features, walls and roofs of buildings and structures have a subdued and non-reflective palette.</p> <p>Note - Examples of suitable colours include shades of green, olive green, blue green, grey green, green blue, indigo, brown, blue grey, and green yellow.</p> <p><b>AO1.6</b> No clearing of native vegetation occurs on land with a slope greater than 1 in 6 (16.5%).</p> <p><b>AO1.7</b> Where for accommodation activities or reconfiguration of a lot in a High landscape value area, development demonstrates that the height, design, scale, positioning on-site, proposed construction materials and external finishes are compatible with the landscape values.</p> <p>Note - A visual impact assessment undertaken in accordance with Planning scheme policy SC6.6 – Landscape values may be required.</p>	

Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>AO1.8</b> Advertising devices do not occur.</p>	
<b>Development within the Medium landscape value area</b>		
<p><b>PO2</b> Development within Medium landscape value areas identified on the Landscape values overlay maps contained in Schedule 2:</p> <p>(a) avoids detrimental impacts on the landscape values of forested skylines, visible hillslopes, ridgelines, the coastal foreshore or the shoreline of other water bodies through the loss of vegetation;</p> <p>(b) is effectively screened from view from a road, lookout or other public place by an existing natural landform or native vegetation, or will be effectively screened by native vegetation within 5 years of construction;</p> <p>(c) retains existing vegetation and incorporates new landscaping to enhance existing vegetation and visually soften built form elements;</p> <p>(d) incorporates development of a scale, design, height, position on site, construction materials and external finishes that are compatible with the landscape values of the locality;</p> <p>(e) avoids detrimental impacts on landscape values and excessive changes to the natural landform as a result of the location, position on site, scale, design and alignment of earthworks, roads, driveways, retaining walls and other on-ground or in-ground infrastructure;</p>	<p><b>AO2.1</b> Buildings and structures are not more than 8.5 metres and two storeys in height.</p> <p>Note - Height is inclusive of the roof height.</p> <p><b>AO2.2</b> Development is screened from view from roads or other public places by an existing natural landform or an existing native vegetation buffer.</p> <p><b>AO2.3</b> Where development on land steeper than 1 in 6 (16.6%) cannot be avoided:</p> <p>(a) development follows the natural; contours of the site;</p> <p>(b) buildings are split level or suspended floor construction, or a combination of the two;</p> <p>(c) lightweight materials are used to areas with suspended floors.</p> <p>Note - Examples of suitable lightweight materials include timber or fibre cement boards or sheeting for walls and factory treated metal sheeting for walls and roofs.</p> <p><b>AO2.4</b> The external features, walls and roofs of buildings and structures have a subdued and non-reflective palette.</p> <p>Note - Examples of suitable colours include shades of green, olive green, blue green, grey green, green blue, indigo, brown, blue grey, and green yellow.</p>	<p><b>Not Applicable.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<p>(f) avoids detrimental impacts on landscape values and views as a result of the location, position on site, scale, design and alignment of telecommunications facilities, electricity towers, poles and lines and other tall infrastructure;</p> <p>(g) extractive industry operations are avoided, or where they cannot be avoided, are screened from view.</p> <p>Note - A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in order to satisfy performance outcomes.</p>	<p><b>AO2.5</b> No clearing of native vegetation occurs on land with a slope greater than 1 in 6 (16.6%).</p> <p><b>AO2.6</b> Advertising devices do not occur.</p>	
<b>Development within a Scenic route buffer / view corridor area</b>		
<p><b>PO3</b> Development within a Scenic route buffer / view corridor area as identified on the Landscape values overlay maps contained in Schedule 2:</p> <p>(a) retains visual access to views of the surrounding landscape, the sea and other water bodies;</p> <p>(b) retains existing vegetation and incorporates landscaping to visually screen and soften built form elements whilst not impeding distant views or view corridors;</p> <p>(c) incorporates building materials and external finishes that are compatible with the visual amenity and the landscape character;</p> <p>(d) minimises visual impacts on the setting and views in terms of:</p> <p>(e) the scale, height and setback of buildings;</p> <p>(f) the extent of earthworks and impacts on the landform including the location and configuration of access roads and driveways;</p>	<p><b>AO3.1</b> Where within a Scenic route buffer / view corridor area, the height of buildings and structures is not more than identified within the acceptable outcomes of the applicable zone code.</p> <p><b>AO3.2</b> No clearing of native vegetation is undertaken within a Scenic route buffer area.</p> <p><b>AO3.3</b> Where within a Scenic route buffer / view corridor area development is set back and screened from view from a scenic route by existing native vegetation with a width of at least 10 metres and landscaped in accordance with the requirements of the landscaping code.</p> <p><b>AO3.4</b> Development does not result in the replacement of, or creation of new, additional, or enlarged advertising devices.</p>	<b>Not Applicable.</b>

Performance outcomes	Acceptable outcomes	Applicant response
<p>(g) the scale, extent and visual prominence of advertising devices.</p> <p>Note - A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in order to satisfy performance outcomes.</p>		
<b>Development within the Coastal scenery area</b>		
<p><b>PO4</b> The landscape values of the Coastal scenery zone as identified on the Landscape values overlay maps contained in Schedule 2 are managed to integrated and limit the visual impact of development.</p> <p>Note - A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in order to satisfy performance outcomes.</p>	<p><b>AO4.1</b> The dominance of the natural character of the coast is maintained or enhanced when viewed from the foreshore.</p> <p><b>AO4.2</b> Where located adjacent to the foreshore buildings and structures are setback:</p> <p>(a) Where no adjoining development, a minimum of 50 metres from the coastal high water mark and the setback area is landscaped with a native vegetation buffer that has a minimum width of 25 metres; or</p> <p>(b) Where there is adjoining development, setbacks will be consistent with that of adjoining buildings and structures, but not less than 10 metres from the coastal high water mark. The setback area is landscaped in accordance with the requirements of the Landscaping code.</p> <p><b>AO4.3</b> Where separated from the foreshore by land contained within public ownership (e.g. unallocated State land, esplanade or other public open space), buildings and structures area setback:</p>	<p><b>Not Applicable.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>(a) where no adjoining development, a minimum of 6 metres from the coastward property boundary. The setback area is landscaped in accordance with the requirements of the Landscaping code; or</p> <p>(b) where there is adjoining development, setbacks will be consistent with that of adjoining buildings and structures. The setback area is landscaped in accordance with the requirements of the Landscaping code.</p>	
<p><b>PO5</b> Development is to maximise opportunities to maintain and/or enhance natural landscape values through the maintenance and restoration of vegetated buffers between development and coastal waters, where practical.</p> <p>Note – A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in satisfaction of a performance outcome.</p>	<p><b>AO5</b> No clearing of native vegetation is undertaken within a Coastal scenery area zone, except for exempt vegetation damage undertaken in accordance with the Vegetation management code</p>	<p><b>Not Applicable.</b></p>

## 8.2.10 Transport network overlay code

### 8.2.10.1 Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Transport network overlay; if:
  - (a) self-assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
  - (b) impact assessable development.
- (2) Land within the Transport network overlay is identified on the Transport network (Road Hierarchy) overlay map and the Transport network (Pedestrian and Cycle) overlay map in Schedule 2 and includes the following sub-categories:
  - (a) Transport network (Road Hierarchy) overlay sub-categories:
    - (i) State controlled road sub-category;
    - (ii) Sub-arterial road sub-category;
    - (iii) Collector road sub-category;
    - (iv) Access road sub-category;
    - (v) Industrial road sub-category;
    - (vi) Major rural road sub-category;
    - (vii) Minor rural road sub-category;
    - (viii) Unformed road sub-category;
    - (ix) Major transport corridor buffer area sub-category.
  - (b) Transport network (Pedestrian and Cycle) overlay sub-categories:
    - (i) Principal route;
    - (ii) Future principal route;
    - (iii) District route;
    - (iv) Neighbourhood route;
    - (v) Strategic investigation route.

**8.2.10.2 Purpose**

- (1) The purpose of the Transport network overlay code is to:
  - (a) implement the policy direction of the Strategic Framework, in particular:
    - (i) Theme 1: Settlement pattern Element 3.4.2 Urban settlement, Element 3.4.3 Activity centres;
    - (ii) Theme 6: Infrastructure and transport Element 3.9.4 Transport;
  - (b) enable an assessment of whether development is suitable on land within the Transport network overlay.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) development provides for transport infrastructure (including active transport infrastructure);
  - (b) development contributes to a safe and efficient transport network;
  - (c) development supports the existing and future role and function of the transport network;
  - (d) development does not compromise the safety and efficiency of major transport infrastructure and facilities.

**Criteria for assessment**

**Table 8.2.10.3 a – Transport network overlay code – assessable development**

Performance outcomes	Acceptable outcomes	Applicant response
<b>For assessable development</b>		
<p><b>PO1</b> Development supports the road hierarchy for the region.</p> <p>Note -A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.</p>	<p><b>AO1.1</b> Development is compatible with the intended role and function of the transport network as identified on the Transport network overlay maps contained in Schedule 2.</p> <p><b>AO1.2</b> Development does not compromise the safety and efficiency of the transport network.</p> <p><b>AO1.3</b> Development is designed to provide access via the lowest order road, where legal and practicable access can be provided to that road.</p>	<p><b>Complies with AO 1.1 – 1.3</b></p> <p><b>Whilst a Traffic Engineering Assessment has not been prepared in support of the application, the modest scale and operational characteristics of the proposed development indicate that traffic generation will be low and comfortably within the capacity of the existing transport network.</b></p> <p><b>The proposal will operate with only three to four staff members and is expected to accommodate a limited number of visitors</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
		<p>each day, typically comprising one to two family appointments. Funeral services themselves will not be conducted from the site, and therefore the development will not generate periodic concentrations of traffic associated with large gatherings. The Applicant anticipates approximately two to three funerals per week, with overall vehicle movements associated with funeral operations and body transfers estimated at approximately eight to ten movements per week.</p> <p>Importantly, the proposal involves the adaptive reuse of an existing developed site that has historically accommodated commercial and industrial activities. The site benefits from an established access arrangement to Theresa Drive and is located within an industrial precinct designed to accommodate vehicle movements associated with business activities.</p> <p>Having regard to the low-intensity nature of the use, the limited staffing levels, modest visitor numbers and the existing function of the surrounding road network, it is considered that the proposal will not adversely impact the safety or efficiency of the transport network. Accordingly, the preparation of a detailed Traffic Engineering Assessment is not considered necessary in this instance, and the development is expected to operate well</p>

Performance outcomes	Acceptable outcomes	Applicant response
		<b>within the capacity of the existing road infrastructure.</b>
<p><b>PO2</b> Transport infrastructure is provided in an integrated and timely manner.</p> <p>Note - A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.</p>	<p><b>AO2</b> Development provides infrastructure (including improvements to existing infrastructure) in accordance with:</p> <p>(a) the Transport network overlay maps contained in Schedule 2; (b) any relevant Local Plan.</p> <p>Note – The Translink Public Transport Infrastructure Manual provides guidance on the design of public transport facilities.</p>	<b>Not Applicable. There is no additional transport infrastructure triggered by the development.</b>
<p><b>PO3</b> Development involving sensitive land uses within a major transport corridor buffer area is located, designed and maintained to avoid or mitigate adverse impacts on amenity for the sensitive land use.</p>	<p><b>AO3</b> No acceptable outcomes are prescribed.</p> <p>Note – Part 4.4 of the Queensland Development Code provides requirements for residential building design in a designated transport noise corridor.</p>	<b>Not Applicable.</b>
<p><b>PO4</b> Development does not compromise the intended role and function or safety and efficiency of major transport corridors.</p> <p>Note - A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.</p>	<p><b>AO4.1</b> Development is compatible with the role and function (including the future role and function) of major transport corridors.</p> <p><b>AO4.2</b> Direct access is not provided to a major transport corridor where legal and practical access from another road is available.</p>	<p><b>Complies with AO4.1-AO4.4.</b></p> <p><b>Refer to assessment comments in response to AO1.1-AO.3.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>AO4.3</b> Intersection and access points associated with major transport corridors are located in accordance with: (a) the Transport network overlay maps contained in Schedule 2; and (b) any relevant Local Plan.</p> <p><b>AO4.4</b> The layout of development and the design of the associated access is compatible with existing and future boundaries of the major transport corridor or major transport facility.</p>	
<p><b>PO5</b> Development retains and enhances existing vegetation between a development and a major transport corridor, so as to provide screening to potential noise, dust, odour and visual impacts emanating from the corridor.</p>	<p><b>AO5</b> No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
<b>Pedestrian and cycle network</b>		
<p><b>PO6</b> Lot reconfiguration assists in the implementation of the pedestrian and cycle movement network to achieve safe, attractive and efficient pedestrian and cycle networks</p>	<p><b>AO6.1</b> Where a lot is subject to, or adjacent to an element of the pedestrian and cycle Movement network (identified on the Transport network overlay maps contained in Schedule 2) the specific location of this element of the pedestrian and cycle network is incorporated in the design of the lot layout.</p> <p><b>AO6.2</b> The element of the pedestrian and cycle network is constructed in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme policy SC6.5 – FNQROC Regional Development Manual.</p>	<p><b>Not Applicable.</b></p> <p><b>The development is not for a lot reconfiguration.</b></p>

## 9.4.1 Access, parking and servicing code

### 9.4.1.1 Application

- (1) This code applies to assessing:
  - (a) operational work which requires a compliance assessment as a condition of a development permit; or
  - (b) a material change of use or reconfiguring a lot if:
    - (i) self-assessable or assessable development where this code is identified in the assessment criteria column of the table of assessment;
    - (ii) impact assessable development, to the extent relevant.
- (2) When using this code, reference should be made to Part 5.

### 9.4.1.2 Purpose

- (1) The purpose of the Access, parking and servicing code is to assess the suitability of access, parking and associated servicing aspects of a development.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) sufficient vehicle parking is provided on-site to cater for all types of vehicular traffic accessing and parking on-site, including staff, guests, patrons, residents and short term delivery vehicles;
  - (b) sufficient bicycle parking and end of trip facilities are provided on-site to cater for customer and service staff;
  - (c) on-site parking is provided so as to be accessible and convenient, particularly for any short term uses;
  - (d) development provides walking and cycle routes through the site which link the development to the external walking and cycling network;
  - (e) the provision of on-site parking, loading / unloading facilities and the provision of access to the site do not impact on the efficient function of street network or on the area in which the development is located;
  - (f) new vehicular access points are safely located and are not in conflict with the preferred ultimate streetscape character and local character and do not unduly disrupt any current or future on-street parking arrangements.

### 9.4.1.3 Criteria for assessment

Table 9.4.1.3.a – Access, parking and servicing code – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		

**PO1**

Sufficient on-site car parking is provided to cater for the amount and type of vehicle traffic expected to be generated by the use or uses of the site, having particular regard to:

- (a) the desired character of the area;
- (b) the nature of the particular use and its specific characteristics and scale;
- (c) the number of employees and the likely number of visitors to the site;
- (d) the level of local accessibility;
- (e) the nature and frequency of any public transport serving the area;
- (f) whether or not the use involves the retention

**AO1.1**

The minimum number of on-site vehicle parking spaces is not less than the number prescribed in Table 9.4.1.3.b for that particular use or uses.

Note - Where the number of spaces calculated from the table is not a whole number, the number of spaces provided is the next highest whole number.

**AO1.2**

Car parking spaces are freely available for the parking of vehicles at all times and are not used for external storage purposes, the display of products or rented/sub-leased.

**Alternative Solution to AO1.1. Complies with PO1.**

**The Access, Parking and Servicing Code prescribes a parking rate of one (1) parking space per 15m<sup>2</sup> of Gross Floor Area for a Funeral Parlour. Based on the proposed Gross Floor Area of approximately 162m<sup>2</sup>, strict application of the acceptable outcome would require the provision of eleven (11) on-site parking spaces.**

**The Applicant considers that compliance with the numerical parking standard would significantly exceed the actual operational requirements of the development and would not represent an efficient or practical use of the site.**

**The proposed Funeral Parlour will operate with only three to four staff members and is anticipated to accommodate a limited number of visitors on an appointment basis. The Applicant advises that the business typically receives one to two family groups per day, generally involving no more than six persons at any one time. Funeral services themselves will not be conducted from the site, with the premises instead functioning as an administration, consultation, mortuary and body preparation facility. Consequently, the development will not generate peak parking demands associated with funeral gatherings or public events.**

**The proposed development also does not represent an expansion of the existing business. Rather, it seeks to relocate and separate funeral administration and mortuary**



functions from the adjoining Sawmill Road premises to provide a more suitable operating environment for families and staff. Based on the Applicant's extensive experience operating the business, the provision of eleven parking spaces would substantially exceed the parking demand likely to be generated by the use.

Importantly, the proposal involves the adaptive reuse of an existing building on an established industrial site. The location, orientation and configuration of the existing building significantly constrain opportunities to provide additional on-site parking without substantial reconstruction works, loss of landscaping or adverse impacts on the functionality of the development. Requiring compliance with the prescribed parking rate would therefore result in an unreasonable outcome that is disproportionate to the operational needs of the use.

The proposal provides three (3) on-site parking spaces, including an accessible parking space, which are considered sufficient to accommodate the anticipated staff and visitor demand associated with the development. In addition, the site benefits from approximately 60 metres of frontage to Theresa Drive, providing ample opportunity for lawful on-street parking immediately adjoining the property. Theresa Drive is a low-volume industrial road with substantial kerbside parking capacity and no parking restrictions that would limit the availability of on-street spaces.

Given the low-intensity nature of the use, the appointment-based operation of the business, the modest staffing levels and the availability of

		<p>on-street parking, the proposed parking provision is considered adequate to meet the reasonable demands of the development. The alternative solution therefore achieves the intent of the Access, Parking and Servicing Code by ensuring sufficient parking is available without imposing an unnecessary and impractical parking requirement that bears little relationship to the actual operation of the use.</p>
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Performance outcomes	Acceptable outcomes	Applicant response
<p>of an existing building and the previous requirements for car parking for the building</p> <p>(g) whether or not the use involves a heritage building or place of local significance;</p> <p>(h) whether or not the proposed use involves the retention of significant vegetation.</p>	<p><b>AO1.3</b> Parking for motorcycles is substituted for ordinary vehicle parking to a maximum level of 2% of total ordinary vehicle parking.</p> <p><b>AO1.4</b> For parking areas exceeding 50 spaces parking, is provided for recreational vehicles as a substitute for ordinary vehicle parking to a maximum of 5% of total ordinary vehicle parking rate.</p>	<p><b>Not Applicable.</b></p> <p><b>No motorcycle parking is provided.</b></p> <p><b>Not Applicable.</b></p>
<p><b>PO2</b> Vehicle parking areas are designed and constructed in accordance with relevant standards.</p>	<p><b>AO2</b> Vehicle parking areas are designed and constructed in accordance with Australian Standard: (a) AS2890.1; (b) AS2890.3; (c) AS2890.6.</p>	<p><b>Complies with AO2.</b></p> <p><b>Vehicle parking is designed in accordance with the Australia Standards.</b></p> <p><b>This can further be conditioned to comply.</b></p>

**PO3**

Access points are designed and constructed:

- (a) to operate safely and efficiently;
- (b) to accommodate the anticipated type and volume of vehicles
- (c) to provide for shared vehicle (including cyclists) and pedestrian use, where appropriate;
- (d) so that they do not impede traffic or pedestrian movement on the adjacent road area;
- (e) so that they do not adversely impact upon existing intersections or future road or intersection improvements;
- (f) so that they do not adversely impact current and future on-street parking arrangements;
- (g) so that they do not adversely impact on existing services within the road reserve adjacent to the site;
- (h) so that they do not involve ramping, cutting of the adjoining road reserve or any built structures (other than what may be necessary to cross over a stormwater channel).

**AO3.1**

Access is limited to one access cross over per site and is an access point located, designed and constructed in accordance with:

- (a) Australian Standard AS2890.1;
- (b) Planning scheme policy SC6.5 – FNQROC Regional Development Manual - access crossovers.

**AO3.2**

Access, including driveways or access crossovers:

- (a) are not placed over an existing:
  - (i) telecommunications pit;
  - (ii) stormwater kerb inlet;
  - (iii) sewer utility hole;
  - (iv) water valve or hydrant.
- (b) are designed to accommodate any adjacent footpath;
- (c) adhere to minimum sight distance requirements in accordance with AS2980.1.

**AO3.3**

Driveways are:

- (a) designed to follow as closely as possible to the existing contours, but are no steeper than the gradients outlined in Planning scheme policy SC6.5 – FNQROC Regional Development Manual;
- (b) constructed such that where there is a grade shift to 1 in 4 (25%), there is an area with a grade of no more than 1 in 6 (16.6%) prior to this area, for a distance of at least 5 metres;
- (c) on gradients greater than 1 in 6 (16.6%) driveways are constructed to ensure the cross-fall of the driveway is one way and directed into the hill, for vehicle safety and drainage purposes;

**Complies with PO3.**

**The proposed development relies on the existing access arrangement. No changes are proposed in this regard.**



Performance outcomes	Acceptable outcomes	Applicant response
	<p>(d) constructed such that the transitional change in grade from the road to the lot is fully contained within the lot and not within the road reserve;</p> <p>(e) designed to include all necessary associated drainage that intercepts and directs storm water runoff to the storm water drainage system.</p> <p><b>A03.4</b> Surface construction materials are consistent with the current or intended future streetscape or character of the area and contrast with the surface construction materials of any adjacent footpath.</p>	
<p><b>PO4</b> Sufficient on-site wheel chair accessible car parking spaces are provided and are identified and reserved for such purposes.</p>	<p><b>A04</b> The number of on-site wheel chair accessible car parking spaces complies with the rates specified in AS2890 Parking Facilities.</p>	<p><b>Not Applicable.</b></p> <p><b>The development does not provide for on-site wheel chair accessible car parking.</b></p>
<p><b>PO5</b> Access for people with disabilities is provided to the building from the parking area and from the street.</p>	<p><b>A05</b> Access for people with disabilities is provided in accordance with the relevant Australian Standard.</p>	<p><b>Not Applicable.</b></p> <p><b>See comments above.</b></p>
<p><b>PO6</b> Sufficient on-site bicycle parking is provided to cater for the anticipated demand generated by the development.</p>	<p><b>A06</b> The number of on-site bicycle parking spaces complies with the rates specified in Table 9.4.1.3.b.</p>	<p><b>Given the nature of the proposal it is submitted that formal bicycle parking is not a priority. Sufficient area exists within the site to support informal parking of staff bicycles should the need arise.</b></p>



<p><b>PO7</b> Development provides secure and convenient bicycle parking which:</p> <ul style="list-style-type: none"> <li>(a) for visitors is obvious and located close to the building's main entrance;</li> <li>(b) for employees is conveniently located to provide secure and convenient access between the bicycle storage area, end-of-trip facilities and the main area of the building;</li> <li>(c) is easily and safely accessible from outside the site.</li> </ul>	<p><b>AO7.1</b> Development provides bicycle parking spaces for employees which are co-located with end-of-trip facilities (shower cubicles and lockers);</p> <p><b>AO7.2</b> Development ensures that the location of visitor bicycle parking is discernible either by direct view or using signs from the street.</p> <p><b>AO7.3</b> Development provides visitor bicycle parking which does not impede pedestrian movement.</p>	<p><b>Given the nature of the proposal it is submitted that formal bicycle parking is not a priority. Sufficient area exists within the site to support informal parking of staff bicycles should the need arise.</b></p>
<p><b>PO8</b> Development provides walking and cycle routes through the site which:</p> <ul style="list-style-type: none"> <li>(a) link to the external network and pedestrian and cyclist destinations such as schools, shopping centres, open space, public transport stations, shops and local activity centres along the safest, most direct and convenient routes;</li> <li>(b) encourage walking and cycling;</li> <li>(c) ensure pedestrian and cyclist safety.</li> </ul>	<p><b>AO8</b> Development provides walking and cycle routes which are constructed on the carriageway or through the site to:</p> <ul style="list-style-type: none"> <li>(a) create a walking or cycle route along the full frontage of the site;</li> <li>(b) connect to public transport and existing cycle and walking routes at the frontage or boundary of the site.</li> </ul>	<p><b>Not Applicable.</b></p>
<p><b>PO9</b> Access, internal circulation and on-site parking for service vehicles are designed and constructed:</p> <ul style="list-style-type: none"> <li>(a) in accordance with relevant standards;</li> </ul>	<p><b>AO9.1</b> Access driveways, vehicle manoeuvring and on-site parking for service vehicles are designed and constructed in accordance with AS2890.1 and</p>	<p><b>Complies with PO9.</b></p> <p><b>Service vehicle parking is not required, except for the Herse. Access and parking for the Herse is accommodated within the design.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<p>(b) so that they do not interfere with the amenity of the surrounding area;</p> <p>(c) so that they allow for the safe and convenient movement of pedestrians, cyclists and other vehicles.</p>	<p>AS2890.2.</p> <p><b>AO9.2</b> Service and loading areas are contained fully within the site.</p> <p><b>AO9.3</b> The movement of service vehicles and service operations are designed so they: (a) do not impede access to parking spaces; (b) do not impede vehicle or pedestrian traffic movement.</p>	
<p><b>PO10</b> Sufficient queuing and set down areas are provided to accommodate the demand generated by the development.</p>	<p><b>AO10.1</b> Development provides adequate area on-site for vehicle queuing to accommodate the demand generated by the development where drive through facilities or drop-off/pick-up services are proposed as part of the use, including, but not limited to, the following land uses: (a) car wash; (b) child care centre; (c) educational establishment where for a school; (d) food and drink outlet, where including a drive-through facility; (e) hardware and trade supplies, where including a drive-through facility; (f) hotel, where including a drive-through facility; (g) service station.</p> <p><b>AO10.2</b> Queuing and set-down areas are designed and constructed in accordance with AS2890.1.</p>	<p><b>Not Applicable.</b></p>

**Table 9.4.1.3.b – Access, parking and servicing requirements**

Note – Where the number of spaces is not a whole number, the number of spaces to be provided is the next highest whole number.

Note – Where the proposed development involves one or more land use, the minimum number of spaces for the proposed development will be calculated using the minimum number of spaces specified for each land use component.



Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
Agricultural supplies store	1 space per 50m <sup>2</sup> of GFA and outdoor display area.	1 space per 200m <sup>2</sup> of GFA.	n/a	LRV
Air services	1 car space per 20m <sup>2</sup> of covered reception area, plus 1 car space per 2 staff, plus a covered bus set down area adjacent to the entry of the reception area and 2 bus parking spaces.	n/a	n/a	LRV



Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
Bulk landscape supplies	1 space per 50m <sup>2</sup> GFA and outdoor display area.	1 space per 200m <sup>2</sup> of GFA.	n/a	MRV
Caretaker's accommodation	A minimum of 1 space	n/a	n/a	n/a
Child care centre	1 space per 10 children to be used for setting down and picking up of children, with a minimum of 3 car spaces to be provided for set down and collection; plus 1 space per employee. Any drive-through facility can provide tandem short term parking for 3 car spaces for setting down/picking up of children, on the basis that a passing lane is provided and line-marked to be kept clear of standing vehicles at all times.	n/a	n/a	VAN
Club	Unlicensed clubrooms: 1 space per 45m <sup>2</sup> of GFA. Licensed clubrooms: 1 space per 15m <sup>2</sup> of GFA.	1 space per 4 employees.	n/a	Licensed and equal or greater than 1500m <sup>2</sup> : RCV Other: VAN
Community care centre	1 space per 20m <sup>2</sup> of GFA.	A minimum of 1 space.	n/a	RCV



Community residence	A minimum of 2 spaces.	A minimum of 1 space.	n/a	VAN
Community use	1 space per 15m <sup>2</sup> GFA.	1 space per 100m <sup>2</sup> of GFA.	n/a	RCV
Dual occupancy	A minimum of 2 spaces per dwelling unit which may be in tandem with a minimum of 1 covered space per dwelling unit.	n/a	n/a	n/a
Dwelling house	A minimum of 2 spaces which may be in tandem plus 1 space for a secondary dwelling	n/a	n/a	n/a
Dwelling unit	1.5 spaces per one or two bedroom unit; or 2 spaces per three bedroom unit.	n/a	n/a	n/a
Educational establishment	Primary school or secondary schools: 1 car space per 2 staff members, plus provision of space to be used	Primary school or secondary schools: 1 space per 5	Required for all educational establishments with a GFA	RCV



Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
	for setting down and picking up of students. Tertiary and further education: 1 car space per 2 staff members, plus 1 car space per 10 students, plus provision of space to be used for setting down and picking up of students.	students over year 4. Tertiary and further education: 2 spaces per 50 full time students.	greater than 2000m <sup>2</sup> .	
Food and drink outlet	1 space per 25m <sup>2</sup> GFA and outdoor dining area. or If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m <sup>2</sup> of GFA, and outdoor dining area.	1 space per 100m <sup>2</sup> of GFA, and outdoor dining area.	n/a	See Table 9.4.1.3.d
Function facility	1 space per 15m <sup>2</sup> GFA.	1 space per 100m <sup>2</sup> of GFA.	n/a	RCV
Funeral parlour	1 space per 15m <sup>2</sup> GFA.	n/a	n/a	RCV
Garden centre	1 space per 50m <sup>2</sup> GFA and outdoor display area	1 space per 200m <sup>2</sup> of GFA.	n/a	AV
Hardware and trade supplies	1 space per 50m <sup>2</sup> GFA and outdoor display area	1 space per 200m <sup>2</sup> of GFA.	n/a	AV



Health care services	1 space per 20m <sup>2</sup> of GFA.	1 space per 100m <sup>2</sup> of GFA.	Required for all health care services with a GFA greater than 2000m <sup>2</sup> .	VAN
High impact industry	1 space per 90m <sup>2</sup> of GFA.	n/a	n/a	AV
Home based business	The parking required for the dwelling house, plus 1 space per bedroom where the Home based business involves the provision of accommodation; or 1 space per 25m <sup>2</sup> GFA for any other Home Based Business.	n/a	n/a	n/a
Hospital	The greater of 1 space per 2 bedrooms or 1 space per 4 beds; plus 1 car space for ambulance parking, designated accordingly.	1 space per 100m <sup>2</sup> of GFA.	Required for all hospitals with a GFA greater than 2000m <sup>2</sup> .	RCV
Hotel	1 space per 10m <sup>2</sup> GFA and	1 space per	n/a	LRV



Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
	licensed outdoor area; plus For 1 space per 50m <sup>2</sup> GFA of floor area of liquor barn or bulk liquor sales area; plus, if a drive in bottle shop is provided, queuing lane/s on site for 12 vehicles. Note - Use standard for any Short Term Accommodation for hotel accommodation use.	100m <sup>2</sup> of GFA.		
Indoor sport and recreation	Squash court or another court game: 4 spaces per court. Basketball, netball, soccer, cricket: 25 spaces per court / pitch. Ten pin bowling: 3 spaces per bowling lane. Gymnasium: 1 space per 15m <sup>2</sup> of GFA.	1 space per 4 employees.	n/a	RCV
Low impact industry	1 space per 90m <sup>2</sup> of GFA.	n/a	n/a	AV
Marine industry	1 space per 90m <sup>2</sup> of GFA.	n/a	n/a	AV
Medium impact industry	1 space per 90m <sup>2</sup> of GFA.	n/a	n/a	AV



Multiple dwelling	If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie Local plan: 1 car space per dwelling unit. If outside Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie Local plan: 1.5 car spaces per dwelling unit In all cases 60% of the car parking area is to be covered.	1 bicycle space per 3 units and 1 visitor bicycle space per 12 units.	n/a	RCV (over 10 units)
Office	1 space per 25m <sup>2</sup> of GFA or If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m <sup>2</sup> of GFA	1 space per 200m <sup>2</sup> GFA	Required for all office development with a GFA greater than 2000m <sup>2</sup> .	See Table 9.4.1.3.e
Outdoor sales	1 space per 50m <sup>2</sup> GFA and outdoor display area	1 space per 200m <sup>2</sup> of GFA.	n/a	AV
Outdoor sport and recreation	Coursing, horse racing, pacing, trotting: 1 space per 5 seated spectators,	Football: 5 space per field.	n/a	RCV



Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
	<p>plus 1 space per 5m<sup>2</sup> of other spectator areas.</p> <p>Football: 50 spaces per field.</p> <p>Lawn bowls: 30 spaces per green.</p> <p>Swimming pool: 15 spaces; plus 1 space per 100m<sup>2</sup> of useable site area.</p> <p>Tennis court or other court game: 4 spaces per court.</p> <p>Golf course: 4 spaces per tee on the course.</p> <p>Note - Use standard for Club for clubhouse component.</p>	<p>Lawn bowls: 5 spaces per green.</p> <p>Swimming pool: 1 space per swimming lane.</p> <p>Tennis court or other court game: 4 space per court.</p> <p>Golf course: 1 space per 15m<sup>2</sup> of GFA for clubhouse component.</p>		
Place of worship	1 space per 15m <sup>2</sup> of GFA.	1 space per 100m <sup>2</sup> of GFA.	n/a	LRV
Relocatable home park	1 space per relocatable home site; plus 0.1 space per relocatable home site for visitor parking; plus 1 space for an on-site manager	n/a	n/a	LRV



Research and technology industry	1 space per 90m <sup>2</sup> of GFA.	n/a	n/a	MRV
Residential care facility	1 visitor car space per 5 bedroom units; plus 1 car space per 2 staff members	n/a	n/a	LRV
Resort complex	Use standard for relevant standard for each component.  For example: Use Short Term Accommodation standard for accommodation component and Food and Drink Outlet for restaurant component.	Use standard for relevant standard for each component.  For example: Use Short Term Accommodation standard for accommodation component and Food and	n/a	RCV



Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
		Drink Outlet for restaurant component.		
Retirement facility	1 space per dwelling unit; plus 1 visitor space per 5 dwelling units; plus 1 visitor car space per 10 hostel units, nursing home or similar beds, plus 1 car space per 2 staff members; plus 1 car parking space for ambulance parking.	n/a	n/a	LRV
Sales office	A minimum of 1 space.	n/a	n/a	n/a
Service industry	1 space per 90m <sup>2</sup> of GFA.	n/a	n/a	SRV
Service station	1 space per 25m <sup>2</sup> of GFA	n/a	n/a	AV
Shop	1 space per 25m <sup>2</sup> of GFA. or If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m <sup>2</sup> of GFA.	1 space per 100m <sup>2</sup> of GFA.	Required for all shops with a GFA greater than 2000m <sup>2</sup> .	See Table 9.4.1.3.d



Shopping centre	1 space per 25m <sup>2</sup> of GFA. or If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m <sup>2</sup> of GFA.	1 space per 200m <sup>2</sup> GFA.	Required for all shopping centres with a GFA greater than 2000m <sup>2</sup> .	See Table 9.4.1.3.d
Short term accommodation	If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan: 0.5 car spaces per dwelling unit.  If outside Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan: For up to 5 units: 1 car space per dwelling unit, plus 1 space for visitors and 1 service/staff spaces. For 5 – 10 units: 1 car space per dwelling unit, plus 2 spaces for visitors and 1 service/staff spaces.	1 space per 10 rooms	n/a	SRV



Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
	<p>For over 10 units: 0.75 car spaces per dwelling unit, plus 3 spaces for visitors and 2 service/staff parking for the first 10 units and 0.5 additional service/staff space per 10 units, there-above.</p> <p>In all cases 60% of the car parking area is to be covered.</p> <p>Note: Where Short term accommodation is to be inter-changeable with a Multiple dwelling land use, multiple dwelling parking rates apply.</p>			
Showroom	1 space per 50m <sup>2</sup> GFA.	1 space per 200m <sup>2</sup> GFA.	n/a	AV
Special industry	1 space per 90m <sup>2</sup> of GFA.	n/a	n/a	AV
Tourist park	1 car space per caravan site, tent site or cabin; plus 1 visitor car space per 10 caravan sites, tent sites or cabins; plus 1 car space for an on-site manager.	n/a	n/a	LRV
Theatre	<p>Indoor: 1 space per 15m<sup>2</sup> of GFA.</p> <p>Outdoor cinema: 1 space per 5m<sup>2</sup> of designated viewing area, plus 1 car space per 2 employees.</p>	1 space per 200m <sup>2</sup> GFA.	n/a	VAN



Veterinary services	1 space per 50m <sup>2</sup> of GFA.	n/a	n/a	VAN
Warehouse	1 space per 90m <sup>2</sup> of GFA.	n/a	n/a	Where self-storage: RCV Other: AV
Any use not otherwise specified in this table.	Sufficient spaces to accommodate number of vehicles likely to be parked at any one time.	Sufficient spaces to accommodate number of vehicles likely to be parked at any one time.		To be determined

**Table 9.4.1.3.c – Design vehicles**

<b>VAN</b>	A 99.8th percentile vehicle equivalent to a large car.
<b>SRV</b>	Small rigid vehicle as in AS2890.2-2002 parking facilities – Off-street commercial vehicle facilities, but incorporating a body width of 2.33m
<b>MRV</b>	Medium rigid vehicle equivalent to an 8-tonne truck.
<b>LRV</b>	Large rigid vehicle described by AS2890.2-2002 parking facilities – Off-street commercial vehicle facilities as heavy rigid vehicle.
<b>RCV</b>	Industrial refuse collection vehicle
<b>AV</b>	19 metre articulated vehicle from AUSTROADS

**Table 9.4.1.3.d – Standard number of service bays required for Food and drink outlet, Shop or Shopping centre**

Gross floor area (m <sup>2</sup> )	Service bays required			
	VAN	SRV	MRV	LRV
0-199	-	1	-	-
200 – 599	1	-	1	-
600 – 999	1	1	1	-
1000 – 1499	2	1	1	-
1500 – 1999	2	2	1	-
2000 – 2799	2	2	2	-
2800 – 3599	2	2	2	1
3600 and over	To be determined via a parking study.			

**Table 9.4.1.3.e – Standard number of service bays required for Office**

Gross floor area (m <sup>2</sup> )	Service bays required			
	VAN	SRV	MRV	LRV
0-999	-	1	-	-

1000 – 2499	1	-	1	-
2500 – 3999	2	1	1	-
4000 – 5999	3	1	1	-
6000 – 7999	4	1	1	-
8000 – 9999	4	2	1	-
10000 and over	To be determined via a parking study.			

### 9.4.3 Environmental performance code

#### 9.4.3.1 Application

- (1) This code applies to assessing:
  - (a) building work for outdoor lighting;
  - (b) a material change of use or reconfiguring a lot if:
    - (i) assessable development where the code is identified in the assessment criteria column of a table of assessment; or
    - (ii) impact assessable development, to the extent relevant.

Note – Where for the purpose of lighting a tennis court in a Residential zone, a compliance statement prepared by a suitably qualified person must be submitted to Council with the development application for building work.

- (2) When using this code, reference should be made to Part 5.

#### 9.4.3.2 Purpose

- (1) The purpose of the Environmental performance code is to ensure development is designed and operated to avoid or mitigate impacts on sensitive receiving environments.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) activities that have potential to cause an adverse impact on amenity of adjacent and surrounding land, or environmental harm is avoided through location, design and operation of the development;
  - (b) sensitive land uses are protected from amenity related impacts of lighting, odour, airborne particles and noise, through design and operation of the development;
  - (c) stormwater flowing over, captured or discharged from development sites is of a quality adequate to enter receiving waters and downstream environments;
  - (d) development contributes to the removal and ongoing management of weed species.

#### 9.4.3.3 Criteria for assessment

Table 9.4.3.3.a – Environmental performance code – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>Lighting</b>		

<p><b>PO1</b> Lighting incorporated within development does not cause an adverse impact on the amenity of adjacent uses and nearby sensitive land uses.</p>	<p><b>AO1.1</b> Technical parameters, design, installation, operation and maintenance of outdoor lighting comply with the requirements of Australian standard AS4282-1997 Control of the obtrusive effects of outdoor lighting.</p> <p><b>AO1.2</b> Development that involves flood lighting is restricted to a type that gives no upward component of light where mounted horizontally.</p> <p><b>AO1.3</b> Access, car parking and manoeuvring areas are designed to shield nearby residential premises from impacts of vehicle headlights.</p>	<p><b>May be conditioned to Comply with AO1.1-AO1.2.</b></p> <p><b>Outdoor lighting will be designed to comply with the requirements of Australian standard AS4282-1997 Control of the obtrusive effects of outdoor lighting.</b></p> <p><b>AO1.3 is Not Applicable.</b></p>
<p><b>Noise</b></p>		
<p><b>PO2</b> Potential noise generated from the development is avoided through design, location and operation of the activity.</p> <p>Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p><b>AO2.1</b> Development does not involve activities that would cause noise related environmental harm or nuisance; or</p>	<p><b>Complies with AO 2.1 and AO2.2.</b></p> <p><b>The proposed Funeral Parlour is not of a nature that is expected to generate significant noise emissions or adversely impact the amenity of surrounding properties.</b></p> <p><b>The primary activities undertaken from the site will comprise administrative functions, family consultations, funeral planning, mortuary operations and body storage. These activities are inherently low intensity and are predominantly conducted within an enclosed building. Unlike many commercial or industrial land uses, the proposal does not involve machinery, manufacturing processes, outdoor operations, entertainment activities or the regular congregation of large numbers of people.</b></p> <p><b>The business will operate with only three to four staff members and is expected to receive a limited number of visitors each day,</b></p>



generally on an appointment basis. Funeral services themselves will not be conducted from the site and therefore the development will not generate noise associated with large gatherings, public events or ceremonial activities.

Vehicle movements associated with the development are also anticipated to be modest. The Applicant estimates that the business will facilitate approximately two to three funerals per week, with overall vehicle movements associated with funeral operations and body transfers remaining low. Any vehicle movements will occur during normal business hours and are not expected to differ significantly from those typically associated with other service-based businesses within the locality.

Furthermore, the site is located within the Industry Zone where a higher level of background activity and operational noise is already anticipated under the Planning Scheme. The proposed Funeral Parlour is substantially less intensive than many industrial or commercial uses that could otherwise establish within the zone.

Accordingly, the development is not expected to generate noise impacts beyond the site boundaries and can operate without adversely affecting the amenity of surrounding properties.

Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>AO2.2</b> Development ensures noise does not emanate from the site through the use of materials, structures and architectural features to not cause an adverse noise impact on adjacent uses.</p> <p><b>AO2.3</b> The design and layout of development ensures car parking areas avoid noise impacting directly on adjacent sensitive land uses through one or more of the following: (a) car parking is located away from adjacent sensitive land uses; (b) car parking is enclosed within a building; (c) a noise ameliorating fence or structure is established adjacent to car parking areas where the fence or structure will not have a visual amenity impact on the adjoining premises; (d) buffered with dense landscaping.</p> <p><i>Editor's note - The Environmental Protection (Noise) Policy 2008, Schedule 1 provides guidance on acoustic quality objectives to ensure environmental harm (including nuisance) is avoided.</i></p>	<p><b>Complies with AO2.3</b></p> <p><b>There are no identified adjacent sensitive uses.</b></p>
<b>Airborne particles and other emissions</b>		
<p><b>PO3</b> Potential airborne particles and emissions generated from the development are avoided through design, location and operation of the activity.</p> <p>Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p><b>AO3.1</b> Development does not involve activities that will result in airborne particles or emissions being generated;</p> <p>or</p> <p><b>AO3.2</b> The design, layout and operation of the development activity ensures that no airborne particles or emissions cause environmental harm or nuisance.</p>	<p><b>Complies with AO 3.1 and AO3.2</b></p> <p><b>The proposed uses on site to do result in any airborne particles or emissions.</b></p>



	<p>Note - examples of activities which generally cause airborne particles include spray painting, abrasive blasting, manufacturing activities and car wash facilities.</p> <p>Examples of emissions include exhaust ventilation from basement or enclosed parking structures, air conditioning/refrigeration ventilation and exhaustion.</p> <p>The <i>Environmental Protection (Air) Policy 2008</i>, Schedule 1 provides guidance on air quality objectives to ensure environmental harm (including nuisance) is avoided.</p>	
<b>Odours</b>		
<p><b>PO4</b> Potential odour causing activities associated with the development are avoided through design, location and operation of the activity.</p> <p>Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p><b>AO4.1</b> The development does not involve activities that create odorous emissions;</p> <p>or</p> <p><b>AO4.2</b> The use does not result in odour that causes</p>	<p><b>Complies with AO 4.1 and AO4.2</b></p> <p><b>The proposed uses on site to do result in any activities that create odorous emissions.</b></p>



Performance outcomes	Acceptable outcomes	Applicant response
	environmental harm or nuisance with respect to surrounding land uses.	
<b>Waste and recyclable material storage</b>		
<p><b>PO5</b> Waste and recyclable material storage facilities are located and maintained to not cause adverse impacts on adjacent uses.</p> <p>Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p><b>AO5.1</b> The use ensures that all putrescent waste is stored in a manner that prevents odour nuisance and is disposed of at regular intervals.</p> <p><b>AO5.2</b> Waste and recyclable material storage facilities are located, designed and maintained to not cause an adverse impact on users of the premises and adjacent uses through consideration of:</p> <ul style="list-style-type: none"> <li>(a) the location of the waste and recyclable material storage areas in relation to the noise and odour generated;</li> <li>(b) the number of receptacles provided in relation to the collection, maintenance and use of the receptacles;</li> <li>(c) the durability of the receptacles, sheltering and potential impacts of local climatic conditions;</li> <li>(d) the ability to mitigate spillage, seepage or leakage from receptacles into adjacent areas and sensitive receiving waters and environments.</li> </ul> <p>Editor's note - the <i>Environmental Protection (Waste Management) Policy 2008</i> provides guidance on the design of waste containers (receptacles) to ensure environmental harm (including nuisance) is avoided.</p>	<p><b>May be conditioned to Comply with AO 5.1 and 5.2</b></p> <p><b>A designated waste storage facility is not specifically detailed within the Architectural Plans. However, there are appropriate areas available within the site to accommodate waste storage.</b></p>
<b>Sensitive land use activities</b>		



<p><b>PO6</b> Sensitive land use activities are not established in areas which will receive potentially incompatible impacts on amenity from surrounding, existing development activities and land uses.</p>	<p><b>AO6.1</b> Sensitive land use activities are not established in areas that will be adversely impacted upon by existing land uses, activities and potential development possible in an area;</p> <p>or</p> <p><b>AO6.2</b> Sensitive land activities are located in areas where potential adverse amenity impacts mitigate all potential impacts through layout, design, operation and maintenance.</p>	<p><b>AO6.1 and AO6.2 are Not Applicable.</b></p>
<p><b>Stormwater quality</b></p>		
<p><b>PO7</b> The quality of stormwater flowing over, through or being discharged from development activities into watercourses and drainage lines is of adequate quality for downstream environments, with respect to:</p> <ul style="list-style-type: none"> <li>(a) the amount and type of pollutants borne from the activity;</li> <li>(b) maintaining natural stream flows;</li> <li>(c) the amount and type of site disturbance;</li> <li>(d) site management and control measures.</li> </ul>	<p><b>AO7.1</b> Development activities are designed to ensure stormwater over roofed and hard stand areas is directed to a lawful point of discharge.</p> <p><b>AO7.2</b> Development ensures movement of stormwater over the site is not impeded or directed through potentially polluting activities.</p>	<p><b>Complies with AO 7.1 to AO 7.3</b></p> <p><b>All stormwater can be directed to a lawful point of discharge. Given the site conditions soil and water control measures are not considered necessary and it is expected that any concerns can be addressed through conditions.</b></p>



Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>A07.3</b> Soil and water control measures are incorporated into the activity's design and operation to control sediment and erosion potentially entering watercourses, drainage lines and downstream receiving waters.</p> <p>Note - Planning scheme policy - FNQROC Regional Development Manual provides guidance on soil and water control measures to meet the requirements of the <i>Environmental Protection Act 1994</i>.</p> <p>During construction phases of development, contractors and builders are to have consideration in their work methods and site preparation for their environmental duty to protect stormwater quality.</p>	
<b>Pest plants (for material change of use on vacant land over 1,000m<sup>2</sup>)</b>		
<p><b>PO8</b> Development activities and sites provide for the removal of all pest plants and implement ongoing measures to ensure that pest plants do not reinfest the site or nearby sites.</p> <p>Editor's note - This does not remove or replace all land owner's obligations or responsibilities under the <i>Land Protection (Pest and Stock Route Management) Act 2002</i>.</p>	<p><b>A08.1</b> The land is free of declared pest plants before development establishes new buildings, structures and practices;</p> <p>or</p> <p><b>A08.2</b> Pest plants detected on a development site are removed in accordance with a management plan prepared by an appropriately qualified person prior to construction of buildings and structures or earthworks.</p> <p>Note - A declaration from an appropriately qualified person validates the land being free from pest plants.</p> <p>Declared pest plants include locally declared and State declared pest plants.</p>	<p><b>Will comply with PO8 and may be conditioned.</b></p>

## 9.4.4 Filling and excavation code

### 9.4.4.1 Application

- (1) This code applies to assessing:
  - (a) operational work for filling or excavation which is self-assessable or code assessable development if this code is an applicable code identified in the assessment criteria column of a table of assessment; or
  - (b) a material change of use or reconfiguring a lot if:
    - (i) assessable development where this code is identified as a prescribed secondary code in the assessment criteria column of a table of assessment; or
    - (ii) impact assessable development, to the extent relevant.

Note—This code does not apply to building work that is regulated under the Building Code of Australia.

- (2) When using this code, reference should be made to Part 5.

### 9.4.4.2 Purpose

- (1) The purpose of the Filling and excavation code is to assess the suitability of development for filling or excavation.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) filling or excavation does not impact on the character or amenity of the site and surrounding areas;
  - (b) filling and excavation does not adversely impact on the environment;
  - (c) filling and excavation does not impact on water quality or drainage of upstream, downstream or adjoining properties;
  - (d) filling and excavation is designed to be fit for purpose and does not create land stability issues;
  - (e) filling and excavation works do not involve complex engineering solutions.

### 9.4.4.3 Criteria for assessment

Table 9.4.4.3.a – Filling and excavation code – for self-assessable and assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<b>Filling and excavation - General</b>		



<p><b>PO1</b> All filling and excavation work does not create a detrimental impact on the slope stability, erosion potential or visual amenity of the site or the surrounding area.</p>	<p><b>AO1.1</b> The height of cut and/or fill, whether retained or not, does not exceed 2 metres in height.</p> <p>and</p> <p>Cuts in excess of those stated in A1.1 above are separated by benches/ terraces with a minimum width of 1.2 metres that incorporate drainage provisions and screen planting.</p> <p><b>AO1.2</b> Cuts are supported by batters, retaining or rock walls and associated benches/terraces are capable of supporting mature vegetation.</p> <p><b>AO1.3</b> Cuts are screened from view by the siting of the building/structure, wherever possible.</p>	<p><b>Not Applicable.</b></p> <p><b>The development relies on existing benched pads within the site. No substantial earthworks are proposed.</b></p>
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Performance outcomes	Acceptable outcomes	Applicant response
	<p><b>AO1.4</b> Topsoil from the site is retained from cuttings and reused on benches/terraces.</p> <p><b>AO1.5</b> No crest of any cut or toe of any fill, or any part of any retaining wall or structure is closer than 600mm to any boundary of the property, unless the prior written approval of the adjoining landowner has been obtained.</p> <p><b>AO1.6</b> Non-retained cut and/or fill on slopes are stabilised and protected against scour and erosion by suitable measures, such as grassing, landscaping or other protective/aesthetic measures.</p>	
<b>Visual Impact and Site Stability</b>		
<p><b>PO2</b> Filling and excavation are carried out in such a manner that the visual/scenic amenity of the area and the privacy and stability of adjoining properties is not compromised.</p>	<p><b>AO2.1</b> The extent of filling and excavation does not exceed 40% of the site area, or 500m<sup>2</sup> whichever is the lesser,  except that AO2.1 does not apply to reconfiguration of 5 lots or more.</p> <p><b>AO2.2</b> Filling and excavation does not occur within 2 metres of the site boundary.</p>	<b>Not Applicable.</b>
<b>Flooding and drainage</b>		



<p><b>PO3</b> Filling and excavation does not result in a change to the run off characteristics of a site which then have a detrimental impact on the site or nearby land or adjacent road reserves.</p>	<p><b>AO3.1</b> Filling and excavation does not result in the ponding of water on a site or adjacent land or road reserves.</p> <p><b>AO3.2</b> Filling and excavation does not result in an increase in the flow of water across a site or any other land or road reserves.</p> <p><b>AO3.3</b> Filling and excavation does not result in an increase in the volume of water or concentration of water in a watercourse and overland flow paths.</p> <p><b>AO3.4</b> Filling and excavation complies with the specifications set out in Planning Scheme Policy No SC5 – FNQROC Development Manual.</p>	<p><b>Not Applicable.</b></p>
<p><b>Water quality</b></p>		
<p><b>PO4</b> Filling and excavation does not result in a reduction of the water quality of receiving waters.</p>	<p><b>AO4</b> Water quality is maintained to comply with the specifications set out in Planning Scheme Policy No SC5 – FNQROC Development Manual.</p>	<p><b>Not Applicable.</b></p>

Performance outcomes	Acceptable outcomes	Applicant response
<b>Infrastructure</b>		
<p><b>PO5</b> Excavation and filling does not impact on Public Utilities.</p>	<p><b>A05</b> Excavation and filling is clear of the zone of influence of public utilities.</p>	<p><b>Not Applicable.</b></p>

## 9.4.5 Infrastructure works code

### 9.4.5.1 Application

- (1) This code applies to assessing:
  - (a) operational work which requires an assessment as a condition of a development permit or is assessable development if this code is identified in the assessment criteria column of a table of assessment;
  - (b) a material change of use or reconfiguring a lot if:
    - (i) assessable development where this code is identified in the assessment criteria column of the table of assessment;
    - (ii) impact assessable development, to the extent relevant.

Note – The Filling and excavation code applies to operational work for filling and excavation.

- (2) When using this code, reference should be made to Part 5.

### 9.4.5.2 Purpose

- (1) The purpose of the Infrastructure works code is to ensure that development is safely and efficiently serviced by, and connected to, infrastructure.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) the standards of water supply, waste water treatment and disposal, stormwater drainage, local electricity supply, telecommunications, footpaths and road construction meet the needs of development and are safe and efficient;
  - (b) development maintains high environmental standards;
  - (c) development is located, designed, constructed and managed to avoid or minimise impacts arising from altered stormwater quality or flow, wastewater discharge, and the creation of non-tidal artificial waterways;
  - (d) the integrity of existing infrastructure is maintained;
  - (e) development does not detract from environmental values or the desired character and amenity of an area.

### 9.4.5.3 Criteria for assessment

Table 9.4.5.3.a – Infrastructure works code –assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<b>Works on a local government road</b>		



<p><b>PO1</b> Works on a local government road do not adversely impact on footpaths or existing infrastructure within the road verge and maintain the flow, safety and efficiency of pedestrians, cyclists and vehicles.</p>	<p><b>AO1.1</b> Footpaths/pathways are located in the road verge and are provided for the hierarchy of the road and located and designed and constructed in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual.</p> <p><b>AO1.2</b> Kerb ramp crossovers are constructed in accordance with Planning scheme policy SC 5 – FNQROC Regional Development Manual.</p> <p><b>AO1.3</b> New pipes, cables, conduits or other similar infrastructure required to cross existing footpaths: (a) are installed via trenchless methods; or (b) where footpath infrastructure is removed to install infrastructure, the new section of footpath is installed to the standard detailed</p>	<p><b>Complies AO 1.1</b></p> <p><b>The site is not serviced by footpath.</b></p> <p><b>Complies with AO 1.2</b></p> <p><b>No new kerb ramps are proposed.</b></p> <p><b>AO1.3 - AO1.5 are Not Applicable.</b></p>
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	<p>in the Planning scheme policy SC5 – FNQROC Regional Development Manual, and is not less than a 1.2 metre section.</p> <p><b>AO1.4</b> Where existing footpaths are damaged as a result of development, footpaths are reinstated ensuring: (a) similar surface finishes are used; (b) there is no change in level at joins of new and existing sections; (c) new sections are matched to existing in terms of dimension and reinforcement.</p> <p>Note – Figure 9.4.5.3.a provides guidance on meeting the outcomes.</p> <p><b>AO1.5</b> Decks, verandahs, stairs, posts and other structures located in the road reserve do not restrict or impede pedestrian movement on footpaths or change the level of the road verges.</p>	<p><b>AO1.5 is not applicable to the proposed development.</b></p>
<b>Accessibility structures</b>		
<p><b>PO2</b> Development is designed to ensure it is accessible for people of all abilities and accessibility features do not impact on the efficient and safe use of footpaths.</p> <p>Note – Accessibility features are those features required to ensure access to premises is provided for people of all abilities and include ramps and lifts.</p>	<p><b>AO2.1</b> Accessibility structures are not located within the road reserve.</p> <p><b>AO2.2</b> Accessibility structures are designed in accordance with AS1428.3.</p> <p><b>AO2.3</b> When retrofitting accessibility features in existing buildings, all structures and changes in grade are contained within the boundaries of the lot and not within the road reserve.</p>	<p><b>Not Applicable.</b></p>
<b>Water supply</b>		

**PO3**

An adequate, safe and reliable supply of potable, fire fighting and general use water is provided.

**AO3.1**

The premises is connected to Council's reticulated water supply system in accordance with the Design Guidelines set out in Section D6 of the Planning scheme policy SC5 – FNQROC Regional Development Manual;

or

**AO3.2**

Where a reticulated water supply system is not available to the premises, on site water storage tank/s with a minimum capacity of 10,000 litres of stored water, with a minimum 7,500 litre tank, with the balance from other sources (e.g. accessible swimming pool, dam etc.) and access to the tank/s for fire trucks is provided for each new house or other development. Tank/s are to be fitted with a 50mm ball valve with a camlock fitting and installed and connected prior to

**Complies with AO 3.1**

**The site is connected to reticulated water supply.**



Performance outcomes	Acceptable outcomes	Applicant response
	occupation of the house and sited to be visually unobtrusive.	
<b>Treatment and disposal of effluent</b>		
<p><b>PO4</b> Provision is made for the treatment and disposal of effluent to ensure that there are no adverse impacts on water quality and no adverse ecological impacts as a result of the system or as a result of increasing the cumulative effect of systems in the locality.</p>	<p><b>AO4.1</b> The site is connected to Council's sewerage system and the extension of or connection to the sewerage system is designed and constructed in accordance with the Design Guidelines set out in Section D7 of the Planning scheme policy SC5 – FNQROC Regional Development Manual;</p> <p>or</p> <p><b>AO4.2</b> Where not in a sewerage scheme area, the proposed disposal system meets the requirements of Section 33 of the <i>Environmental Protection Policy (Water) 1997</i> and the proposed on site effluent disposal system is designed in accordance with the <i>Plumbing and Drainage Act (2002)</i>.</p>	<p><b>Will comply with AO4.2.</b></p> <p><b>The site is connected to reticulated sewerage infrastrucutre.</b></p>
<b>Stormwater quality</b>		

**PO5**

Development is planned, designed, constructed and operated to avoid or minimise adverse impacts on stormwater quality in natural and developed catchments by:

- (a) achieving stormwater quality objectives;
- (b) protecting water environmental values;
- (c) maintaining waterway hydrology.

**AO5.1**

A connection is provided from the premises to Council's drainage system;

or

**AO5.2**

An underground drainage system is constructed to convey stormwater from the premises to Council's drainage system in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.

**AO5.3**

A stormwater quality management plan is prepared, and provides for achievable stormwater quality treatment measures meeting design objectives listed in Table 9.4.5.3.b and Table 9.4.5.3.c, reflecting land use constraints, such as:

- (a) erosive, dispersive and/or saline soil types;
- (b) landscape features (including landform);
- (c) acid sulfate soil and management of nutrients of concern;
- (d) rainfall erosivity.

**AO5.4**

Erosion and sediment control practices are designed, installed, constructed, monitored, maintained, and carried out in accordance with an erosion and sediment control plan.

**AO5.5**

Development incorporates stormwater flow control measures to achieve the design objectives set out in Table 9.4.5.3.b and Table 9.4.5.3.c, including management of frequent flows, peak flows, and construction phase hydrological impacts.

Note – Planning scheme policy SC5 – FNQROC Regional

**Complies with PO5.**

**The site is connected to drainage infrastructure within the road reserve.**



Development Manual provides guidance on soil and water control measures to meet the requirements of the *Environmental Protection Act 1994*.

Note – During construction phases of development, contractors and builders are to have consideration in their work methods and site preparation for their environmental duty to protect stormwater quality.

**Non-tidal artificial waterways**

**PO6**

Development involving non-tidal artificial waterways is planned, designed, constructed and operated to:

- (a) protect water environmental values;
- (b) be compatible with the land use constraints for the site for protecting water environmental values;
- (c) be compatible with existing tidal and non-tidal waterways;
- (d) perform a function in addition to stormwater management;
- (e) achieve water quality objectives.

**AO6.1**

Development involving non-tidal artificial waterways ensures:

- (a) environmental values in downstream waterways are protected;
- (b) any ground water recharge areas are not affected;
- (c) the location of the waterway incorporates low lying areas of the catchment connected to an existing waterway;
- (d) existing areas of ponded water are included.

**AO6.2**

Non-tidal artificial waterways are located:

- (a) outside natural wetlands and any associated buffer areas;
- (b) to minimise disturbing soils or sediments;
- (c) to avoid altering the natural hydrologic regime in acid sulfate soil and nutrient hazardous areas.

**AO6.3**

Non-tidal artificial waterways located adjacent to, or connected to a tidal waterway by means of a weir, lock, pumping system or similar ensures:

- (a) there is sufficient flushing or a tidal range of >0.3 m; or
- (b) any tidal flow alteration does not adversely impact on the tidal waterway; or
- (c) there is no introduction of salt water into freshwater environments.

**AO6.4**

Non-tidal artificial waterways are designed and managed for any of the following end-use purposes:

- (a) amenity (including aesthetics), landscaping or recreation; or
- (b) flood management, in accordance with a drainage catchment management plan; or
- (c) stormwater harvesting plan as part of an

**Not Applicable.**



	<p>integrated water cycle management plan; or (d) aquatic habitat.</p> <p><b>AO6.5</b> The end-use purpose of the non-tidal artificial waterway is designed and operated in a way that protects water environmental values.</p> <p><b>AO6.6</b> Monitoring and maintenance programs adaptively manage water quality to achieve relevant water quality objectives downstream of the waterway.</p> <p><b>AO6.7</b> (e) Aquatic weeds are managed to achieve a low percentage of coverage of the water surface area, and pests and vectors are managed through design and maintenance.</p>	
<b>Wastewater discharge</b>		
<p><b>PO7</b> Discharge of wastewater to waterways, or off site: (a) meets best practice environmental management; (b) is treated to: (i) meet water quality objectives for its receiving waters; (ii) avoid adverse impact on ecosystem health or waterway health; (iii) maintain ecological processes, riparian vegetation and waterway integrity; (iv) offset impacts on high ecological value waters.</p>	<p><b>AO7.1</b> A wastewater management plan is prepared and addresses: (a) wastewater type; (b) climatic conditions; (c) water quality objectives; (d) best practice environmental management.</p> <p><b>AO7.2</b> The waste water management plan is managed in accordance with a waste management hierarchy that: (a) avoids wastewater discharge to waterways; or (b) if wastewater discharge cannot practicably be avoided, minimises wastewater discharge to waterways by re-use, recycling, recovery and treatment for disposal to sewer, surface water and ground water.</p>	<p><b>Not Applicable.</b></p> <p><b>The development does not discharge directly to waterways.</b></p>

**AO7.3**

Wastewater discharge is managed to avoid or minimise the release of nutrients of concern so as to minimise the occurrence, frequency and intensity of algal blooms.

**AO7.4**

Development in coastal catchments avoids or minimises and appropriately manages soil disturbance or altering natural hydrology and:

- (a) avoids lowering ground water levels where potential or actual acid sulfate soils are present;
- (b) manages wastewater so that:
  - (i) the pH of any wastewater discharges is maintained between 6.5 and 8.5 to avoid mobilisation of acid, iron, aluminium and other metals;
  - (ii) holding times of neutralised wastewater ensures the flocculation and removal of any dissolved iron prior to release; visible iron floc is not present in any discharge;
  - (iv) precipitated iron floc is contained and disposed of;
  - (iii) wastewater and precipitates that cannot be contained and treated for discharge on site are removed and disposed of through trade waste or another lawful method.



Electricity supply		
<p><b>PO8</b> Development is provided with a source of power that will meet its energy needs.</p>	<p><b>AO8.1</b> A connection is provided from the premises to the electricity distribution network;</p> <p>or</p> <p><b>AO8.2</b> The premises is connected to the electricity distribution network in accordance with the Design Guidelines set out in Section D8 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p> <p>Note - Areas north of the Daintree River have a different standard.</p>	<p><b>Complies with AO8.1.</b></p> <p><b>The site is connected to underground electricity supply within the road network.</b></p>
<p><b>PO9</b> Development incorporating pad-mount electricity infrastructure does not cause an adverse impact on amenity.</p>	<p><b>AO9.1</b> Pad-mount electricity infrastructure is:</p> <ul style="list-style-type: none"> <li>(a) not located in land for open space or sport and recreation purposes;</li> <li>(b) screened from view by landscaping or fencing;</li> <li>(c) accessible for maintenance.</li> </ul> <p><b>AO9.2</b> Pad-mount electricity infrastructure within a building, in a Town Centre is designed and located to enable an active street frontage.</p> <p>Note – Pad-mounts in buildings in activity centres should not be located on the street frontage.</p>	<p><b>Not Applicable.</b></p>
Telecommunications		
<p><b>PO10</b> Development is connected to a telecommunications service approved by the relevant telecommunication regulatory authority.</p>	<p><b>AO10</b> The development is connected to telecommunications infrastructure in accordance with the standards of the relevant regulatory authority.</p>	<p><b>Complies with AO10.</b></p> <p><b>The site is connected to telecommunications.</b></p>



<p><b>PO11</b> Provision is made for future telecommunications services (e.g. fibre optic cable).</p>	<p><b>AO11</b> Conduits are provided in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual.</p>	<p><b>Not Applicable.</b></p>
<p><b>Road construction</b></p>		
<p><b>PO12</b> The road to the frontage of the premises is constructed to provide for the safe and efficient movement of: (a) pedestrians and cyclists to and from the site; (b) pedestrians and cyclists adjacent to the site; (c) vehicles on the road adjacent to the site; (d) vehicles to and from the site; (e) emergency vehicles.</p>	<p><b>AO12.1</b> The road to the frontage of the site is constructed in accordance with the Design Guidelines set out in Sections D1 and D3 of the Planning scheme policy SC5 – FNQROC Regional Development Manual, for the particular class of road, as identified in the road hierarchy.</p> <p><b>AO12.2</b> There is existing road, kerb and channel for the full road frontage of the site.</p> <p><b>AO12.3</b> Road access minimum clearances of 3.5 metres wide and 4.8 metres high are provided for the safe passage of emergency vehicles.</p>	<p><b>Not Applicable.</b></p>
<p><b>Alterations and repairs to public utility services</b></p>		
<p><b>PO13</b> Infrastructure is integrated with, and efficiently extends, existing networks.</p>	<p><b>AO13</b> Development is designed to allow for efficient connection to existing infrastructure networks.</p>	<p><b>Complies with AO13.</b></p>



<p><b>PO14</b> Development and works do not affect the efficient functioning of public utility mains, services or installations.</p>	<p><b>AO14.1</b> Public utility mains, services and installations are not required to be altered or repaired as a result of the development;</p> <p>or</p> <p><b>AO14.2</b> Public utility mains, services and installations are altered or repaired in association with the works so that they continue to function and satisfy the relevant Design Guidelines set out in Section D8 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p>	<p><b>Complies with AO14.1.</b></p>
<p><b>Construction management</b></p>		
<p><b>PO15</b> Work is undertaken in a manner which minimises adverse impacts on vegetation that is to be retained.</p>	<p><b>AO15</b> Works include, at a minimum:</p> <ul style="list-style-type: none"> <li>(a) installation of protective fencing around retained vegetation during construction;</li> <li>(b) erection of advisory signage;</li> <li>(c) no disturbance, due to earthworks or storage of plant, materials and equipment, of ground level and soils below the canopy of any retained vegetation;</li> <li>(d) removal from the site of all declared noxious weeds.</li> </ul>	<p><b>The proposed development can be conditioned to comply with AO15.</b></p>
<p><b>PO16</b> Existing infrastructure is not damaged by construction activities.</p>	<p><b>AO16</b> Construction, alterations and any repairs to infrastructure is undertaken in accordance with the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p> <p>Note - Construction, alterations and any repairs to State-controlled roads and rail corridors are undertaken in accordance with the Transport Infrastructure Act 1994.</p>	<p><b>The proposed development can be conditioned to comply with AO16.</b></p>



Performance outcomes	Acceptable outcomes	Applicant response
<b>For assessable development</b>		
<b>High speed telecommunication infrastructure</b>		
<p><b>PO17</b> Development provides infrastructure to facilitate the roll out of high speed telecommunications infrastructure.</p>	<p><b>AO17</b> No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
<b>Trade waste</b>		
<p><b>PO18</b> Where relevant, the development is capable of providing for the storage, collection treatment and disposal of trade waste such that:</p> <ul style="list-style-type: none"> <li>(a) off-site releases of contaminants do not occur;</li> <li>(b) the health and safety of people and the environment are protected;</li> <li>(c) the performance of the wastewater system is not put at risk.</li> </ul>	<p><b>AO18</b> No acceptable outcomes are prescribed.</p>	<p><b>Development can be conditioned to comply with PO18.</b></p>
<b>Fire services in developments accessed by common private title</b>		
<p><b>PO19</b> Hydrants are located in positions that will enable fire services to access water safely, effectively and efficiently.</p>	<p><b>AO19.1</b> Residential streets and common access ways within a common private title places hydrants at intervals of no more than 120 metres and at each intersection. Hydrants may have a single outlet and be situated above or below ground.</p> <p><b>AO19.2</b> Commercial and industrial streets and access ways within a common private title serving commercial properties such as factories and warehouses and offices are provided with above or below ground fire hydrants located at not more than 90 metre intervals and at each intersection. Above ground fire hydrants have dual-valved outlets.</p>	<p><b>Not Applicable.</b></p>



<p><b>PO20</b> Hydrants are suitable identified so that fire services can locate them at all hours.</p> <p>Note – Hydrants are identified as specified in the Department of Transport and Main Roads Technical Note: 'Identification of street hydrants for fire fighting purposes' available under 'Publications'.</p>	<p><b>AO20</b> No acceptable outcomes are prescribed.</p>	<p><b>Not Applicable.</b></p>
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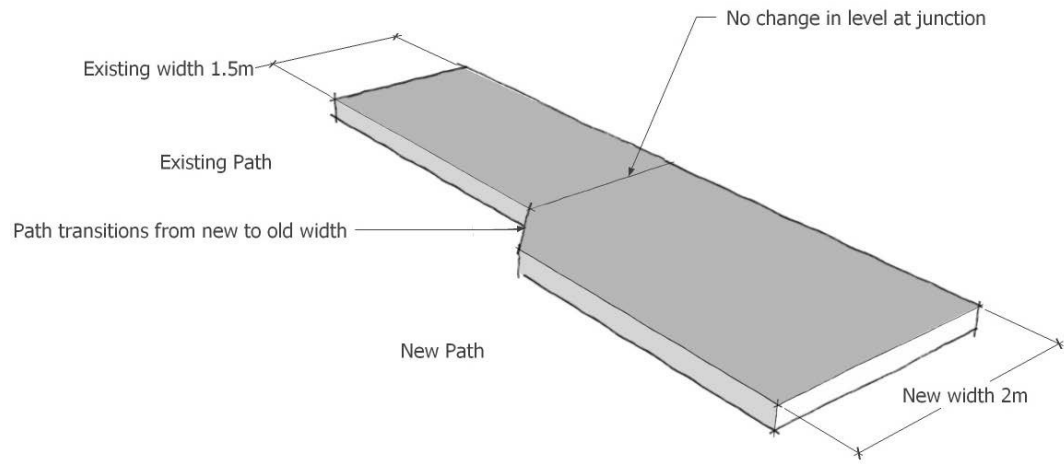
Table 9.4.5.3.b – Stormwater management design objectives (Construction phase).

Issue	Design objectives
<b>Drainage control</b> (Temporary drainage works)	(a) Design life and design storm for temporary drainage works: (i) Disturbed open area for <12 months – 1 in 2 year ARI event; (ii) Disturbed open area for 12-24 months – 1 in 5 year ARI event; (iii) Disturbed open area for >24 months – 1 in 10 year ARI event. (b) Design capacity excludes minimum 150mm freeboard. (c) Temporary culvert crossing – minimum of 1 in 1-year ARI hydraulic capacity.
<b>Erosion control</b> (Erosion control measures)	(a) Minimise exposure of disturbed soils at any time. (b) Divert water run-off from undisturbed areas around disturbed areas. (c) Determine erosion risk rating using local rainfall erosivity, rainfall depth, soil loss rate or other acceptable methods. (d) Implement erosion control methods corresponding to identified erosion risk rating.
<b>Sediment control measures</b> (sediment control measures, design storm for sediment control basins, Sediment basin dewatering)	(a) Determine appropriate sediment control measures using: (i) potential soil loss rate; or (ii) monthly erosivity; or (iii) average monthly rainfall. (b) Collect and drain stormwater from disturbed soils to sediment basin for design storm event: (i) design storm for sediment basin sizing is 80th% five-day event or similar. (c) Site discharge during sediment basin dewatering: (i) TSS < 50mg/L TSS; (ii) Turbidity not > 10% receiving water's turbidity; (iii) pH 6.5-8.5.
<b>Water quality</b> (Litter and other waste, hydrocarbons and other contaminants)	(a) Avoid wind-blown litter; remove grass pollutants. (b) Ensure there is no visible oil or grease sheen on released waters. (c) Dispose of waste containing contaminants at authorised facilities.
<b>Waterway stability and flood flow management</b> (Changes to the natural hydraulics and hydrology)	(a) For peak flow for the 100% AEP event and 1% AEP event, use constructed sediment basins to attenuate the discharge rate of stormwater from the site.

Table 9.4.5.3.c – Stormwater management design objectives (post-construction phase)

Design objectives				Application
Minimum reductions in mean annual load from unmitigated development (%)				
Total suspended solids (TSS)	Total phosphorus (TP)	Total nitrogen (TN)	Gross pollutants >5mm	
80	60	40	90	<p>Development for urban purposes</p> <p>Excludes development that is less than 25% pervious.</p> <p>In lieu of modelling, the default bio-retention treatment area to comply with load reduction targets of 1.5% of contributing catchment area.</p>
<p>Water stability management</p> <p>(a) Limit peak 100% AEP event discharge within the receiving waterway to the pre-development peak 100% AEP event discharge.</p>				<p>Catchments contributing to un-lined receiving waterway. Degraded waterways may seek alternative discharge management objectives to achieve waterway stability.</p> <p>For peak flow for the 100% AEP event, use co-located storages to attenuate site discharge rate of stormwater.</p>

Figure 9.4.5.3.a – New footpath sections



## 9.4.6 Landscaping code

### 9.4.6.1 Application

- (1) This code applies to assessing:
  - (a) operational work which requires a compliance assessment as a condition of a development permit; or
  - (b) a material change of use or reconfiguring a lot if:
    - (i) assessable development where this code is identified in the assessment criteria column of the table of assessment;
    - (ii) impact assessable development, to the extent relevant.
- (2) When using this code, reference should be made to Part 5.

### 9.4.6.2 Purpose

- (1) The purpose of the Landscaping code is to assess the landscaping aspects of a development.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) The tropical, lush landscape character of the region is retained, promoted and enhanced through high quality landscape works;
  - (b) The natural environment of the region is enhanced;
  - (c) The visual quality, amenity and identity of the region is enhanced;
  - (d) Attractive streetscapes and public places are created through landscape design;
  - (e) As far as practical, existing vegetation on site is retained, and protected during works and integrated with the built environment;
  - (f) Landscaping is provided to enhance the tropical landscape character of development and the region;
  - (g) Landscaping is functional, durable, contributes to passive energy conservation and provides for the efficient use of water and ease of ongoing maintenance;
  - (h) Landscaping takes into account utility service protection;
  - (i) Weed species and invasive species are eliminated from development sites;
  - (j) Landscape design enhances personal safety and incorporates CPTED principles.

### 9.4.6.3 Criteria for assessment

Table 9.4.6.3.a – Landscaping code – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		
<b>Landscape design</b>		

**PO1**

Development provides landscaping that contributes to and creates a high quality landscape character for the site, street and local areas of the Shire by:

- (a) promoting the Shire's character as a tropical environment;
- (b) softening the built form of development;
- (c) enhancing the appearance of the development from within and outside the development and makes a positive contribution to the streetscape;
- (d) screening the view of buildings, structures, open storage areas, service equipment, machinery plant and the like from public places, residences and other sensitive development;
- (e) where necessary, ensuring the privacy of habitable rooms and private outdoor recreation areas;
- (f) contributing to a comfortable living environment and improved energy efficiency, by providing shade to reduce glare and heat absorption and re-radiation from buildings, parking areas and other hard surfaces;
- (g) ensuring private outdoor recreation space is useable;
- (h) providing long term soil erosion protection;
- (i) providing a safe environment;
- (j) integrating existing vegetation and other natural features of the premises into the development;
- (k) not adversely affecting vehicular and pedestrian sightlines and road safety.

**AO1**

Development provides landscaping:

- (a) in accordance with the minimum area, dimensions and other requirements of applicable development codes;
- (b) that is designed and planned in a way that meets the guidelines for landscaping outlined in Planning Scheme Policy SC6.7 – Landscaping;
- (c) that is carried out and maintained in accordance with a landscaping plan that meets the guidelines for landscaping outlined in Planning Scheme Policy SC6.7 – Landscaping.

Note - Planning scheme policy SC6.7 – Landscaping provides guidance on meeting the outcomes of this code. A landscape plan submitted for approval in accordance with the Planning policy is one way to achieve this outcome.

**May be conditioned to Comply with AO1.**

**The site provides more than sufficient area to accommodate landscaping that is commensurate with the scale and nature of the proposed development. The proposal involves the adaptive reuse of an existing building on an established industrial allotment and retains substantial areas of land capable of supporting landscape treatment along site boundaries, within setback areas and adjacent to parking and access areas.**

**Given the preliminary nature of the application and the limited extent of external works proposed, detailed landscape plans have not been prepared at this stage. Notwithstanding, there are no physical constraints that would prevent the provision of appropriate landscaping outcomes consistent with the intent of the Landscaping Code.**

**The development footprint occupies only a portion of the site, leaving ample opportunity for the establishment of landscaping that will soften the appearance of the development, contribute to the streetscape and provide visual screening where required. The proposed use is also of a relatively low intensity, reducing the need for extensive landscape buffers typically associated with larger commercial or industrial developments.**

**Should Council consider it necessary, the preparation and implementation of a detailed Landscape Plan can readily be addressed by way of a condition of approval. This would allow the landscaping design to be finalised following approval while ensuring that an appropriate**



		<p>landscaping outcome is achieved prior to commencement of the use.</p> <p>Accordingly, the proposal is capable of achieving the outcomes sought by the Landscaping Code and complies with the intent of the assessment benchmarks.</p>
<p><b>PO2</b> Landscaping contributes to a sense of place, is functional to the surroundings and enhances the streetscape and visual appearance of the development.</p>	<p><b>AO2.1</b> No acceptable outcomes are specified.</p> <p>Note - Landscaping is in accordance with the requirements specified in Planning scheme policy SC6.7 – Landscaping.</p> <p><b>AO2.2</b> Tropical urbanism is incorporated into building design.</p> <p>Note – ‘Tropical urbanism’ includes many things such as green walls, green roofs, podium planting and vegetation incorporated into the design of a building.</p>	<p>Will comply with PO2.</p> <p>Refer to discussion above.</p>
<p><b>PO3</b> Development provides landscaping that is , as far as practical, consistent with the existing desirable landscape character of the area and protects trees, vegetation and other features of ecological, recreational, aesthetic and cultural value.</p>	<p><b>AO3.1</b> Existing vegetation on site is retained and incorporated into the site design, wherever possible, utilising the methodologies and principles outline in AS4970-2009 Protection of Trees on Development Sites.</p> <p><b>AO3.2</b> Mature vegetation on the site that is removed or damaged during development is replaced with advanced species.</p> <p><b>AO3.3</b> Where there is an existing landscape character in a street or locality which results from existing vegetation, similar species are incorporated into new development.</p> <p><b>AO3.4</b></p>	<p>Will comply with PO3.</p> <p>Refer to discussion above.</p>



	<p>Street trees are species which enhance the landscape character of the streetscape, with species chosen from the Planning scheme policy SC6.7 – Landscaping.</p>	
<p><b>PO4</b> Plant species are selected with consideration to the scale and form of development, screening, buffering, streetscape, shading and the locality of the area.</p>	<p><b>AO4</b> Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.</p>	<p><b>Will comply with AO4.</b>  <b>Refer to discussion above.</b></p>
<p><b>PO5</b> Shade planting is provided in car parking areas where uncovered or open, and adjacent to driveways and internal roadways.</p>	<p><b>AO5</b> Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.</p>	<p><b>Will comply with PO5.</b>  <b>Refer to discussion above.</b></p>
<p><b>PO6</b> Landscaped areas are designed in order to allow for efficient maintenance.</p>	<p><b>AO6.1</b> A maintenance program is undertaken in accordance with Planning scheme policy SC6.7 – Landscaping.</p> <p><b>AO6.2</b> Tree maintenance is to have regard to the 'Safe Useful Life Expectancy of Trees (SULE).</p> <p>Note – It may be more appropriate to replace trees with a SULE of less than 20 years (as an example), and replant with younger healthy species.</p>	<p><b>The Development can be conditioned to comply with AO6.1 &amp; 6.2.</b></p>



<p><b>PO7</b> Podium planting is provided with appropriate species for long term survival and ease of maintenance, with beds capable of proper drainage.</p>	<p><b>A07.1</b> Podium planting beds are provided with irrigation and are connected to stormwater infrastructure to permit flush out.</p> <p><b>A07.2</b> Species of plants are selected for long term performance designed to suit the degree of access to podiums and roof tops for maintenance.</p>	<p><b>Not Applicable.</b></p>
<p><b>PO8</b> Development provides for the removal of all weed and invasive species and implement on-going measures to ensure that weeds and invasive species do not reinfest the site and nearby premises.</p>	<p><b>A08</b> Weed and invasive species detected on a development site are removed in accordance with a management plan prepared by an appropriately qualified person.</p>	<p><b>Will comply with AO8.</b></p> <p><b>Any weed species detected on the development site will be removed during construction and prior to landscaping.</b></p>
<p><b>PO9</b> The landscape design enhances personal safety and reduces the potential for crime and vandalism.</p>	<p><b>A09</b> No acceptable outcomes are specified.</p> <p>Note - Planning scheme policy SC6.3 – Crime prevention through environmental design (CPTED) provides guidance on meeting this outcome.</p>	<p><b>Will comply with PO9.</b></p> <p><b>The landscape design will adopt CPTED principles in design.</b></p>
<p><b>PO10</b> The location and type of plant species does not adversely affect the function and accessibility of services and facilities and service areas.</p>	<p><b>A010</b> Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.</p>	<p><b>Will comply with PO10.</b></p> <p><b>The landscaping will be designed to specifically ensure continued access to and function of services.</b></p>

## 9.4.9 Vegetation management code

### 9.4.9.1 Application

- (1) This code applies to assessing operational works for vegetation damage if:
  - (a) assessable development where the code is an applicable code identified in the assessment criteria column of a table of assessment;
  - (b) impact assessable development, to the extent relevant.
- (2) When using this code, reference should be made to Part 5.

### 9.4.9.2 Purpose

- (1) The purpose of the Vegetation management code is achieved through the overall outcomes.
- (2) The purpose of the code will be achieved through the following overall outcomes:
  - (a) vegetation is protected from inappropriate damage;
  - (b) where vegetation damage does occur it is undertaken in a sustainable manner;
  - (c) significant trees are maintained and protected;
  - (d) biodiversity and ecological values are protected and maintained;
  - (e) habitats for rare, threatened and endemic species of flora and fauna are protected and maintained;
  - (f) landscape character and scenic amenity is protected and maintained;
  - (g) heritage values are protected and maintained.

### 9.4.9.3 Criteria for assessment

**Table 9.4.9.3.a – Vegetation management –assessable development**

Note – All vegetation damage is to have regard to the provisions of AS4373-2009 Pruning of Amenity Trees

Performance outcomes	Acceptable outcomes	Applicant response
<b>For self-assessable and assessable development</b>		



<p><b>PO1</b> Vegetation is protected to ensure that:</p> <ul style="list-style-type: none"> <li>(a) the character and amenity of the local area is maintained;</li> <li>(b) vegetation damage does not result in fragmentation of habitats;</li> <li>(c) vegetation damage is undertaken in a sustainable manner;</li> <li>(d) the Shire's biodiversity and ecological values are maintained and protected;</li> <li>(e) vegetation of historical, cultural and / or visual significance is retained;</li> <li>(f) vegetation is retained for erosion prevention and slope stabilisation.</li> </ul>	<p><b>AO1.1</b> Vegetation damage is undertaken by a statutory authority on land other than freehold land that the statutory authority has control over;</p> <p>or</p> <p><b>AO1.2</b> Vegetation damage is undertaken by or on behalf of the local government on land controlled, owned or operated by the local government;</p> <p>or</p> <p><b>AO1.3</b> Vegetation damage, other than referenced in AO1.1 or AO1.2 is the damage of:</p> <ul style="list-style-type: none"> <li>(a) vegetation declared as a pest pursuant to the <i>Land Protection (Pest and Stock Route Management) Act 2002</i>; or</li> <li>(b) vegetation identified within the local government's register of declared plants pursuant to the local government's local laws; or</li> <li>(c) vegetation is located within a Rural zone and the trunk is located within ten metres of an existing building; or</li> <li>(d) vegetation is located within the Conservation zone or Environmental management zone</li> </ul>	<p><b>May be conditioned to comply with PO1.</b></p> <p><b>Not vegetation is proposed for removal at this stage. Any vegetation removal can be detailed within a Landscape Plan.</b></p>
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and the trunk is located within three metres of an existing or approved structure, not including a boundary fence;

or

**AO1.4**

Vegetation damage that is reasonably necessary for carrying out work that is:

- (a) authorised or required under legislation or a local law;
- (b) specified in a notice served by the local government or another regulatory authority;

or

**AO1.5**

Vegetation damage for development where the damage is on land the subject of a valid development approval and is necessary to give effect to the development approval;

or

**AO1.6**

Vegetation damage is in accordance with an approved Property Map of Assessable Vegetation issued under the *Vegetation Management Act 1999*;

or

**AO1.7**

Vegetation damage is essential to the maintenance of an existing fire break;

or

**AO1.8**

Vegetation damage is essential to prevent interference to overhead service cabling;



or

**AO1.9**

Vegetation damage is for an approved Forest practice, where the lot is subject to a scheme approved under the *Vegetation Management Act 1999*;

or

**AO1.10**

Vegetation damage is undertaken in accordance with section 584 of the *Sustainable Planning Act 2009*.

**AO1.11**

Vegetation damage where it is necessary to remove one tree in order to protect an adjacent more significant tree (where they are growing close to one another).

**AO1.12**

Private property owners may only remove dead, dying, structurally unsound vegetation following receipt of written advice from, at minimum, a fully qualified Certificate V Arborist. A copy of the written advice is to be submitted to Council for its records, a minimum of seven business days prior to the vegetation damage work commencing.



<p><b>PO2</b> Vegetation damaged on a lot does not result in a nuisance</p>	<p><b>AO2.1</b> Damaged vegetation is removed and disposed of at an approved site; or</p> <p><b>AO2.2</b> Damaged vegetation is mulched or chipped if used onsite.</p>	<p><b>The development can be conditioned to comply with AO2.1 and AO 2.2.</b></p>
<p><b>For assessable development</b></p>		
<p><b>PO3</b> Vegetation damage identified on the Places of significance overlay lot does not result in a negative impact on the site's heritage values.</p>	<p><b>AO3</b> No acceptable outcomes are prescribed.</p>	<p><b>Not applicable.</b></p>