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Application for Development Permit for Material Change of Use for 'Dwelling House' over Land at 14 Murphy Street, Port Douglas, described as Lot 114 on PTD2094

March 2022

Prepared by:

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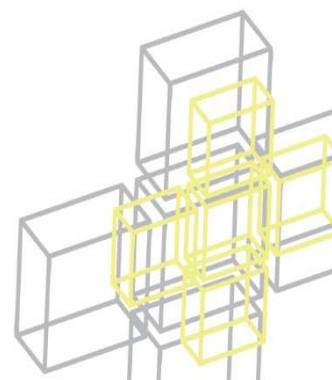
PATAGORANG PTY LTD

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town planning, project management & development consultants



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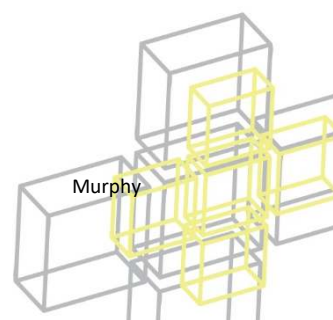
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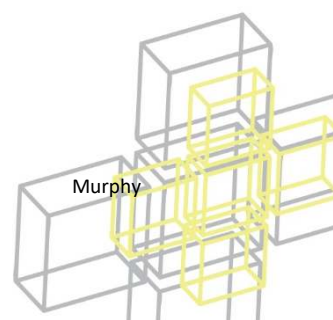


Contents

Executive Summary	4
1.0. Application Summary	5
2.0. Site	6
2.1. Details and Description	6
2.2. Locality	6
2.3. Planning Context	6
3.0. Proposal	7
4.0. Legislative Considerations.....	9
4.1. Planning Act 2016	9
5.0. Statutory Planning Assessment	10
5.1. Overview	10
5.2. State Planning Policy	10
5.3. State Development Assessment Provisions	10
5.4. Douglas Shire Planning Scheme 2018	10
6.0. Conclusion	11

Annexures

Annexure 1:	DA Form 1
Annexure 2:	Title Searches
Annexure 3:	QLD Globe Aerial Overlay
Annexure 4:	SmartMap
Annexure 5:	Detail and Contour Survey Plan
Annexure 6:	Planning Scheme Property Report
Annexure 7:	SDAP Mapping
Annexure 8:	Architectural Plans
Annexure 9:	3D View
Annexure 10:	Landscape Plans
Annexure 11:	Driveway Crossover Plans
Annexure 12:	Douglas Shire Planning Scheme Code Assessment



Executive Summary

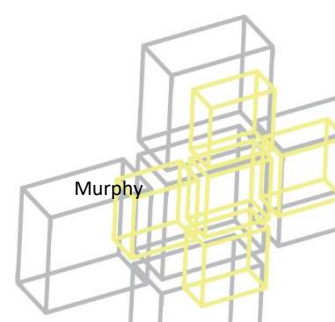
This Planning Report has been prepared by Planning Plus (QLD) Pty Ltd (Planning Plus) on behalf of Patagorang Pty Ltd (the 'applicant') in support of an application to Douglas Shire Council seeking a Development Permit for Material Change of Use for a 'Dwelling House' over land at 14 Murphy Street, Port Douglas, described as Lot 114 on PTD2094 (the 'site').

The proposal comprises a high-quality residential dwelling which is to be set into the hillside and designed to integrate into the vegetated surrounds through the use of natural colours, materials and landscaping. Overall the proposal is considered to appropriately reflect the existing pattern of development on Flagstaff Hill and achieves an appropriate tropical vernacular as is desired for Port Douglas.

The development proposed is 'assessable development' as defined under Section 44 of the *Planning Act 2016* and thus requires assessment against local Planning Scheme provisions and relevant State legislation. This report provides an overview of the development proposed and addresses the various planning considerations relevant to Council and other agencies' assessment of the proposal.

In summary, the report concludes that:

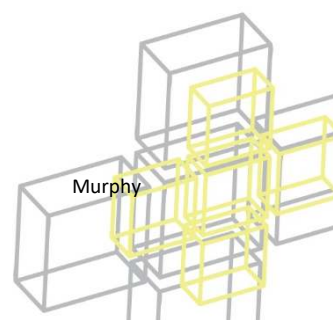
- The submitted information conforms to the requirements for making a 'code-assessable' development application under the *Planning Act 2016*; and
- The proposed development is considered to be consistent with the outcomes sought by the Douglas Shire Planning Scheme 2018 and associated policies, and adequate justification is considered to exist to facilitate the intended development including any relaxations sought against relevant technical code provisions.



1.0. Application Summary

DA Form 1 is included as **Annexure 1**. A summary of relevant application details is provided in the following:

Applicant:	Patagorang Pty Ltd
Registered Owner/s:	Patagorang Pty Ltd <i>Refer to Title Searches (Annexure 2)</i>
Contact:	Mr Evan Yelavich Planning Plus (QLD) Pty Ltd PO Box 399 REDLYNCH QLD 4870 E: Info@planningplusqld.com.au P: 4039 3409
Real Property Description:	Lot 114 on PTD2094
Location:	14 Murphy Street, Port Douglas
Tenure:	Freehold
Total Area:	2,833m2
Local Government Authority:	Douglas Shire Council
Easements and Encumbrances:	Nil
Proposal:	Development Permit for Material Change of Use for a 'Dwelling House'.
Our Reference:	22-04



2.0. Site

2.1. Details and Description

The land the subject of this application is situated at 14 Murphy Street, Port Douglas, and is described as Lot 114 on PTD2094. A QLD Globe Aerial Overlay and SmartMap of the site are included for reference as **Annexures 3 & 4**, respectively.

The site covers a total area of 2,833m² with frontage to Murphy Street and is sloping and vegetated. A Detail Survey Plan is included as **Annexure 5**.

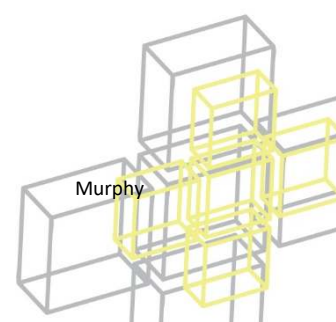
2.2. Locality

The site is located a very short distance from the main activity centre of Port Douglas, Macrossan Street, where a range of residential and commercial land use exist. The site is located in the foothills of Flagstaff Hill which typically comprises large upmarket residential dwellings, although several adjoining lots currently remain vacant.

2.3. Planning Context

The planning context of the site includes:

Regional Plan Designation:	Urban Footprint
Planning Scheme Local Plan Area:	Port Douglas/Craiglie <i>Refer to Douglas Shire Council Planning Scheme Property Report included as Annexure 6</i>
Planning Scheme Zone:	Environmental Management <i>Refer to Douglas Shire Council Planning Scheme Property Report included as Annexure 6</i>
Planning Scheme Overlays:	Bushfire Hazard Overlay; Coastal Processes Overlay; Hillslopes Overlay; Landscape Values Overlay; Natural Areas Overlay; and Transport Road Hierarchy Overlay. <i>Refer to Douglas Shire Council Planning Scheme Property Report included as Annexure 6</i>
State Assessment and Referral Agency Mapping:	Native Vegetation Clearing Coastal Protection <i>Refer to SARA Mapping included as Annexure 7</i>



3.0. Proposal

This application seeks a Development Permit for Material Change of Use for a 'Dwelling House' over the subject site. The following proposal plans are provided:

- Architectural Plans included as **Annexure 8**;
- 3D View included as **Annexure 9**; and
- Landscape Plans included as **Annexure 10**.

In summary, the proposal includes a modern tropical dwelling comprising:

- Three (3) levels which are recessed into the hillslope (building height discussed below);
- Side setbacks of 3m and significant front and rear setbacks;
- Site cover of 23.7%;
- Sustainability features including solar and hydroelectric systems and significant stormwater and greywater storage;
- Driveway from Murphy Street (further discussion below); and
- Significant retention of existing vegetation and additional landscaping, including rooftop and vertical landscaping.

As evidenced by the plans and imagery, the proposal is somewhat unique in the way in which it seeks to integrate with the existing landscape through the use of materials and landscaping. Rooftop and vertical landscaping will significantly soften the built form and ensure minimal visual impact. External walls are proposed to comprise a raw concrete finish which will blend with the vegetated backdrop.

Building height at various points is shown below in Figure 1. In the worst-case assessment, the maximum building height is 13.3m from the lowest floor level to the top of the parapet. It should be noted though that the parapet will be largely screened by rooftop landscaping. The building height from the lowest floor level to the roof level where the rooftop landscaping is to exist is 11.8m.

Measurements have also been shown below from the approximate mean natural ground level given the sloping nature of the site and the stepped nature of the building (noting that the roof level does not extend all the way to the front of the building and really only sits above the approximate mean natural ground level in the worst case). Measurement from this point show a height of 11.23m to the top of the parapet and 9.75m to the roof level.

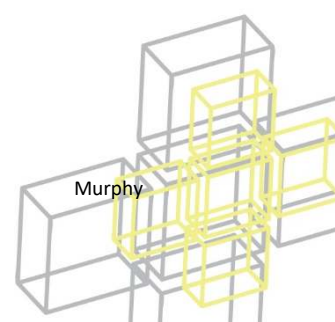
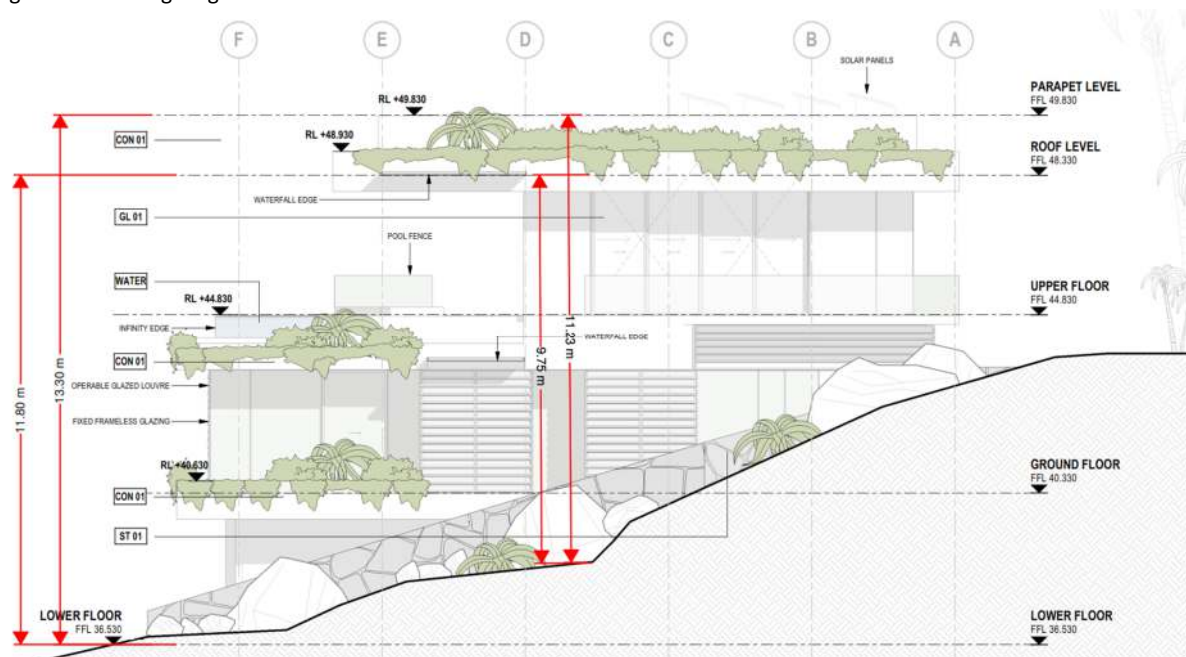
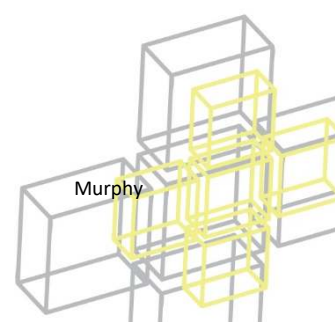


Figure 1 – building height



Irrespective of the above approach taken to determine the effective building height, we submit that building height is appropriate in this instance given the overall treatment of the built form, being that it is largely integrated into the site. It is further submitted that Flagstaff Hill is characterised by large upmarket dwellings which reflect the status and desirability of Port Douglas, and that the proposed development is complimentary to this existing character.

Access to the proposed dwelling is via a new driveway off Murphy Street with typical maximum grades of 1:8 – 1:5. Vegetation will be retained where possible on the site and additional landscaping will be established to ensure appropriate screening. The crossover/integration of the driveway with Murphy Street is proposed to occur in conjunction with plans for a Dwelling House on adjoining Lot 113 on PTD2094. Plans of this arrangement are included as **Annexure 11** and show that access to the subject site would use the same crossover as adjoining Lot 113.



4.0. Legislative Considerations

4.1. Planning Act 2016

This section provides an overview of the legislative context of the application under the provisions of the *Planning Act 2016*.

4.1.1. Assessable Development

The proposed development is identified as 'assessable' under the *Planning Act 2016* due to the effect of the *Planning Regulation 2017*.

4.1.2. Assessment Manager

The Assessment Manager for this development application is Douglas Shire Council as determined by Schedule 8 of the *Planning Regulation 2017*.

4.1.3. Assessment Categories & Benchmarks

The following table outlines the Assessment Categories and Assessment Benchmarks applicable to the various aspects of development proposed:

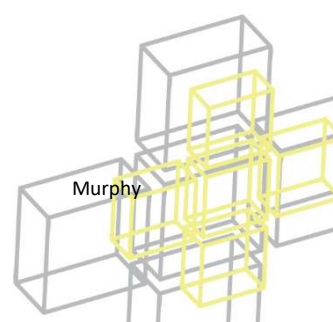
Aspect of Development	Categories of Assessment	Assessment Benchmarks
Material Change of Use for Dwelling House	Code Assessment	Relevant Codes of Douglas Shire Planning Scheme

4.1.4. Referral Agencies

A review of Schedule 10 of the *Planning Regulation 2017* indicates that the application will not trigger any State agency referrals.

4.1.5. Public Notification

This application is 'code-assessable' and therefore does not require public notification.



5.0. Statutory Planning Assessment

5.1. Overview

This section provides an assessment of the application against relevant statutory planning provisions and assessment benchmarks.

5.2. State Planning Policy

The State Planning Policy (SPP) is relevant to the assessment of this application where a State interest is not appropriately reflected in the Planning Scheme relevant to the site. It is understood that relevant all State interests are adequately reflected in the Planning Scheme.

5.3. State Development Assessment Provisions

No State Development Assessment Provisions are identified as being relevant to the application.

5.4. Douglas Shire Planning Scheme 2018

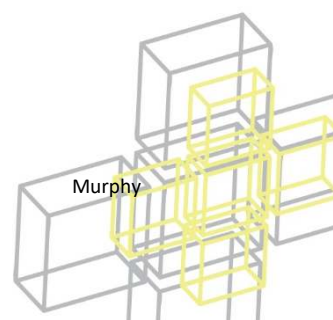
The proposed development is identified as being subject to 'code assessment' under the Douglas Shire Planning Scheme 2018. Assessment of the proposal against the relevant code provisions is provided below.

5.4.1. Code Assessment

By reference to the Environmental Management Zone Assessment Table, the following codes have been identified as being applicable to this proposal:

- Environmental Management Zone Code;
- Port Douglas/Craigie Local Plan Code;
- Dwelling House Code;
- Bushfire Hazards Overlay Code;
- Coastal Environment Overlay Code;
- Hillslopes Overlay Code;
- Natural Areas Overlay Code;
- Access, Parking and Servicing Code; and
- Infrastructure Works Code.

A detailed assessment against the above codes is included as **Annexure 12** to this report.



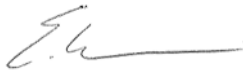
6.0. Conclusion

This Planning Report has been prepared by Planning Plus on behalf of Patagorang Pty Ltd (the 'applicant') in support of an application to Douglas Shire Council seeking a Development Permit for Material Change of Use for a 'Dwelling House' on land at 14 Murphy Street, Port Douglas, described as Lot 114 on PTD2094 (the 'site').

It is submitted that the application has demonstrated that the proposed development is consistent with the outcomes sought by the Douglas Shire Planning Scheme 2018 and associated policies, and includes adequate justification to facilitate the intended development proposal, including any relaxations sought against technical code provisions.

We trust the information provided herein is sufficient for Council's purpose, however should you require any further information, please do not hesitate to contact the undersigned.

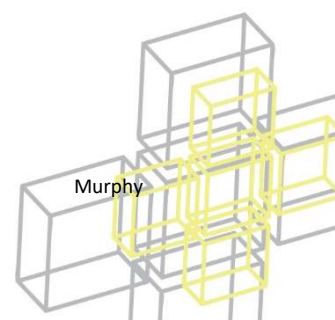
Yours faithfully



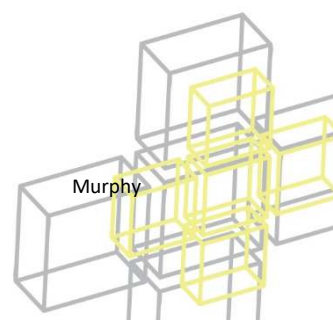
Evan Yelavich
Director / Planner
Planning Plus (QLD) Pty Ltd

Encl.

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Annexure 1: DA Form 1



DA Form 1 – Development application details

Approved form (version 1.3 effective 28 September 2020) made under section 282 of the Planning Act 2016.

This form **must** be used to make a development application **involving code assessment or impact assessment**, except when applying for development involving only building work.

For a development application involving **building work only**, use *DA Form 2 – Building work details*.

For a development application involving **building work associated with any other type of assessable development (i.e. material change of use, operational work or reconfiguring a lot)**, use this form (*DA Form 1*) and parts 4 to 6 of *DA Form 2 – Building work details*.

Unless stated otherwise, all parts of this form **must** be completed in full and all required supporting information **must** accompany the development application.

One or more additional pages may be attached as a schedule to this development application if there is insufficient space on the form to include all the necessary information.

This form and any other form relevant to the development application must be used to make a development application relating to strategic port land and Brisbane core port land under the *Transport Infrastructure Act 1994*, and airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*. For the purpose of assessing a development application relating to strategic port land and Brisbane core port land, any reference to a planning scheme is taken to mean a land use plan for the strategic port land, Brisbane port land use plan for Brisbane core port land, or a land use plan for airport land.

Note: All terms used in this form have the meaning given under the Planning Act 2016, the Planning Regulation 2017, or the Development Assessment Rules (DA Rules).

PART 1 – APPLICANT DETAILS

1) Applicant details	
Applicant name(s) (individual or company full name)	Patagorang Pty Ltd C/- Planning Plus
Contact name (only applicable for companies)	Evan Yelavich
Postal address (P.O. Box or street address)	PO Box 399
Suburb	Redlynch
State	QLD
Postcode	4870
Country	
Contact number	(07) 40393409
Email address (non-mandatory)	Evan@planningplusqld.com.au
Mobile number (non-mandatory)	
Fax number (non-mandatory)	
Applicant's reference number(s) (if applicable)	22-04

2) Owner's consent	
2.1) Is written consent of the owner required for this development application?	
<input checked="" type="checkbox"/> Yes – the written consent of the owner(s) is attached to this development application	
<input type="checkbox"/> No – proceed to 3)	

PART 2 – LOCATION DETAILS

3) Location of the premises (complete 3.1) or 3.2), and 3.3) as applicable)

Note: Provide details below and attach a site plan for any or all premises part of the development application. For further information, see [DA Forms Guide: Relevant plans](#).

3.1) Street address and lot on plan

- ☒ Street address **AND** lot on plan (all lots must be listed), **or**
☐ Street address **AND** lot on plan for an adjoining or adjacent property of the premises (appropriate for development in water but adjoining or adjacent to land e.g. jetty, pontoon. All lots must be listed).

a)	Unit No.	Street No.	Street Name and Type	Suburb
		14	Murphy Street	Port Douglas
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)
	4877	114	PTD2094	Douglas Shire
b)	Unit No.	Street No.	Street Name and Type	Suburb
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)

3.2) Coordinates of premises (appropriate for development in remote areas, over part of a lot or in water not adjoining or adjacent to land e.g. channel dredging in Moreton Bay)

Note: Place each set of coordinates in a separate row.

- ☐ Coordinates of premises by longitude and latitude

Longitude(s)	Latitude(s)	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

- ☐ Coordinates of premises by easting and northing

Easting(s)	Northing(s)	Zone Ref.	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> 54 <input type="checkbox"/> 55 <input type="checkbox"/> 56	<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

3.3) Additional premises

- ☐ Additional premises are relevant to this development application and the details of these premises have been attached in a schedule to this development application
☒ Not required

4) Identify any of the following that apply to the premises and provide any relevant details

- ☐ In or adjacent to a water body or watercourse or in or above an aquifer

Name of water body, watercourse or aquifer:

- ☐ On strategic port land under the *Transport Infrastructure Act 1994*

Lot on plan description of strategic port land:

Name of port authority for the lot:

- ☐ In a tidal area

Name of local government for the tidal area (if applicable):

Name of port authority for tidal area (if applicable):

- ☐ On airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*

Name of airport:

<input type="checkbox"/> Listed on the Environmental Management Register (EMR) under the <i>Environmental Protection Act 1994</i>
EMR site identification: <input type="text"/>
<input type="checkbox"/> Listed on the Contaminated Land Register (CLR) under the <i>Environmental Protection Act 1994</i>
CLR site identification: <input type="text"/>

5) Are there any existing easements over the premises?

Note: Easement uses vary throughout Queensland and are to be identified correctly and accurately. For further information on easements and how they may affect the proposed development, see [DA Forms Guide](#).

- ☐ Yes – All easement locations, types and dimensions are included in plans submitted with this development application
- ☒ No

PART 3 – DEVELOPMENT DETAILS

Section 1 – Aspects of development

6.1) Provide details about the first development aspect

a) What is the type of development? *(tick only one box)*

- ☒ Material change of use ☐ Reconfiguring a lot ☐ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☒ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☒ Code assessment ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

Dwelling House

e) Relevant plans

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms guide: Relevant plans](#).

- ☒ Relevant plans of the proposed development are attached to the development application

6.2) Provide details about the second development aspect

a) What is the type of development? *(tick only one box)*

- ☐ Material change of use ☐ Reconfiguring a lot ☐ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☐ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☐ Code assessment ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

e) Relevant plans

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).

- ☐ Relevant plans of the proposed development are attached to the development application

6.3) Additional aspects of development

☐ Additional aspects of development are relevant to this development application and the details for these aspects that would be required under Part 3 Section 1 of this form have been attached to this development application

- ☒ Not required

Section 2 – Further development details

7) Does the proposed development application involve any of the following?	
Material change of use	<input checked="" type="checkbox"/> Yes – complete division 1 if assessable against a local planning instrument
Reconfiguring a lot	<input type="checkbox"/> Yes – complete division 2
Operational work	<input type="checkbox"/> Yes – complete division 3
Building work	<input type="checkbox"/> Yes – complete DA Form 2 – Building work details

Division 1 – Material change of use

Note: This division is only required to be completed if any part of the development application involves a material change of use assessable against a local planning instrument.

8.1) Describe the proposed material change of use			
Provide a general description of the proposed use	Provide the planning scheme definition (include each definition in a new row)	Number of dwelling units (if applicable)	Gross floor area (m ²) (if applicable)
Residential dwelling	Dwelling House		
8.2) Does the proposed use involve the use of existing buildings on the premises?			
<input type="checkbox"/> Yes			
<input checked="" type="checkbox"/> No			

Division 2 – Reconfiguring a lot

Note: This division is only required to be completed if any part of the development application involves reconfiguring a lot.

9.1) What is the total number of existing lots making up the premises?	
9.2) What is the nature of the lot reconfiguration? (tick all applicable boxes)	
<input type="checkbox"/> Subdivision (complete 10))	<input type="checkbox"/> Dividing land into parts by agreement (complete 11))
<input type="checkbox"/> Boundary realignment (complete 12))	<input type="checkbox"/> Creating or changing an easement giving access to a lot from a constructed road (complete 13))

10) Subdivision				
10.1) For this development, how many lots are being created and what is the intended use of those lots:				
Intended use of lots created	Residential	Commercial	Industrial	Other, please specify:
Number of lots created				
10.2) Will the subdivision be staged?				
<input type="checkbox"/> Yes – provide additional details below				
<input type="checkbox"/> No				
How many stages will the works include?				
What stage(s) will this development application apply to?				

11) Dividing land into parts by agreement – how many parts are being created and what is the intended use of the parts?				
Intended use of parts created	Residential	Commercial	Industrial	Other, please specify:
Number of parts created				

12) Boundary realignment			
12.1) What are the current and proposed areas for each lot comprising the premises?			
Current lot		Proposed lot	
Lot on plan description	Area (m ²)	Lot on plan description	Area (m ²)
12.2) What is the reason for the boundary realignment?			

13) What are the dimensions and nature of any existing easements being changed and/or any proposed easement? (attach schedule if there are more than two easements)				
Existing or proposed?	Width (m)	Length (m)	Purpose of the easement? (e.g. pedestrian access)	Identify the land/lot(s) benefitted by the easement

Division 3 – Operational work

Note: This division is only required to be completed if any part of the development application involves operational work.

14.1) What is the nature of the operational work?	
<input type="checkbox"/> Road work <input type="checkbox"/> Drainage work <input type="checkbox"/> Landscaping <input type="checkbox"/> Other – please specify:	<input type="checkbox"/> Stormwater <input type="checkbox"/> Earthworks <input type="checkbox"/> Signage <input type="checkbox"/> Water infrastructure <input type="checkbox"/> Sewage infrastructure <input type="checkbox"/> Clearing vegetation
14.2) Is the operational work necessary to facilitate the creation of new lots? (e.g. subdivision)	
<input type="checkbox"/> Yes – specify number of new lots:	
<input type="checkbox"/> No	
14.3) What is the monetary value of the proposed operational work? (include GST, materials and labour)	
\$	

PART 4 – ASSESSMENT MANAGER DETAILS

15) Identify the assessment manager(s) who will be assessing this development application
Douglas Shire Council
16) Has the local government agreed to apply a superseded planning scheme for this development application?
<input type="checkbox"/> Yes – a copy of the decision notice is attached to this development application <input type="checkbox"/> The local government is taken to have agreed to the superseded planning scheme request – relevant documents attached <input checked="" type="checkbox"/> No

PART 5 – REFERRAL DETAILS

17) Does this development application include any aspects that have any referral requirements?

Note: A development application will require referral if prescribed by the Planning Regulation 2017.

☒ No, there are no referral requirements relevant to any development aspects identified in this development application – proceed to Part 6

Matters requiring referral to the **Chief Executive of the Planning Act 2016:**

- ☐ Clearing native vegetation
- ☐ Contaminated land (*unexploded ordnance*)
- ☐ Environmentally relevant activities (ERA) (*only if the ERA has not been devolved to a local government*)
- ☐ Fisheries – aquaculture
- ☐ Fisheries – declared fish habitat area
- ☐ Fisheries – marine plants
- ☐ Fisheries – waterway barrier works
- ☐ Hazardous chemical facilities
- ☐ Heritage places – Queensland heritage place (*on or near a Queensland heritage place*)
- ☐ Infrastructure-related referrals – designated premises
- ☐ Infrastructure-related referrals – state transport infrastructure
- ☐ Infrastructure-related referrals – State transport corridor and future State transport corridor
- ☐ Infrastructure-related referrals – State-controlled transport tunnels and future state-controlled transport tunnels
- ☐ Infrastructure-related referrals – near a state-controlled road intersection
- ☐ Koala habitat in SEQ region – interfering with koala habitat in koala habitat areas outside koala priority areas
- ☐ Koala habitat in SEQ region – key resource areas
- ☐ Ports – Brisbane core port land – near a State transport corridor or future State transport corridor
- ☐ Ports – Brisbane core port land – environmentally relevant activity (ERA)
- ☐ Ports – Brisbane core port land – tidal works or work in a coastal management district
- ☐ Ports – Brisbane core port land – hazardous chemical facility
- ☐ Ports – Brisbane core port land – taking or interfering with water
- ☐ Ports – Brisbane core port land – referable dams
- ☐ Ports – Brisbane core port land – fisheries
- ☐ Ports – Land within Port of Brisbane's port limits (*below high-water mark*)
- ☐ SEQ development area
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – tourist activity or sport and recreation activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – community activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – indoor recreation
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – urban activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – combined use
- ☐ Tidal works or works in a coastal management district
- ☐ Reconfiguring a lot in a coastal management district or for a canal
- ☐ Erosion prone area in a coastal management district
- ☐ Urban design
- ☐ Water-related development – taking or interfering with water
- ☐ Water-related development – removing quarry material (*from a watercourse or lake*)
- ☐ Water-related development – referable dams
- ☐ Water-related development – levees (*category 3 levees only*)
- ☐ Wetland protection area

Matters requiring referral to the **local government:**

- ☐ Airport land
- ☐ Environmentally relevant activities (ERA) (*only if the ERA has been devolved to local government*)

<input type="checkbox"/> Heritage places – Local heritage places
Matters requiring referral to the Chief Executive of the distribution entity or transmission entity:
<input type="checkbox"/> Infrastructure-related referrals – Electricity infrastructure
Matters requiring referral to:
<ul style="list-style-type: none"> • The Chief Executive of the holder of the licence, if not an individual • The holder of the licence, if the holder of the licence is an individual
<input type="checkbox"/> Infrastructure-related referrals – Oil and gas infrastructure
Matters requiring referral to the Brisbane City Council:
<input type="checkbox"/> Ports – Brisbane core port land
Matters requiring referral to the Minister responsible for administering the Transport Infrastructure Act 1994:
<input type="checkbox"/> Ports – Brisbane core port land <i>(where inconsistent with the Brisbane port LUP for transport reasons)</i>
<input type="checkbox"/> Ports – Strategic port land
Matters requiring referral to the relevant port operator , if applicant is not port operator:
<input type="checkbox"/> Ports – Land within Port of Brisbane's port limits <i>(below high-water mark)</i>
Matters requiring referral to the Chief Executive of the relevant port authority:
<input type="checkbox"/> Ports – Land within limits of another port <i>(below high-water mark)</i>
Matters requiring referral to the Gold Coast Waterways Authority:
<input type="checkbox"/> Tidal works or work in a coastal management district <i>(in Gold Coast waters)</i>
Matters requiring referral to the Queensland Fire and Emergency Service:
<input type="checkbox"/> Tidal works or work in a coastal management district <i>(involving a marina (more than six vessel berths))</i>

18) Has any referral agency provided a referral response for this development application?		
<input type="checkbox"/> Yes – referral response(s) received and listed below are attached to this development application		
<input type="checkbox"/> No		
Referral requirement	Referral agency	Date of referral response
Identify and describe any changes made to the proposed development application that was the subject of the referral response and this development application, or include details in a schedule to this development application <i>(if applicable)</i> .		

PART 6 – INFORMATION REQUEST

19) Information request under Part 3 of the DA Rules
<input checked="" type="checkbox"/> I agree to receive an information request if determined necessary for this development application
<input type="checkbox"/> I do not agree to accept an information request for this development application
Note: By not agreeing to accept an information request I, the applicant, acknowledge:
<ul style="list-style-type: none"> • that this development application will be assessed and decided based on the information provided when making this development application and the assessment manager and any referral agencies relevant to the development application are not obligated under the DA Rules to accept any additional information provided by the applicant for the development application unless agreed to by the relevant parties • Part 3 of the DA Rules will still apply if the application is an application listed under section 11.3 of the DA Rules.
Further advice about information requests is contained in the DA Forms Guide .

PART 7 – FURTHER DETAILS

20) Are there any associated development applications or current approvals? (e.g. a preliminary approval)			
<input type="checkbox"/> Yes – provide details below or include details in a schedule to this development application <input checked="" type="checkbox"/> No			
List of approval/development application references	Reference number	Date	Assessment manager
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			

21) Has the portable long service leave levy been paid? (only applicable to development applications involving building work or operational work)		
<input type="checkbox"/> Yes – a copy of the receipted QLeave form is attached to this development application <input type="checkbox"/> No – I, the applicant will provide evidence that the portable long service leave levy has been paid before the assessment manager decides the development application. I acknowledge that the assessment manager may give a development approval only if I provide evidence that the portable long service leave levy has been paid <input checked="" type="checkbox"/> Not applicable (e.g. building and construction work is less than \$150,000 excluding GST)		
Amount paid	Date paid (dd/mm/yy)	QLeave levy number (A, B or E)
\$		

22) Is this development application in response to a show cause notice or required as a result of an enforcement notice?	
<input type="checkbox"/> Yes – show cause or enforcement notice is attached <input checked="" type="checkbox"/> No	

23) Further legislative requirements			
<u>Environmentally relevant activities</u>			
23.1) Is this development application also taken to be an application for an environmental authority for an Environmentally Relevant Activity (ERA) under section 115 of the <i>Environmental Protection Act 1994</i>?			
<input type="checkbox"/> Yes – the required attachment (form ESR/2015/1791) for an application for an environmental authority accompanies this development application, and details are provided in the table below <input checked="" type="checkbox"/> No <small>Note: Application for an environmental authority can be found by searching "ESR/2015/1791" as a search term at www.qld.gov.au. An ERA requires an environmental authority to operate. See www.business.qld.gov.au for further information.</small>			
Proposed ERA number:		Proposed ERA threshold:	
Proposed ERA name:			
<input type="checkbox"/> Multiple ERAs are applicable to this development application and the details have been attached in a schedule to this development application.			
<u>Hazardous chemical facilities</u>			
23.2) Is this development application for a hazardous chemical facility?			
<input type="checkbox"/> Yes – Form 69: Notification of a facility exceeding 10% of schedule 15 threshold is attached to this development application <input checked="" type="checkbox"/> No <small>Note: See www.business.qld.gov.au for further information about hazardous chemical notifications.</small>			

Clearing native vegetation

23.3) Does this development application involve **clearing native vegetation** that requires written confirmation that the chief executive of the *Vegetation Management Act 1999* is satisfied the clearing is for a relevant purpose under section 22A of the *Vegetation Management Act 1999*?

☐ Yes – this development application includes written confirmation from the chief executive of the *Vegetation Management Act 1999* (s22A determination)

☒ No

Note: 1. Where a development application for operational work or material change of use requires a s22A determination and this is not included, the development application is prohibited development.
2. See <https://www.qld.gov.au/environment/land/vegetation/applying> for further information on how to obtain a s22A determination.

Environmental offsets

23.4) Is this development application taken to be a prescribed activity that may have a significant residual impact on a **prescribed environmental matter** under the *Environmental Offsets Act 2014*?

☐ Yes – I acknowledge that an environmental offset must be provided for any prescribed activity assessed as having a significant residual impact on a prescribed environmental matter

☒ No

Note: The environmental offset section of the Queensland Government's website can be accessed at www.qld.gov.au for further information on environmental offsets.

Koala habitat in SEQ Region

23.5) Does this development application involve a material change of use, reconfiguring a lot or operational work which is assessable development under Schedule 10, Part 10 of the Planning Regulation 2017?

☐ Yes – the development application involves premises in the koala habitat area in the koala priority area

☐ Yes – the development application involves premises in the koala habitat area outside the koala priority area

☒ No

Note: If a koala habitat area determination has been obtained for this premises and is current over the land, it should be provided as part of this development application. See koala habitat area guidance materials at www.des.qld.gov.au for further information.

Water resources

23.6) Does this development application involve **taking or interfering with underground water through an artesian or subartesian bore, taking or interfering with water in a watercourse, lake or spring, or taking overland flow water under the Water Act 2000?**

☐ Yes – the relevant template is completed and attached to this development application and I acknowledge that a relevant authorisation or licence under the *Water Act 2000* may be required prior to commencing development

☒ No

Note: Contact the Department of Natural Resources, Mines and Energy at www.dnrme.qld.gov.au for further information.

DA templates are available from <https://planning.dsdmip.qld.gov.au/>. If the development application involves:

- Taking or interfering with underground water through an artesian or subartesian bore: complete DA Form 1 Template 1
- Taking or interfering with water in a watercourse, lake or spring: complete DA Form 1 Template 2
- Taking overland flow water: complete DA Form 1 Template 3.

Waterway barrier works

23.7) Does this application involve **waterway barrier works?**

☐ Yes – the relevant template is completed and attached to this development application

☒ No

DA templates are available from <https://planning.dsdmip.qld.gov.au/>. For a development application involving waterway barrier works, complete DA Form 1 Template 4.

Marine activities

23.8) Does this development application involve **aquaculture, works within a declared fish habitat area or removal, disturbance or destruction of marine plants?**

☐ Yes – an associated resource allocation authority is attached to this development application, if required under the *Fisheries Act 1994*

☒ No

Note: See guidance materials at www.daf.qld.gov.au for further information.

Quarry materials from a watercourse or lake

23.9) Does this development application involve the **removal of quarry materials from a watercourse or lake** under the *Water Act 2000*?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
☒ No

Note: Contact the Department of Natural Resources, Mines and Energy at www.dnrme.qld.gov.au and www.business.qld.gov.au for further information.

Quarry materials from land under tidal waters

23.10) Does this development application involve the **removal of quarry materials from land under tidal water** under the *Coastal Protection and Management Act 1995*?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
☒ No

Note: Contact the Department of Environment and Science at www.des.qld.gov.au for further information.

Referable dams

23.11) Does this development application involve a **referable dam** required to be failure impact assessed under section 343 of the *Water Supply (Safety and Reliability) Act 2008* (the *Water Supply Act*)?

- ☐ Yes – the 'Notice Accepting a Failure Impact Assessment' from the chief executive administering the *Water Supply Act* is attached to this development application
☒ No

Note: See guidance materials at www.dnrme.qld.gov.au for further information.

Tidal work or development within a coastal management district

23.12) Does this development application involve **tidal work or development in a coastal management district**?

- ☐ Yes – the following is included with this development application:
- ☐ Evidence the proposal meets the code for assessable development that is prescribed tidal work (*only required if application involves prescribed tidal work*)
 - ☐ A certificate of title
- ☒ No

Note: See guidance materials at www.des.qld.gov.au for further information.

Queensland and local heritage places

23.13) Does this development application propose development on or adjoining a place entered in the **Queensland heritage register** or on a place entered in a local government's **Local Heritage Register**?

- ☐ Yes – details of the heritage place are provided in the table below
☒ No

Note: See guidance materials at www.des.qld.gov.au for information requirements regarding development of Queensland heritage places.

Name of the heritage place:		Place ID:	
-----------------------------	--	-----------	--

Brothels

23.14) Does this development application involve a **material change of use for a brothel**?

- ☐ Yes – this development application demonstrates how the proposal meets the code for a development application for a brothel under Schedule 3 of the *Prostitution Regulation 2014*
☒ No

Decision under section 62 of the Transport Infrastructure Act 1994

23.15) Does this development application involve new or changed access to a state-controlled road?

- ☐ Yes – this application will be taken to be an application for a decision under section 62 of the *Transport Infrastructure Act 1994* (subject to the conditions in section 75 of the *Transport Infrastructure Act 1994* being satisfied)
☒ No

Walkable neighbourhoods assessment benchmarks under Schedule 12A of the Planning Regulation

23.16) Does this development application involve reconfiguring a lot into 2 or more lots in certain residential zones (except rural residential zones), where at least one road is created or extended?

☐ Yes – Schedule 12A is applicable to the development application and the assessment benchmarks contained in schedule 12A have been considered

☒ No

Note: See guidance materials at www.planning.dsdmip.qld.gov.au for further information.

PART 8 – CHECKLIST AND APPLICANT DECLARATION

24) Development application checklist

I have identified the assessment manager in question 15 and all relevant referral requirement(s) in question 17

☒ Yes

Note: See the Planning Regulation 2017 for referral requirements

If building work is associated with the proposed development, Parts 4 to 6 of [DA Form 2 – Building work details](#) have been completed and attached to this development application

☐ Yes

☒ Not applicable

Supporting information addressing any applicable assessment benchmarks is with the development application

Note: This is a mandatory requirement and includes any relevant templates under question 23, a planning report and any technical reports required by the relevant categorising instruments (e.g. local government planning schemes, State Planning Policy, State Development Assessment Provisions). For further information, see [DA Forms Guide: Planning Report Template](#).

☒ Yes

Relevant plans of the development are attached to this development application

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).

☒ Yes

The portable long service leave levy for QLeave has been paid, or will be paid before a development permit is issued (see 21)

☐ Yes

☒ Not applicable

25) Applicant declaration

☒ By making this development application, I declare that all information in this development application is true and correct

☒ Where an email address is provided in Part 1 of this form, I consent to receive future electronic communications from the assessment manager and any referral agency for the development application where written information is required or permitted pursuant to sections 11 and 12 of the *Electronic Transactions Act 2001*

Note: It is unlawful to intentionally provide false or misleading information.

Privacy – Personal information collected in this form will be used by the assessment manager and/or chosen assessment manager, any relevant referral agency and/or building certifier (including any professional advisers which may be engaged by those entities) while processing, assessing and deciding the development application. All information relating to this development application may be available for inspection and purchase, and/or published on the assessment manager's and/or referral agency's website.

Personal information will not be disclosed for a purpose unrelated to the *Planning Act 2016*, Planning Regulation 2017 and the DA Rules except where:

- such disclosure is in accordance with the provisions about public access to documents contained in the *Planning Act 2016* and the Planning Regulation 2017, and the access rules made under the *Planning Act 2016* and Planning Regulation 2017; or
- required by other legislation (including the *Right to Information Act 2009*); or
- otherwise required by law.

This information may be stored in relevant databases. The information collected will be retained as required by the *Public Records Act 2002*.

PART 9 – FOR COMPLETION OF THE ASSESSMENT MANAGER – FOR OFFICE USE ONLY

Date received: Reference number(s):

Notification of engagement of alternative assessment manager

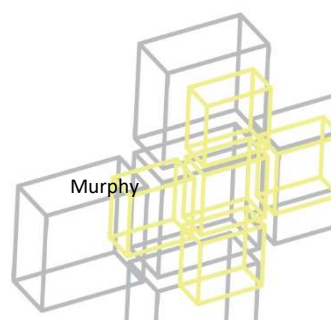
Prescribed assessment manager	
Name of chosen assessment manager	
Date chosen assessment manager engaged	
Contact number of chosen assessment manager	
Relevant licence number(s) of chosen assessment manager	

QLeave notification and payment

Note: For completion by assessment manager if applicable

Description of the work	
QLeave project number	
Amount paid (\$)	Date paid (dd/mm/yy)
Date receipted form sighted by assessment manager	
Name of officer who sighted the form	

Annexure 2: Title Searches



Queensland Titles Registry Pty Ltd
ABN 23 648 568 101

Title Reference:	20572145	Search Date:	25/02/2022 14:42
Date Title Created:	24/03/1959	Request No:	40273687
Previous Title:	20436040		

ESTATE AND LAND

Estate in Fee Simple

LOT 114 CROWN PLAN PTD2094
Local Government: DOUGLAS

REGISTERED OWNER

Dealing No: 706055188 22/10/2002
PATAGORANG PTY LTD A.C.N. 005 178 432

EASEMENTS, ENCUMBRANCES AND INTERESTS

1. Rights and interests reserved to the Crown by
Deed of Grant No. 10369038 (ALLOT 4 SEC 11)
2. CAVEAT No 721419350 19/01/2022 at 11:37
GEORGE ARGYROU

ADMINISTRATIVE ADVICES

NIL

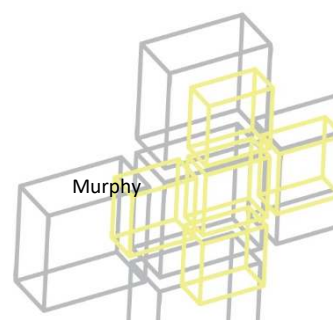
UNREGISTERED DEALINGS

NIL

Caution - Charges do not necessarily appear in order of priority

** End of Current Title Search **

Annexure 3: QLD Globe Aerial Overlay



16°28'49"S 145°27'51"E

16°28'49"S 145°27'55"E

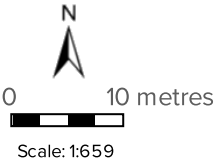


16°28'53"S 145°27'51"E

16°28'53"S 145°27'55"E

A product of
 **Queensland Globe**

Legend located on next page



Scale: 1:659

Printed at: A4

Print date: 17/3/2022

Projection: Web Mercator EPSG 102100 (3857)

For more information, visit
<https://qldglobe.information.qld.gov.au/help-info/Contact-us.html>



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Road parcel



Land parcel



Land parcel - gt 1 ha



Land parcel - gt 10 ha



Easement parcel



Strata parcel



Volumetric parcel



Land parcel - gt 1000 ha



Land parcel label

Land parcel label - gt 1 ha

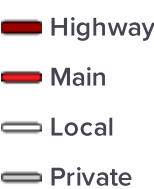
Land parcel label - gt 10 ha

Land parcel label - gt 1000 ha

Road Crossing



Road



Cities and Towns



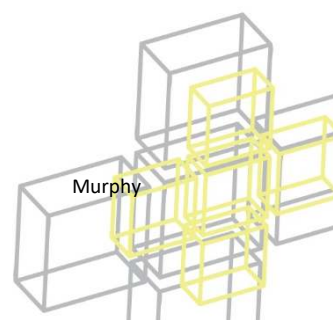
Railway



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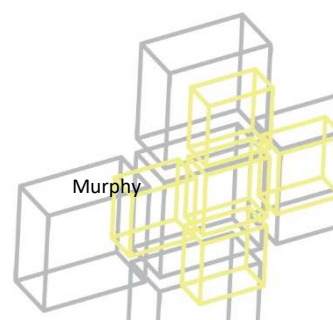
© State of Queensland (Department of Resources) 2021

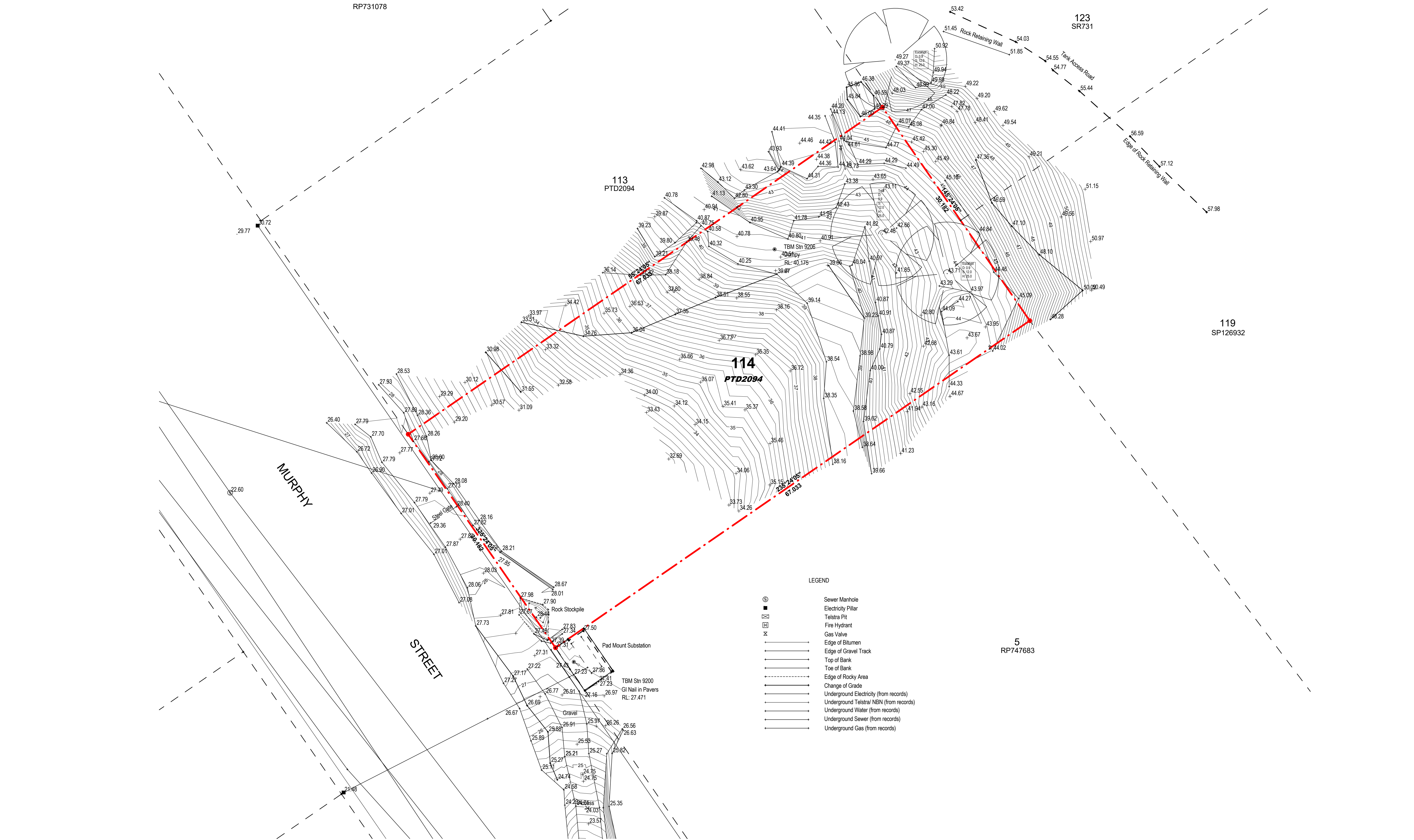
Annexure 4: SmartMap



Annexure 5:

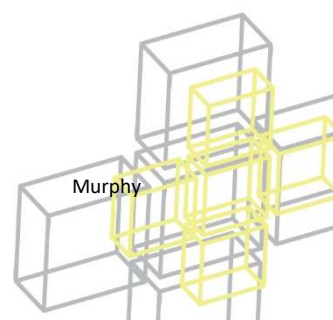
Detail and Contour Survey Plan





Annexure 6:

Planning Scheme Property Report



2018 Douglas Shire Council Planning Scheme Property Report

The following report has been automatically generated to provide a general indication of development related information applying to the premise.

For more information and to determine if the mapping layers are applicable, refer to the [2018 Douglas Shire Council Planning Scheme](#). This report is not intended to replace the need for carrying out a detailed assessment of Council and State controls or the need to seek your own professional advice on any town planning instrument, local law or other controls that may impact on the existing or intended use of the premise mentioned in this report. For further information please contact Council by phone: [07 4099 9444](tel:0740999444) or [1800 026 318](tel:1800026318) or email enquiries@douglas.qld.gov.au.

Visit Council's website to apply for an [official property search or certificate](#), or contact the [Department of Natural Resources, Mines and Energy](#) to undertake a title search to ascertain how easements may affect a premise.

Property Information

Property Address [14 Murphy Street PORT DOUGLAS](#)



Lot Plan [114PTD2094](#) (Freehold - 2023m²)



☒ Selected Property

☐ Easements

☐ Land Parcels

Douglas Shire Planning Scheme 2018 version 1.0

The table below provides a summary of the Zones and Overlays that apply to the selected property.

Zoning








Applicable Zone
Environmental Management

More Information

- [View Section 6.2.4 Environmental Management Zone Code](#)
- [View Section 6.2.4 Environmental Management Zone Compliance table](#)
- [View Section 6.2.4 Environmental Management Zone Assessment table](#)

Douglas Shire Planning Scheme 2018 version 1.0

The table below provides a summary of the Zones and Overlays that apply to the selected property.

 <u>Local Plans</u>	Applicable Precinct or Area Port Douglas - Craiglie Precinct 1 - 1f Flagstaff Hill	More Information <ul style="list-style-type: none"> • View Section 7.2.4 Port Douglas/Craiglie Local Plan Code • View Section 7.2.4 Port Douglas/Craiglie Local Plan Compliance table
 <u>Bushfire Hazard</u>	Applicable Precinct or Area Very High Potential Bushfire Intensity High Potential Bushfire Intensity	More Information <ul style="list-style-type: none"> • View Section 8.2.2 Bushfire Hazard Overlay Code • View Section 8.2.2 Bushfire Hazard Overlay Compliance table
 <u>Coastal Processes</u>	Applicable Precinct or Area Erosion Prone Area	More Information <ul style="list-style-type: none"> • View Section 8.2.3 Coastal Environment Overlay Code • View Section 8.2.3 Coastal Environment Overlay Compliance table
 <u>Hillslopes</u>	Applicable Precinct or Area Area Affected by Hillslopes	More Information <ul style="list-style-type: none"> • View Section 8.2.5 Hillslopes Overlay Code • View Section 8.2.5 Hillslopes Overlay Compliance table
 <u>Landscape Values</u>	Landscape Values High landscape values	More Information <ul style="list-style-type: none"> • View Section 8.2.6 Landscape Values Overlay Code • View Section 8.2.6 Landscape Values Overlay Compliance table
 <u>Natural Areas</u>	Applicable Precinct or Area MSES - Regulated Vegetation	More Information <ul style="list-style-type: none"> • View Section 8.2.7 Natural Areas Overlay Code • View Section 8.2.7 Natural Areas Overlay Compliance table
 <u>Transport Road Hierarchy</u>	Applicable Precinct or Area Access Road	More Information <ul style="list-style-type: none"> • View Section 8.2.10 Transport Network Overlay Code • View Section 8.2.10 Transport Network Overlay Compliance table

Zoning

Applicable Zone
Environmental Management

- More Information**
- [View Section 6.2.4 Environmental Management Zone Code](#)
 - [View Section 6.2.4 Environmental Management Zone Compliance table](#)
 - [View Section 6.2.4 Environmental Management Zone Assessment table](#)



☒ Selected Property

☐ Land Parcels

<input type="checkbox"/> Centre	<input type="checkbox"/> Community Facilities	<input type="checkbox"/> Conservation	<input type="checkbox"/> Environmental Management
<input type="checkbox"/> Industry	<input type="checkbox"/> Low Density Residential	<input type="checkbox"/> Low-medium Density Residential	<input type="checkbox"/> Medium Density Residential
<input type="checkbox"/> Recreation and Open Space	<input type="checkbox"/> Rural	<input type="checkbox"/> Rural Residential	<input type="checkbox"/> Special Purpose
<input type="checkbox"/> Tourism	<input type="checkbox"/> Tourist Accommodation		

Local Plans

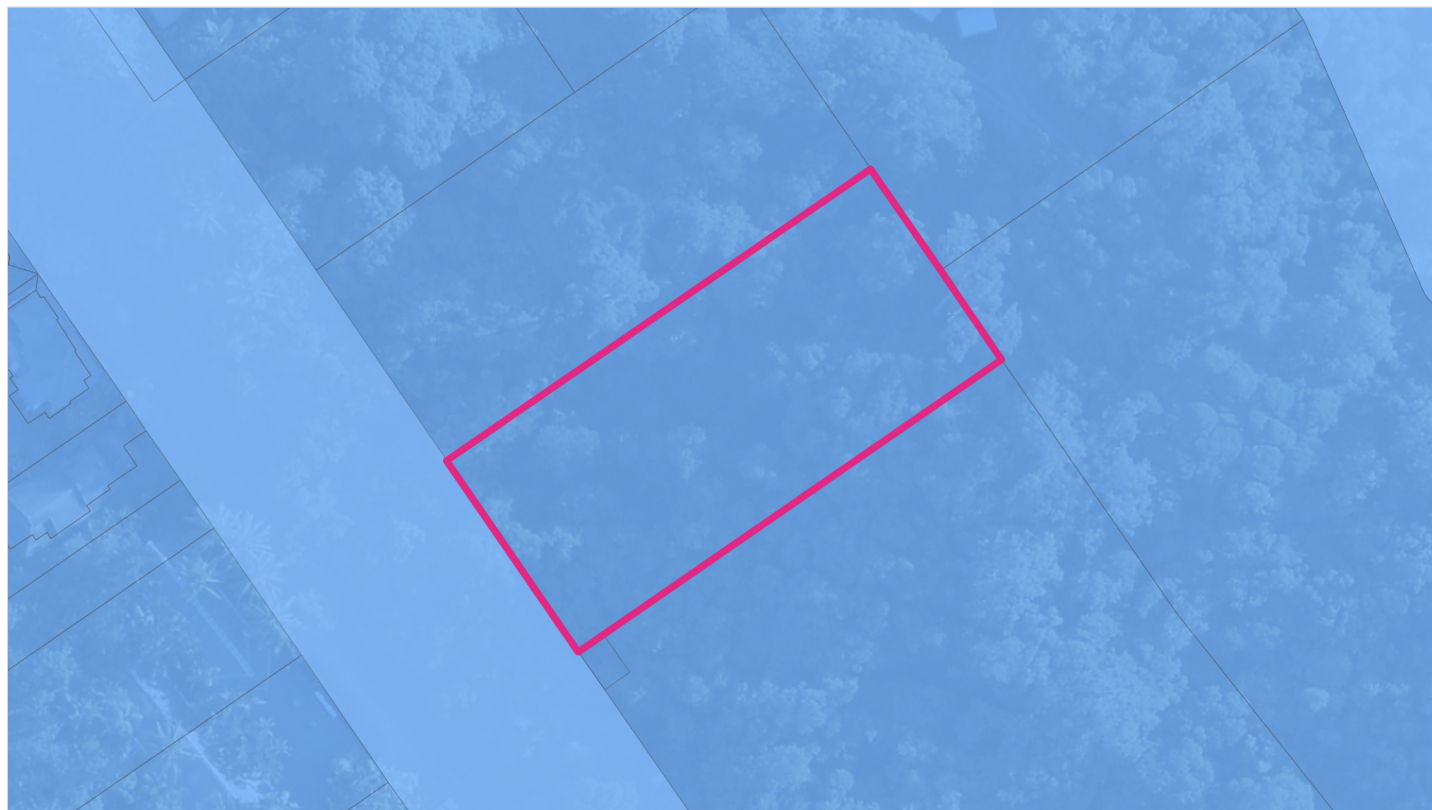
Applicable Precinct or Area

Port Douglas - Craiglie

Precinct 1 - 1f Flagstaff Hill

More Information

- [View Section 7.2.4 Port Douglas/Craiglie Local Plan Code](#)
- [View Section 7.2.4 Port Douglas/Craiglie Local Plan Compliance table](#)



 Selected Property

 Land Parcels

Transport Investigation Corridor

 Transport Investigation Corridors

Major Road Connections

 Major Road Connections

Major Road Connections (No Arrow)

 Major Road Connections

Daintree River to Bloomfield

 Daintree River to Bloomfield

Creb Track and Quaid Road

 Creb Track







60 metre contour

 60 metre contour


Local Plan Boundary

 Local Plan Boundary

Local Plan Sub Precincts

 1a Town Centre	 1b Waterfront North	 1c Waterfront South
 1d Limited Development	 1e Community and Recreation	 1f Flagstaff Hill

Local Plan Precincts

Not Part of a Precinct	 Precinct 1	 Precinct 2	 Precinct 3
 Precinct 4	 Precinct 5	 Precinct 6	 Precinct 7
 Precinct 8	 Precinct 9		

Live Entertainment Precinct

 Live Entertainment Precinct

Indicative Future Open Space

 Indicative Future Open Space

 Road Reserve Esplanade

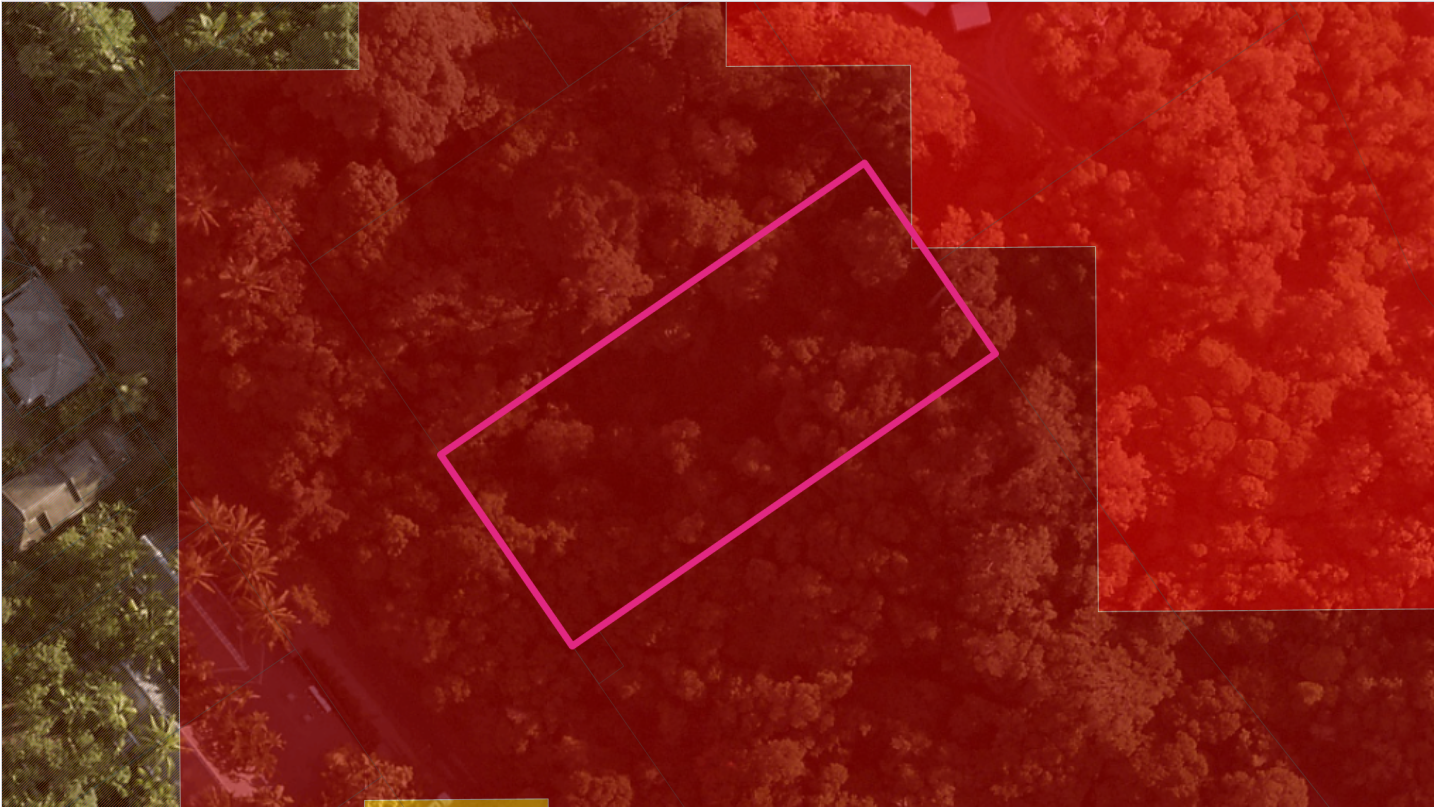
Bushfire Hazard

Applicable Precinct or Area

Very High Potential Bushfire Intensity
High Potential Bushfire Intensity

More Information

- [View Section 8.2.2 Bushfire Hazard Overlay Code](#)
- [View Section 8.2.2 Bushfire Hazard Overlay Compliance table](#)



☒ Selected Property

☐ Land Parcels

Bushfire_Hazard

☒ High Potential Bushfire Intensity

☒ Medium Potential Bushfire Intensity

☒ Potential Impact Buffer

☒ Very High Potential Bushfire Intensity

☐ all others

Coastal Processes

Applicable Precinct or Area
Erosion Prone Area

- More Information
- [View Section 8.2.3 Coastal Environment Overlay Code](#)
 - [View Section 8.2.3 Coastal Environment Overlay Compliance table](#)



☒ Selected Property

☐ Land Parcels

☐ Coastal Management District

☒ Erosion Prone Area

Hillslopes

Applicable Precinct or Area
Area Affected by Hillslopes

- More Information
- [View Section 8.2.5 Hillslopes Overlay Code](#)
 - [View Section 8.2.5 Hillslopes Overlay Compliance table](#)



☒ Selected Property

☐ Land Parcels

☐ Area Affected by Hillslopes

Landscape Values

Landscape Values

High landscape values

- More Information

 - [View Section 8.2.6 Landscape Values Overlay Code](#)
 - [View Section 8.2.6 Landscape Values Overlay Compliance table](#)



Selected Property

Land Parcels

Scenic Buffer Area

Gateway
 View corridor

Lookout
 all others

Scenic route
 Scenic route buffer

Landscape Values

Coastal scenery
 High landscape values
 Medium Landscape Value
 all others

Natural Areas

Applicable Precinct or Area

MSES - Regulated Vegetation

More Information

- [View Section 8.2.7 Natural Areas Overlay Code](#)
- [View Section 8.2.7 Natural Areas Overlay Compliance table](#)



Selected Property



Land Parcels



MSES - Regulated Vegetation (Intersecting a Watercourse)



MSES - High Ecological Value Waters (Watercourse)



MSES - Wildlife Habitat



MSES - Regulated Vegetation



MSES - Protected Area



MSES - Marine Park



MSES - Legally Secured Offset Area



MSES - High Ecological Value Waters (Wetland)



MSES - High Ecological Significance Wetlands

Transport Road Hierarchy

Applicable Precinct or Area

Access Road

More Information

- [View Section 8.2.10 Transport Network Overlay Code](#)
- [View Section 8.2.10 Transport Network Overlay Compliance table](#)



☒ Selected Property

☐ Land Parcels

Road Hierarchy

— Access Road

— Arterial Road

— Collector Road

— Industrial Road

— Major Rural Road

— Minor Rural Road

— Sub Arterial Road

— Unformed Road

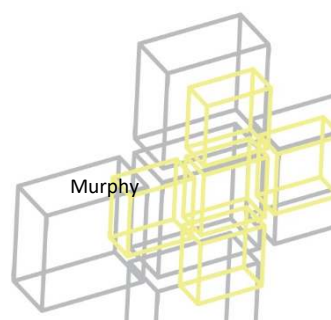
— all others

☐ Major Transport Corridor Buffer Area

Disclaimer

This report is not a substitute for a Planning and Development Certificate and should not be relied upon where the reliance may result in loss, damage or injury. While every effort is taken to ensure the information in this report is accurate and up to date, Douglas Shire Council makes no representations or warranties about its accuracy, reliability, completeness or suitability for any particular purpose and disclaims all responsibility and all liability (including without limitation, liability in negligence) for all expenses, losses, damages (including indirect or consequential damage) and costs that may occur as a result of the report being inaccurate or incomplete in any way or for any reason.

Annexure 7: SDAP Mapping



State Assessment and Referral Agency

Date: 28/02/2022



Queensland Government

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Matters of Interest for all selected Lot Plans

Coastal area - erosion prone area
Regulated vegetation management map (Category A and B extract)

Matters of Interest by Lot Plan

Lot Plan: 114PTD2094 (Area: 2023 m²)
Coastal area - erosion prone area
Regulated vegetation management map (Category A and B extract)



State Assessment and Referral Agency

Date: 28/02/2022





Queensland Government

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Legend

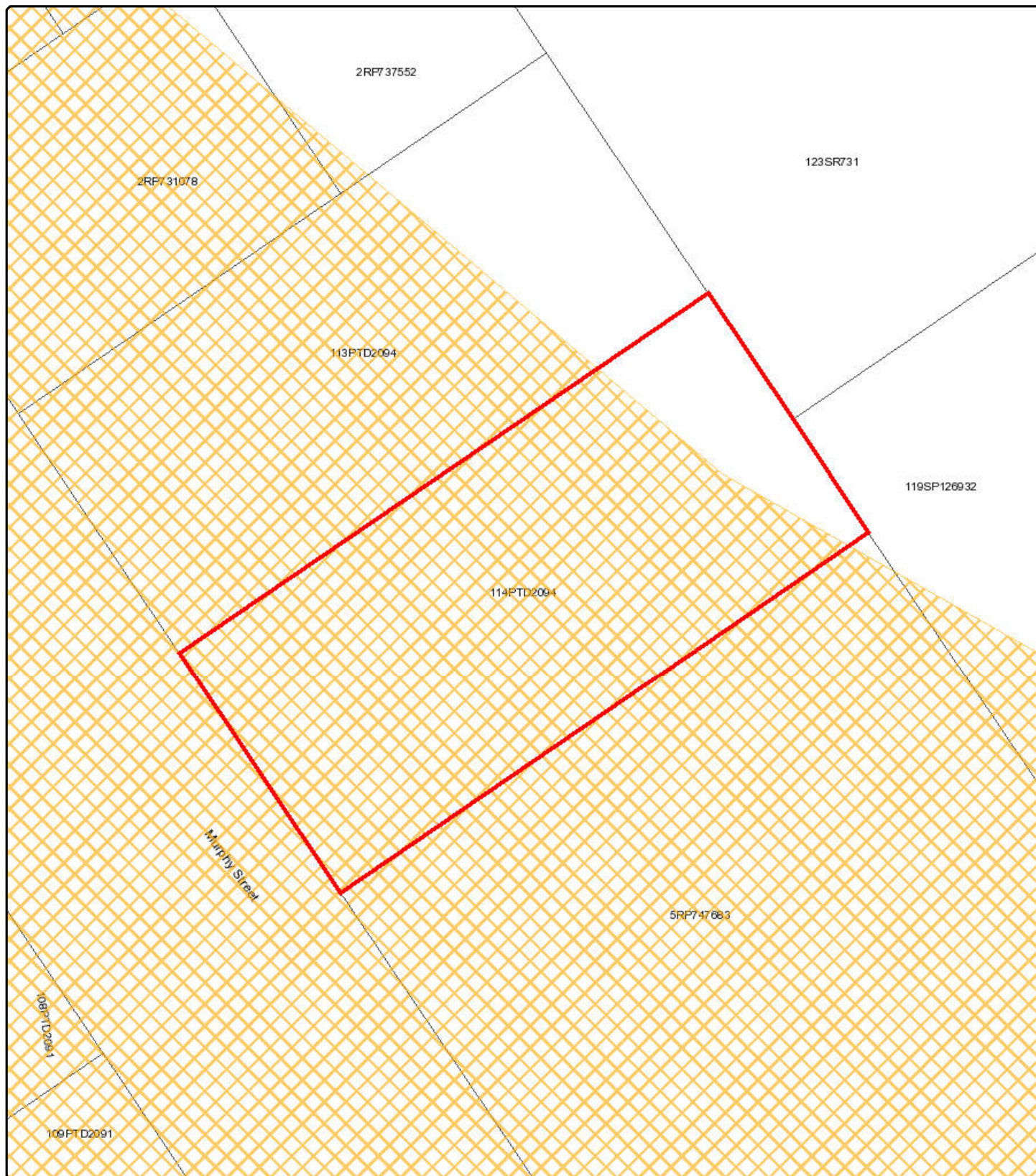
Regulated vegetation management map
(Category A and B extract)

-  Category A on the regulated vegetation management map
-  Category B on the regulated vegetation management map

0 10 20 30 40
Metres

Disclaimer:

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State Assessment and Referral Agency

Date: 28/02/2022



Queensland Government

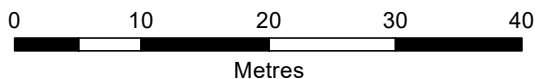
© The State of Queensland 2022.

Legend

Coastal area - erosion prone area



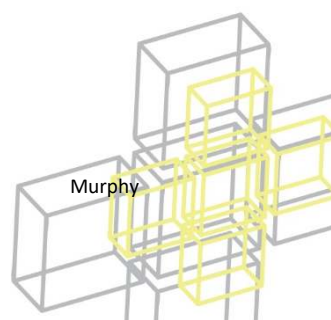
Coastal area - erosion prone area



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Annexure 8: Architectural Plans



PROPOSED DEVELOPMENT AT

14 MURPHY STREET, PORT DOUGLAS

SHEET LIST_TP		
SHEET #	SHEET NAME	Current Revision
TP000 - GENERAL INFO		
TP001	COVER PAGE	
TP010 - SITE INFO		
TP012	LOCALITY PLAN	
TP013	SURVEY	
TP020 - SITE PLAN		
TP021	SITE PLAN	
TP100 - GENERAL ARRANGEMENT		
TP101	GA PLAN - LOWER GROUND FL	
TP102	GA PLAN - GROUND FL	
TP103	GA PLAN - UPPER LEVEL	
TP104	GA PLAN - ROOF PLAN	
TP200 - BUILDING ELEVATION		
TP201	SOUTH WEST AND NORTH EAST ELEVATIONS	
TP202	NORTH WEST ELEVATION	
TP203	SOUTH EAST ELEVATION	
TP204	INTERNAL ELEVATIONS	
TP300 - BUILDING SECTION		
TP301	BUILDING SECTION A-A	
TP302	BUILDING SECTION B-B	
TP303	BUILDING SECTION C-C & D-D	

Grand total: 15

PROJECT INFORMATION

ADDRESS: 14 Murphy Street, Port Douglas, QLD,
LOT ID: PTD2094
LOCAL GOVERNMENT: Douglas Shire Council
OWNER: c/- George Argyrou
email: g.argyrou@hickory.com.au

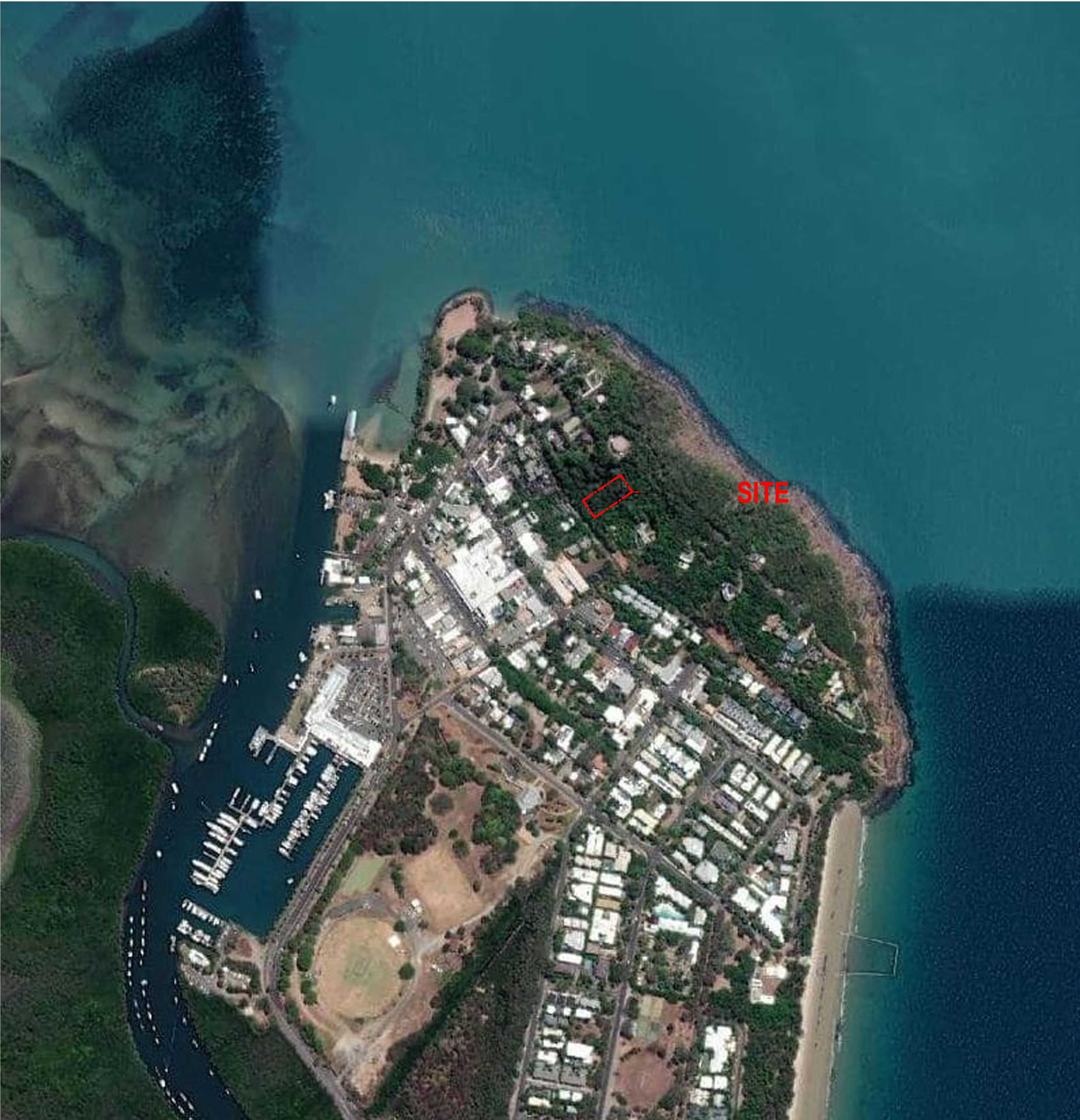
PROJECT DESCRIPTION

Three storey private residential home with subterranean level.
The development incorporates sustainable initiatives such as; PV
panels, hydroelectric power system, grey water collection.

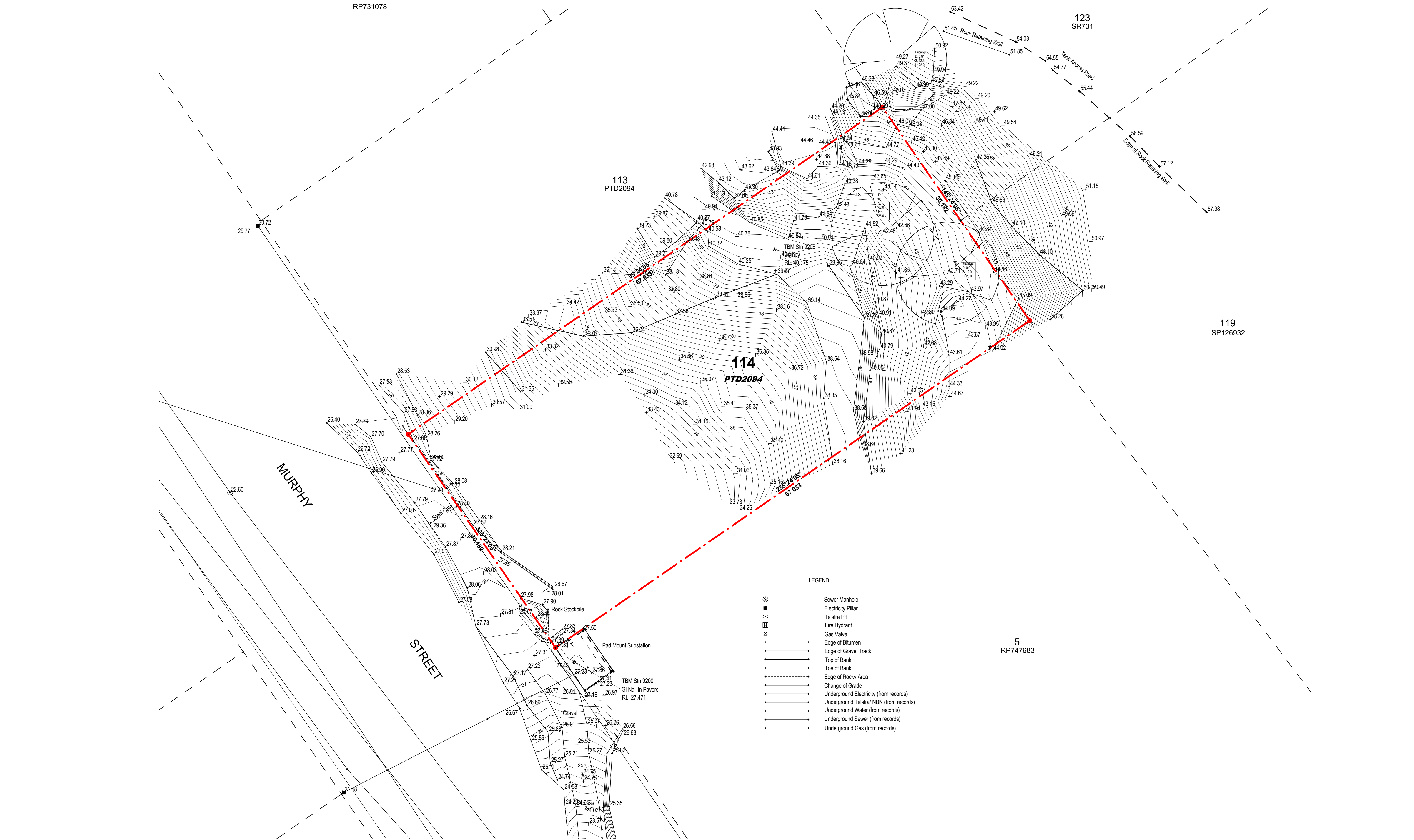
DEVELOPMENT SUMMARY

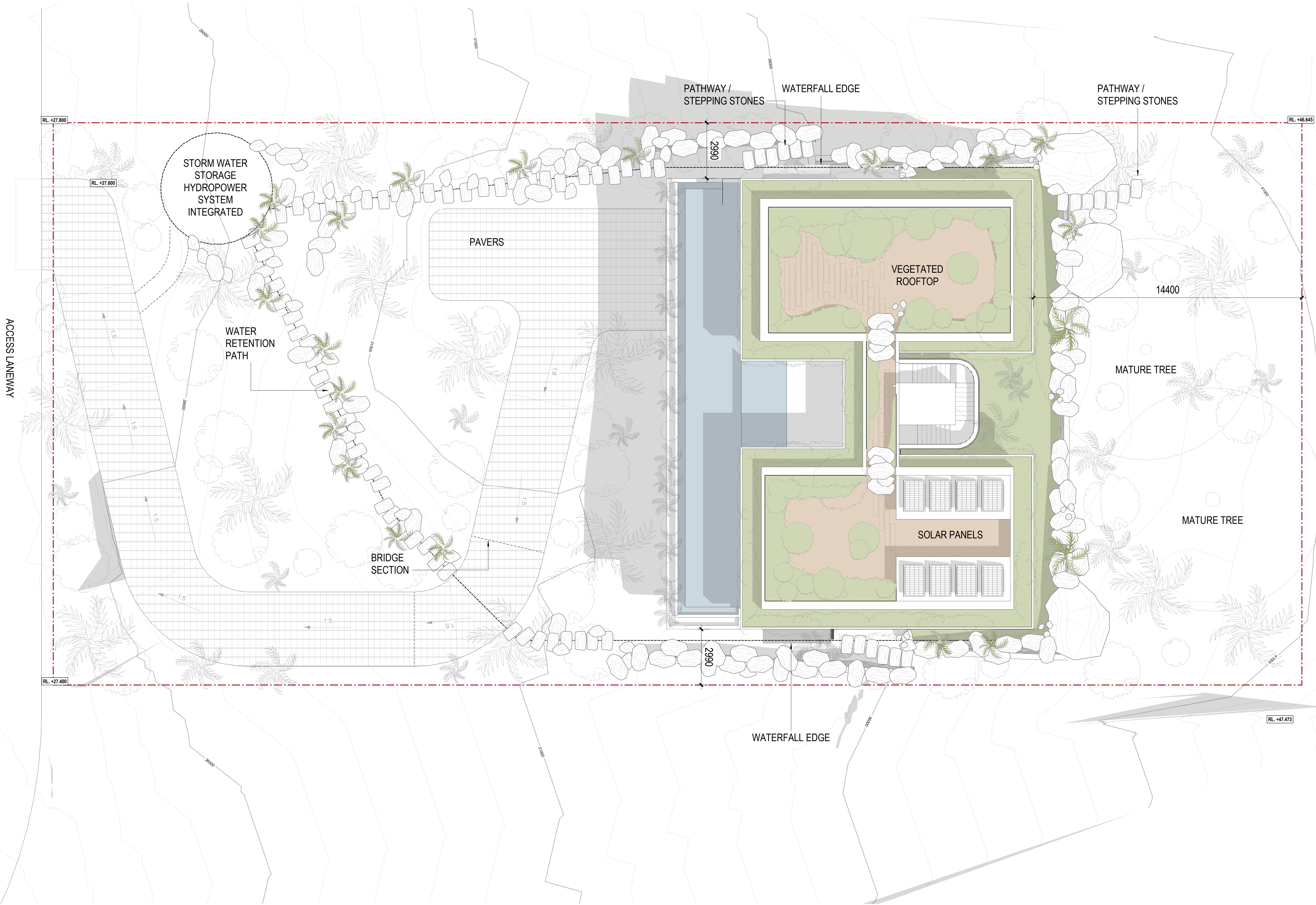
SITE AREA: 2023.179m²
SITE COVER: 476.741 m² (23.71%)

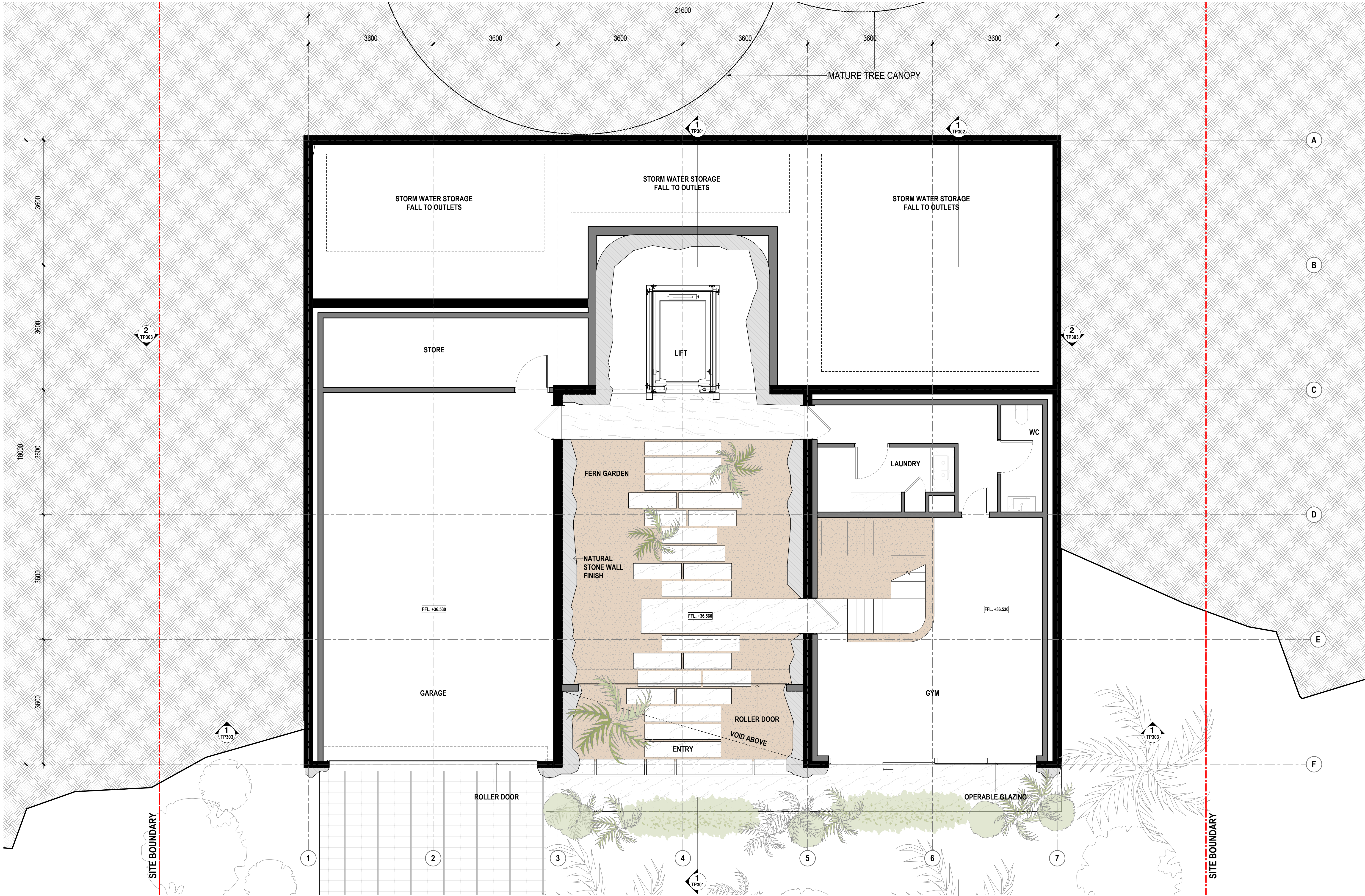
GFA		
Area	Area Type	Level
475 m ²	Floor Area	LOWER FLOOR
339 m ²	Floor Area	GROUND FLOOR
100 m ²	Floor Area	UPPER FLOOR
64 m ²	Floor Area	UPPER FLOOR
978 m ²		

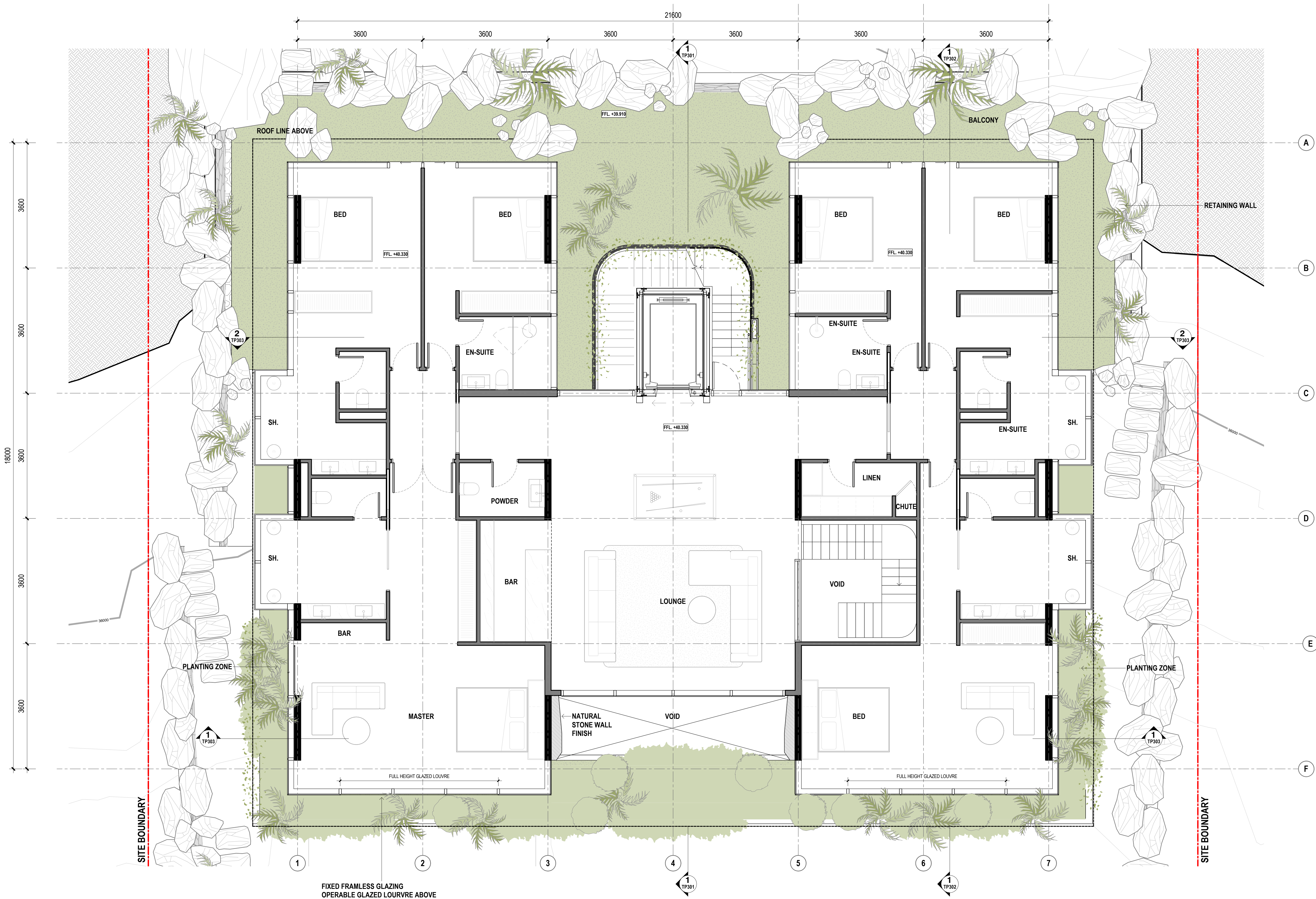


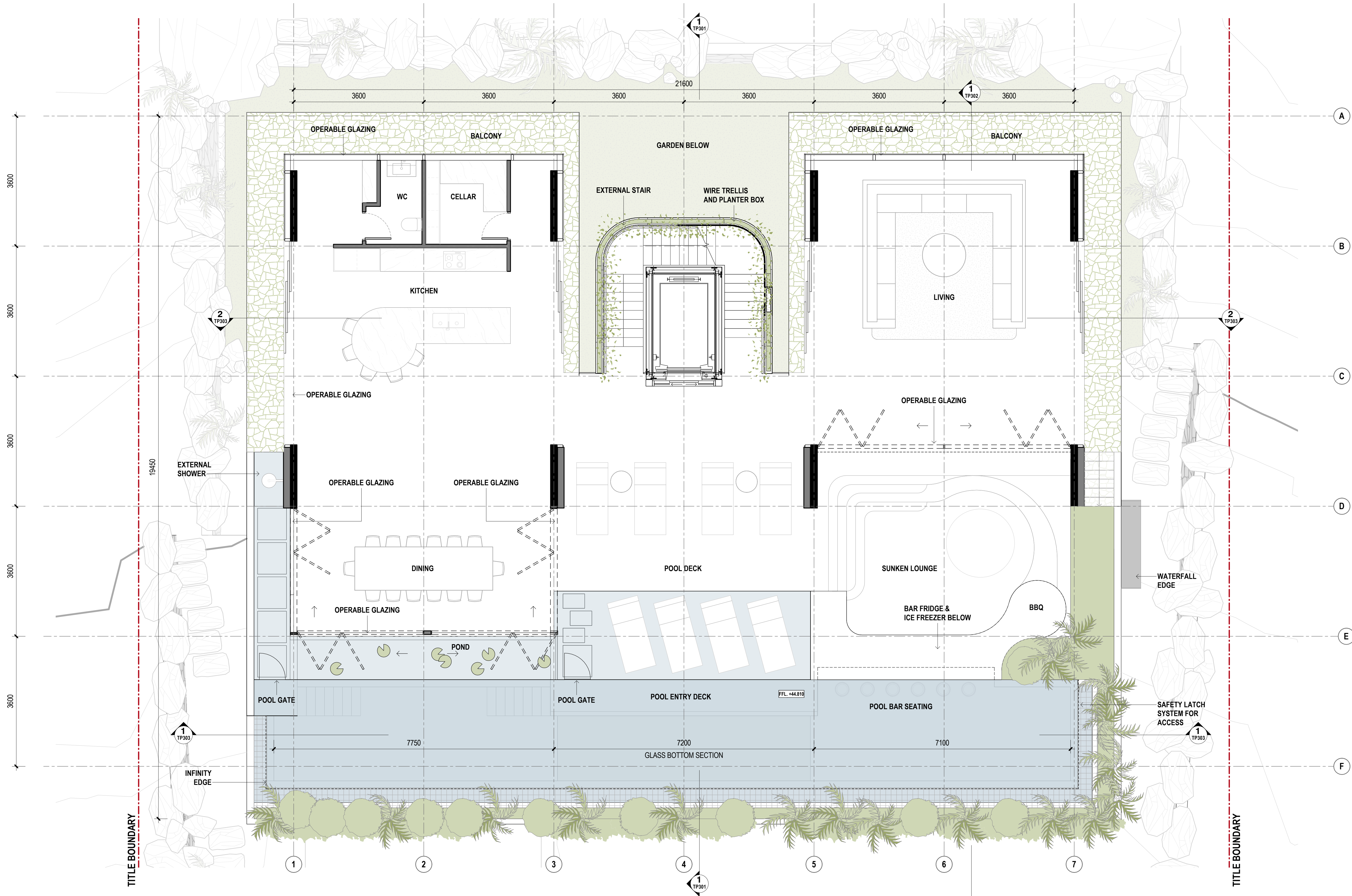
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01. DO NOT SCALE FROM DRAWINGS. USE FIGURED DIMENSIONS ONLY. 02. CONTRACTOR TO VERIFY ALL DIMENSIONS ON SITE PRIOR TO COMMENCEMENT OF WORK OR PREPARATION OF SHOP DRAWINGS. 03. ALL DIMENSIONS, AMBIGUITIES AND OCCURRENCES TO BE REPORTED TO THE ARCHITECT IMMEDIATELY.			REV	DESCRIPTION	DATE	DEVELOPMENT APPLICATION		George Argyrou		1789		14 MURPHY ST, PORT DOUGLAS			LOCALITY PLAN		TP012						
NOT TO BE USED FOR CONSTRUCTION															SCALE		@ A1 / 50% @ A3	DRAWN BY	Author	CHECKED BY	Checker	REVISION	
COPYRIGHT © 2021 BAYLEYWARD																							

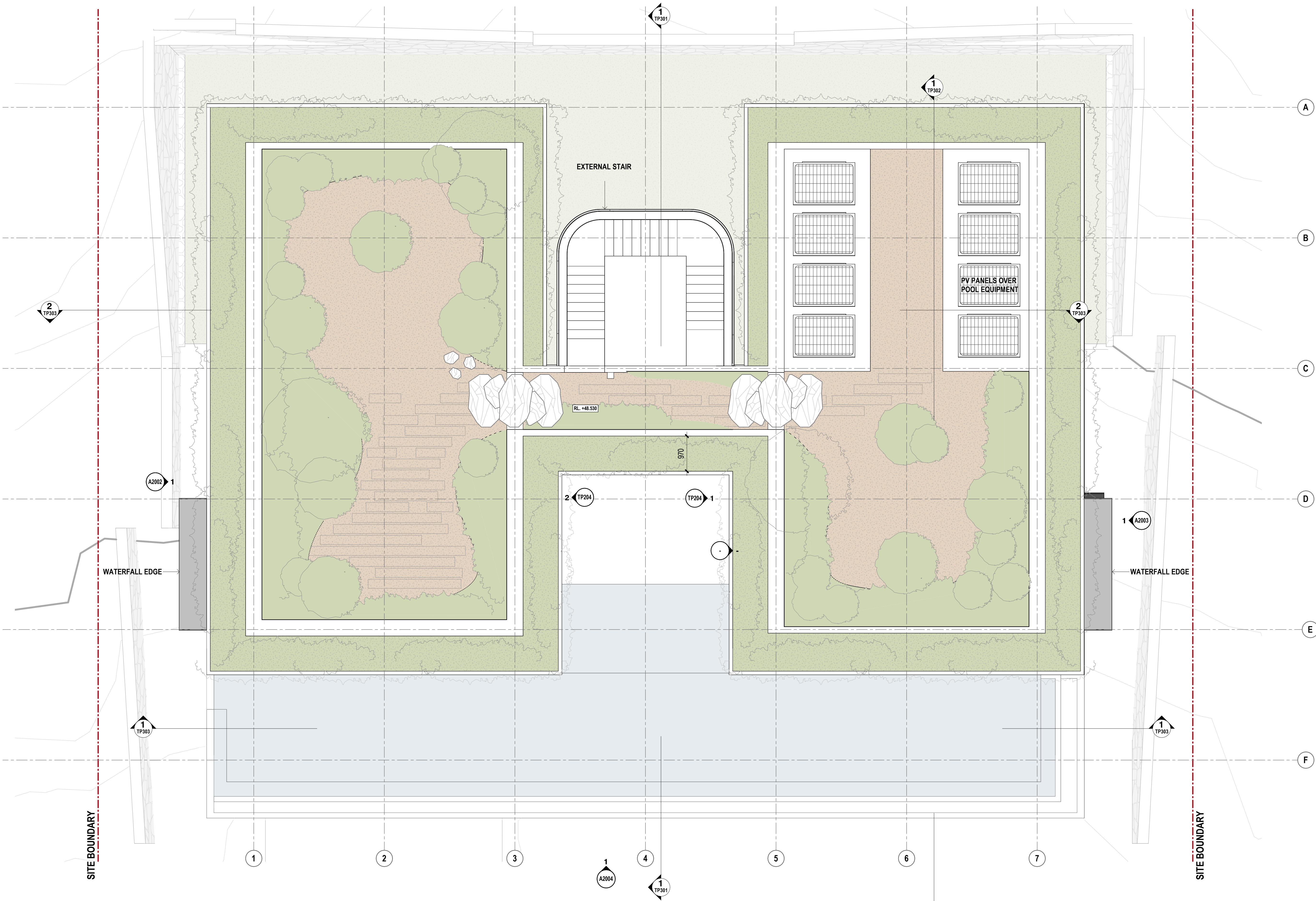


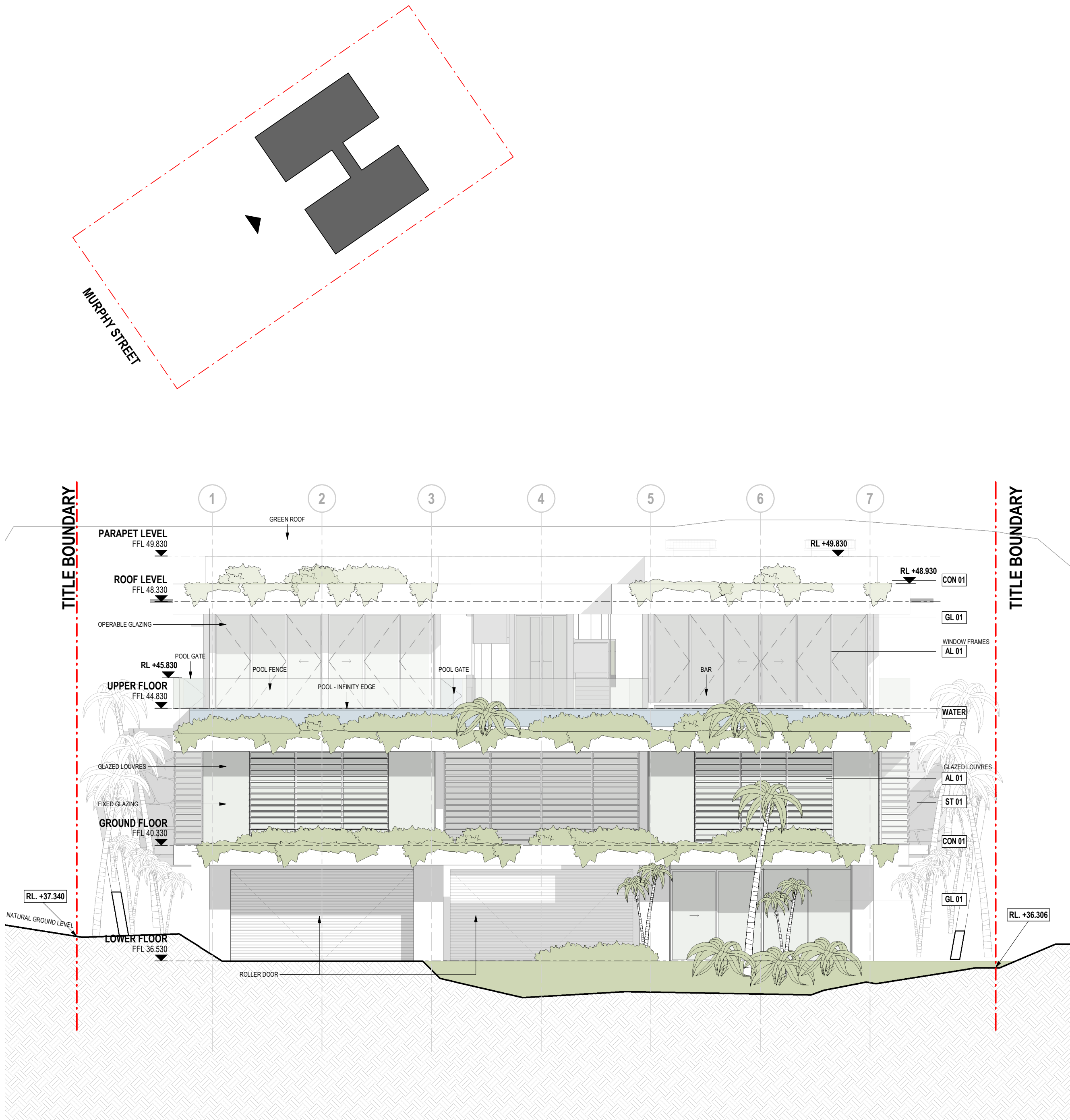




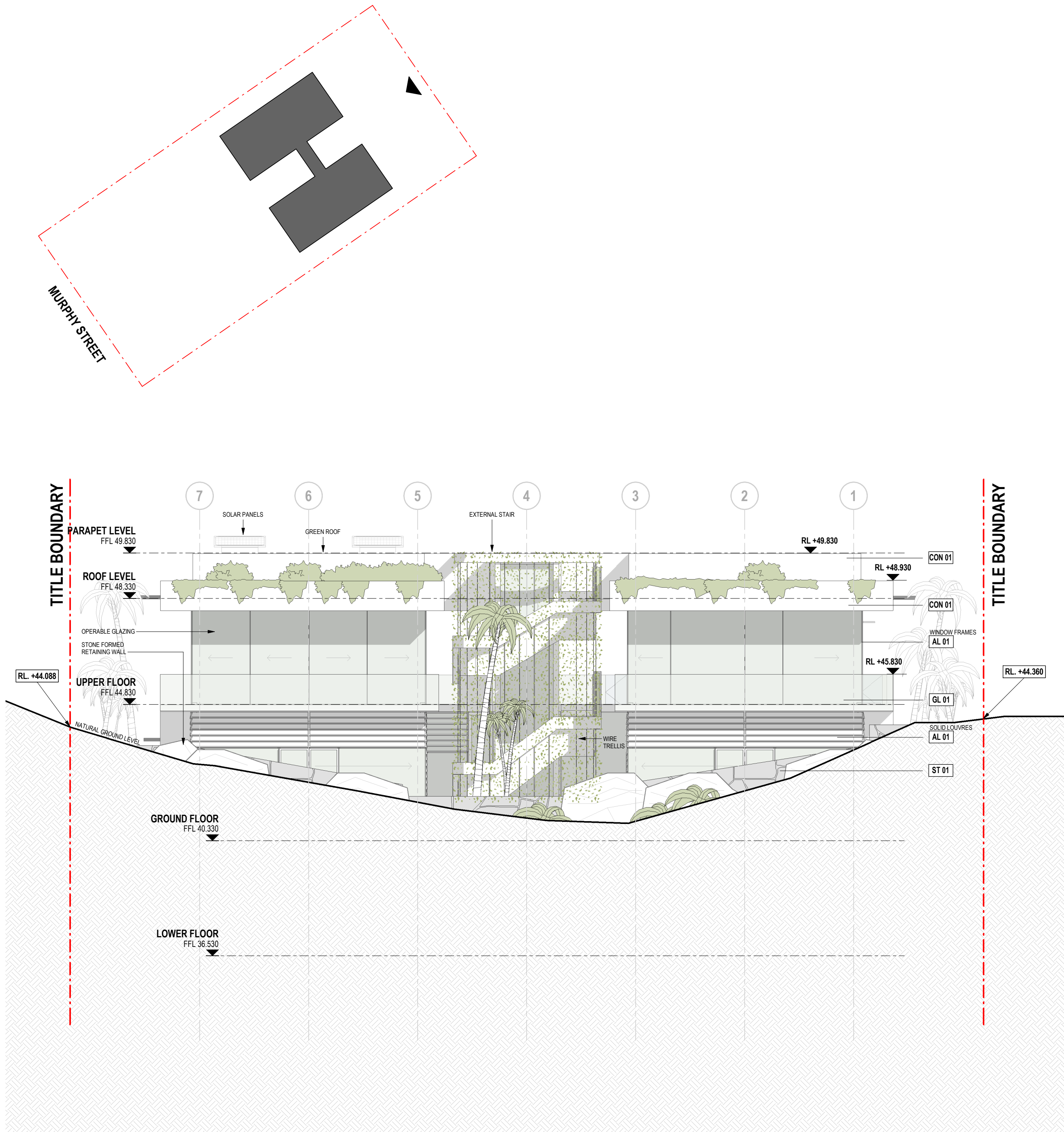








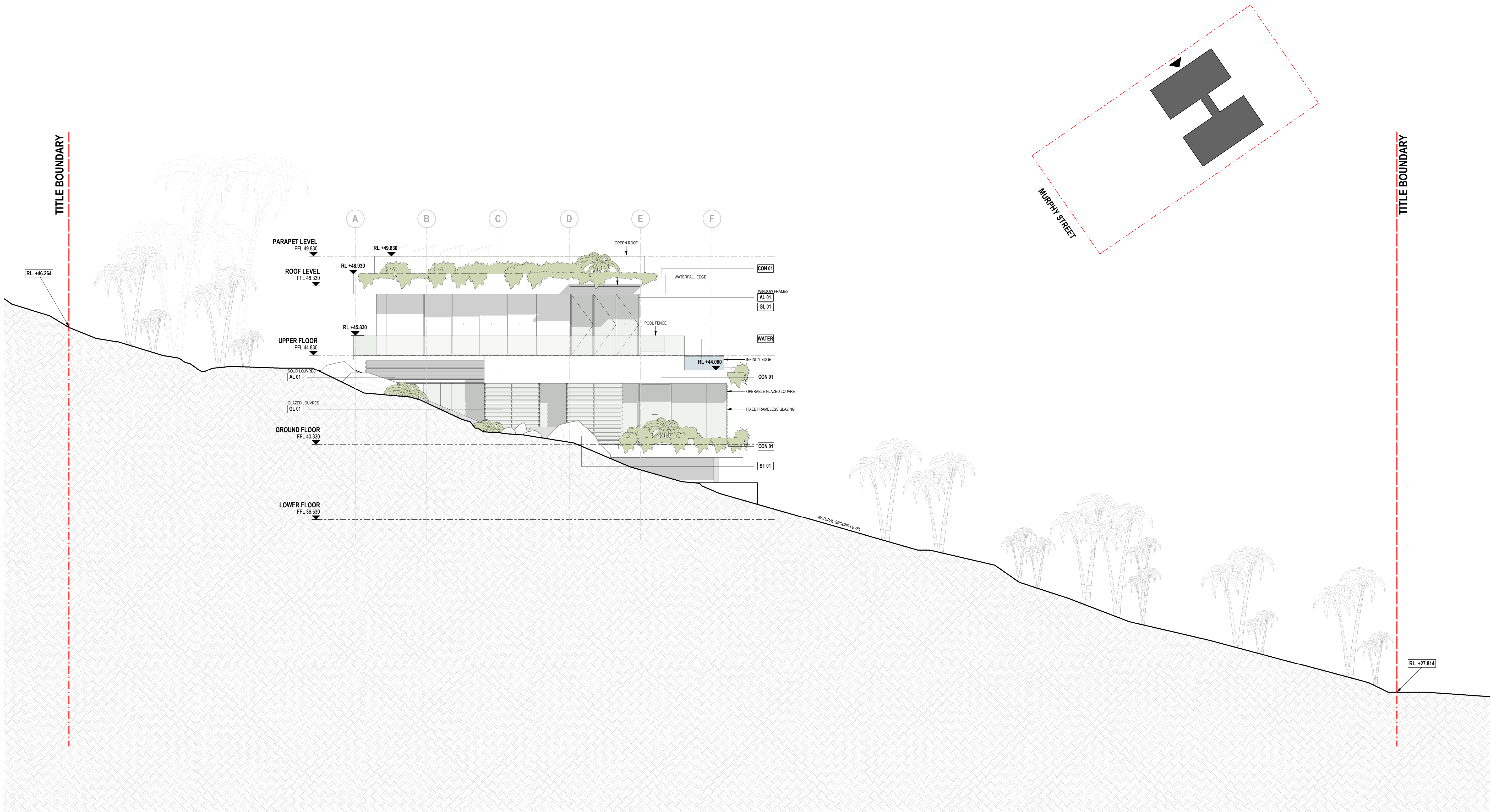
1 SOUTH WEST ELEVATION
TP104
1:100



2 NORTH EAST ELEVATION
1:100



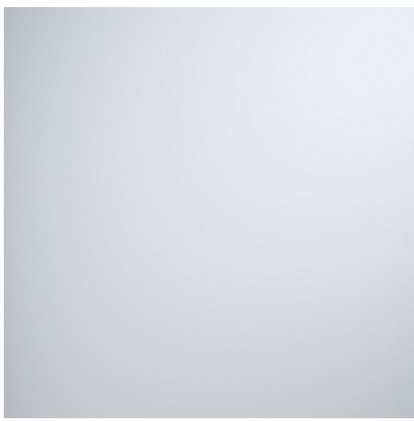
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	REV	DESCRIPTION	DATE									



ST 01
LOCAL NATURAL STONE



CON 01
CONCRETE



GL 01
CLEAR GLAZING



LANDSCAPING



WATER



AL 01
ALUMINIUM - LIGHT COLOURED

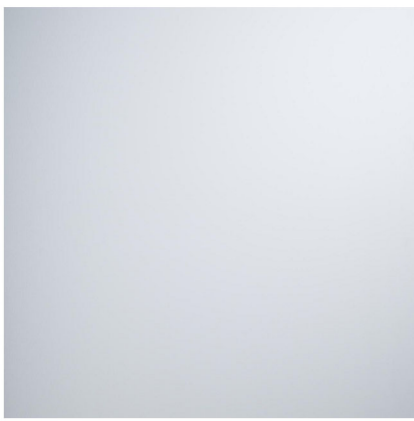
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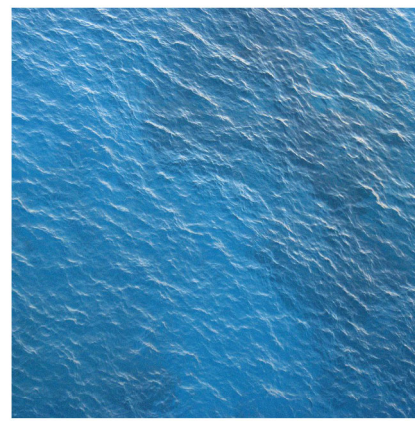
CON 01
CONCRETE



GL 01
CLEAR GLAZING



LANDSCAPING



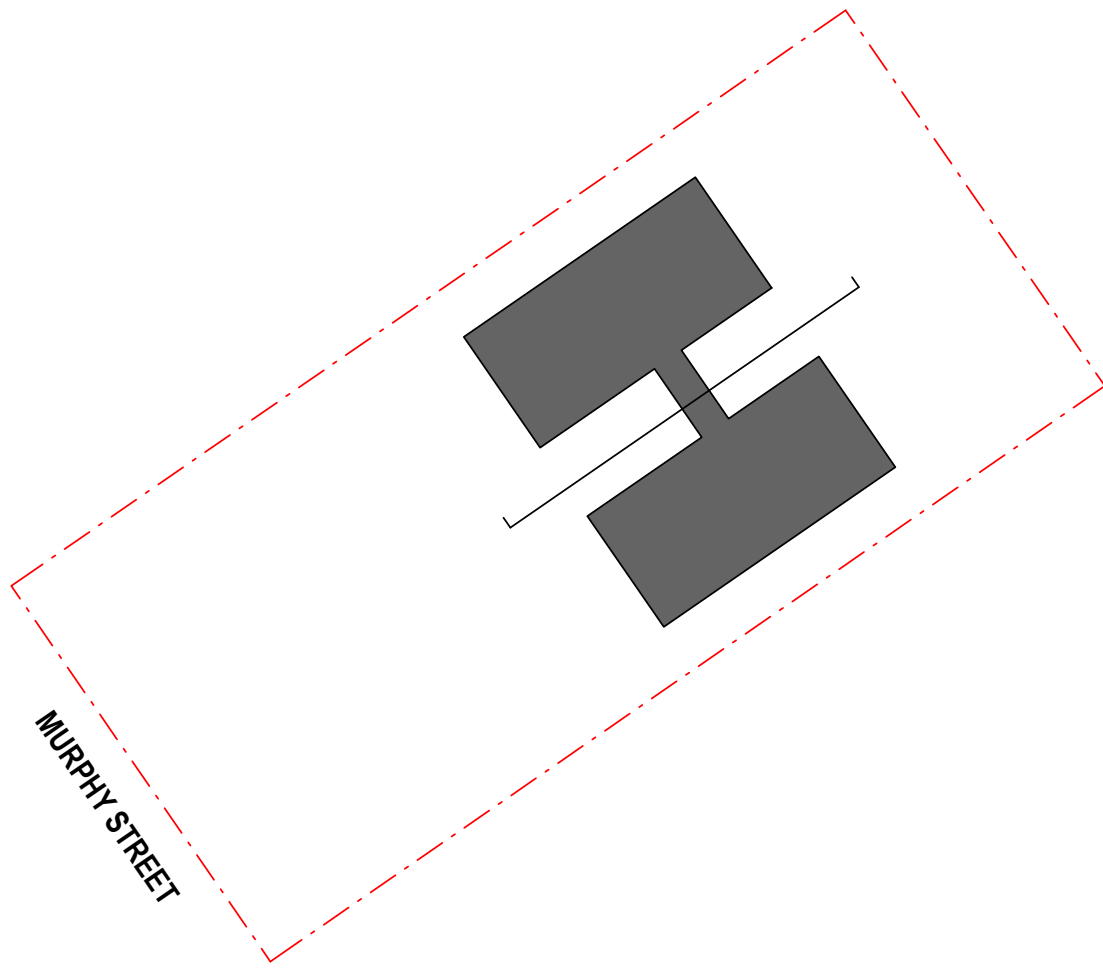
WATER



AL 01
ALUMINIUM - LIGHT COLOURED

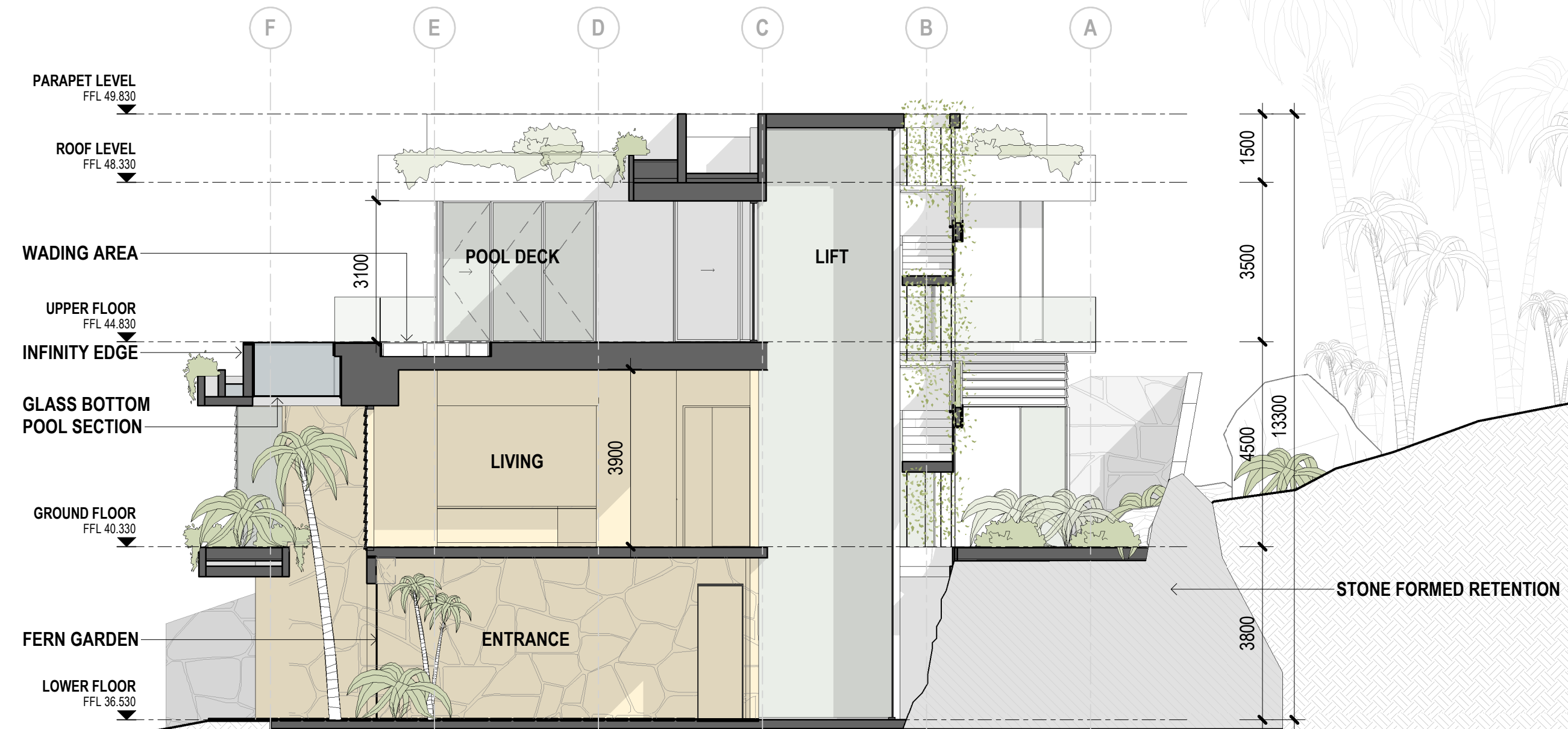
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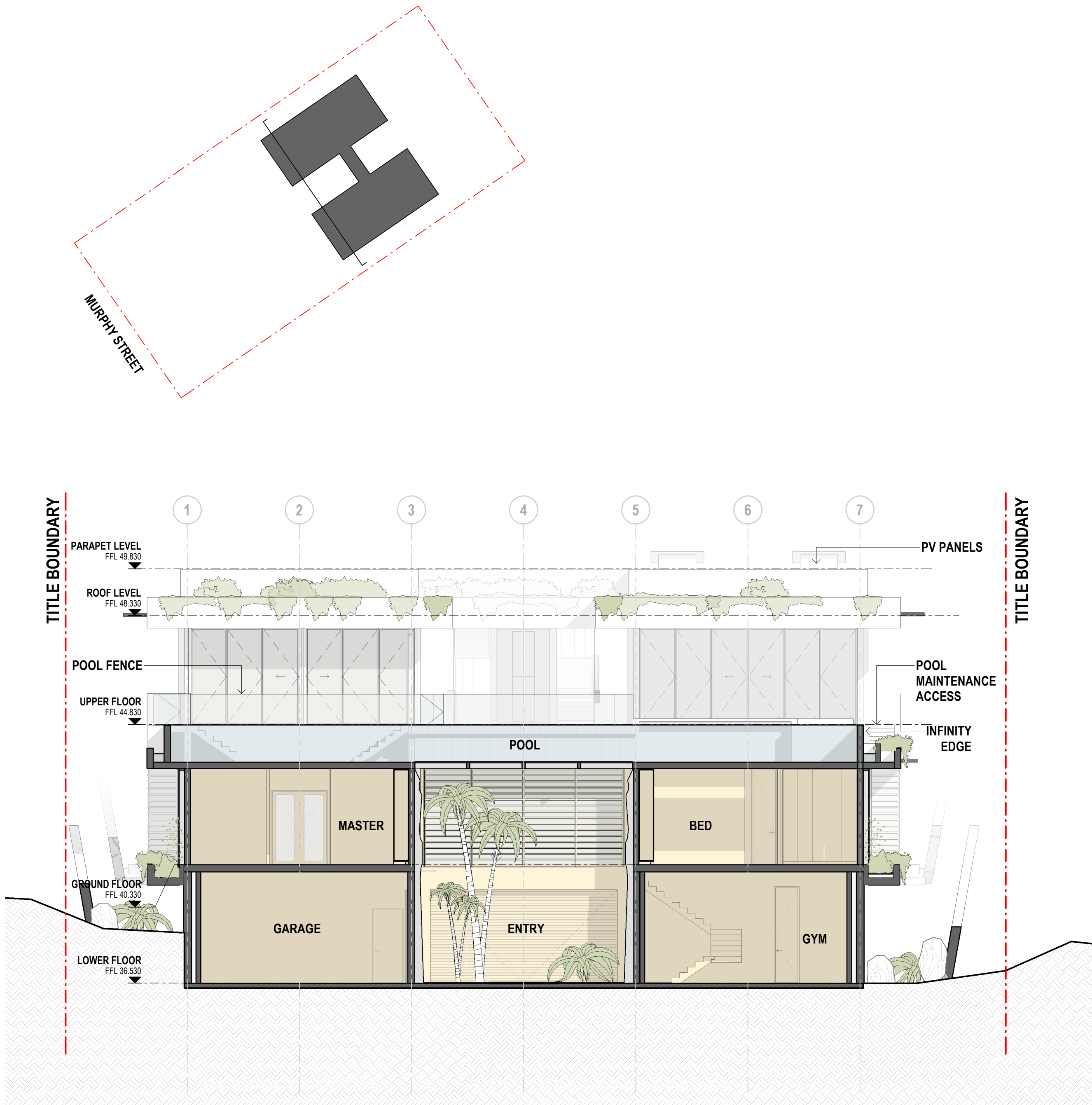


MURPHY STREET

TITLE BOUNDARY





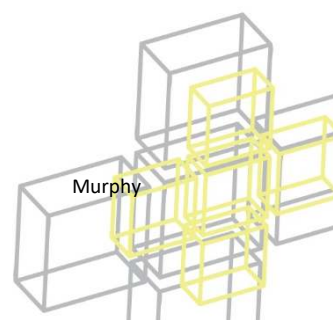


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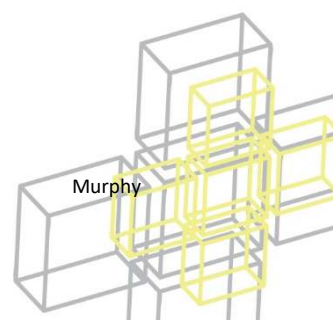
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TP101
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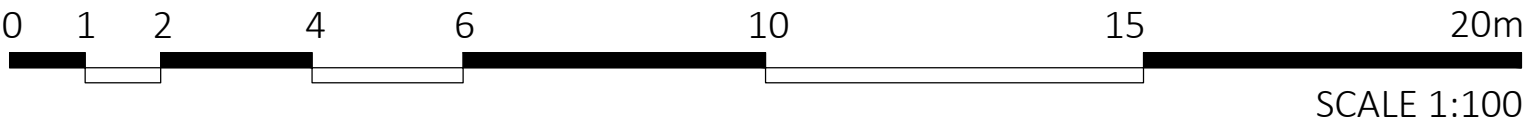
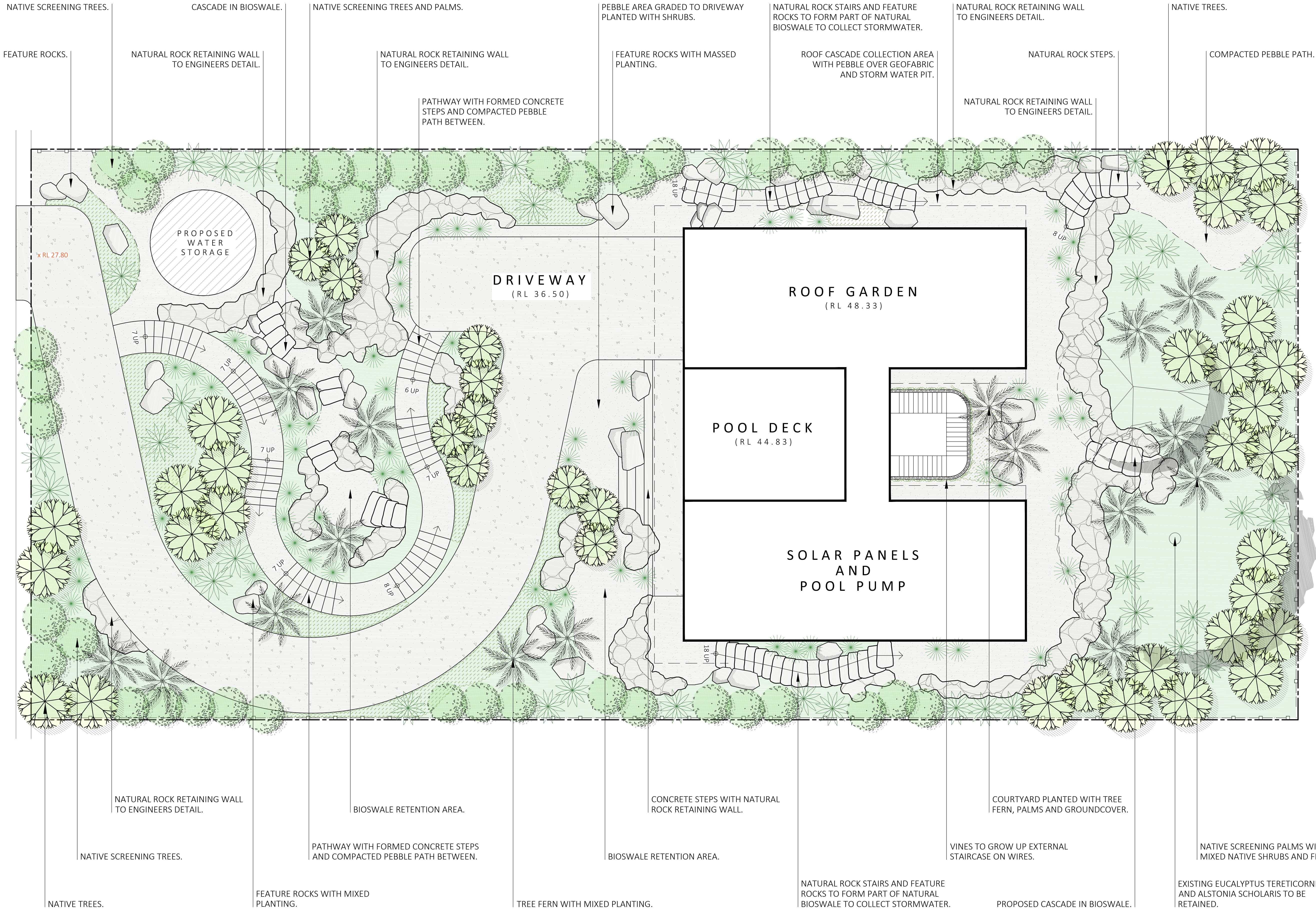
Annexure 9: 3D View





Annexure 10: Landscape Plans





ARGYROU RESIDENCE

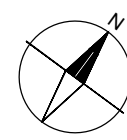
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DRAWING TITLE

LANDSCAPE CONCEPT PLAN

SCALE: 1:100@A1

DATE: 14 March 2022



DRAWING NO. 1
REV. -

HORTULUS AUSTRALIA PTY LTD

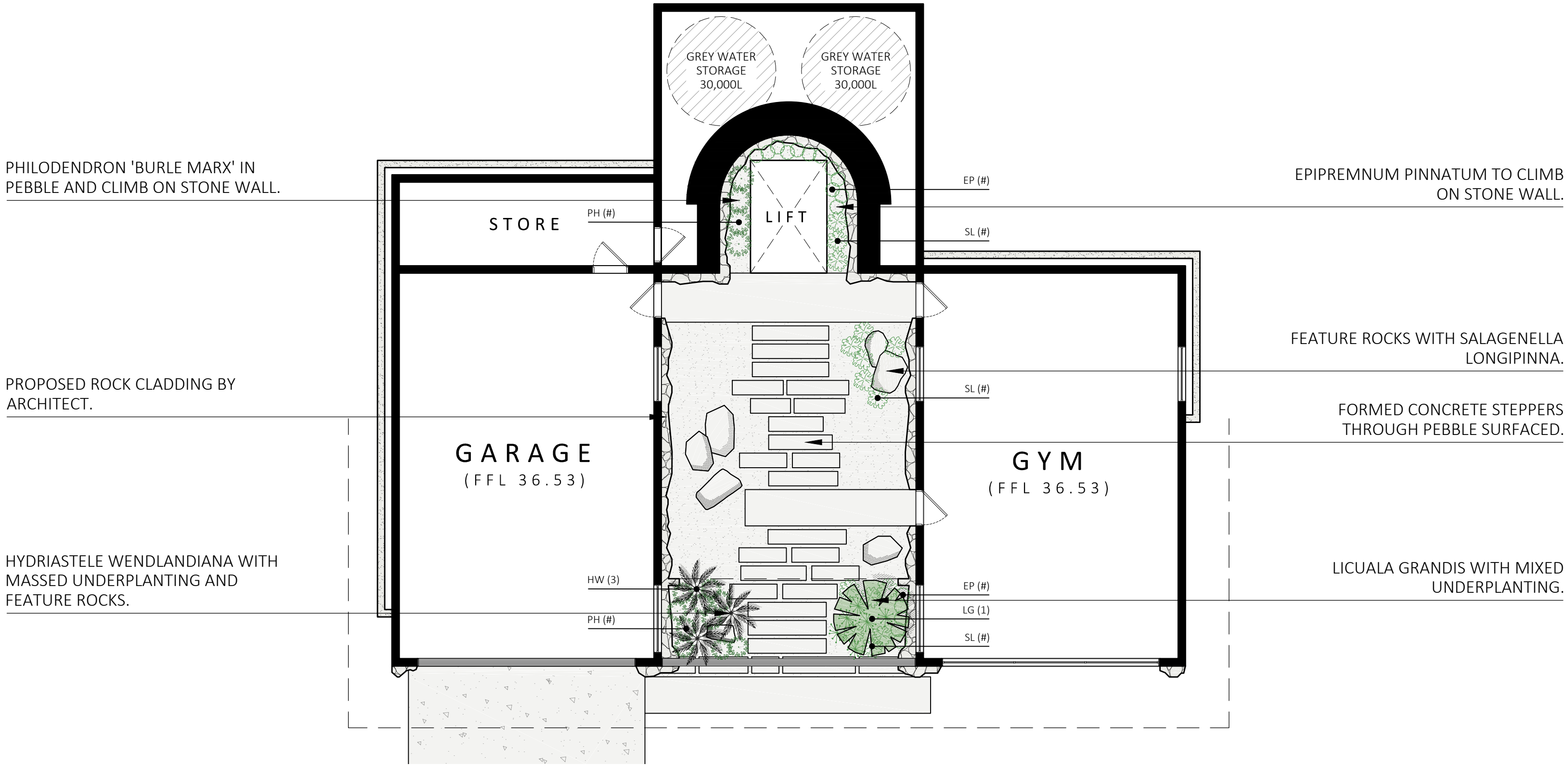
338 Port Douglas Road | Port Douglas QLD Australia 4877

T: 0412944793

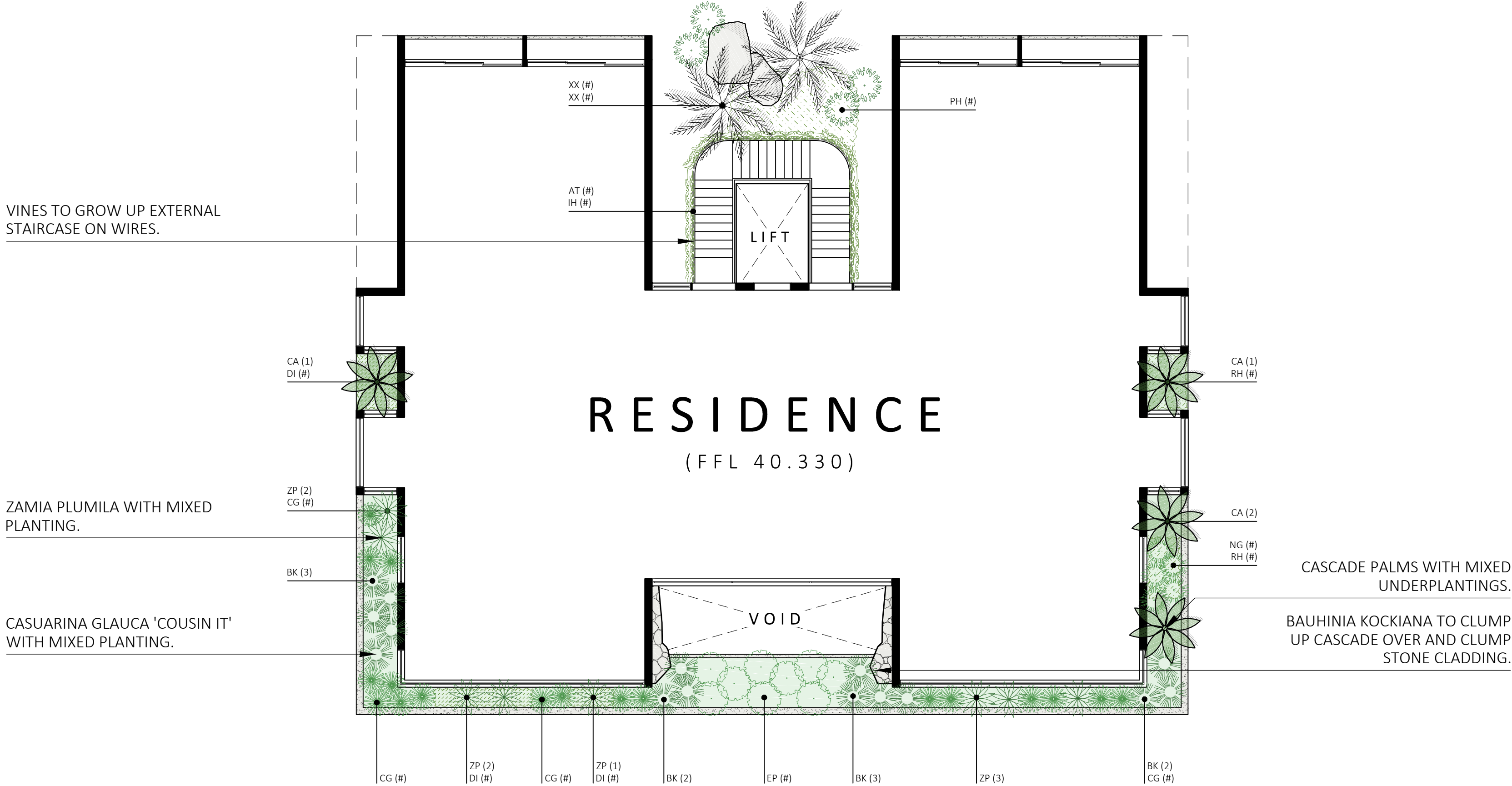
W: www.hortulus.com.au

E: design@hortulus.com.au





1 LOWER GROUND FLOOR PLAN
1:100

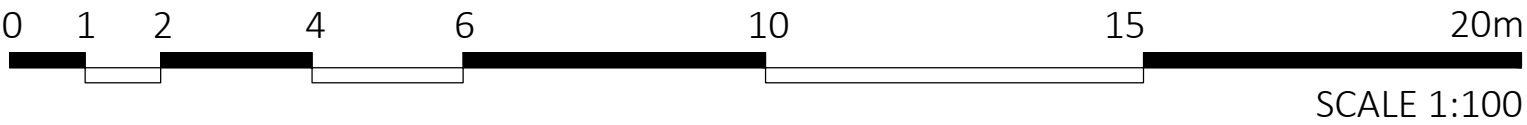
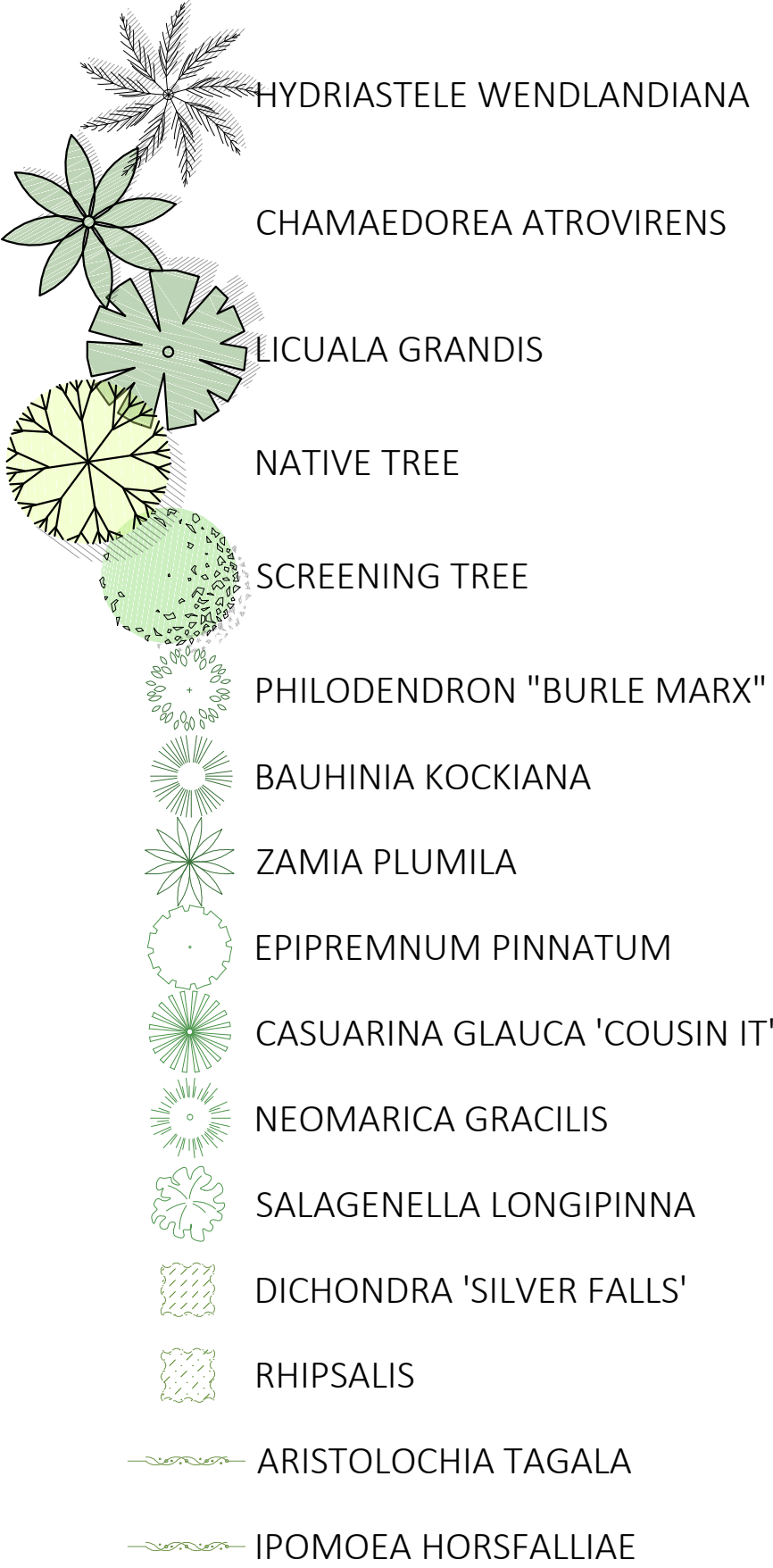


2 GROUND FLOOR PLAN
1:100

PLANT SCHEDULE

Code	Botanical Name	Common name	Qty.	Pot Size	Type	H x W
TREES & PALMS						
HW	Hydriastele wendlandiana	Elegant swamp palm	3	400mm	P	8x1.5
LG	Licuala grandis	Vanuatu fan palm	1	300mm	P	8x4
CA	Chamaedorea atrovirens	Cascade palm	4	200mm	P	1.5x1.5
SHRUBS, GRASSES, GROUNDCOVERS & CLIMBERS						
EP	Epipremnum pinnatum	Native monstera		200mm	S	5x2
PH	Philodendron 'Burle Marx'	Burle marx philodendron		140mm	S	1x0.5
SL	Salagenella longipinna	Native club fern		140mm	GC	0.1x0.2
BK	Bauhinia kockiana	Kock's bauhinia	10	200mm	C	3x3
CG	Casuarina glauca 'Cousin It'	Cousin it plant		140mm	S	0.3x1
DI	Dichondra 'Silver Falls'	Silver falls		140mm	GC	0.1x1
NG	Neomarica gracilis	Brazilian walking iris		140mm	S	0.3x0.3
RH	Rhipsalis	Mistletoe cacti				
ZP	Zamia plumila	Coontie	6	200mm	S	1x1
AT	Aristolochia tagala	Dutchman's pipe vine		140mm	C	5x5
IH	Ipomoea horsfalliae	Cardinal creeper		140mm	C	5x5
C = Climber P = Palm GC = Groundcover S = Shrub						

KEY TO SYMBOLS



ARGYROU RESIDENCE

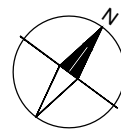
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DRAWING TITLE

LANDSCAPE CONCEPT PLAN

SCALE: 1:100@A1

DATE: 14 March 2022



DRAWING NO.

2

REV.

-

HORTULUS AUSTRALIA PTY LTD

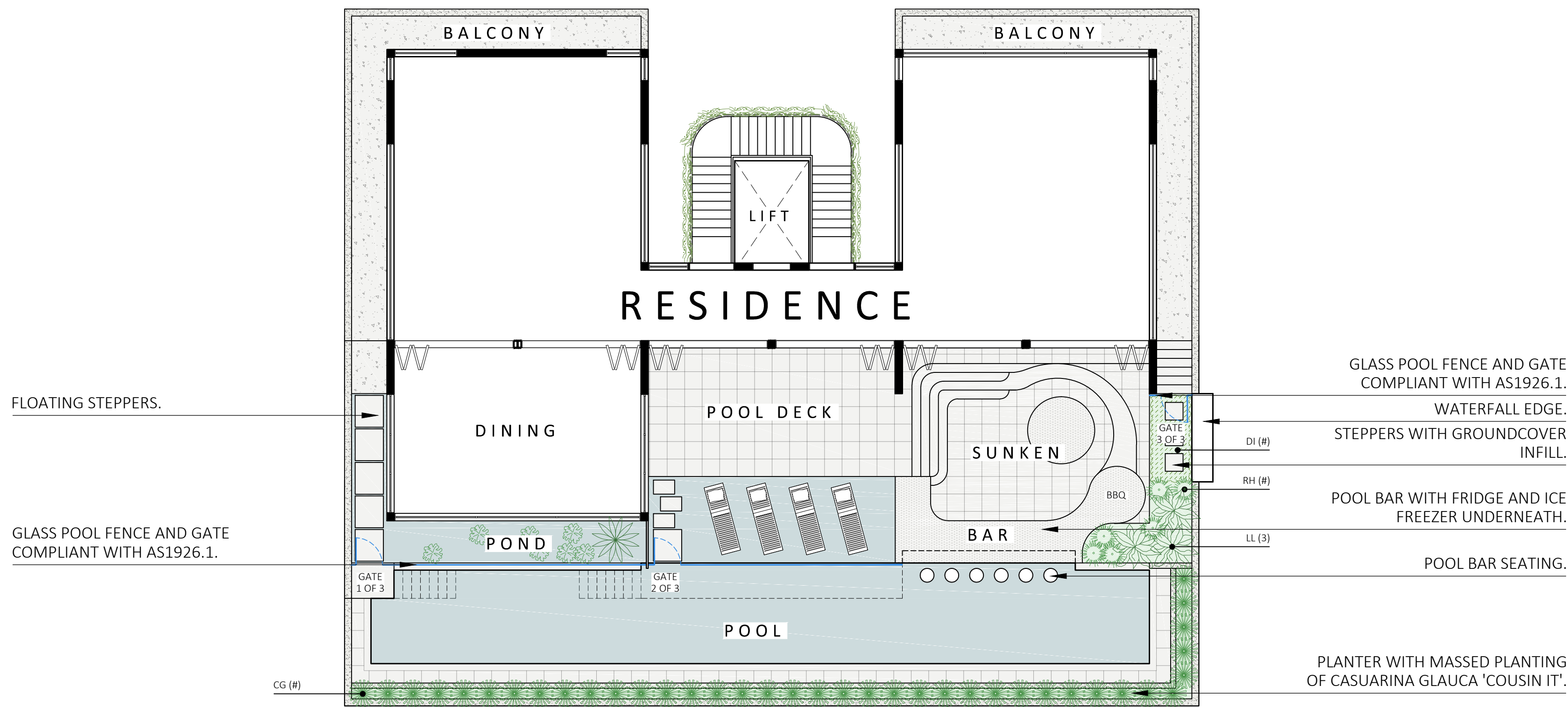
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T: 0412944793

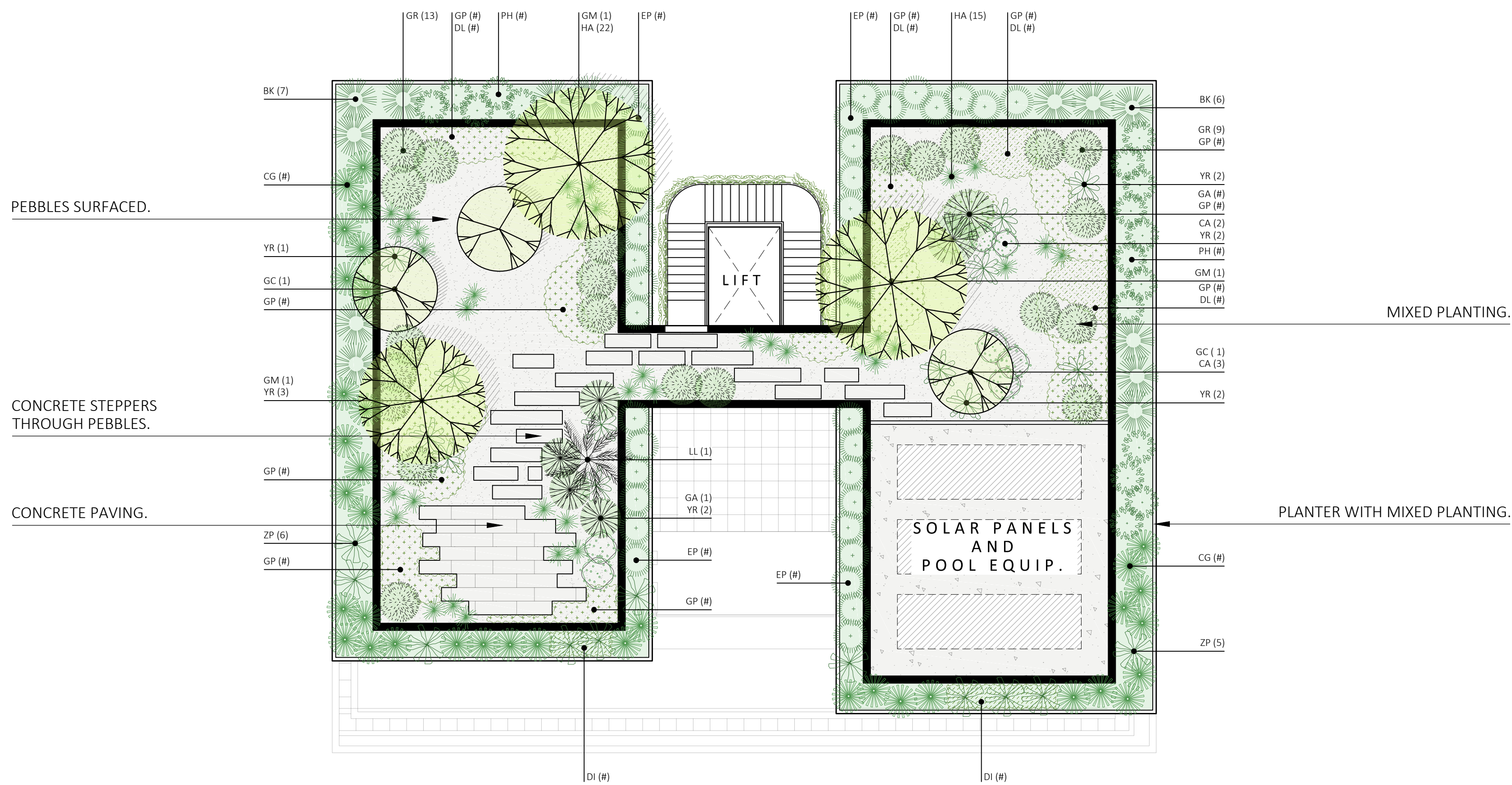
W: www.hortulus.com.au

E: design@hortulus.com.au





1 UPPER LEVEL PLAN
1:100

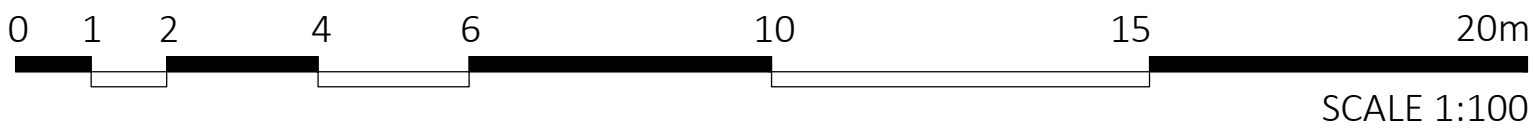


2 ROOF LEVEL PLAN
1:100

PLANT SCHEDULE

[illegible]

KEY TO SYMBOLS



LANDSCAPE SPECIFICATIONS:

1. EXCAVATION

ALL GARDEN AREAS ARE TO BE STRIPPED OF UNWANTED PLANTS, BUILDING MATERIAL AND OTHER MATTER.

- a. EXCAVATE-
ALL PAVED AREAS TO A DEPTH OF 200mm BELOW FINISHED LEVEL.
ALL LAWN AREAS TO A DEPTH OF 100mm BELOW FINISHED LEVEL.
ALL GARDEN BED AREAS TO A DEPTH OF 250mm BELOW FINISHED LEVEL.

2. SOIL PREPARATION

ALL GARDEN AREAS ARE TO BE CULTIVATED AND AERATED PRIOR TO APPLICATION OF TOP SOILS. GYPSUM SHALL BE APPLIED TO ALL HEAVY CLAY AREAS AT THE RATE OF 1.5KG PER SQ. METRE.

3. TOP SOIL

IMPORT 5 WAY ORGANIC GARDEN SOIL BLEND TO FILL ALL RETAINING WALLS AND GARDEN BEDS. ENSURE MIN. 250mm LAYER SOIL IS IMPORTED TO ALL PLANTED AREAS.

4. MULCH

A 50mm LAYER 'MULCH-MASTER' COMPOST SHALL BE SPREAD EVENLY OVER ALL GARDEN AREAS AFTER PLANTING.

5. FERTILISING

ADDITION OF A SUITABLE FERTILISER (EG. OSMOCOTE) WILL BE APPLIED TO ALL PLANTS AS PER MANUFACTURERS' SPECIFICATIONS AT TIME OF PLANTING. SLOW AND QUICK RELEASE FERTILISERS WILL BE APPLIED TO LAWN AREAS AND WELL WATERED IN AT TIME OF PLANTING.

6. PLANT MATERIAL

TO BE VIGOROUS, WELL-ESTABLISHED OF GOOD FORM AND NOT ROOT BOUND.

7. PLANTING

BEFORE PLANTING, THOROUGHLY WATER PLANTS AND PLANTING AREAS. WATER AGAIN IMMEDIATELY AFTER PLANTING.

8. DRAINAGE

THE FOLLOWING IS INTENDED AS A GUIDE ONLY:

- a. A.G. DRAINAGE
A SINGLE A.G. DRAINAGE LINE SHALL BE INSTALLED BEHIND ALL RETAINING WALLS AND PARALLEL LINES AT 3 METRE INTERVALS THROUGH LAWN AREAS. 90mm DIAM. SLOTTED PVC PIPE SHALL BE LAID AT A MINIMUM DEPTH OF 600mm WITH A 400mm LAYER OF 20mm SCREENINGS SHALL SURROUND ALL LINES. INSTALL FLUSHING POINTS AT THE END OF EACH LINE IN THE FORM OF 90mm DIAM. STORMWATER RISERS WITH END CAPS IN NEAREST GARDEN BED.
- b. DRAINAGE OF PAVED AREAS
ALL PAVED AREAS ARE TO BE GRADED AWAY FROM HOUSE. SURFACE WATER IS TO BE COLLECTED BY PITS THROUGHOUT. ENSURE EVEN FALL ON PAVING TOWARDS PITS, PITS SHALL BE CONNECTED TO STORMWATER LINE.

9. RETAINING WALLS

PLEASE NOTE - FOOTING SIZES WILL VARY DEPENDING ON HEIGHT OF WALLS:
POUR 400-500mm WIDE, 450-650mm DEEP 25MPA CONCRETE FOOTINGS WITH 2 LAYERS 12mm TRENCH MESH, Y12 STARTER BARS TO BE INSERTED AT 400 CENTRES. LAY 190 WIDE REINFORCED CEMENT BLOCK WALLS. IN ALL FOOTINGS, EXTENDING BARS THROUGH WALL CAVITIES TO TOP OF WALLS. FILL WALL CAVITIES WITH 20MPA CONCRETE. APPLY 2 COATS 'GRIPSET' BITUMEN-BASED MEMBRANE TO ALL BURIED SURFACES OF WALL, COVERING 150mm OF FOOTINGS ALSO.

10. SPRINKLER SYSTEM

A FULLY AUTOMATIC IRRIGATION SYSTEM SHALL BE INSTALLED. 20mm PVC SHALL BE USED FOR ALL LATERAL PIPING. DRIP LINE SYSTEM TO BE USED THROUGHOUT GARDEN BEDS WITH GEAR DRIVEN POP UP SPRINKLERS FOR LAWN AREAS.

PLANT SCHEDULE

Code	Botanical Name	Common name	Qty.	Pot Size	Type	H x W
TREES & PALMS						
AR	Archidendron lucyi	Scarlet bean tree		300mm	T	6x4
AF	Atractocarpus fitzalanni	Brown gardenia		300mm	T	8x4
CF	Cupaniopsis flagelliformis	Brown tuckeroo		300mm	T	6x6
DD	Darlingia darlingiana	Brown silky oak		300mm	T	10x6
DT	Deplanchea tetraphylla	Daintree gardenia		300mm	T	8x6
GA	Gardenia actinocarpa	Licorice plant		300mm	T	5x3
LR	Licuala ramsayii	Daintree fan palm		300mm	P	8x4
ME	Melicope elleryana	Ulysses tree		200mm	T	10x6
MI	Mimusops elengi	Mimusops tree		300mm	T	6x5
PE	Ptycosperma elegans	Solitaire palm		200mm	P	8x3
XC	Xanthostemon chrysanthus 'Fairhill Gold'	Dwarf golden penda		300mm	T	6x5
AA	Arenga Australasica	Native sugar palm		300mm	P	8x4
CA	Callistemon sp. 'Slim'	Bottlebrush		200mm	S	4x2
GS	Gardenia scabrella	Cape york gardenia		200mm	S	2x1
PM	Ptycosperma macartheri	Macarther palm		300mm	P	8x3
SA	Syzygium australe 'Straight & Narrow'	Straight & Narrow lilly pillly		200mm	T	4x2
SW	Syzygium wilsonii	Powder puff lilly pillly		200mm	T	8x6
SHRUBS, GRASSES, GROUNDCOVERS & CLIMBERS						
AE	Angiopteris evecta	King fern		300mm	F	4x4
AS	Asplenium australasicum	Bird's nest fern		140mm	S	1x1
BS	Bowenia spectabilis	Zamia fern		200mm	S	1x1
CM	Cordyline manners-suttoniae	Native swamp cordyline		140mm	S	3x1.5
CP	Cinnum pedunculatum	Native swamp lily		140mm	S	1x1
CC	Cyathea cooperi	Sun tree fern		300mm	F	6x4
DA	Dianella atraxis	Blue flax lily		140mm	S	0.3x0.3
GP	Gardenia psidiodes 'Glennie River'	Glennie river gardenia		140mm	GC	0.5x1
LH	Lomandra hystrix	Mat rush		140mm	S	1x1
MC	Molinieria capitulate	Weevil lily		140mm	S	1x1
XV	Xanthostemon verticillatus	Bloomfeild penda		140mm	S	2x1
AP	Alpinia purpurata	Red shell ginger		200mm	G	2x2
AZ	Alpinia zerumbet variegata	Variegated shell ginger		200mm	G	1.2x1.2
CZ	Calathea zebrina	Zebra plant		140mm	G	0.5x0.5
HC	Heliconia chartacea 'Sexy Pink'	Sexy pink crab's claw		200mm	G	3x2
IH	Ixora hookeri	Fragrant ixora		200mm	S	2x2
OJ	Ophiopogon japonicus 'Nana'	Dwarf mondo grass		140mm	GC	0.1x0.2
PH	Philodendron 'Burle Marx'	Burle marx philodendron		140mm	S	1x0.5
PG	Philodendron gloriosum	Quilted philodendron		200mm	S	1x2
TC	Tacca chantrieri	Black bat plant		200mm	S	0.7x0.7
AT	Aristolochia tagala	Dutchman's pipe vine		140mm	C	5x5
IP	Ipomoea horsfalliae	Cardinal creeper		140mm	C	5x5
C = Climber G = Ginger SE = Sedge F = Fern P = Palm T = Tree GC = Groundcover S = Shrub						

LANDSCAPE SURFACE PERMEABILITY

32% NON-PERMEABLE (498.00m²)
68% PERMEABLE (1,048.00m²)

TOTAL LANDSCAPE AREA (1,546.00m²)

ALL TREES TO BE HEALTHY, WELL-GROWN SPECIMENS FREE OF PEST & DISEASE. TREE ROOT BALL TO BE WATERED THE DAY PRIOR TO PLANTING.

SOIL BERM FORMING A WATERING SAUCER AROUND IMMEDIATE CROWN OF PLANT TO HOLD AT LEAST 5~10% OF ROOTBALL SIZE OF WATER.

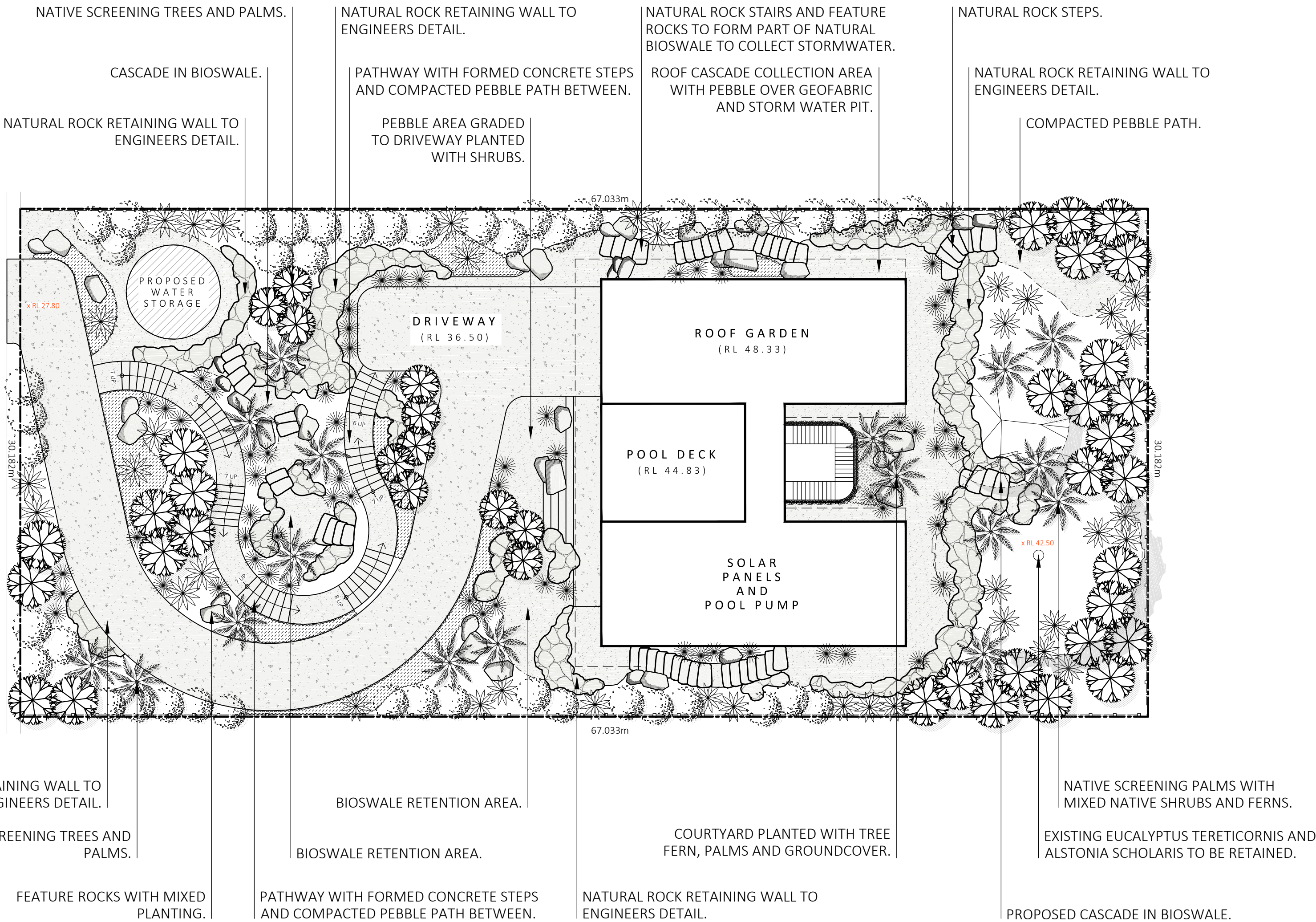
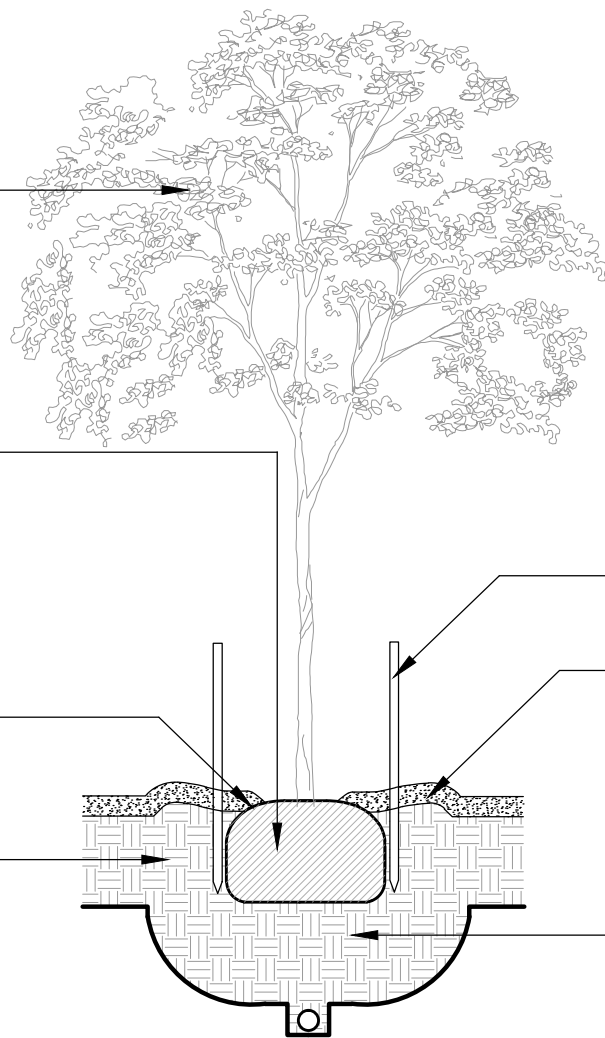
TREES TO BE PLACED TOP ENSURE TOP OF THE ROOTBALL IS AT THE SAME LEVEL AS FINISHED SOIL LEVEL. SOIL OR MULCH SHOULD NOT COVER THE PLANTS TRUNK TO AVOID COLLAR ROT.

RE-CONDITION SOIL TO 300mm DEEP WITH COMPOSTED WEED-FREE GARDEN SOIL.

NOTES:

GARDEN BED TO BE WELL DRAINED WITH DRAINAGE AS PER DRAWING. BEFORE PLANTING, THOROUGHLY WATER TREES TO BE TRANSPLANTED. AFTER PLANTING WATER IS TO BE CONCENTRATED WITH DIPPERS PLACED DIRECTLY ON TOP OF ROOT BALL. AFTER PLANTING IT IS ESSENTIAL MOISTURE LEVELS OF ROOTBALL ARE REGULARLY CHECKED TO ENSURE ROOTBALL DOES NOT DRY OUT.

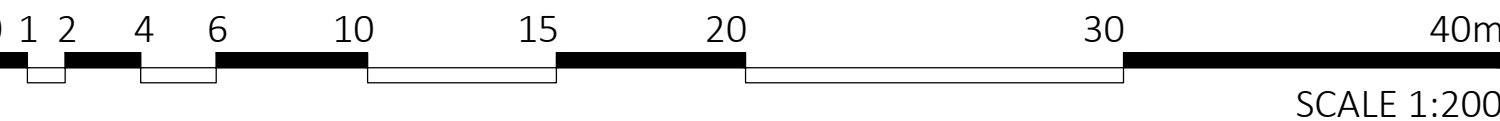
1 TREE PLANTING DETAIL
SECTION NTS



NOTES:

- ERADICATE WEEDS BY SPRAYING WITH GLYPHOSATE OR REMOVING BY HAND.
- LOOSELY BREAK UP SOIL WITH A FORK AND COMBINE WITH A 75mm DEPTH OF ORGANIC MATTER.
- DIG A HOLE TWICE THE WIDTH AND ONE AND A HALF THE DEPTH OF THE POT SIZE.
- FILL THE HOLE WITH WATER BEFORE PLANTING AND ALLOW TO DRAIN THOROUGHLY.
- PLACE THE PLANT IN THE HOLE & BACKFILL WITH FRESH TOPSOIL.
- SPREAD WITH A 40MM LAYER OF FINE COMPOSTED MULCH AND WATER DEEPLY AFTER PLANTING.
- IF PLANTING IS TALLER THAN 1M, SUPPORT PLANT WITH 750mm LONG X 25 X 25 HARDWOOD STAKE TIED WITH JUTE WEBBING TIES.
- GARDEN BEDS TO BE WELL DRAINED WITH GEO-FABRIC SOCKED AGRICULTURAL DRAINS WITH SCREENINGS SURROUNDING CONNECTED TO SILT PIT PRIOR TO STORMWATER SYSTEM.

2 SHRUB PLANTING DETAIL
SECTION NTS



ARGYROU RESIDENCE

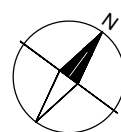
SITE ADDRESS: 14 Murphy Street | Port Douglas

DRAWING TITLE

LANDSCAPE PLAN FOR TOWN PLANNING APPROVAL

SCALE: 1:200@A1

DATE: 14 March 2022



DRAWING NO.

1

REV.

-

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338 Port Douglas Road | Port Douglas QLD Australia 4877

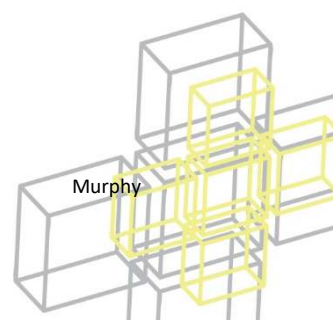
T: 0412944793

W: www.hortulus.com.au

E: design@hortulus.com.au



Annexure 11: Driveway Crossover Plans



MURPHY STREET

CONNECTION TO
MURPHY STREET

2
RP731078

30.72

325°24'05"

30.182

55°24'05"

67.033

BOUNDARY

C
040102

CARDEN
TERRACE

B
040101

E2
030101

23,890

+36,900

DRIVEWAY

+28,509

PLANTER

VISITORS
PARKING

STORAGE

PLANTER

+28,392

113
PTD2094

235°24'05"

67.033

BOUNDARY

ADJOINING
DRIVEWAY
ACCESS

1:100

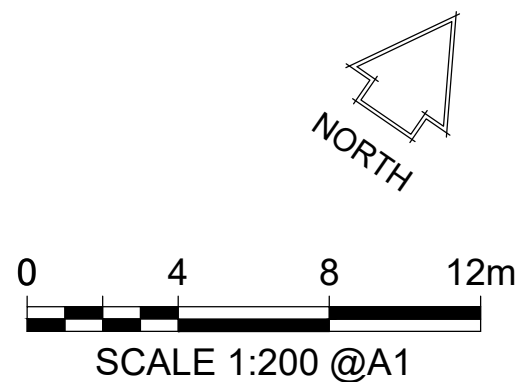
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PTD2094

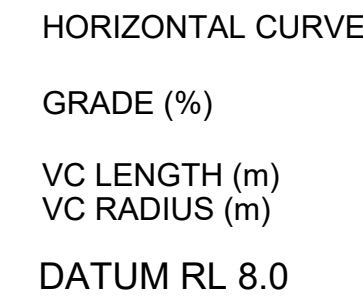
5
RP747683

1:8



THE CONTRACTOR SHALL CONFIRM THE LOCATION OF ALL EXISTING SERVICES ON AND EXTERIOR TO THE SITE INCLUDING WATER MAINS, SEWER MAINS, GAS MAINS, TELECOMMUNICATIONS CABLES, ELECTRICAL CABLES, AND STORMWATER PIPES. ANY DAMAGE TO EXISTING SERVICES SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE.

[illegible]



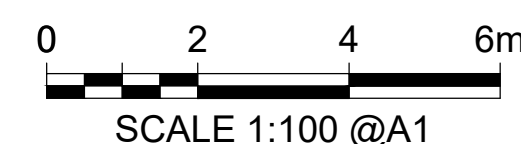
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1 : 250 Vert.



EDGE CE
RETENTION INTENT
SKETCHES
03/03/22
MW
REV1.0

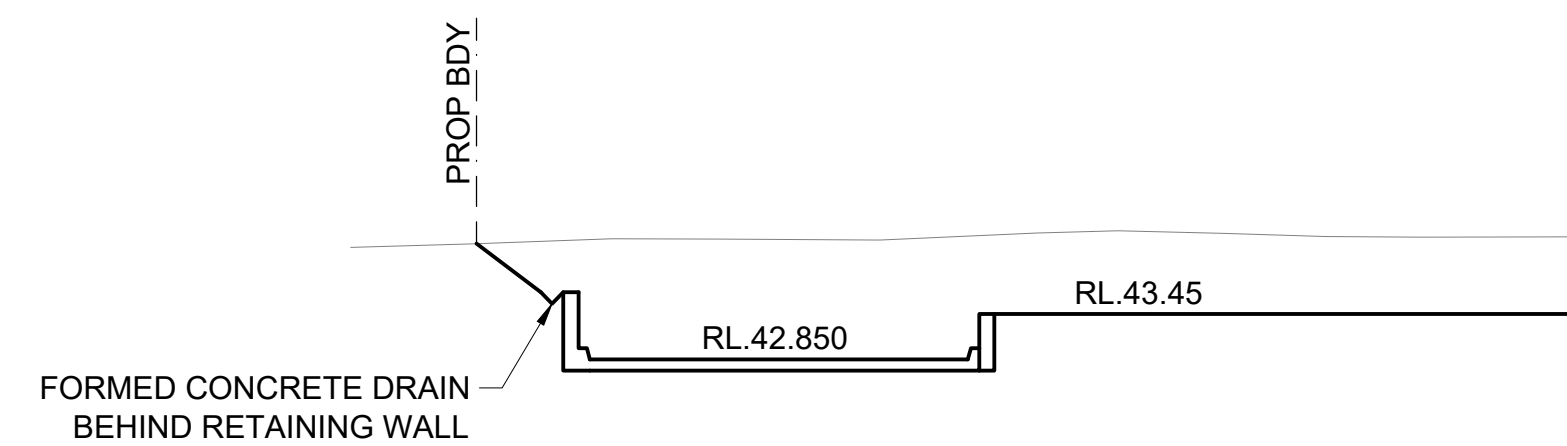
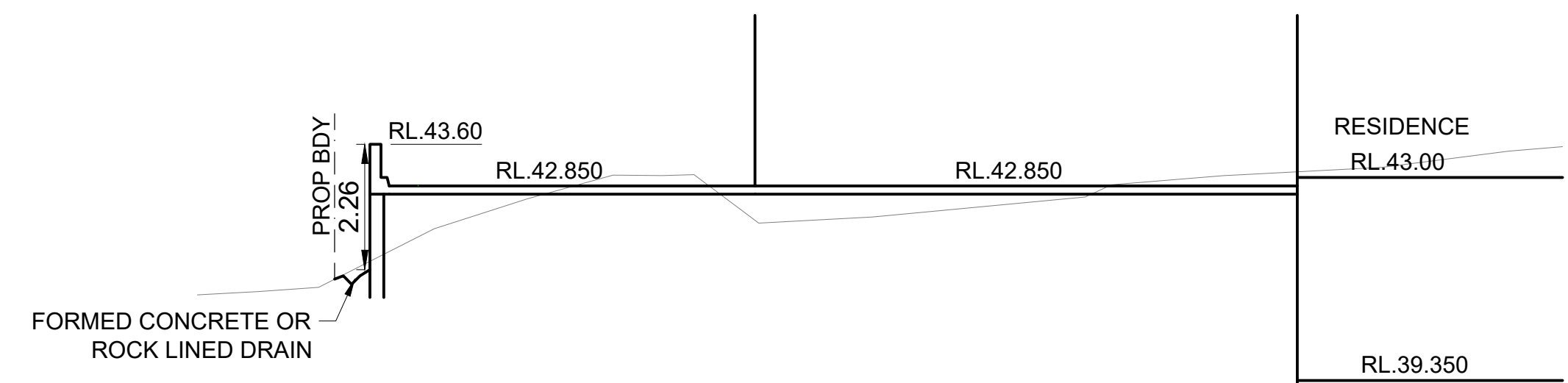
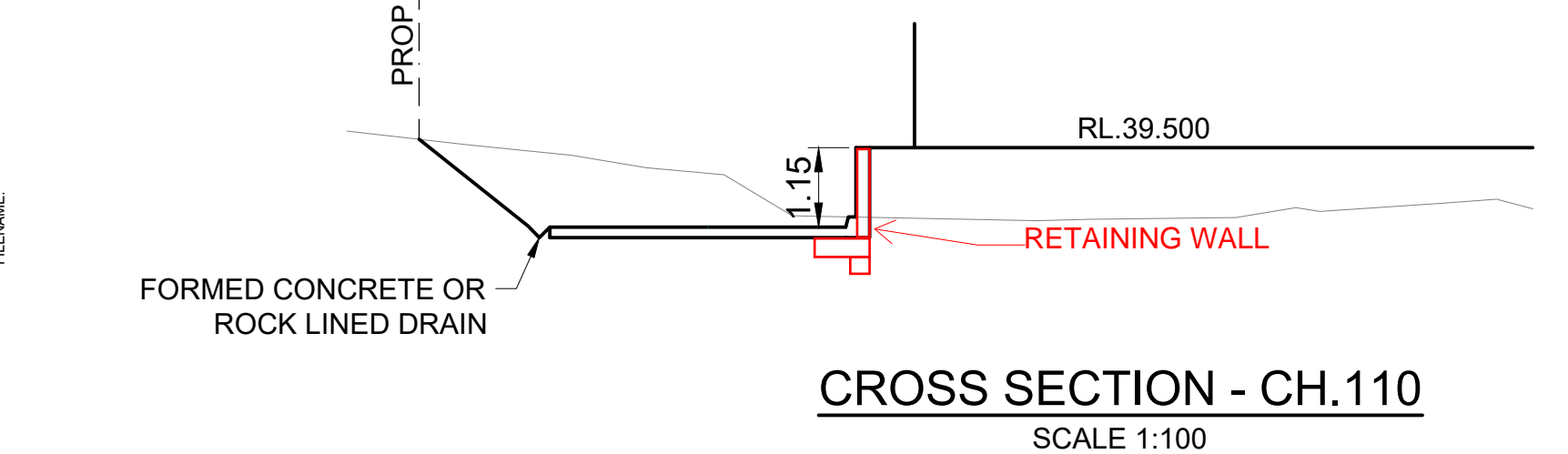
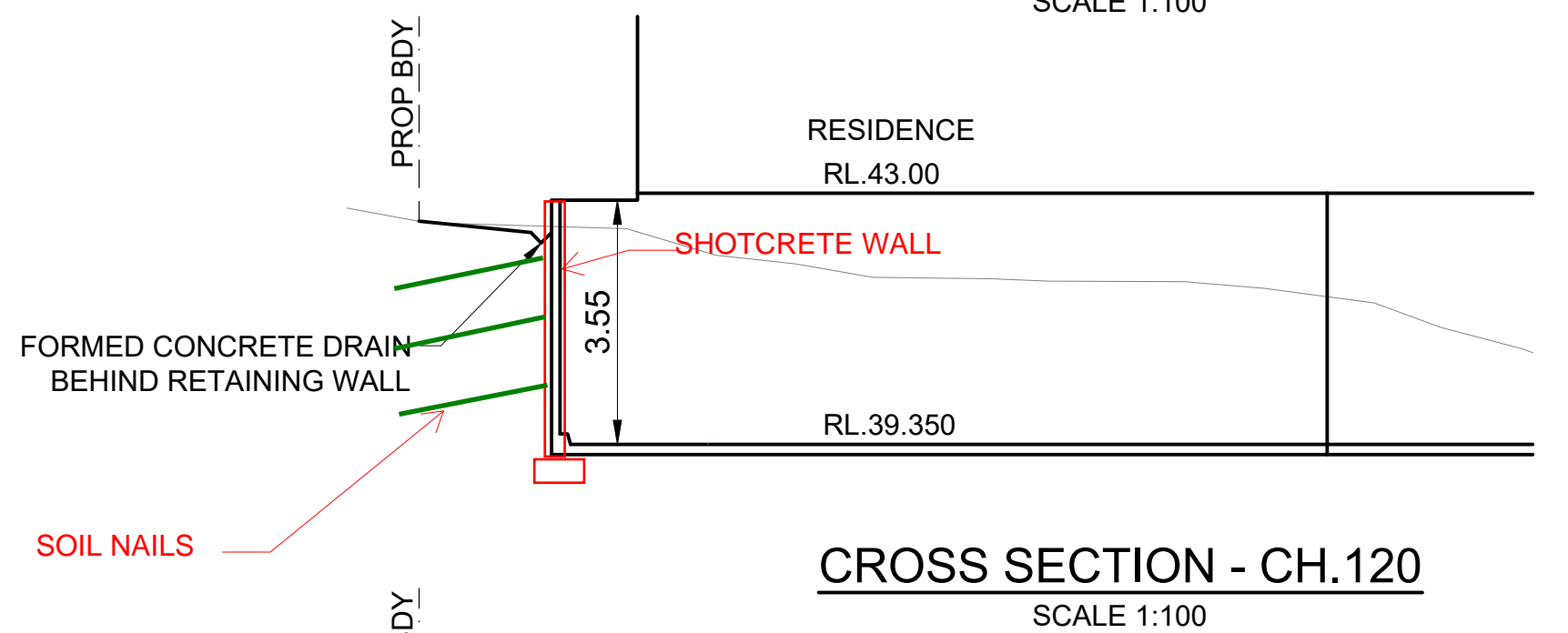
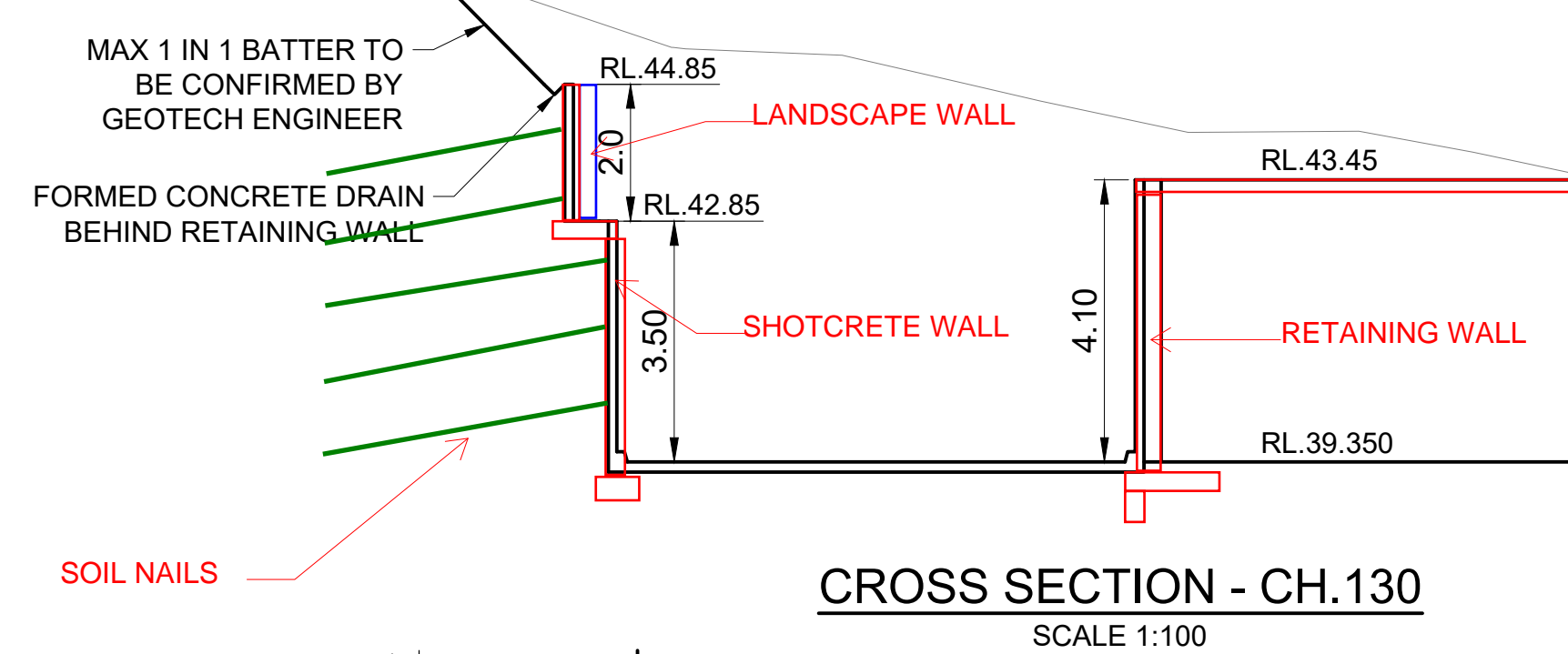
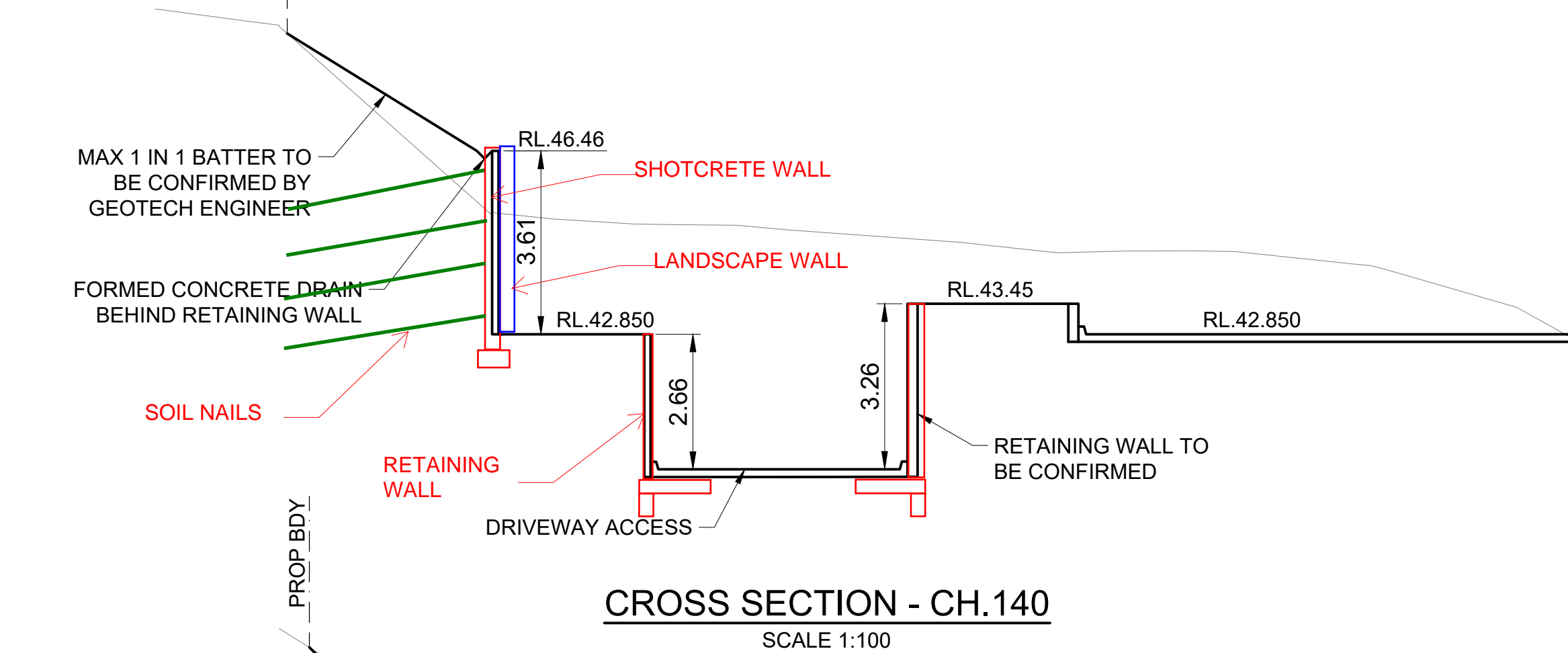
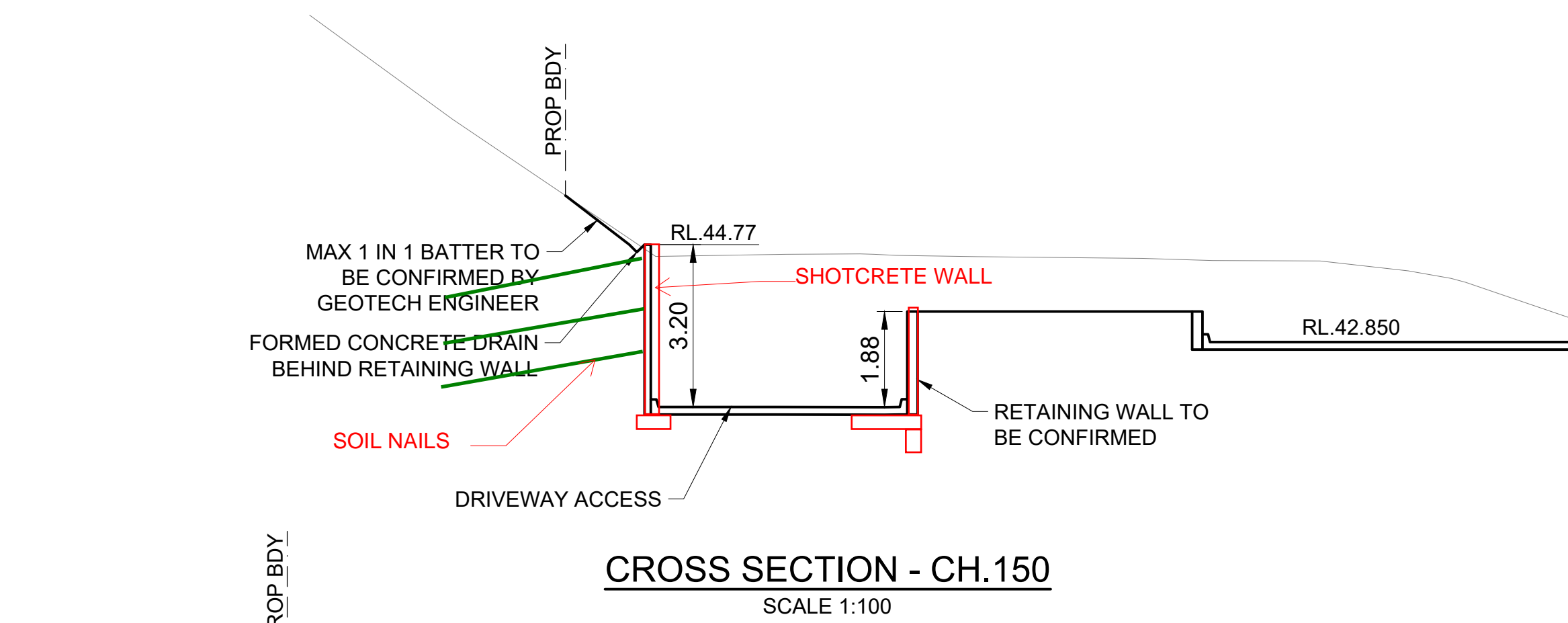


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SIGN:

MINIARY



EDGE CE
RETENTION INTENT
SKETCHES
03/03/22
MW
REV1.0

REVISIONS

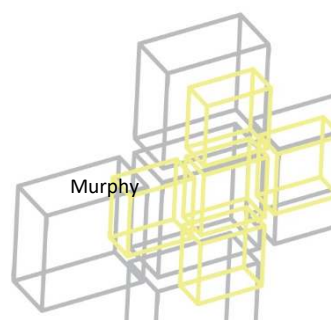
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0 2 4 6m
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RPEQ NAME:		EDGE CONSULTING ENGINEERS www.edgece.com	Brisbane, Australia 131 Floor, 26 Balaclava Street, Woolloongabba, Queensland, 4102, Australia T: +61 7 3392 3671 E: brisbane@edgece.com	Project Name MURPHY STREET RESIDENCE LOT113 PTD2094 MURPHY STREET PORT DOUGLAS Client KIM CULLEN & NEIL BIDDLE	Drawing Title PRELIMINARY DRIVEWAY CROSS SECTIONS SHEET 2	Project No. 220150	Drawing No. CSK004	Revision P2
RPEQ No:								
DATE:								
SIGN:								
Rev	Date	Description	By	Chk	Designed	Drawn	Checked	Scale @ A1
P2	02.03.22	ISSUED FOR INFORMATION	MS	MW				
P1	22.02.22	ISSUED FOR INFORMATION	MS	MW				
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Annexure 12:

Douglas Shire Planning Scheme Code Assessment



Port Douglas/Craigie local plan code

Application

- (1) This code applies to assessing development within the Port Douglas/Craigie local plan area as identified on the Port Douglas/Craigie local plan maps contained in Schedule 2.
- (2) When using this code, reference should be made to Part 5.

Context and setting

Editor's note - This section is extrinsic material under section 15 of the *Statutory Instruments Act 1992* and is intended to assist in the interpretation of the Port Douglas/Craigie local plan code.

The Port Douglas/Craigie local plan encompasses the traditional Port Douglas town centre and surrounding tourist and residential areas, including Four Mile Beach and Craigie.

Port Douglas was officially named in 1877. It was initially settled as the port of entry and supply for the Hodgkinson goldfield on the Hann Tableland which was proclaimed in 1876. It was the dominant port in Far North Queensland until a decision was made to establish Cairns as the terminus for a new railway in 1884. This ended the town's dominance, and it gradually became a small centre for local residents and fishing activities. During the 1970s and 1980s, a renewed interest in Far North Queensland as a holiday destination led to a boom in large scale tourism and residential development with Port Douglas re-emerging as a premium destination.

The Captain Cook Highway runs north-south to the west of Port Douglas through Craigie (Four Mile). Craigie caters for the permanent resident population associated with Port Douglas, as well as providing for service industries to support business in the town. The majority of urban development is confined to the eastern side of the highway. The main entrance to Port Douglas at the intersection of Port Douglas Road is accentuated by mature oil palms lining both sides of the street for almost the entire length of the corridor into the heart of Port Douglas.

Flagstaff Hill is a prominent headland on the northern side of the Port Douglas town centre providing a green tropical backdrop to the town. Island Point Road runs to the top of Flagstaff Hill and provides access to the iconic lookout overlooking the sweep of Four Mile Beach.

Macrossan Street is the main shopping area in Port Douglas running in a general east-west direction at the base of Flagstaff Hill connecting Four Mile Beach to Dickson Inlet. Tourist and commercial development is concentrated towards the western side of Macrossan Street, with marine orientated activity focussed around the inlet. The western side of the inlet provides unspoiled views across mangroves to the distinctive formations and features of the coastal range.

The street pattern in the town centre is based on the original grid pattern survey of 1878. While the town has lost many of its original buildings to cyclones and redevelopment, a number of important built features remain including the Central Hotel, the Court House Hotel, a number of relocated buildings such as St Mary's Church, the former Clink Theatre and the Court House Museum and scattered memorials such as the Carstens memorial in Macrossan Street

and the Port Douglas War memorial in Wharf Street. The Sugar Wharf on Dickson Inlet was the original terminus of the tramline to Mossman. The tramline now terminates adjacent to the Port Douglas marina and operates as the Balley Hooley passenger service on four kilometres of track between the Port Douglas Marina and St Crispins Station.

A particular characteristic of the local plan area is its high quality, lush landscaping complementing the tropical resort town atmosphere. This theme will be carried throughout the local plan area with gateways, nodes and corridor planting emphasising the role of the town as a tropical tourist destination.

Purpose

- (1) The purpose of the Port Douglas/Craigie local plan code is to facilitate development outcomes consistent with community values, the local tropical built-form and protection of the natural environment within the Port Douglas/Craigie local plan area, while providing a platform for investment and prosperity.
 - (a) In addition, the purpose of the code is supported by the Port Douglas Waterfront Master Plan which provides a clear strategic direction for the incremental transformation of the Port Douglas Waterfront, including the following objectives:
 - (b) To set out a vision for revitalisation of the waterfront;
 - (c) To protect and enhance the environmental attributes; and
- (2) To provide a flexible framework, expressed through several key strategies that will assist the Council and community in managing change.
- (3) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Port Douglas will continue to develop as the premium destination for international and domestic tourists in the Far North Queensland Region, while also acting for permanent residents attracted to the associated lifestyle.
 - (b) Major tourist, retail, dining and entertainment facilities will consolidate in the Town Centre and the Waterfront North sub-precincts, with improved pedestrian connections between the town centre and the waterfront.
 - (c) Craigie will develop as an integrated residential community with some low scale tourism development opportunities in appropriate locations. Craigie will also function as small scale commercial and light industry node, providing employment opportunities for the Shire's permanent resident population.
 - (d) All forms of development will complement the tropical image of the town through distinctive tropical vernacular, urban design and landscaping.
 - (e) Character will be enhanced through the identification of gateway sites, landmarks, main approach routes and pedestrian thoroughfares and view corridors;
 - (f) The Flagstaff Hill, Dickson Inlet, Four Mile Beach and other areas of scenic and environmental significance will be protected from development. Vegetation cover will dominate over built form.
 - (g) Vegetation, iconic to the character of Port Douglas, including the avenues of Oil Palms, is retained and where appropriate supplemented.

- (h) Development will be indistinguishable from view from Four Mile Beach. In addition, any development on Flagstaff Hill will be indistinguishable when viewed from vantage points in Port Douglas.
 - (i) Residential areas are designed as pleasant, functional and distinctive, in visually well-defined areas.
- (4) The purpose of the code will be further achieved through the following overall outcomes:
- (a) Precinct 1 – Port Douglas precinct
 - (i) Sub-precinct 1a – Town Centre sub-precinct
 - (ii) Sub-precinct 1b – Waterfront North sub-precinct
 - (iii) Sub-precinct 1c – Waterfront South sub-precinct
 - (iv) Sub-precinct 1d – Limited Development sub-precinct
 - (v) Sub-precinct 1e – Community and recreation sub-precinct
 - (vi) Sub-precinct 1f – Flagstaff Hill sub-precinct
 - (b) Precinct 2 – Integrated Resort precinct
 - (c) Precinct 3 – Craiglie Commercial and Light Industry precinct
 - (d) Precinct 4 – Old Port Road / Mitre Street precinct
 - (e) Precinct 5 – Very Low Density Residential/ Low Scale Recreation/Low Scale Educational/Low Scale Entertainment Uses precinct

Precinct 1 – Port Douglas precinct

- (5) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) development will contribute to the incremental transformation of the township, preserving and enhancing maritime activities and environmental areas, delivering tropical open spaces and a high quality public realm, and allowing for tourism opportunities and investment.
 - (b) development contributes to the enhancement of the Port Douglas precinct through the following development outcomes:
 - (i) access and connectivity throughout the township is enhanced through a series of improvements to circulation and mobility, including:
 - (A) access to, and connectivity along, the waterfront and foreshore areas is maintained and, where appropriate, enhanced;
 - (B) reducing reliance on the waterfront as a car parking resource.
 - (ii) the use of land in the Port Douglas precinct improves the cohesive layout of the township through:
 - (A) the establishment of distinct sub-precincts that reinforce the character and built form of the Port Douglas local plan area including:
 - Port Douglas centre sub-precinct 1a – Town Centre sub-precinct;
 - Port Douglas centre sub-precinct 1b – Waterfront North sub-precinct;
 - Port Douglas centre sub-precinct 1c – Waterfront South sub-precinct;
 - Port Douglas centre sub-precinct 1d – Limited development sub-precinct;
 - Port Douglas centre sub-precinct 1e – Community and recreation precinct;

- Port Douglas centre sub-precinct 1f – Flagstaff Hill sub-precinct;
- (B) facilitating marina facilities and supporting marine industry uses as a key part of the local economy;
- (C) reducing conflict between industry, community and commercial activities in the waterfront, without diminishing the marine industry capacity in the Port Douglas precinct;
- (i) environment and sustainability is integrated into the township through:
 - (A) preservation and enhancement of the qualities and characteristics of environmental areas of the township;
 - (B) water sensitive urban design is considered as a means of water quality improvement and management of overland flow to ensure hard infrastructure solutions in Warner Street can be mitigated;
 - (C) design of buildings and access way improvements prioritises walking and cycling modes of transport.
- (ii) the tropical character of the Port Douglas precinct is enhanced by ensuring development:
 - (A) maintains and enhances the built form, local character, streetscapes and natural elements of the township;
 - (B) is compatible with the desired character and amenity of local places and neighbourhoods;
 - (C) does not exceed the height of buildings designations which contribute to the desired form of the township which contains three storey development heights in sub-precinct 1a – Town Centre sub-precinct and part of sub-precinct 1b – Waterfront North sub-precinct;
 - (D) implements high quality landscaped environments around buildings and on streets;
 - (E) protects the recognisable character and locally significance sites throughout the precinct.
- (iii) public spaces and the streetscape are enhanced through:
 - (A) an increase in the quantity and quality of public land and places throughout the precinct;
 - (B) consolidating community recreation and sporting uses to create a precinct of community focussed activity between Mudlo Street and Wharf Street;
 - (C) improved connections between the town centre and the waterfront marina, including an investigation of a plaza on the waterfront;
 - (D) improved streetscapes with high quality landscaping, surface treatments and shaded pedestrian environments;
 - (E) the creation of a sense of place through aesthetic streetscapes and built-form character;
 - (F) managing vegetation to ensure succession of planting and the ongoing presence of significant trees.
- (iv) advertising signage is small scale, low-key and complements the tropical character of the town.

Sub-precinct 1a – Town Centre sub-precinct

- (6) In addition to other overall development outcomes, development in the Town Centre sub-precinct facilitates the following development outcomes:
 - (a) tourist, retail, dining and entertainment activities are facilitated at an appropriate pedestrian scale;
 - (b) drive-through developments, bulky goods showrooms, outdoor sales, saleyards and other big-box retailing or entertainment facilities are not established;
 - (c) development contributes to a high quality public realm;
 - (d) parking (and associated infrastructure) does not undermine the relationship between buildings and street or pedestrian circulation patterns;

- (e) consolidation of community and cultural land use activities along Mowbray Street between Wharf Street and Mudlo Street;
- (f) active street frontages are established along Macrossan and Wharf Streets and other nearby streets as shown on the Port Douglas Centre Active Frontages and Pedestrian and Cycle Network Plan;
- (g) Live entertainment activities are concentrated within the Live Entertainment Precinct and are subject to the recommendations of a suitably qualified acoustic engineer.

Sub-precinct 1b - Waterfront North sub-precinct

- (7) In addition to other overall development outcomes, development in the Waterfront North sub-precinct facilitates the following development outcomes:
 - (a) the precinct evolves as a revitalised open space and waterside development precinct;
 - (b) development within the precinct is designed to be sympathetic to the environmentally sensitive Dickson Inlet and mitigates any adverse impacts;
 - (c) the establishment of mixed-use development is facilitated to promote activity and vitality;
 - (d) public pedestrian access is maximised along the extent of the edge of the waterfront, consisting of a boardwalk or similar structure available for 24-hour use;
 - (e) development contributes to a high quality public realm;
 - (f) built form provides an attractive point of arrival from both land and sea;
 - (g) pedestrian connectivity is safe, efficient and provides for the needs of all users of the Port Douglas waterfront;
 - (h) parking (and associated infrastructure) does not undermine the relationship between buildings and street or pedestrian circulation patterns;
 - (i) the importance of existing marine-based industries to the area is recognised, not diminished and protected from incompatible uses. Relocation of marine based industries to an alternative precinct does not occur until such time that agreement has been reached among all relevant stakeholders such that development does not diminish the viability of marine based industrial uses that directly serve the Port Douglas tourist and fishing operators and private boat owners;
 - (j) marine infrastructure is established to service the tourism, fishing and private boating community;
 - (k) Live entertainment activities are concentrated within the Live Entertainment Precinct and are subject to the recommendations of a suitably qualified acoustic engineer;
 - (l) the functionality of the Balley Hooley tourist rail is retained.

Sub-precinct 1c – Waterfront South sub-precinct

- (8) In addition to all other overall development outcomes, development in the Waterfront South sub-precinct facilitates the following development outcomes:
 - (a) any use of land in the precinct does not affect the environmental, habitat, conservation or scenic values of Dickson Inlet and surrounding land;
 - (b) marine-based industries are established on appropriate land having regard to site suitability, accessibility, surrounding land uses, and location of utilities and services;
 - (c) marine-based industry achieves appropriate environmental standards;
 - (d) industrial buildings have a high standard of layout and building design;
 - (e) landscaping provides an attractive streetscape and screens utility, storage and car parking from the street and other public areas;

- (f) the precinct is protected from encroachment of incompatible land use activities.

Sub-precinct 1d – Limited Development sub-precinct

- (9) In addition to all other overall development outcomes, development in the Limited Development sub-precinct facilitates the following development outcomes:
 - (a) any use of land in the precinct does not affect the environmental, habitat, conservation or scenic values of Dickson Inlet and surrounding land;
 - (b) the open nature and character of the precinct is retained maintaining view lines across the inlet;
 - (c) community and recreation land use activities are established that promote public access to the foreshore.

Sub-precinct 1e – Community and recreation sub-precinct

- (10) In addition to all other overall development outcomes, development in the Community and recreation sub-precinct facilitates the following development outcomes:
 - (a) development for community uses, including sport and recreation is facilitated.
 - (b) sport and recreation activities predominantly involve outdoor activities;
 - (c) areas of natural vegetation are protected from further development;
 - (d) shade trees are increased, in appropriate locations, surrounding the sports fields.

Sub-precinct 1f – Flagstaff Hill sub-precinct

- (11) In addition to all other overall development outcomes, development in the Flagstaff Hill sub-precinct facilitates the following development outcomes:
 - (a) development is not established where it results in detriment to the vegetated and scenic qualities of Flagstaff Hill;
 - (b) development minimises excavation and filling;
 - (c) buildings and other works are unobtrusive when viewed from vantage points in Port Douglas and are designed and constructed of colours and materials which complement the hill's vegetated state;
 - (d) views from public viewing points within the precinct are protected.

Precinct 2 – Integrated Resort precinct

- (12) In addition to the overall outcomes, development in the Integrated Resort precinct facilitates development in accordance with the *Integrated Development Resort Act, 1987*.

Editor's note – The development of land within this precinct is subject to the Integrated Development Resort Act 1987 (IDRA). Where a conflict exists between this planning scheme and the IDRA, the IDRA prevails.

Precinct 3 – Craiglie Commercial and Light Industry precinct

- (13) In addition to the overall outcomes, development in the Craiglie Commercial and Light Industry precinct facilitates the following overall outcomes:
- (a) development supports the tourism and marine industries in Port Douglas, along with the small-scale commercial and light industry land uses that support the local economy that would otherwise be better suited to a location outside the Port Douglas Centre Precinct unless they pose a safety issue;
 - (b) development adjacent to the Captain Cook Highway presents an attractive appearance to the highway. The rain-trees, melaleucas and eucalypt trees along the Captain Cook Highway are retained where possible, taking into account the Department of Transport and main Road's requirements;
 - (c) retailing activities are generally restricted to those which are ancillary and necessarily associated with the primary service and light industry nature of the area;
 - (d) adjacent residential areas are protected from industry nuisances;
 - (e) lots fronting Downing Street, between Dickson Street and Beor Street, are provided with an appropriate standard of road access and infrastructure, prior to development occurring.

Precinct 4 – Old Port Road / Mitre Street precinct

- (14) In addition to the overall outcomes, development in the Old Port Road / Mitre Street precinct facilitates the following overall outcomes:
- (a) the precinct is intended to be used for outdoor recreational land use activity, primarily as a golf course;
 - (b) areas of significant vegetation are protected from development and retained;
 - (c) other forms of development will only be considered if substantial areas of open space are retained adjacent to existing residential areas to maintain the existing residential amenity of open views across open space.

Precinct 5 – Very Low Density Residential/Low Scale Recreation/Low Scale Educational/Low Scale Entertainment Uses precinct

- (15) In addition to the overall outcomes, development in the Very Low Residential Density/Low Scale Recreation/Low Scale Educational/Low Scale Entertainment Uses precinct facilitates the following overall outcomes:
- (a) residential accommodation does not exceed a maximum of 8.5 metres in building height;
 - (b) minimum lot sizes exceed 2 hectares;
 - (c) very low scale and intensity recreation/ very low scale and intensity educational/ and very low scale entertainment uses may be appropriate in areas of the precinct subject to erosion and other flooding constraints.

Note - Undeveloped lots in this precinct are located on very low-lying land. Council may consider a consolidation of existing land titles via lot reconfiguration to lot sizes less than 2 hectares, where the reconfigured lots are consolidated onto the highest terrain, to avoid a pattern of development consisting of dwelling houses located on isolated islands of raised building pads.

Criteria for assessment

Table Error! No text of specified style in document..a –Port Douglas / Craiglie local plan – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
For self assessable and assessable development		
Development in the Port Douglas / Craiglie local plan area generally		
PO1 Pedestrians, cyclists, motorists and public transport users can easily move into and through the precinct along planned connectivity routes, identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.	AO1 A pedestrian and cycle movement network is integrated and delivered through development.	Subject site is not located such as to include opportunities for pedestrian and cycle movements through the site.
PO2 Development retains and enhances key landscape elements including character trees and areas of significant vegetation contributing to the character and quality of the local plan area and significant views and vistas and other landmarks important to the context of Port Douglas / Craiglie (as identified on the Port Douglas/ Craiglie Townscape Plan map contained in Schedule 2).	AO2.1 Development provides for the retention and enhancement of existing mature trees and character vegetation that contribute to the lush tropical character of the town, including: <ul style="list-style-type: none"> (a) the tree covered backdrop of Flagstaff Hill; (b) natural vegetation along watercourses, in particular the Mowbray River, Beor Creek and Dickson Inlet; (c) the tidal vegetation along the foreshore; 	Proposal will include significant vegetation retention and additional landscape treatment.

Performance outcomes	Acceptable outcomes	Applicant response
	<p>(d) beachfront vegetation along Four Mile Beach, including the fringe of Coconut Palms;</p> <p>(e) the oil palm avenues along the major roads;</p> <p>(f) the lush landscaping within major roundabouts at key nodes;</p> <p>(g) Macrossan Street and Warner Street;</p> <p>(h) Port Douglas waterfront.</p> <p>AO2.2 Development protects and does not intrude into important views and vistas as identified on the Port Douglas Townscape Plan map contained in Schedule 2, in particular:</p> <p>(a) Flagstaff Hill;</p> <p>(a) Four Mile Beach;</p> <p>(b) Across to the ranges over Dickson Inlet;</p> <p>(c) Mowbray Valley.</p> <p>AO2.3 Important landmarks, memorials and monuments are retained.</p>	<p>Proposal does not negatively intrude on any of the identified views.</p> <p>N/A</p>
<p>PO3 Development contributes to the protection, reinforcement and where necessary enhancement of gateways and key intersections identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.</p>	<p>AO3 Development adjacent to the gateways and nodes as identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 incorporates architectural features and landscaping treatments and design elements that enhance the sense of arrival and way finding within the town.</p>	<p>N/A</p>
<p>PO4 Landscaping of development sites complements the existing tropical character of Port Douglas and Craiglie.</p>	<p>AO4 Landscaping incorporates the requirements of Planning scheme policy SC6.7 – Landscaping, in particular landscaping should</p>	<p>Proposal includes sufficient areas for landscaping which will be capable of achieving compliance with Planning scheme policy SC6.7 – Landscaping.</p>

Performance outcomes	Acceptable outcomes	Applicant response
	be capable of achieving a 60% screening of development within 5 years and predominantly consists of endemic vegetation.	
P05 Development does not compromise the safety and efficiency of the State-controlled road network.	A05 Direct access is not provided to a State-controlled road where legal and practical access from another road is available.	Proposal complies.
For assessable development		
Additional requirements in Precinct 1 – Port Douglas precinct		
P06 The views and vistas identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 are maintained.	A06.1 Development does not impede continued views to scenic vistas and key streetscapes within the local plan area. A06.2 Unless otherwise specified within this Local Plan, buildings are set back not less than 6 metres from the primary street frontage.	N/A N/A
P07 Vehicle access, parking and service areas: (a) do not undermine the relationship between buildings and street or dominate the streetscape; (b) are designed to minimise pedestrian vehicle conflict; (c) are clearly identified and maintain ease of access at all times.	A07.1 For all buildings, parking is: (a) to the side of buildings and recessed behind the main building line; or (b) behind buildings; or (c) wrapped by the building façade, and not visible from the street. A07.2 Ground level parking incorporates clearly defined pedestrian routes. A07.3	N/A N/A N/A

Performance outcomes	Acceptable outcomes	Applicant response
	<p>Any porte-cocheres, disabled and pedestrian accesses are accommodated within the boundary of new or refurbished development.</p> <p>A07.4 Where the development is an integrated mixed-use development incorporating short term accommodation or multiple dwellings and either food and drink outlet or hotel or shop or shopping centre or office, on-site parking spaces are provided as per the number prescribed in the Parking and access code with a relaxation of 30% of spaces required for the non-residential uses.</p> <p>A07.5 On-site car parking available for public use is clearly signed at the site frontage.</p> <p>A07.6 Boom gates, pay machines or other regulatory devices to control access to a publicly available car parking area are not constructed or installed.</p>	<p>N/A</p> <p>N/A</p> <p>N/A</p>
<p>PO8 Precinct 1 – Port Douglas precinct is not characterised by a proliferation of advertising signs.</p>	<p>A08 No acceptable outcomes are prescribed.</p>	<p>N/A</p>
Additional requirements for Sub-precinct 1a – Town Centre sub-precinct		
<p>PO9 Building heights: (a) do not overwhelm or dominate the town centre; (b) respect the desired streetscape;</p>	<p>A09 Buildings and structures are not more than 3 storeys and 13.5 metres in height, with a roof height of not less than 3 metres.</p> <p>Note – Height is inclusive of the roof height.</p>	<p>N/A</p>

Performance outcomes	Acceptable outcomes	Applicant response
(c) ensure a high quality appearance when viewed from both within the town centre sub-precinct and external to the town centre sub-precinct; (d) remain subservient to the natural environment and the backdrop of Flagstaff Hill. (e) do not exceed 3 storeys.		
PO10 Building design, the streetscape, pedestrian paths and street front spaces promote integration with the surrounding area and the rest of Precinct 1 – Port Douglas Precinct.	AO10 No acceptable outcomes are prescribed.	N/A
PO11 Buildings: (a) address street frontages; (b) ensure main entrances front the street or public spaces; (c) do not focus principally on internal spaces or parking areas.	AO11 No acceptable outcomes are prescribed.	N/A
PO12 Setbacks at ground level provide for: (a) connection between pedestrian paths and public places; (b) areas for convenient movement of pedestrians; (c) changes in gradient of the street.	AO12 Setbacks at ground level: (a) are clear of columns and other obstructions; (b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths.	N/A
AO13 Buildings do not result in a reduction of views and vistas from public places to: (a) Flagstaff Hill; (b) Dickson Inlet; (c) public open space; (d) places of significance.	AO13 No acceptable outcomes are prescribed.	N/A

Performance outcomes	Acceptable outcomes	Applicant response
PO14 Development enhances the distinctive tropical resort town and identity of Port Douglas and encourages pedestrian activity at street level including shade protection across the footpath for the length of the building.	AO14 Development is built up to the street frontage/s at the street level and incorporates a light frame awning, a minimum of 3 metres in width for the length of the street frontage/s; Or If a development includes an outdoor dining area at ground/footpath level, the dining area has a maximum setback of 3 metres and the required awning is still maintained along the length of the street frontage/s. Note – PO24 provides more detail on awning design.	N/A
PO15 Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street-level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.	AO15.1 Centre activities establish: at street level on active street frontages; a maximum of one level above street level. AO15.2 Any residential development activities or short term accommodation is located above street level of the active frontage, but not on or up to the street frontage in any development, including mixed use development.	N/A N/A
PO16 Detailed building design: (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements; (c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the	AO16 No acceptable outcomes are prescribed.	N/A

Performance outcomes	Acceptable outcomes	Applicant response
lush, vegetated character of the Town Centre sub-precinct is maintained.		
P017 Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through: <ul style="list-style-type: none"> (a) surface decoration; (b) wall recesses and projections; (c) a variation in wall finishes; windows, balconies, awnings and other visible structural elements. (d) differentiating between the lower, middle and upper parts of the building by varying the façade and/or the shape of the built form, where comprised of more than two storeys. 	AO17 No acceptable outcomes are prescribed.	N/A
P018 Roofs are not characterised by a cluttered display of plant and equipment, in particular: <ul style="list-style-type: none"> (a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Town Centre sub-precinct; (b) service structures, lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view; (c) rooftops are not used for advertising. 	AO18 No acceptable outcomes are prescribed.	N/A
P019 Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to: <ul style="list-style-type: none"> (a) shade windows; (b) reduce glare; (c) assist in maintaining comfortable indoor temperatures; (d) minimising heat loads; 	AO19 No acceptable outcomes are prescribed.	N/A

Performance outcomes	Acceptable outcomes	Applicant response
	(d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development.	
PO23 Building facades that face public spaces at ground level: (a) complement the appearance of the development and surrounding streetscape; (b) enhance the visual amenity of the public place; (c) include a variety of human scale architectural elements and details; (d) provide an opportunity for the casual and convenient surveillance of public space from within the development.	AO23 Building facades at the ground floor of development that face public space are designed to ensure: (a) a minimum of 70% of the façade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development; (b) a visually prominent main entrance that faces the principal public place; (c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the façade.	N/A
PO24 Awnings for pedestrian shelter are consistent with the character setting of the Town Centre sub-precinct and: (a) extend and cover the footpath to provide protection from the sun and rain; (b) include lighting under the awning; (c) are continuous across the frontage of the site; (d) align to provide continuity with existing or future awnings on adjoining sites; (e) are a minimum of 3.0 metres in width and generally not more than 3.5 metres above pavement height; (f) do not extend past a vertical plane, 1.2 metres inside the kerb-line to enable street trees to be planted and grow;	AO24 No acceptable outcomes are prescribed.	N/A

Performance outcomes	Acceptable outcomes	Applicant response
(g) are cantilevered from the main building with any posts within the footpath being non load-bearing.		
PO25 Development integrates with the streetscape and landscaping improvements for Port Douglas.	AO25 Development fronting Davidson Street, Macrossan Street, Wharf Street, Mowbray Street and Warner Street is designed to integrate with the on-street landscaping and design improvements as outlined within the Port Douglas landscape master plan contained within Planning scheme policy SC6.7 – Landscaping. Note - Planning scheme policy SC6.7 - Landscaping provides guidance on meeting the Performance Outcome.	N/A
Additional requirements for Sub-precinct 1b – Waterfront North sub-precinct		
PO26 The establishment of uses is consistent with the outcomes sought for sub-precinct 1b – Waterfront North.	AO26 Uses identified as inconsistent uses in Table Error! No text of specified style in document..b — Inconsistent uses in sub-precinct 1b - Waterfront North sub-precinct are not established in sub-precinct 1b - Waterfront North	N/A
PO27 The bulk and scale of buildings is consistent with surrounding development and steps down to complement the open space areas in the adjoining limited development sub-precinct.	AO27 Buildings and structures are not more than: (a) 3 storeys and 13.5 metres in height , with a roof height of not less than 3 metres, in those parts of the precinct south of Inlet Street; (b) 2 storeys and 8.5 metres in height, with a roof height of not less than 3 metres, in those parts of the precinct north of Inlet Street. Note – Height is inclusive of roof height.	N/A
PO28	AO28	N/A

Performance outcomes	Acceptable outcomes	Applicant response
	(b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths.	
PO32 Buildings do not result in a reduction of views and vistas from public places to: (a) Dickson Inlet; (b) public open space; (c) places of significance.	AO32 No acceptable outcomes are prescribed.	N/A
PO33 Development enhances the distinctive tropical resort town and identity of Port Douglas and encourages pedestrian activity at ground level including shade protection across the footpath and open space areas.	AO33 No acceptable outcomes are prescribed.	N/A
PO34 Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street-level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.	AO34.1 Centre activities establish: (a) at street level on active street frontages; (b) a maximum of one level above street level. AO34.2 Residential development activities or short term accommodation is located above street /ground floor level of the active frontage, but not on or up to the street / public frontage in any development, including mixed use development.	N/A
PO35 Detailed building design: (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements;	AO35 No acceptable outcomes are prescribed.	N/A

Performance outcomes	Acceptable outcomes	Applicant response
(c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the lush, vegetated character of the Waterfront North sub-precinct is maintained.		
PO36 Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through: (a) surface decoration; (b) wall recesses and projections; (c) a variation in wall finishes; windows, balconies, awnings and other visible structural elements. (d) differentiating between the lower, middle and upper parts of the building by varying the façade and/or the shape of the built form, where comprised of more than two storeys.	AO36 No acceptable outcomes are prescribed.	N/A
PO37 Roofs are not characterised by a cluttered display of plant and equipment, in particular: (a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Waterfront North sub-precinct; (b) service structures, lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view; (c) rooftops are not used for advertising.	AO37 No acceptable outcomes are prescribed.	N/A
PO38 Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to: (a) shade windows;	AO38 No acceptable outcomes are prescribed.	N/A

Performance outcomes	Acceptable outcomes	Applicant response
(b) reduce glare; (c) assist in maintaining comfortable indoor temperatures; (d) minimising heat loads; (e) enriching the North Queensland tropical character of the Waterfront North sub-precinct; (f) architectural interest to building façades.		
PO39 Buildings are finished with high quality materials, selected for: (a) their ability to contribute the character of Waterfront North sub-precinct; (b) easy maintenance, durability and an ability not to readily stain, discolour or deteriorate.	AO39 No acceptable outcomes are prescribed.	N/A
PO40 Buildings do not incorporate any type of glass or other materials that are likely to reflect the sun's rays in a manner that may create a nuisance, discomfort or a hazard.	AO40 No acceptable outcomes are prescribed.	N/A
PO41 Façades and elevations do not include large blank walls and openings and setbacks are used to articulate vertical building surfaces.	AO41.1 Development has a maximum length of unbroken building facade of 20 metres and a maximum extent of overall development in the same style/design along the street frontage/s of 40 metres. AO41.2 Any break in the building façade varies the alignment by a 1 metre minimum deviation. AO41.3 A minimum of three of the following building design features and architectural elements	N/A

Performance outcomes	Acceptable outcomes	Applicant response
	<p>detailed below are incorporated to break the extended facade of a development:</p> <ul style="list-style-type: none"> (a) a change in roof profile; (b) a change in parapet coping; (c) a change in awning design; (d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development. 	
<p>PO42 Building facades that face public spaces at ground level:</p> <ul style="list-style-type: none"> (a) complement the appearance of the development and surrounding streetscape; (b) enhance the visual amenity of the public place; (c) include a variety of human scale architectural elements and details; (d) provide an opportunity for the casual and convenient surveillance of public space from within the development. 	<p>AO42 Building facades at the ground floor of development that face public space are designed to ensure:</p> <ul style="list-style-type: none"> (a) a minimum of 70% of the façade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development; (b) a visually prominent main entrance that faces the principal public place; (c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the façade. 	N/A
<p>PO43 Awnings for pedestrian shelter are consistent with the character setting of the Waterfront North sub-precinct and:</p> <ul style="list-style-type: none"> (a) extend and cover the footpath to provide protection from the sun and rain; (b) include lighting under the awning; (c) are continuous across pedestrian circulation areas; (d) align to provide continuity with existing or future awnings on adjoining sites; 	<p>AO43 No acceptable outcomes are prescribed.</p>	N/A

Performance outcomes	Acceptable outcomes	Applicant response
(e) are a minimum of 3 metres in width and generally not more than 3.5 metres above pavement height; (f) do not extend past a vertical plane, 1.2 metres inside the street kerb-line to enable street trees to be planted and grow; (g) are cantilevered from the main building with any posts within the footpath being non load-bearing.		
PO44 The Balley Hooley rail line and turn-table is retained and incorporated into development and maintains its functionality.	AO44.1 Bally Hooley rail line and turn-table is retained and incorporated into development to maintain its functionality. AO44.2 Where development provides floor area for the Bally Hooley rail station, the gross floor area of the rail line and station does not generate a requirement for additional vehicle parking.	N/A N/A
PO45 Development recognises the importance of and relationship between the marina, commercial and residential development in the Waterfront North sub-precinct, and includes measures to mitigate the impact of: (a) noise; (b) odour; (c) hazardous materials; (d) waste and recyclable material storage.	AO45 No acceptable outcomes are prescribed.	N/A
PO46 Formalised public spaces and pedestrian paths/areas on freehold land are made accessible to the public.	AO46 No acceptable outcomes are prescribed.	N/A

Performance outcomes	Acceptable outcomes	Applicant response
PO47 Buildings, civic spaces, roads and pedestrian links are enhanced by: (a) appropriate landscape design and planting; (b) themed planting that defines entry points, and creates strong 'entry corridors' into the waterfront; (c) lighting and well-considered discrete signage that complements building and landscape design; (d) public artwork and other similar features that reflect the heritage and character of the Port Douglas Waterfront.	AO47 No acceptable outcomes are prescribed.	N/A
PO48 Buildings are designed and sited to provide vistas along shared pedestrian/open space and movement areas in suitable locations.	AO48 No acceptable outcomes are prescribed.	N/A
PO49 Development does not diminish the viability of marine-based industrial uses that directly serve the Port Douglas tourist and fishing operators and private boat owners, particularly with respect to the slipway operation.	AO49 No acceptable outcomes are prescribed.	N/A
PO50 Marine infrastructure to service the tourism, fishing and private boating community is provided.	AO50 No acceptable outcomes are prescribed.	N/A
PO51 Changes to the Port Douglas Waterfront quay-line do not cause adverse impacts to the environmentally sensitive Dickson Inlet.	AO51 Development that results in changes to the Port Douglas Waterfront quay-line is only established where an Ecological assessment report provides support to the changes. Note - Planning scheme policy SC6.8 – Natural environment provides guidance on preparing an ecological assessment report.	N/A
Additional requirements for Sub-precinct 1c – Waterfront South sub-precinct		

Performance outcomes	Acceptable outcomes	Applicant response
PO52 The establishment of uses is consistent with the outcomes sought for Precinct 1c – Waterfront South.	AO52 Uses identified as inconsistent uses Table Error! No text of specified style in document..c are not established in Precinct 1c – Waterfront South.	N/A
PO53 Development does not adversely impact on the natural environment, natural vegetation or watercourses.	AO53.1 An Ecological assessment report is prepared identifying the environmental qualities of the surrounding natural and built features which are to be managed. Note - Planning scheme policy SC6.8 – Natural environment provides guidance on preparing an ecological assessment report. AO53.2 An Environmental Management Plan is prepared to manage potential impacts of the operation of the development on surrounding natural areas. Note - Planning scheme policy SC6.4 – Environmental management plans contains information to demonstrate compliance and guidance on preparing an Environmental Management Plan.	N/A
PO54 Development of land at the end of Port Street adjacent to Dickson Inlet incorporates a slipway, or an alternative functioning facility, with capacity to service the Port Douglas marine and tourism industry.	AO54 A master plan for the development is provided and implemented to demonstrate the integration of the slipway, or an alternative functioning facility, with other supporting service industry activities that service the marine and tourism industry of Port Douglas.	N/A
PO55	AO55.1	N/A

Performance outcomes	Acceptable outcomes	Applicant response
Buildings and structures are of a height, and are set back from side boundaries and other sensitive areas to ensure the scenic amenity and environmental qualities of the adjacent area are not adversely affected.	Development has a height of not more than 10 metres. AO55.2 Development is setback from all property boundaries not less than 3 metres.	
PO56 The site coverage of all buildings and structures ensures development: (a) is sited in an existing cleared area or in an area approved for clearing; (b) has sufficient area for the provision of services; (c) development does not have an adverse effect on the environmental, habitat, conservation or landscape values of the on-site and surrounding sensitive areas.	AO56 No acceptable outcomes are prescribed.	N/A
PO57 Premises include adequate provision for service vehicles, to cater for generated demand. Loading areas for service vehicles are designed to: (a) be accommodated on-site; (b) maximise safety and efficiency of loading; (c) protect the visual and acoustic amenity of sensitive land use activities; (d) minimise adverse impacts on natural characteristics of adjacent areas.	AO57.1 Sufficient manoeuvring area is provided on-site to allow a Medium Rigid Vehicle to enter and leave the site in a forward gear. AO57.2 Development is designed to ensure all service vehicles are contained within the site when being loaded/unloaded. AO57.3 Driveways, parking and manoeuvring areas are constructed and maintained to: (a) minimise erosion from storm water runoff; (a) retain all existing vegetation.	N/A N/A N/A
PO58	AO58	N/A

Performance outcomes	Acceptable outcomes	Applicant response
Development ensures adverse impacts from service vehicles on the road network, external to the site, are minimised.	No acceptable outcomes are prescribed.	
PO59 Entry to the site is landscaped to enhance the amenity of the area and provide a pleasant working environment.	AO59 Areas used for loading and unloading, storage, utilities and car parking are screened from public view: (a) by a combination of landscaping and screen fencing; (b) dense planting along any road frontage is a minimum width of 3 metres.	N/A
PO60 Landscaping is informal in character and complementary to the existing natural environment, provides screening and enhances the visual appearance of the development.	AO60 For any development landscaping is in accordance with the Plant species schedule in Planning scheme policy SC6.7– Landscaping.	N/A
Additional requirements for Sub-precinct 1d – Limited Development sub-precinct		
PO61 The height of buildings and structures contributes to the desired form and outcomes for the sub-precinct and are limited to a single storey.	AO61 Buildings and structures are not more than one storey and 4 metres in height. Note - Height is inclusive of the roof height.	N/A
Additional requirements for Sub-precinct 1e – Community and recreation sub-precinct		
PO62 The precinct is developed for organised sporting activities and other community uses.	AO62 No acceptable outcomes are prescribed.	N/A
Additional requirements for Sub-precinct 1f – Flagstaff Hill sub-precinct		
PO63 Flagstaff Hill is protected from inappropriate development to protect the hill as an important natural landmark feature of Port Douglas and as a vegetated backdrop to the Town centre.	AO63 No acceptable outcomes are prescribed.	Proposal is consistent with the existing character of development on Flagstaff Hill and includes significant vegetation retention and landscaping to protect the vegetated backdrop to the town centre.

Performance outcomes	Acceptable outcomes	Applicant response
PO64 All development on Flagstaff Hill is designed to minimise the visibility of the development and to ensure development is subservient to the natural landscape and topography of the site, including through: <ul style="list-style-type: none"> (a) building design which minimises excavation and filling; (b) buildings being designed to step down the site and incorporate foundations and footings on piers or poles; (c) buildings being visually unobtrusive and incorporating exterior finishes and muted colours which are non-reflective and complement the colours of the surrounding vegetation and viewshed; (d) protection of the views from public viewing points in the Port Douglas precinct. 	AO64 No acceptable outcomes are prescribed.	Proposal achieves all of the outcomes of PO64.
Additional requirements for Precinct 3 – Craiglie Commercial and Light Industry precinct		
PO65 Development supports the tourism and marine industries in Port Douglas, along with the small-scale commercial and light industry land uses that support the local economy that would otherwise be better suited to a location outside the Port Douglas Town Centre Precinct.	AO65 Development consists of service and light industries and associated small scale commercial activities.	N/A
PO66 Development on lots adjacent to the Captain Cook Highway is sited, designed and landscaped to provide an attractive visual approach to Port Douglas with all buildings, structures and car parking areas setback a sufficient distance from the frontage to enable landscaping to soften or screen the appearance of the development.	AO66.1 Buildings and structures are setback 8 metres from the Captain Cook Highway frontage, or no closer to the Captain Cook Highway frontage than buildings and structures on adjoining sites (averaged), whichever is the greater.	N/A
	AO66.2	N/A

Performance outcomes	Acceptable outcomes	Applicant response
	<p>The setback area to the Captain Cook Highway frontage is landscaped with advanced dense planting including tree species (100 litre bag stock), which will, at maturity, exceed the height of the building(s) on the site.</p> <p>AO66.3 Advertising signs are discreet in appearance with no large advertising signs, including tenancy signs, located on or near the Captain Cook Highway frontage, or within any landscaped setback area.</p> <p>AO66.4 Car parking areas, loading and other service areas are designed to be screened from the Captain Cook Highway and are located so as to not be visually prominent from the Captain Cook Highway.</p>	<p>N/A</p> <p>N/A</p>
Additional requirements for Precinct 6 – Very Low Residential Density / Low Scale Recreation / Low Scale Educational / Low Scale Entertainment Uses precinct		
PO67 No additional lots are created within the precinct.	AO67 No acceptable outcomes are prescribed.	N/A
PO68 Reconfigured lots have a minimum lot size of 2 hectares, unless the lot reconfiguration transfers lots to the higher parts of the land, to avoid the need to fill existing lots to accommodate dwelling houses.	AO68 No acceptable outcomes are prescribed.	N/A

Table Error! No text of specified style in document..b — Inconsistent uses in sub-precinct 1b - Waterfront North sub-precinct

Inconsistent uses

<ul style="list-style-type: none"> • Agricultural supplies store • Air services • Animal husbandry • Animal keeping • Aquaculture • Brothel • Bulk landscape supplies • Car wash • Cemetery • Crematorium • Cropping • Detention facility • Dual occupancy • Dwelling house 	<ul style="list-style-type: none"> • Extractive industry • Funeral parlour • High impact industry • Intensive animal industry • Intensive horticulture • Major electricity infrastructure • Major sport, recreation and entertainment facility • Medium impact industry • Motor sport facility, • Outstation • Permanent plantation 	<ul style="list-style-type: none"> • Relocatable home park • Roadside stall • Rural industry • Rural workers accommodation • Service station • Showroom • Special industry • Tourist park • Transport depot • Veterinary services • Warehouse • Wholesale nursery • Winery
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Table Error! No text of specified style in document..c — Inconsistent uses in sub-precinct 1c - Waterfront South sub-precinct

Inconsistent uses		
<ul style="list-style-type: none"> • Adult store • Agricultural supplies store • Air services • Animal husbandry • Animal keeping • Brothel • Bulk landscape supplies • Car wash • Cemetery • Child care centre • Community care centre • Community residence • Community use • Crematorium • Cropping • Detention facility • Dual occupancy 	<ul style="list-style-type: none"> • Hardware and trade supplies • Health care services • Home based business • Hospital • Hotel • Indoor sport and recreation • Intensive animal industry • Intensive horticulture • Major electricity infrastructure • Major sport, recreation and entertainment facility • Market • Motor sport facility • Multiple dwelling • Nature-based tourism • Nightclub entertainment facility • Outdoor sales 	<ul style="list-style-type: none"> • Permanent plantation • Place of worship • Relocatable home park • Residential care facility • Resort complex • Retirement facility • Roadside stall • Rooming accommodation • Rural industry • Rural workers accommodation • Sales office • Shopping centre • Short-term accommodation • Showroom • Special industry • Theatre • Tourist attraction

<ul style="list-style-type: none"> • Dwelling house • Dwelling unit • Extractive industry • Function facility • Funeral parlour • Garden centre 	<ul style="list-style-type: none"> • Outdoor sport and recreation • Outstation 	<ul style="list-style-type: none"> • Tourist park • Transport depot • Veterinary services • Warehouse • Wholesale nursery • Winery
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Note - **Table Error! No text of specified style in document..b** or **Table Error! No text of specified style in document..c** do not imply that all other uses not listed in the table are automatically consistent uses within the zone. Assessable development must still demonstrate consistency through the assessment process.

Dwelling house code

Application

- (1) This code applies to assessing development for a dwelling house if:
 - (a) self-assessable development or assessable development where this code identified in the assessment criteria column of a table of assessment;
or
 - (b) impact assessable development.
- (2) When using this code, reference should be made to Part 5.

Note—Where the land is identified in an overlay map, additional provisions relating to that overlay also apply. For example, minimum floor levels for a dwelling house on a site subject to certain types of flooding are identified in the Flood and storm tide inundation overlay code.

Note – For a proposal to be self-assessable, it must meet all of the self-assessable outcomes of this code and any other applicable code. Where it does not meet all the self-assessable outcomes, the proposal becomes assessable development and a development application is required. Where a development application is triggered, only the specific acceptable outcomes that the proposal fails to meet need to be assessed against the corresponding performance outcomes. Other self-assessable outcomes that are met are not assessed as part of the development application.

Purpose

- (1) The purpose of the Dwelling house code is to assess the suitability of development to which this code applies.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) The dwelling house, including all habitable buildings on site, is occupied by a single household;
 - (b) A dwelling house, including a secondary dwelling or domestic out-buildings; ensures that the secondary dwelling is sub-ordinate to the primary dwelling house;
 - (c) Development of a dwelling house provides sufficient and safe vehicle access and parking for residents;
 - (d) The built form, siting, design and use of each dwelling is consistent with the desired neighbourhood character and streetscape elements of the area.

Criteria for assessment

Table Error! No text of specified style in document..a – Dwelling house code –assessable development

Performance outcomes	Acceptable outcomes	Applicant response
For self-assessable and assessable development		
PO1	AO1	N/A

Performance outcomes	Acceptable outcomes	Applicant response
<p>Secondary dwellings:</p> <ul style="list-style-type: none"> (a) are subordinate, small-scaled dwellings; (b) contribute to a safe and pleasant living environment; (c) are established on appropriate sized lots; (d) do not cause adverse impacts on adjoining properties. 	<p>The secondary dwelling:</p> <ul style="list-style-type: none"> (a) has a total gross floor area of not more than 80m², excluding a single carport or garage; (b) is occupied by 1 or more members of the same household as the dwelling house. 	
<p>PO2</p> <p>Resident's vehicles are accommodated on- site.</p>	<p>AO2</p> <p>Development provides a minimum number of on-site car parking spaces comprising:</p> <ul style="list-style-type: none"> (a) 2 car parking spaces which may be in tandem for the dwelling house; (b) 1 car parking space for any secondary dwelling on the same site. 	<p>Proposal complies.</p>
<p>PO3</p> <p>Development is of a bulk and scale that:</p> <ul style="list-style-type: none"> (a) is consistent with and complements the built form and front boundary setbacks prevailing in the street and local area; (b) does not create an overbearing development for adjoining dwelling houses and their private open space; (c) does not impact on the amenity and privacy of residents in adjoining dwelling houses; (d) ensures that garages do not dominate the appearance of the street. 	<p>AO3</p> <p>Development meets the acceptable outcome for building height in the applicable Zone code associated with the site.</p>	<p>Discussion is provided within the planning report to demonstrate that the proposal meets the relevant performance outcomes with respect to building height.</p>

Bushfire hazard overlay code

Note - Land shown on the bushfire hazard overlay map is designated as the bushfire prone area for the purposes of section 12 of the Building Regulations 2006. The bushfire hazard area (bushfire prone area) includes land covered by the high and medium hazard areas as well as the buffer area category on the overlay map.

Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational works or building work in the Bushfire hazard overlay, if:
 - (a) self-assessable or assessable where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
 - (b) impact assessable development.
- (2) Land in the Bushfire hazard overlay is identified on the Bushfire hazard overlay map in Schedule 2 and includes the following sub-categories:
 - (a) Medium bushfire risk sub-category;
 - (b) High bushfire risk sub-category;
 - (c) Very high bushfire risk sub-category;
 - (d) Potential impact buffer sub-category.
- (3) When using this code, reference should be made to Part 5.

Purpose

- (1) The purpose of the Bushfire overlay code is to:
 - (a) implement the policy direction in the Strategic Framework, in particular:
 - (i) Theme 1 Settlement pattern: Element 3.4.7 Mitigation of hazards;
 - (ii) Theme 6 Infrastructure and transport: Element 3.9.2 Energy.
 - (b) enable an assessment of whether development is suitable on land within the Bushfire risk overlay sub-categories.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) development avoids the establishment or intensification of vulnerable activities within or near areas that are subject to bushfire hazard;
 - (b) development is designed and located to minimise risks to people and property from bushfires;
 - (c) bushfire risk mitigation treatments are accommodated in a manner that avoids or minimises impacts on the natural environment and ecological processes;
 - (d) development involving the manufacture or storage of hazardous materials does not increase the risk to public safety or the environment in a bushfire event;
 - (e) development contributes to effective and efficient disaster management response and recovery capabilities.

Note - A site based assessment may ground-truth the extent of hazardous vegetation and extent and nature of the bushfire hazard area (bushfire prone area). Such assessments should be undertaken using the methodology set out in Planning scheme policy SC6.9 - Natural Hazards.

Criteria for assessment

Table Error! No text of specified style in document..a – Bushfire hazard overlay code –assessable development

Performance outcomes	Acceptable outcomes	Applicant response
For self-assessable and assessable development		
Compatible development		
<p>PO1 A vulnerable use is not established or materially intensified within a bushfire hazard area (bushfire prone area) unless there is an overriding need or other exceptional circumstances.</p> <p>Note - See the end of this code for examples of vulnerable uses.</p>	<p>AO1 Vulnerable uses are not established or expanded.</p> <p>Note – Where, following site inspection and consultation with Council, it is clear that the mapping is in error in identifying a premises as being subject to a medium, high, very high bushfire hazard or potential impact buffer sub-category, Council may supply a letter exempting the need for a Bushfire Management Plan.</p> <p>Note – Where the assessment manager has not previously approved a Bushfire Management Plan (either by condition of a previous development approval), the development proponent will be expected to prepare such a plan.</p> <p>Note – Planning scheme policy SC6.9 - Natural hazards, provides a guide to the preparation of a Bushfire Management Plan.</p>	N/A
<p>PO2 Emergency services and uses providing community support services are able to function effectively during and immediately after a bushfire hazard event.</p>	<p>AO2 Emergency Services and uses providing community support services are not located in a bushfire hazard sub-category and have direct access to low hazard evacuation routes.</p>	N/A
<p>PO3 Development involving hazardous materials manufactured or stored in bulk is not located in bushfire hazard sub-category.</p>	<p>AO3 The manufacture or storage of hazardous material in bulk does not occur within bushfire hazard sub-category.</p>	N/A
Development design and separation from bushfire hazard – reconfiguration of lots		

Performance outcomes	Acceptable outcomes	Applicant response
<p>PO4.1 Where reconfiguration is undertaken in an urban area or is for urban purposes or smaller scale rural residential purposes, a separation distance from hazardous vegetation is provided to achieve a radiant heat flux level of 29kW/m² at the edge of the proposed lot(s).</p> <p>Note - "Urban purposes" and "urban area" are defined in the <i>Sustainable Planning Regulations 2009</i>. Reconfiguration will be taken to be for rural residential purposes where proposed lots are between 2000m² and 2ha in area. "Smaller scale" rural residential purposes will be taken to be where the average proposed lot size is 6000m² or less.</p> <p>Note - The radiant heat levels and separation distances are to be established in accordance with method 2 set out in AS3959-2009.</p> <p>PO4.2 Where reconfiguration is undertaken for other purposes, a building envelope of reasonable dimensions is provided on each lot which achieves radiant heat flux level of 29kW/m² at any point.</p>	<p>AO4.1 No new lots are created within a bushfire hazard sub-category.</p> <p>or</p> <p>AO4.2 Lots are separated from hazardous vegetation by a distance that: (a) achieves radiant heat flux level of 29kW/m² at all boundaries; and (b) is contained wholly within the development site.</p> <p>Note - Where a separation distance is proposed to be achieved by utilising existing cleared developed areas external to the site, certainty must be established (through tenure or other means) that the land will remain cleared of hazardous vegetation. For staged developments, temporary separation distances, perimeter roads or fire trails may be absorbed as part of subsequent stages. Note - The achievement of a cleared separation distance may not be achievable where other provisions within the planning scheme require protection of certain ecological, slope, visual or character features or functions.</p>	N/A
<p>PO5 Where reconfiguration is undertaken in an urban area or is for urban purposes, a constructed perimeter road with reticulated water supply is established between the lots and the hazardous vegetation and is readily accessible at all times for urban fire fighting vehicles.</p> <p>The access is available for both fire fighting and maintenance/defensive works.</p>	<p>AO5.1 Lot boundaries are separated from hazardous vegetation by a public road which: (a) has a two lane sealed carriageway; (b) contains a reticulated water supply; (c) is connected to other public roads at both ends and at intervals of no more than 500m; (d) accommodates geometry and turning radii in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines;</p>	N/A

Performance outcomes	Acceptable outcomes	Applicant response
	<p>(e) has a minimum of 4.8m vertical clearance above the road;</p> <p>(f) is designed to ensure hydrants and water access points are not located within parking bay allocations; and</p> <p>(g) incorporates roll-over kerbing.</p> <p>AO5.2 Fire hydrants are designed and installed in accordance with AS2419.1 2005, unless otherwise specified by the relevant water entity.</p> <p>Note - Applicants should have regard to the relevant standards set out in the reconfiguration of a lot code and works codes in this planning scheme.</p>	
<p>PO6 Where reconfiguration is undertaken for smaller scale rural residential purposes, either a constructed perimeter road or a formed, all weather fire trail is established between the lots and the hazardous vegetation and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>The access is available for both fire fighting and maintenance/hazard reduction works.</p>	<p>AO6 Lot boundaries are separated from hazardous vegetation by a public road or fire trail which has:</p> <p>(a) a reserve or easement width of at least 20m;</p> <p>(b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15 tonne vehicle and which is at least 6m clear of vegetation;</p> <p>(c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path;</p> <p>(d) a minimum of 4.8m vertical clearance;</p> <p>(e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines;</p> <p>(f) a maximum gradient of 12.5%;</p> <p>(g) a cross fall of no greater than 10 degrees;</p> <p>(h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy;</p>	N/A

Performance outcomes	Acceptable outcomes	Applicant response
	<ul style="list-style-type: none"> (i) vehicular access at each end which is connected to the public road network at intervals of no more than 500m; (j) designated fire trail signage; (k) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and (l) if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services. 	
<p>PO7 Where reconfiguration is undertaken for other purposes, a formed, all weather fire trail is provided between the hazardous vegetation and either the lot boundary or building envelope, and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>However, a fire trail will not be required where it would not serve a practical fire management purpose.</p>	<p>AO7 Lot boundaries are separated from hazardous vegetation by a public road or fire trail which has:</p> <ul style="list-style-type: none"> (a) a reserve or easement width of at least 20m; (b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15 tonne vehicle and which is at least 6m clear of vegetation; (c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path; (d) a minimum of 4.8m vertical clearance; (e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; (f) a maximum gradient of 12.5%; (g) a cross fall of no greater than 10 degrees; (h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy; (i) vehicular access at each end which is connected to the public road network; (j) designated fire trail signage; <p>if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and</p>	N/A

Performance outcomes	Acceptable outcomes	Applicant response
	if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services.	
PO8 The development design responds to the potential threat of bushfire and establishes clear evacuation routes which demonstrate an acceptable or tolerable risk to people.	AO8 The lot layout: (a) minimises the length of the development perimeter exposed to, or adjoining hazardous vegetation; (b) avoids the creation of potential bottle-neck points in the movement network; (c) establishes direct access to a safe assembly /evacuation area in the event of an approaching bushfire; and (d) ensures roads likely to be used in the event of a fire are designed to minimise traffic congestion. Note - For example, developments should avoid finger-like or hour-glass subdivision patterns or substantive vegetated corridors between lots. In order to demonstrate compliance with the performance outcome, a bushfire management plan prepared by a suitably qualified person may be required. The bushfire management plan should be developed in accordance with the Public Safety Business Agency (PSBA) guideline entitled "Undertaking a Bushfire Protection Plan. Advice from the Queensland Fire and Emergency Services (QFES) should be sought as appropriate	N/A
PO9 Critical infrastructure does not increase the potential bushfire hazard.	AO9 Critical or potentially hazardous infrastructure such as water supply, electricity, gas and telecommunications are placed underground.	N/A
Development design and separation from bushfire hazard – material change of use		
PO10	AO10	Proposal is capable of complying, however the planning scheme requires vegetation to be

Performance outcomes	Acceptable outcomes	Applicant response
<p>Development is located and designed to ensure proposed buildings or building envelopes achieve a radiant heat flux level at any point on the building or envelope respectively, of:</p> <p>(a) 10kW/m² where involving a vulnerable use; or 29kW/m² otherwise.</p> <p>The radiant heat flux level is achieved by separation unless this is not practically achievable.</p> <p>Note - The radiant heat levels and separation distances are to be established in accordance with method 2 set out in AS3959-2009.</p>	<p>Buildings or building envelopes are separated from hazardous vegetation by a distance that:</p> <p>(a) achieves a radiant heat flux level of at any point on the building or envelope respectively, of 10kW/m² for a vulnerable use or 29kW/m² otherwise; and</p> <p>(b) is contained wholly within the development site.</p> <p>Note - Where a separation distance is proposed to be achieved by utilising existing cleared developed areas external to the site, certainty must be established (through tenure or other means) that the land will remain cleared of hazardous vegetation.</p> <p>For staged developments, temporary separation distances, perimeter roads or fire trails may be absorbed as part of subsequent stages.</p> <p>Note - The achievement of a cleared separation distance may not be achievable where other provisions within the planning scheme require protection of certain ecological, slope, visual or character features or functions.</p>	<p>retained for visual amenity purposes. It is submitted that bushfire risk is appropriately mitigated in the circumstances by the proposed development including appropriate opportunity for evacuation, as well as appropriate water supply both via the reticulated system and significant onsite storage systems.</p>
<p>PO11</p> <p>A formed, all weather fire trail is provided between the hazardous vegetation and the site boundary or building envelope, and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>However, a fire trail will not be required where it would not serve a practical fire management purpose.</p> <p>Note - Fire trails are unlikely to be required where a development site involves less than 2.5ha</p>	<p>AO11</p> <p>Development sites are separated from hazardous vegetation by a public road or fire trail which has:</p> <p>(a) a reserve or easement width of at least 20m;</p> <p>(b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15 tonne vehicle and which is at least 6m clear of vegetation;</p> <p>(c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path;</p> <p>(d) a minimum of 4.8m vertical clearance;</p> <p>(e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines;</p>	<p>As above.</p>

Performance outcomes	Acceptable outcomes	Applicant response
	<ul style="list-style-type: none"> (f) a maximum gradient of 12.5%; (g) a cross fall of no greater than 10 degrees; (h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy; (i) vehicular access at each end which is connected to the public road network which is connected to the public road network at intervals of no more than 500m; (j) designated fire trail signage; if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services. 	
All development		
PO12 All premises are provided with vehicular access that enables safe evacuation for occupants and easy access by fire fighting appliances.	AO12 Private driveways: <ul style="list-style-type: none"> (a) do not exceed a length of 60m from the street to the building; (b) do not exceed a gradient of 12.5%; (c) have a minimum width of 3.5m; (d) have a minimum of 4.8m vertical clearance; (e) accommodate turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; and (f) serve no more than 3 dwellings or buildings. 	Proposed driveway complies with FNQROC.
PO13 Development outside reticulated water supply areas includes a dedicated static supply that is available solely for fire fighting purposes and can be accessed by fire fighting appliances.	AO13 A water tank is provided within 10m of each building (other than a class 10 building) which: <ul style="list-style-type: none"> (a) is either below ground level or of non-flammable construction; 	Proposal includes both reticulated water supply and significant onsite storage.

Performance outcomes	Acceptable outcomes	Applicant response
	<p>(b) has a take off connection at a level that allows the following dedicated, static water supply to be left available for access by fire fighters:</p> <p>(i) 10,000l for residential buildings</p> <p>Note – A minimum of 7,500l is required in a tank and the extra 2,500l may be in the form of accessible swimming pools or dams.</p> <p>(ii) 45,000l for industrial buildings; and</p> <p>(iii) 20,000l for other buildings;</p> <p>(c) includes shielding of tanks and pumps in accordance with the relevant standards;</p> <p>(d) includes a hardstand area allowing medium rigid vehicle (15 tonne fire appliance) access within 6m of the tank;</p> <p>(e) is provided with fire brigade tank fittings – 50mm ball valve and male camlock coupling and, if underground, an access hole of 200mm (minimum) to accommodate suction lines; and</p> <p>(f) is clearly identified by directional signage provided at the street frontage.</p>	
<p>PO14</p> <p>Landscaping does not increase the potential bushfire risk.</p>	<p>AO14</p> <p>Landscaping uses species that are less likely to exacerbate a bushfire event, and does not increase fuel loads within separation areas.</p>	Proposal complies.
<p>PO15</p> <p>The risk of bushfire and the need to mitigate that risk is balanced against other factors (such as but not limited to, biodiversity or scenic amenity).</p>	<p>AO15</p> <p>Bushfire risk mitigation treatments do not have a significant impact on the natural environment or landscape character of the locality where this has value.</p>	Proposal complies.

Note – 'Vulnerable activities' are those involving:

- (1) the accommodation or congregation of vulnerable sectors of the community such as child care centres, community care centre, educational establishments, detention facilities, hospitals, rooming accommodation, retirement facilities or residential care facilities; or
- (2) the provision of essential services including community uses, emergency services, utility installation, telecommunications facility, substations and major electricity infrastructure.

Coastal environment overlay code

Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Coastal environment overlay, if:
 - (a) self assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
 - (b) impact assessable development.
- (2) Land in the Coastal hazard overlay is identified on the Coastal environment overlay map in Schedule 2 and includes the following sub-categories:
 - (a) Coastal management district sub-category;
 - (b) Erosion prone area sub-category.
- (3) When using this code, reference should be made to Part 5.

Purpose

- (1) The purpose of the Coastal environment overlay code is to:
 - (a) implement the policy direction in the Strategic Framework, in particular:
 - (i) Theme 1 Settlement pattern: Element 3.4.7 Mitigation of hazards;
 - (ii) Theme 2 Environment and landscape values: Element 3.5.4 Coastal zones;
 - (iii) Theme 3 Natural resource management: Element 3.6.2 Land and catchment management.
 - (b) enable an assessment of whether development is suitable on land within the Coastal processes sub-categories.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) facilitate the protection of both coastal processes and coastal resources;
 - (b) facilitating coastal dependent development on the foreshore over other development;
 - (c) public access to the foreshore protects public safety;
 - (d) maintain the erosion prone area as a development free buffer zone (other than for coastal dependent, temporary or relocatable development);
 - (e) require redevelopment of existing permanent buildings or structures in an erosion prone area to avoid coastal erosion risks, manage coastal erosion risks through a strategy of planned retreat or mitigate coastal erosion risks;
 - (f) require development to maintain or enhance natural processes and the protective function of landforms and vegetation that can mitigate risks associated with coastal erosion;
 - (g) locate and design community infrastructure to maintain the required level of functionality during and immediately after a coastal hazard event.

Criteria for assessment

Table Error! No text of specified style in document..a – Coastal environment overlay code – self-assessable and assessable development

Performance outcomes		Acceptable outcomes	Applicant response
For self-assessable and assessable development			
<p>PO1</p> <p>No works other than coastal protection works extend seaward of the coastal building line.</p>	<p>AO1.1</p> <p>Development (including all buildings and other permanent structures such as swimming pools and retaining walls) does not extend seaward of a coastal building line.</p> <p>Note – Coastal building lines are declared under the Coastal Protection and Management Act 1995 and are administered by the State Department of Environment and Heritage Protection.</p> <p>AO1.2</p> <p>Coastal protection works are only undertaken as a last resort where coastal erosion presents an immediate threat to public safety or existing buildings or structures and the property cannot be relocated or abandoned.</p> <p>AO1.3</p> <p>Coastal protection works are as far landward as practicable on the lot containing the property to the maximum extent reasonable.</p> <p>AO1.4</p> <p>Coastal protection work mitigates any increase in the coastal hazard.</p>	N/A	
<p>PO2</p> <p>Where a coastal building line does not exist on a lot fronting the coast or a reserve adjoining the coast, development is setback to maintain the amenity and use of the coastal resource.</p>	<p>AO2</p> <p>Where a coastal building line does not exist on a lot fronting the coast or a reserve adjoining the coast, development (including all buildings and structures such as swimming pools) and retaining walls are set back not less than 6 metres from the seaward boundary of the lot.</p>	N/A	
For assessable development			
Erosion prone areas			
<p>PO3</p>	<p>AO3</p>		

Performance outcomes	Acceptable outcomes
Development identifies erosion prone areas (coastal hazards).	No acceptable outcomes are prescribed.
DPO4 Erosion prone areas are free from development to allow for natural coastal processes.	AO4.1 Development is not located within the Erosion prone area, unless it can be demonstrated that the development is for: (a) community infrastructure where no suitable alternative location or site exists for this infrastructure; or development that reflects the preferred development outcomes in accordance with the zoning of the site (i.e. in the Low density residential zone, a dwelling house is a preferred development outcome in accordance with the zoning of the site) AO4.2 Development involving existing permanent buildings and structures within an erosion prone area does not increase in intensity of its use by: (a) adding additional buildings or structures; or incorporating a land use that will result in an increase in the number of people or employees occupying the site.
Coastal management districts	
PO5 Natural processes and protective functions of landforms and vegetation are maintained.	PO5.1 Development within the coastal management district: (a) maintains vegetation on coastal land forms where its removal or damage may: (i) destabilise the area and increase the potential for coastal erosion, or interrupt the natural sediment trapping processes or dune or land building processes; maintains sediment volumes of dunes and near-shore coastal landforms, or where a reduction in sediment volumes cannot be avoided, increased risks to development from coastal erosion are mitigated by location, design and construction and operating standards; minimises the need for erosion control structures or riverine hardening through location, design and construction standards;

Subject site is mapped as being impacted by the Erosion Prone Area however is located above 27m AHD so it is unclear why. In any case, the proposed dwelling is located high on the site out of the EPA.

N/A

N/A

Performance outcomes	Acceptable outcomes	
	<p>maintains physical coastal processes outside the development footprint for the development, including longshore transport of sediment along the coast;</p> <p>reduces the risk of shoreline erosion for areas adjacent to the development footprint to the maximum extent feasible in the case of erosion control structures.</p> <p>PO5.2 Where development proposes the construction of an erosion control structure: (a) it is demonstrated that it is the only feasible option for protecting permanent structures from coastal erosion; and those permanent structures cannot be abandoned or relocated in the event of coastal erosion occurring.</p> <p>PO5.3 Development involving reclamation: (a) does not alter, or otherwise minimises impacts on, the physical characteristics of a waterway or the seabed near the reclamation, including flow regimes, hydrodynamic forces, tidal water and riverbank stability; is located outside active sediment transport area, or otherwise maintains sediment transport processes as close as possible to their natural state; ensures activities associated with the operation of the development maintain the structure and condition of vegetation communities and avoid wind and water run-off erosion.</p>	<p>N/A</p> <p>N/A</p>
<p>PO6 Development avoids or minimises adverse impacts on coastal resources and their values to the maximum extent reasonable.</p>	<p>AO6.1 Coastal protection work that is in the form of beach nourishment uses methods of placement suitable for the location that do not interfere with the long-term use of the locality, or natural values within or neighbouring the proposed placement site.</p> <p>and</p> <p>AO6.2</p>	<p>N/A</p> <p>N/A</p>

Performance outcomes	Acceptable outcomes	
	<p>Marine development is located and designed to expand on or redevelop existing marine infrastructure unless it is demonstrated that it is not practicable to co-locate the development with existing marine infrastructure;</p> <p>and</p> <p>AO6.3 Measures are incorporated as part of siting and design of the development to maintain or enhance water quality to achieve the environmental values and water quality objectives outlined in the Environmental Protection (Water) Policy 2009.</p> <p>and</p> <p>AO6.4 Development avoids the disturbance of acid sulfate soils, or where it is demonstrated that this is not possible, the disturbance of acid sulfate soils is carefully managed to minimise and mitigate the adverse effects of disturbance on coastal resources.</p> <p>and</p> <p>AO6.4 Design and siting of development protects and retains identified ecological values and underlying ecosystem processes within the development site to the greatest extent practicable.</p>	
<p>PO7 Development is to maintain access to and along the foreshore for general public access.</p>	<p>AO7.1 Development provides for regular access points for pedestrians including approved walking tracks, boardwalks and viewing platforms.</p> <p>and</p> <p>AO7.2</p>	N/A

Performance outcomes	Acceptable outcomes	
	<p>Development provides for regular access points for vehicles including approved roads and tracks.</p> <p>or</p> <p>AO7.3 Development demonstrates an alternative solution to achieve an equivalent standard of performance.</p>	
<p>PO8 Public access to the coast is appropriately located, designed and operated.</p>	<p>AO8.1 Development maintains or enhances public access to the coast.</p> <p>or</p> <p>AO8.2 Development is located adjacent to state coastal land or tidal water and minimises and offsets any loss of access to and along the foreshore within 500 metres.</p> <p>or</p> <p>AO8.3 Development adjacent to state coastal land or tidal water demonstrates an alternative solution to achieve an equivalent standard and quality of access.</p>	N/A
<p>PO9 Development adjacent to state coastal land or tidal water is located, designed and operated to: (a) maintain existing access to and along the foreshore; minimise any loss of access to and along the foreshore, or offset any loss of access to and along the foreshore by providing for enhanced alternative access in the general location.</p>	<p>AO9.1 Development adjacent to state coastal land or tidal water: (a) demonstrates that restrictions to public access are necessary for: (i) the safe and secure operation of development; the maintenance of coastal landforms and coastal habitat; or (a) maintains public access (including public access infrastructure that has been approved by the local government or relevant authority) through the site to the foreshore for: (i) pedestrians via access points including approved walking tracks, boardwalks and viewing platforms; vehicles via access points including approved roads or tracks.</p>	N/A

Performance outcomes	Acceptable outcomes	
	<p>AO9.2 Development adjacent to state coastal land or tidal water:</p> <p>(a) is located and designed to:</p> <ul style="list-style-type: none"> (i) allow safe unimpeded access to, over, under or around built infrastructure located on, over or along the foreshore, for example through the provision of esplanades or easement corridors to preserve future access; ensure emergency vehicles can access the area near the development. <p>or</p> <p>(a) minimises and offsets any loss of access to and along the foreshore within 500m of existing access points and development is located and designed to:</p> <ul style="list-style-type: none"> (i) allow safe unimpeded access to, over, under or around built infrastructure located on, over or along the foreshore, and ensure emergency vehicles can access the area near the development. 	
<p>AO10 Development that involves reconfiguring a lot for urban purposes adjacent to the coast is designed to ensure public access to the coast in consideration of public access demand from a whole-of-community basis and the maintenance of coastal landforms and coastal habitat.</p>	<p>AO10.1 Development complies if consideration of public access demand from a whole-of-community basis and the maintenance of coastal landforms and coastal habitat is undertaken.</p> <p>or</p> <p>AO10.2 Development demonstrates an alternative solution to achieve an equivalent standard and quality of access.</p>	N/A
<p>PO11 Development maintains public access to State coastal land by avoiding private marine development attaching to, or extending across, non-tidal State coastal land.</p>	<p>AO11 Private marine access structures and other structures such as decks or boardwalks for private use do not attach to or extend across State coastal land that is situated above high water mark</p>	N/A
PO12	AO12	N/A

Performance outcomes		Acceptable outcomes
Development in connection with an artificial waterway enhances public access to coastal waters.		The artificial waterway avoids intersecting with or connection to inundated land or leased land where the passage, use or movement of vessels in water on the land could be restricted or prohibited by the registered proprietor of the inundated land or leased land.
Coastal landscapes, views and vistas		
PO13 Development maintains and / or enhances natural coastal landscapes, views and vistas.	AO13 No acceptable outcomes are prescribed.	Proposal complies.
PO14 Coastal settlements are consolidated through the concentration of development within the existing urban areas through infill and conserving the natural state of the coastal area outside existing urban areas.	AO14 No acceptable outcomes are prescribed.	Proposal complies.
Private marine development		
PO15 Private marine development is to avoid attaching to, or extending across, non-tidal State coastal land.	AO15 Private marine development and other structures such as decks or boardwalks for private use do not attach to, or extend across, State coastal land that is situated above high water mark. Note – For occupation permits or allocations of State land, refer to the <i>Land Act 1994</i> .	N/A
PO16 The location and design of private marine development does not adversely affect the safety of members of the public access to the foreshore.	AO16 Private marine development does not involve the erection or placement of any physical barrier preventing existing access, along a public access way to the foreshores.	N/A
PO17 Private marine development is of a height and scale and size compatible with the character and amenity of the location.	AO17 Private marine development has regard to: (a) the height, scale and size of the natural features of the immediate surroundings and locality; the height, scale and size of existing buildings or other structures in the immediate surroundings and the locality;	N/A

Performance outcomes	Acceptable outcomes	
	<p>if the relevant planning scheme states that desired height, scale or size of buildings or other structures in the immediate surroundings or locality – the stated desired height, scale or size.</p> <p>Note – The prescribed tidal works code in the <i>Coastal Protection and Management Regulation 2003</i> outlines design and construction requirements that must be complied with.</p>	
<p>PO18 Private marine development avoids adverse impacts on coastal landforms and coastal processes.</p>	<p>AO18 Private marine development does not require the construction of coastal protection works, shoreline or riverbank hardening or dredging for marine access.</p>	N/A
For dry land marinas and artificial waterways		
<p>PO19 Dry land marinas and artificial waterways: (a) avoid impacts on coastal resources; do not contribute to the degradation of water quality; do not increase the risk of flooding; do not result in the degradation or loss of MSES; do not result in an adverse change to the tidal prism of the natural waterway to which development is connected. does not involve reclamation of tidal land other than for the purpose of: (i) coastal dependent development, public marine development; or community infrastructure, where there is no feasible alternative; or strategic ports, boat harbours or strategic airports and aviation facilities in accordance with a statutory land use plan; or coastal protection works or works necessary to protect coastal resources and processes.</p>	<p>AO19 No acceptable solutions are prescribed.</p>	N/A

Hillslopes overlay code

Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Hillslopes overlay, if:
 - (a) self assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
 - (b) impact assessable development.
- (2) Land in the Hillslopes overlay is identified on the Hillslopes overlay map in Schedule 2 and includes the following sub-categories:
 - (a) Hillslopes constraint sub-category.
- (3) When using this code, reference should be made to Part 5.

Purpose

- (1) The purpose of the Hillslopes overlay code is to:
 - (a) implement the policy direction in the Strategic Framework, in particular:
 - (i) Theme 1 - Settlement pattern: Element 3.4.7 Mitigation of hazards;
 - (ii) Theme 2 – Environment and landscape values: Element 3.5.5 Scenic amenity.
 - (b) enable an assessment of whether development is suitable on land within the Hillslopes sub-categories.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) development on hillslopes is safe, serviceable and accessible;
 - (b) the ecological values, landscape character and visual quality of the hillslopes are protected from development so as to retain the scenic backdrop to the region;
 - (c) Development on hillslopes is appropriate, having regard to the topographic constraints and environmental characteristics of the land;
 - (d) Development responds to the constraints of the site including gradient and slope stability;
 - (e) Works do not involve complex engineering solutions.

Criteria for assessment

Table Error! No text of specified style in document..a – Hillslopes overlay code –assessable development

Performance outcomes	Acceptable outcomes	Applicant response
For self-assessable development		

Performance outcomes	Acceptable outcomes	Applicant response
PO1 The landscape character and visual amenity quality of hillslopes areas is retained to protect the scenic backdrop to the region.	AO1.1 Development is located on parts of the site that are not within the Hillslopes constraint sub-category as shown on the Hillslopes overlay Maps contained in schedule 2.	N/A
For assessable development		

<p>PO2 The landscape character and visual amenity quality of hillslopes areas is retained to protect the scenic backdrop to the region.</p>	<p>A02.1 Development does not occur on land with a gradient in excess of 1 in 6 (16.6%)</p> <p>or</p> <p>A02.2 Where development on land steeper than 1 in 6 (16.6%) cannot be avoided, development follows the natural contours of the site.</p> <p>A02.3 Access ways and driveways are: (a) constructed with surface materials that blend with the surrounding environment; (b) landscaped with dense planting to minimise the visual impact of the construction; (c) provided with erosion control measures immediately after construction.</p> <p>A02.4 The clearing or disturbance of vegetation is limited to clearing and disturbance that: (a) is necessary for the construction of driveways; (b) is necessary to contain the proposed development; (c) minimises canopy clearing or disturbance; (d) minimises riparian clearing or disturbance.</p> <p>A02.5 On land with slopes greater than 1 in 6 (16.6%) or greater, alternative construction methods to concrete slab on ground are utilised (i.e. split level or post and beam constructed buildings that minimise modification to the natural terrain of the land).</p>	<p>Proposed development is stepped into the slope.</p> <p>Proposal complies.</p> <p>Proposal complies.</p> <p>Proposal comprises split level design which is stepped into the slope.</p>
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Performance outcomes	Acceptable outcomes	Applicant response
	<p>AO2.6 Development does not alter the sky line.</p> <p>AO2.7 Buildings and structures: (a) are finished predominantly in the following exterior colours or surfaces: (i) moderately dark to darker shades of olive green, brown, green, blue, or charcoal; or (ii) moderately dark to darker wood stains that blend with the colour and hues of the surrounding vegetation and landscape; (b) are not finished in the following exterior colours or surfaces: (i) pastel or terracotta colours, reds, yellows, shades of white or beige, or other bright colours that do not blend with the surrounding vegetation and landscape; (ii) reflective surfaces.</p> <p>AO2.8 Exterior colour schemes limit the use of white or other light colours to exterior trim and highlighting of architectural features.</p> <p>AO2.9 Areas between the first floor (including outdoor deck areas) and ground level are screened from view.</p> <p>AO2.10 Recreational or ornamental features (including tennis courts, ponds or swimming pools) do not occur on land:</p>	<p>Proposal does not break the ridgeline of Flagstaff Hill.</p> <p>Proposal comprises appropriate materials and earthy tones which are consistent with the existing character of the area, as well as rooftop and vertical landscaping. Note that the external finish is to be raw concrete.</p> <p>Proposal complies. External finish is to be raw concrete.</p> <p>Proposal complies.</p> <p>Proposal complies.</p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>except where a rectangular area of land of lesser grade is contained within the new lots to accommodate the intended land use, with the balance left in its natural state to the greatest extent possible.</p> <p>Note – The size of rectangular areas is outlined within each zone code.</p> <p>AO4.3 Development does not alter ridgelines.</p> <p>AO4.4 Lots are designed to ensure rooflines of future buildings and structures do not protrude above a ridgeline.</p>	<p>N/A</p> <p>N/A</p>

Natural areas overlay code

Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Natural areas overlay, if:
 - (a) self-assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
 - (b) impact assessable development.
- (2) Land in the Natural areas overlay is identified on the Natural areas overlay map in Schedule 2 and includes the following sub-categories:
 - (a) MSES – Protected area;
 - (b) MSES – Marine park;
 - (c) MSES – Wildlife habitat;
 - (d) MSES – Regulated vegetation;
 - (e) MSES – Regulated vegetation (intersecting a Watercourse);
 - (f) MSES – High ecological significance wetlands;
 - (g) MSES – High ecological value waters (wetlands);
 - (h) MSES – High ecological value waters (watercourse);
 - (i) MSES – Legally secured off set area.

Note – MSES = Matters of State Environmental Significance.

- (3) When using this code, reference should be made to Part 5.

Purpose

- (1) The purpose of the Natural areas overlay code is to:
 - (a) implement the policy direction in the Strategic Framework, in particular:
 - (i) Theme 2: Environment and landscape values, Element 3.5.3 Biodiversity, Element 3.5.4 Coastal zones;
 - (ii) Theme 3: Natural resource management Element 3.6.2 Land and catchment management, Element 3.6.3 Primary production, forestry and fisheries.
 - (b) enable an assessment of whether development is suitable on land within the Biodiversity area overlay sub-categories.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) development is avoided within:

- (i) areas containing matters of state environmental significance (MSES);
- (ii) other natural areas;
- (iii) wetlands and wetland buffers;
- (iv) waterways and waterway corridors.
- (b) where development cannot be avoided, development:
 - (i) protects and enhances areas containing matters of state environmental significance;
 - (ii) provides appropriate buffers;
 - (iii) protects the known populations and supporting habitat of rare and threatened flora and fauna species, as listed in the relevant State and Commonwealth legislation;
 - (iv) ensures that adverse direct or indirect impacts on areas of environmental significance are minimised through design, siting, operation, management and mitigation measures;
 - (v) does not cause adverse impacts on the integrity and quality of water in upstream or downstream catchments, including the Great Barrier Reef World Heritage Area;
 - (vi) protects and maintains ecological and hydrological functions of wetlands, waterways and waterway corridors;
 - (vii) enhances connectivity across barriers for aquatic species and habitats;
 - (viii) rehabilitates degraded areas to provide improved habitat condition, connectivity, function and extent;
 - (ix) protects areas of environmental significance from weeds, pests and invasive species.
- (c) strategic rehabilitation is directed to areas on or off site, where it is possible to achieve expanded habitats and increased connectivity.

Criteria for assessment

Table Error! No text of specified style in document..a – Natural areas overlay code – assessable development

Performance outcomes		Acceptable outcomes
For self-assessable and assessable development		
Protection of matters of environmental significance		
PO1 Development protects matters of environmental significance.	AO1.1 Development avoids significant impact on the relevant environmental values. or AO1.2 A report is prepared by an appropriately qualified person demonstrating to the satisfaction of the assessment manager, that the development site does	The subject site is located within an existing urban area and is intended to accommodate a residential dwelling, therefore the proposed disturbance of environmental values is considered to be warranted. The proposal seeks to utilise a flatter part of the site for the dwelling and this has dictated the overall design, however existing vegetation will be retained where practicable to minimise impacts.

Performance outcomes	Acceptable outcomes	
	<p>not contain any matters of state and local environmental significance.</p> <p>or</p> <p>AO1.3 Development is located, designed and operated to mitigate significant impacts on environmental values. For example, a report certified by an appropriately qualified person demonstrating to the satisfaction of the assessment manager, how the proposed development mitigates impacts, including on water quality, hydrology and biological processes.</p>	
Management of impacts on matters of environmental significance		
<p>PO2 Development is located, designed and constructed to avoid significant impacts on matters of environmental significance.</p>	<p>AO2 The design and layout of development minimises adverse impacts on ecologically important areas by: (a) focusing development in cleared areas to protect existing habitat; utilising design to consolidate density and preserve existing habitat and native vegetation; aligning new property boundaries to maintain ecologically important areas; ensuring that alterations to natural landforms, hydrology and drainage patterns on the development site do not negatively affect ecologically important areas; ensuring that significant fauna habitats are protected in their environmental context; and incorporating measures that allow for the safe movement of fauna through the site.</p>	<p>The proposal seeks to utilise a flatter part of the site for the dwelling and this has dictated the overall design, however existing vegetation will be retained where practicable to minimise impacts.</p>
<p>PO3</p>	<p>AO3.1</p>	<p>N/A</p>

Performance outcomes	Acceptable outcomes	
An adequate buffer to areas of state environmental significance is provided and maintained.	<p>A buffer for an area of state environmental significance (Wetland protection area) has a minimum width of:</p> <p>(a) 100 metres where the area is located outside Urban areas; or</p> <p>50 metres where the area is located within a Urban areas.</p> <p>or</p> <p>AO3.2 A buffer for an area of state environmental significance is applied and maintained, the width of which is supported by an evaluation of environmental values, including the function and threats to matters of environmental significance.</p>	
<p>PO4 Wetland and wetland buffer areas are maintained, protected and restored.</p> <p>Note – Wetland buffer areas are identified in AO3.1.</p>	<p>AO4.1 Native vegetation within wetlands and wetland buffer areas is retained.</p> <p>AO4.2 Degraded sections of wetlands and wetland buffer areas are revegetated with endemic native plants in patterns and densities which emulate the relevant regional ecosystem.</p>	N/A
<p>PO5 Development avoids the introduction of non-native pest species (plant or animal), that pose a risk to ecological integrity.</p>	<p>AO5.1 Development avoids the introduction of non-native pest species.</p> <p>AO5.2 The threat of existing pest species is controlled by adopting pest management practices for long-term ecological integrity.</p>	Proposal is capable of complying.

Performance outcomes		Acceptable outcomes
Ecological connectivity		
PO6 Development protects and enhances ecological connectivity and/or habitat extent.	AO6.1 Development retains native vegetation in areas large enough to maintain ecological values, functions and processes. and AO6.2 Development within an ecological corridor rehabilitates native vegetation. and AO6.3 Development within a conservation corridor mitigates adverse impacts on native fauna, feeding, nesting, breeding and roosting sites and native fauna movements.	Subject site is located in an existing urban area and on the periphery of a vegetated area, therefore loss of connectivity is minimal.
PO7 Development minimises disturbance to matters of state environmental significance (including existing ecological corridors).	AO7.1 Development avoids shading of vegetation by setting back buildings by a distance equivalent to the height of the native vegetation. and AO7.2 Development does not encroach within 10 metres of existing riparian vegetation and watercourses.	Proposal will not cause undue shading of vegetation.
Waterways in an urban area		

Performance outcomes	Acceptable outcomes	
PO8 Development is set back from waterways to protect and maintain: (a) water quality; hydrological functions; ecological processes; biodiversity values; riparian and in-stream habitat values and connectivity; in-stream migration.	AO8.1 Where a waterway is contained within an easement or a reserve required for that purpose, development does not occur within the easement or reserve; or AO8.2 Development does not occur on the part of the site affected by the waterway corridor. Note – Waterway corridors are identified within Table Error! No text of specified style in document..b.	N/A
Waterways in a non-urban area		
PO9 Development is set back from waterways to protect and maintain: (a) water quality; hydrological functions; ecological processes; biodiversity values; riparian and in-stream habitat values and connectivity; in-stream migration.	AO9 Development does not occur on that part of the site affected by a waterway corridor. Note – Waterway corridors are identified within Table Error! No text of specified style in document..b.	N/A

Table Error! No text of specified style in document..b — Widths of waterway corridors for waterways

Waterways classification	Waterway corridor width
Waterways in Urban areas	10 metres measured perpendicular from the top of the high bank.

Waterways in Other areas	For a dwelling house, 10 metres measured perpendicular from the top of the high bank. For all other development, 20 metres measured perpendicular from the top of the high bank.
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Access, parking and servicing code

Application

- (1) This code applies to assessing:
 - (a) operational work which requires a compliance assessment as a condition of a development permit; or
 - (b) a material change of use or reconfiguring a lot if:
 - (i) self-assessable or assessable development where this code is identified in the assessment criteria column of the table of assessment;
 - (ii) impact assessable development, to the extent relevant.
- (2) When using this code, reference should be made to Part 5.

Purpose

- (1) The purpose of the Access, parking and servicing code is to assess the suitability of access, parking and associated servicing aspects of a development.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) sufficient vehicle parking is provided on-site to cater for all types of vehicular traffic accessing and parking on-site, including staff, guests, patrons, residents and short term delivery vehicles;
 - (b) sufficient bicycle parking and end of trip facilities are provided on-site to cater for customer and service staff;
 - (c) on-site parking is provided so as to be accessible and convenient, particularly for any short term uses;
 - (d) development provides walking and cycle routes through the site which link the development to the external walking and cycling network;
 - (e) the provision of on-site parking, loading / unloading facilities and the provision of access to the site do not impact on the efficient function of street network or on the area in which the development is located;
 - (f) new vehicular access points are safely located and are not in conflict with the preferred ultimate streetscape character and local character and do not unduly disrupt any current or future on-street parking arrangements.

Criteria for assessment

Table Error! No text of specified style in document..a – Access, parking and servicing code – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
For self-assessable and assessable development		
PO1 Sufficient on-site car parking is provided to cater for the amount and type of vehicle traffic expected to be	AO1.1 The minimum number of on-site vehicle parking spaces is not less than the number prescribed in	Proposal exceeds requirements of Table Error! No text of specified style in document..b.

Performance outcomes	Acceptable outcomes	Applicant response
<p>generated by the use or uses of the site, having particular regard to:</p> <ul style="list-style-type: none"> (a) the desired character of the area; (b) the nature of the particular use and its specific characteristics and scale; (c) the number of employees and the likely number of visitors to the site; (d) the level of local accessibility; (e) the nature and frequency of any public transport serving the area; (f) whether or not the use involves the retention of an existing building and the previous requirements for car parking for the building; (g) whether or not the use involves a heritage building or place of local significance; (h) whether or not the proposed use involves the retention of significant vegetation. 	<p>Table Error! No text of specified style in document..b for that particular use or uses.</p> <p>Note - Where the number of spaces calculated from the table is not a whole number, the number of spaces provided is the next highest whole number.</p> <p>AO1.2 Car parking spaces are freely available for the parking of vehicles at all times and are not used for external storage purposes, the display of products or rented/sub-leased.</p> <p>AO1.3 Parking for motorcycles is substituted for ordinary vehicle parking to a maximum level of 2% of total ordinary vehicle parking.</p> <p>AO1.4 For parking areas exceeding 50 spaces parking, is provided for recreational vehicles as a substitute for ordinary vehicle parking to a maximum of 5% of total ordinary vehicle parking rate.</p>	<p>Proposal complies,</p> <p>N/A</p> <p>N/A</p>
<p>PO2 Vehicle parking areas are designed and constructed in accordance with relevant standards.</p>	<p>AO2 Vehicle parking areas are designed and constructed in accordance with Australian Standard: (a) AS2890.1; AS2890.3; AS2890.6.</p>	<p>Proposal complies or is capable of complying.</p>
<p>PO3 Access points are designed and constructed: (a) to operate safely and efficiently;</p>	<p>AO3.1</p>	<p>Subject site includes challenging topography within Murphy Street frontage so requires a custom solution as per proposed plans.</p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>directed into the hill, for vehicle safety and drainage purposes;</p> <p>(d) constructed such that the transitional change in grade from the road to the lot is fully contained within the lot and not within the road reserve;</p> <p>(e) designed to include all necessary associated drainage that intercepts and directs storm water runoff to the storm water drainage system.</p> <p>AO3.4 Surface construction materials are consistent with the current or intended future streetscape or character of the area and contrast with the surface construction materials of any adjacent footpath.</p>	Proposal is capable of complying.
<p>P04 Sufficient on-site wheel chair accessible car parking spaces are provided and are identified and reserved for such purposes.</p>	<p>AO4 The number of on-site wheel chair accessible car parking spaces complies with the rates specified in AS2890 Parking Facilities.</p>	N/A
<p>P05 Access for people with disabilities is provided to the building from the parking area and from the street.</p>	<p>AO5 Access for people with disabilities is provided in accordance with the relevant Australian Standard.</p>	N/A
<p>P06 Sufficient on-site bicycle parking is provided to cater for the anticipated demand generated by the development.</p>	<p>AO6 The number of on-site bicycle parking spaces complies with the rates specified in</p> <p>Table Error! No text of specified style in document..b.</p>	N/A
P07	AO7.1	N/A

Performance outcomes	Acceptable outcomes	Applicant response
<p>Development provides secure and convenient bicycle parking which:</p> <ul style="list-style-type: none"> (a) for visitors is obvious and located close to the building's main entrance; (b) for employees is conveniently located to provide secure and convenient access between the bicycle storage area, end-of-trip facilities and the main area of the building; (c) is easily and safely accessible from outside the site. 	<p>Development provides bicycle parking spaces for employees which are co-located with end-of-trip facilities (shower cubicles and lockers);</p> <p>A07.2 Development ensures that the location of visitor bicycle parking is discernible either by direct view or using signs from the street.</p> <p>A07.3 Development provides visitor bicycle parking which does not impede pedestrian movement.</p>	<p>N/A</p> <p>N/A</p>
<p>PO8 Development provides walking and cycle routes through the site which:</p> <ul style="list-style-type: none"> (a) link to the external network and pedestrian and cyclist destinations such as schools, shopping centres, open space, public transport stations, shops and local activity centres along the safest, most direct and convenient routes; (b) encourage walking and cycling; (c) ensure pedestrian and cyclist safety. 	<p>AO8 Development provides walking and cycle routes which are constructed on the carriageway or through the site to:</p> <ul style="list-style-type: none"> (a) create a walking or cycle route along the full frontage of the site; (b) connect to public transport and existing cycle and walking routes at the frontage or boundary of the site. 	<p>N/A</p>
<p>PO9 Access, internal circulation and on-site parking for service vehicles are designed and constructed:</p> <ul style="list-style-type: none"> (a) in accordance with relevant standards; (b) so that they do not interfere with the amenity of the surrounding area; (c) so that they allow for the safe and convenient movement of pedestrians, cyclists and other vehicles. 	<p>AO9.1 Access driveways, vehicle manoeuvring and on-site parking for service vehicles are designed and constructed in accordance with AS2890.1 and AS2890.2.</p> <p>AO9.2 Service and loading areas are contained fully within the site.</p> <p>AO9.3 The movement of service vehicles and service operations are designed so they:</p>	<p>N/A</p> <p>N/A</p> <p>N/A</p>

Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
Agricultural supplies store	1 space per 50m ² of GFA and outdoor display area.	1 space per 200m ² of GFA.	n/a	LRV
Air services	1 car space per 20m ² of covered reception area, plus 1 car space per 2 staff, plus a covered bus set down area adjacent to the entry of the reception area and 2 bus parking spaces.	n/a	n/a	LRV
Bulk landscape supplies	1 space per 50m ² GFA and outdoor display area.	1 space per 200m ² of GFA.	n/a	MRV
Caretaker's accommodation	A minimum of 1 space	n/a	n/a	n/a
Child care centre	1 space per 10 children to be used for setting down and picking up of children, with a minimum of 3 car spaces to be provided for set down and collection; plus 1 space per employee. Any drive-through facility can provide tandem short term parking for 3 car spaces for setting down/picking up of children, on the basis that a passing lane is provided and line-marked to be kept clear of standing vehicles at all times.	n/a	n/a	VAN
Club	Unlicensed clubrooms: 1 space per 45m ² of GFA. Licensed clubrooms: 1 space per 15m ² of GFA.	1 space per 4 employees.	n/a	Licensed and equal or greater than 1500m ² : RCV Other: VAN
Community care centre	1 space per 20m ² of GFA.	A minimum of 1 space.	n/a	RCV
Community residence	A minimum of 2 spaces.	A minimum of 1 space.	n/a	VAN
Community use	1 space per 15m ² GFA.	1 space per 100m ² of GFA.	n/a	RCV

Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
Dual occupancy	A minimum of 2 spaces per dwelling unit which may be in tandem with a minimum of 1 covered space per dwelling unit.	n/a	n/a	n/a
Dwelling house	A minimum of 2 spaces which may be in tandem plus 1 space for a secondary dwelling	n/a	n/a	n/a
Dwelling unit	1.5 spaces per one or two bedroom unit; or 2 spaces per three bedroom unit.	n/a	n/a	n/a
Educational establishment	Primary school or secondary schools: 1 car space per 2 staff members, plus provision of space to be used for setting down and picking up of students. Tertiary and further education: 1 car space per 2 staff members, plus 1 car space per 10 students, plus provision of space to be used for setting down and picking up of students.	Primary school or secondary schools: 1 space per 5 students over year 4. Tertiary and further education: 2 spaces per 50 full time students.	Required for all educational establishments with a GFA greater than 2000m ² .	RCV
Food and drink outlet	1 space per 25m ² GFA and outdoor dining area. or If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m ² of GFA, and outdoor dining area.	1 space per 100m ² of GFA, and outdoor dining area.	n/a	See Table Error! No text of specified style in document..d
Function facility	1 space per 15m ² GFA.	1 space per 100m ² of GFA.	n/a	RCV
Funeral parlour	1 space per 15m ² GFA.	n/a	n/a	RCV
Garden centre	1 space per 50m ² GFA and outdoor display area	1 space per 200m ² of GFA.	n/a	AV
Hardware and trade supplies	1 space per 50m ² GFA and outdoor display area	1 space per 200m ² of GFA.	n/a	AV

Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
Health care services	1 space per 20m ² of GFA.	1 space per 100m ² of GFA.	Required for all health care services with a GFA greater than 2000m ² .	VAN
High impact industry	1 space per 90m ² of GFA.	n/a	n/a	AV
Home based business	The parking required for the dwelling house, plus 1 space per bedroom where the Home based business involves the provision of accommodation; or 1 space per 25m ² GFA for any other Home Based Business.	n/a	n/a	n/a
Hospital	The greater of 1 space per 2 bedrooms or 1 space per 4 beds; plus 1 car space for ambulance parking, designated accordingly.	1 space per 100m ² of GFA.	Required for all hospitals with a GFA greater than 2000m ² .	RCV
Hotel	1 space per 10m ² GFA and licensed outdoor area; plus For 1 space per 50m ² GFA of floor area of liquor barn or bulk liquor sales area; plus, if a drive in bottle shop is provided, queuing lane/s on site for 12 vehicles. Note - Use standard for any Short Term Accommodation for hotel accommodation use.	1 space per 100m ² of GFA.	n/a	LRV
Indoor sport and recreation	Squash court or another court game: 4 spaces per court. Basketball, netball, soccer, cricket: 25 spaces per court / pitch. Ten pin bowling: 3 spaces per bowling lane. Gymnasium: 1 space per 15m ² of GFA.	1 space per 4 employees.	n/a	RCV
Low impact industry	1 space per 90m ² of GFA.	n/a	n/a	AV

Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
Marine industry	1 space per 90m ² of GFA.	n/a	n/a	AV
Medium impact industry	1 space per 90m ² of GFA.	n/a	n/a	AV
Multiple dwelling	If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie Local plan: 1 car space per dwelling unit. If outside Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie Local plan: 1.5 car spaces per dwelling unit In all cases 60% of the car parking area is to be covered.	1 bicycle space per 3 units and 1 visitor bicycle space per 12 units.	n/a	RCV (over 10 units)
Office	1 space per 25m ² of GFA or If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m ² of GFA	1 space per 200m ² GFA	Required for all office development with a GFA greater than 2000m ² .	See Table Error! No text of specified style in document..e
Outdoor sales	1 space per 50m ² GFA and outdoor display area	1 space per 200m ² of GFA.	n/a	AV
Outdoor sport and recreation	Coursing, horse racing, pacing, trotting: 1 space per 5 seated spectators, plus 1 space per 5m ² of other spectator areas. Football: 50 spaces per field. Lawn bowls: 30 spaces per green. Swimming pool: 15 spaces; plus	Football: 5 space per field. Lawn bowls: 5 spaces per green. Swimming pool: 1 space per swimming lane. Tennis court or other court game:	n/a	RCV

Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
	<p>1 space per 100m² of useable site area.</p> <p>Tennis court or other court game: 4 spaces per court. Golf course: 4 spaces per tee on the course.</p> <p>Note - Use standard for Club for clubhouse component.</p>	<p>4 space per court.</p> <p>Golf course: 1 space per 15m² of GFA for clubhouse component.</p>		
Place of worship	1 space per 15m ² of GFA.	1 space per 100m ² of GFA.	n/a	LRV
Relocatable home park	1 space per relocatable home site; plus 0.1 space per relocatable home site for visitor parking; plus 1 space for an on-site manager	n/a	n/a	LRV
Research and technology industry	1 space per 90m ² of GFA.	n/a	n/a	MRV
Residential care facility	1 visitor car space per 5 bedroom units; plus 1 car space per 2 staff members	n/a	n/a	LRV
Resort complex	<p>Use standard for relevant standard for each component.</p> <p>For example: Use Short Term Accommodation standard for accommodation component and Food and Drink Outlet for restaurant component.</p>	<p>Use standard for relevant standard for each component.</p> <p>For example: Use Short Term Accommodation standard for accommodation component and Food and Drink Outlet for restaurant component.</p>	n/a	RCV
Retirement facility	1 space per dwelling unit;	n/a	n/a	LRV

Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
	plus 1 visitor space per 5 dwelling units; plus 1 visitor car space per 10 hostel units, nursing home or similar beds, plus 1 car space per 2 staff members; plus 1 car parking space for ambulance parking.			
Sales office	A minimum of 1 space.	n/a	n/a	n/a
Service industry	1 space per 90m ² of GFA.	n/a	n/a	SRV
Service station	1 space per 25m ² of GFA	n/a	n/a	AV
Shop	1 space per 25m ² of GFA. or If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m ² of GFA.	1 space per 100m ² of GFA.	Required for all shops with a GFA greater than 2000m ² .	See Table Error! No text of specified style in document..d
Shopping centre	1 space per 25m ² of GFA. or If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m ² of GFA.	1 space per 200m ² GFA.	Required for all shopping centres with a GFA greater than 2000m ² .	See Table Error! No text of specified style in document..d
Short term accommodation	If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan: 0.5 car spaces per dwelling unit. If outside Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan: For up to 5 units: 1 car space per dwelling unit, plus 1 space for visitors and 1 service/staff spaces. For 5 – 10 units: 1 car space per dwelling unit, plus 2 spaces for visitors and 1 service/staff spaces.	1 space per 10 rooms	n/a	SRV

Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
	<p>For over 10 units: 0.75 car spaces per dwelling unit, plus 3 spaces for visitors and 2 service/staff parking for the first 10 units and 0.5 additional service/staff space per 10 units, there-above.</p> <p>In all cases 60% of the car parking area is to be covered.</p> <p>Note: Where Short term accommodation is to be inter-changeable with a Multiple dwelling land use, multiple dwelling parking rates apply.</p>			
Showroom	1 space per 50m ² GFA.	1 space per 200m ² GFA.	n/a	AV
Special industry	1 space per 90m ² of GFA.	n/a	n/a	AV
Tourist park	1 car space per caravan site, tent site or cabin; plus 1 visitor car space per 10 caravan sites, tent sites or cabins; plus 1 car space for an on-site manager.	n/a	n/a	LRV
Theatre	<p>Indoor: 1 space per 15m² of GFA.</p> <p>Outdoor cinema: 1 space per 5m² of designated viewing area, plus 1 car space per 2 employees.</p>	1 space per 200m ² GFA.	n/a	VAN
Veterinary services	1 space per 50m ² of GFA.	n/a	n/a	VAN
Warehouse	1 space per 90m ² of GFA.	n/a	n/a	Where self-storage: RCV Other: AV
Any use not otherwise specified in this table.	Sufficient spaces to accommodate number of vehicles likely to be parked at any one time.	Sufficient spaces to accommodate number		To be determined

Land use	Minimum number of ordinary vehicle parking spaces	Minimum number of bicycle spaces	End of trip facilities	Minimum standard design service vehicle (refer to Table 9.4.1.3c)
		of vehicles likely to be parked at any one time.		

Table Error! No text of specified style in document..c – Design vehicles

VAN	A 99.8th percentile vehicle equivalent to a large car.
SRV	Small rigid vehicle as in AS2890.2-2002 parking facilities – Off-street commercial vehicle facilities, but incorporating a body width of 2.33m
MRV	Medium rigid vehicle equivalent to an 8-tonne truck.
LRV	Large rigid vehicle described by AS2890.2-2002 parking facilities – Off-street commercial vehicle facilities as heavy rigid vehicle.
RCV	Industrial refuse collection vehicle
AV	19 metre articulated vehicle from AUSTROADS

Table Error! No text of specified style in document..d – Standard number of service bays required for Food and drink outlet, Shop or Shopping centre

Gross floor area (m ²)	Service bays required			
	VAN	SRV	MRV	LRV
0-199	-	1	-	-
200 – 599	1	-	1	-
600 – 999	1	1	1	-
1000 – 1499	2	1	1	-
1500 – 1999	2	2	1	-
2000 – 2799	2	2	2	-
2800 – 3599	2	2	2	1
3600 and over	To be determined via a parking study.			

Table Error! No text of specified style in document..e – Standard number of service bays required for Office

Gross floor area (m ²)	Service bays required			
	VAN	SRV	MRV	LRV
0-999	-	1	-	-
1000 – 2499	1	-	1	-
2500 – 3999	2	1	1	-
4000 – 5999	3	1	1	-
6000 – 7999	4	1	1	-
8000 – 9999	4	2	1	-
10000 and over	To be determined via a parking study.			

Environmental management zone code

Application

- (1) This code applies to assessing development in the Environmental management zone.
- (2) When using this code, reference should be made to Part 5.

Purpose

- (1) The purpose of the Environmental management zone code is to recognise environmentally sensitive areas and provide for houses on lots and other low impact activities where suitable.

These areas are protected from intrusion of any urban, suburban, centre or industrial land use.

- (2) The local government purpose of the code is to:
 - (a) implement the policy direction set in the Strategic Framework, in particular:
 - (i) Theme 2 : Environment and landscape values, Element 3.5.3 – Biodiversity, Element 3.5.5 – Scenic amenity.
 - (b) protect and buffer areas of environmental significance from inappropriate development.
- (3) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Development is generally restricted to a dwelling house;
 - (b) Adverse impacts on natural systems, both on-site and on adjoining land are minimised through the location, design and management of development;
 - (c) Development reflects and responds to the natural features and environmental values of the area;
 - (d) Visual impacts are minimised through the location and design of development;
 - (e) Development does not adversely affect water quality;
 - (f) Development responds to land constraints, including but not limited to topography, vegetation, bushfire, landslide and flooding.

Criteria for assessment

Table Error! No text of specified style in document..a – Environmental management zone – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
For self-assessable and assessable development		

Performance outcomes	Acceptable outcomes	Applicant response
PO1 The height of all buildings and structures is in keeping with the natural characteristics of the site. Buildings and structures are low-rise and not unduly visible from external sites.	AO1.1 Buildings and structures are not more than 8.5 metres and two storeys in height. Note – Height is inclusive of the roof height. AO1.2 Buildings have a roof height not less than 2 metres.	Refer to Section 3.0 of the Planning Report for discussion.
PO2 Buildings and structures are set back to: (a) maintain the natural character of the area; (b) achieve separation from neighbouring buildings and from road frontages.	AO2 Buildings and structures are set back not less than: (a) 40 metres from the frontage of a state controlled road; (b) 25 metres from the frontage to Cape Tribulation Road; (c) 6 metres from any other road; (d) 6 metres from the side and rear boundaries of the site.	Proposed development is set back 3m from side boundaries which is considered appropriate given the site and surrounding pattern of development.
For assessable development		
PO3 Development is consistent with the purpose of the Environmental management zone and protects the zone from the intrusion of inconsistent uses.	AO3 Inconsistent uses as identified in Table Error! No text of specified style in document..b are not established in the Environmental management zone.	N/A
PO4 The site coverage of all buildings and structures and associated services do not have an adverse effect on the environmental or scenic values of the site.	PO4 No acceptable outcomes are prescribed.	Proposal includes a low site cover of 23.7%.
PO5 Development is located, designed, operated and managed to respond to the characteristics,	AO5.1 Buildings, structures and associated access, infrastructure and private open space are sited:	All of the site is vegetated. Proposed development is located on the flatter part of the site.

Performance outcomes	Acceptable outcomes	Applicant response
<p>features and constraints of the site and its surrounds.</p> <p>Note - Planning scheme policy – Site assessments provides guidance on identifying the characteristics, features and constraints of a site and its surrounds.</p>	<p>(a) within areas of the site which are already cleared; or</p> <p>(b) within areas of the site which are environmentally degraded;</p> <p>(c) to minimise additional vegetation clearing.</p> <p>AO5.2 Buildings and structures and associated infrastructure are not located on slopes greater than 1 in 6 (16.6%) or on a ridgeline.</p>	Refer below.
<p>PO6 Buildings and structures are responsive to steep slope through innovative construction techniques so as to:</p> <p>(a) maintain the geotechnical stability of slopes;</p> <p>(b) minimise cut and/or fill;</p> <p>(c) minimise the overall height of development.</p>	<p>AO6.1 Where development on land steeper than 1 in 6 (16.6%) cannot be avoided, development follows the natural contours of the land and single plane concrete slab on-ground methods of construction are not utilised.</p> <p>AO6.2 Access and vehicle manoeuvring and parking areas are constructed and maintained to:</p> <p>(a) minimise erosion;</p> <p>(b) minimise cut and fill;</p> <p>(c) follow the natural contours of the site.</p>	<p>Proposed development is located on the flatter part of the site and comprises a split-level design which is stepped into the slope.</p> <p>Proposed driveway follows the site contours as far as practicable, noting the challenging topography within Murphy Street.</p>
<p>PO7 The exterior finishes of buildings and structures are consistent with the surrounding natural environment.</p>	<p>PO7 The exterior finishes and colours of buildings and structures are non-reflective and are moderately dark to darker shades of grey, green, blue and brown or the development is not visible external to the site.</p>	Proposal complies. Rooftop and vertical landscaping is also employed.
<p>PO8 Development does not adversely affect the amenity of the zone and adjoining land uses in terms of traffic, noise, dust, odour, lighting or other physical or environmental impacts.</p>	<p>AO8 No acceptable outcomes are prescribed.</p>	Proposal complies.

Performance outcomes	Acceptable outcomes	Applicant response
PO9 The density of development ensures that the environmental and scenic amenity values of the site and surrounding area are not adversely affected.	AO9 The maximum residential density is one dwelling house per lot.	Proposal complies.
PO10 Lot reconfiguration results in no additional lots. Note - Boundary realignments to resolve encroachments and lot amalgamation are considered appropriate.	AO10 No acceptable outcomes are prescribed.	N/A

Table Error! No text of specified style in document..b — Inconsistent uses within the Environmental management zone

Inconsistent uses		
<ul style="list-style-type: none"> • Adult store • Agricultural supplies store • Air services • Aquaculture • Bar • Brothel • Bulk landscape supplies • Car wash • Caretaker's accommodation • Cemetery • Child care centre • Club • Community care centre • Community residence • Community use • Crematorium • Cropping • Detention facility • Dual occupancy 	<ul style="list-style-type: none"> • Hardware and trade supplies • Health care services • High impact industry • Hospital • Hotel • Indoor sport and entertainment • Intensive animal industry • Intensive horticulture • Landing • Low impact industry • Major electricity infrastructure • Major sport, recreation and entertainment facility • Marine industry • Market • Motor sport facility • Multiple dwelling • Nightclub entertainment facility • Office 	<ul style="list-style-type: none"> • Renewable energy facility • Relocatable home park • Research and technology industry • Residential care facility • Resort complex • Retirement facility • Rooming accommodation • Rural industry • Rural workers accommodation • Sales office • Service Station • Shop • Shopping centre • Short-term accommodation • Showroom • Special industry • Substation • Theatre • Transport depot

<ul style="list-style-type: none"> • Dwelling unit • Educational establishment • Food and drink outlet • Function facility • Garden centre 	<ul style="list-style-type: none"> • Outdoor sales • Outstation • Parking station • Place of worship • Port services 	<ul style="list-style-type: none"> • Utility installation • Veterinary services • Warehouse • Wholesale nursery • Winery
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Note – This table does not imply that all other uses not listed in the table are automatically consistent uses within the zone. Assessable development must still demonstrate consistency through the assessment process.

Infrastructure works code

Application

- (1) This code applies to assessing:
 - (a) operational work which requires an assessment as a condition of a development permit or is assessable development if this code is identified in the assessment criteria column of a table of assessment;
 - (b) a material change of use or reconfiguring a lot if:
 - (i) assessable development where this code is identified in the assessment criteria column of the table of assessment;
 - (ii) impact assessable development, to the extent relevant.

Note – The Filling and excavation code applies to operational work for filling and excavation.

- (2) When using this code, reference should be made to Part 5.

Purpose

- (1) The purpose of the Infrastructure works code is to ensure that development is safely and efficiently serviced by, and connected to, infrastructure.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) the standards of water supply, waste water treatment and disposal, stormwater drainage, local electricity supply, telecommunications, footpaths and road construction meet the needs of development and are safe and efficient;
 - (b) development maintains high environmental standards;
 - (c) development is located, designed, constructed and managed to avoid or minimise impacts arising from altered stormwater quality or flow, wastewater discharge, and the creation of non-tidal artificial waterways;
 - (d) the integrity of existing infrastructure is maintained;
 - (e) development does not detract from environmental values or the desired character and amenity of an area.

Criteria for assessment

Table Error! No text of specified style in document..a – Infrastructure works code –assessable development

Performance outcomes	Acceptable outcomes	Applicant response
For self-assessable and assessable development		
Works on a local government road		

Performance outcomes	Acceptable outcomes	Applicant response
<p>PO1 Works on a local government road do not adversely impact on footpaths or existing infrastructure within the road verge and maintain the flow, safety and efficiency of pedestrians, cyclists and vehicles.</p>	<p>AO1.1 Footpaths/pathways are located in the road verge and are provided for the hierarchy of the road and located and designed and constructed in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual.</p> <p>AO1.2 Kerb ramp crossovers are constructed in accordance with Planning scheme policy SC 5 – FNQROC Regional Development Manual.</p> <p>AO1.3 New pipes, cables, conduits or other similar infrastructure required to cross existing footpaths: (a) are installed via trenchless methods; or (b) where footpath infrastructure is removed to install infrastructure, the new section of footpath is installed to the standard detailed in the Planning scheme policy SC5 – FNQROC Regional Development Manual, and is not less than a 1.2 metre section.</p> <p>AO1.4 Where existing footpaths are damaged as a result of development, footpaths are reinstated ensuring: (a) similar surface finishes are used; (b) there is no change in level at joins of new and existing sections; (c) new sections are matched to existing in terms of dimension and reinforcement.</p>	<p>N/A</p> <p>Subject site includes challenging topography within Murphy Street frontage so requires a custom solution as per proposed plans.</p> <p>Proposal is capable of complying.</p> <p>N/A</p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>Note – Figure Error! No text of specified style in document..a provides guidance on meeting the outcomes.</p> <p>A01.5 Decks, verandahs, stairs, posts and other structures located in the road reserve do not restrict or impede pedestrian movement on footpaths or change the level of the road verges.</p>	N/A
Accessibility structures		
<p>PO2 Development is designed to ensure it is accessible for people of all abilities and accessibility features do not impact on the efficient and safe use of footpaths.</p> <p>Note – Accessibility features are those features required to ensure access to premises is provided for people of all abilities and include ramps and lifts.</p>	<p>A02.1 Accessibility structures are not located within the road reserve.</p> <p>A02.2 Accessibility structures are designed in accordance with AS1428.3.</p> <p>A02.3 When retrofitting accessibility features in existing buildings, all structures and changes in grade are contained within the boundaries of the lot and not within the road reserve.</p>	<p>N/A</p> <p>N/A</p> <p>N/A</p>
Water supply		
<p>PO3 An adequate, safe and reliable supply of potable, fire fighting and general use water is provided.</p>	<p>A03.1 The premises is connected to Council's reticulated water supply system in accordance with the Design Guidelines set out in Section D6 of the Planning scheme policy SC5 – FNQROC Regional Development Manual;</p> <p>or</p> <p>A03.2</p>	Proposal is capable of complying.

Performance outcomes	Acceptable outcomes	Applicant response
	Where a reticulated water supply system is not available to the premises, on site water storage tank/s with a minimum capacity of 10,000 litres of stored water, with a minimum 7,500 litre tank, with the balance from other sources (e.g. accessible swimming pool, dam etc.) and access to the tank/s for fire trucks is provided for each new house or other development. Tank/s are to be fitted with a 50mm ball valve with a camlock fitting and installed and connected prior to occupation of the house and sited to be visually unobtrusive.	
Treatment and disposal of effluent		
PO4 Provision is made for the treatment and disposal of effluent to ensure that there are no adverse impacts on water quality and no adverse ecological impacts as a result of the system or as a result of increasing the cumulative effect of systems in the locality.	AO4.1 The site is connected to Council's sewerage system and the extension of or connection to the sewerage system is designed and constructed in accordance with the Design Guidelines set out in Section D7 of the Planning scheme policy SC5 – FNQROC Regional Development Manual; or AO4.2 Where not in a sewerage scheme area, the proposed disposal system meets the requirements of Section 33 of the <i>Environmental Protection Policy (Water) 1997</i> and the proposed on site effluent disposal system is designed in accordance with the <i>Plumbing and Drainage Act (2002)</i> .	Proposal is capable of complying.
Stormwater quality		

Performance outcomes	Acceptable outcomes	Applicant response
<p>PO5 Development is planned, designed, constructed and operated to avoid or minimise adverse impacts on stormwater quality in natural and developed catchments by:</p> <ul style="list-style-type: none"> (a) achieving stormwater quality objectives; (b) protecting water environmental values; (c) maintaining waterway hydrology. 	<p>AO5.1 A connection is provided from the premises to Council's drainage system;</p> <p>or</p> <p>AO5.2 An underground drainage system is constructed to convey stormwater from the premises to Council's drainage system in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p>	<p>Proposal is capable of complying.</p>
	<p>AO5.3 A stormwater quality management plan is prepared, and provides for achievable stormwater quality treatment measures meeting design objectives listed in Table Error! No text of specified style in document..b and Table Error! No text of specified style in document..c, reflecting land use constraints, such as:</p> <ul style="list-style-type: none"> (a) erosive, dispersive and/or saline soil types; (b) landscape features (including landform); (c) acid sulfate soil and management of nutrients of concern; (d) rainfall erosivity. 	<p>Proposal is capable of complying.</p>
	<p>AO5.4 Erosion and sediment control practices are designed, installed, constructed, monitored, maintained, and carried out in accordance with an erosion and sediment control plan.</p>	<p>Proposal is capable of complying.</p>
	<p>AO5.5</p>	<p>Proposal is capable of complying.</p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>Development incorporates stormwater flow control measures to achieve the design objectives set out in Table Error! No text of specified style in document..b and Table Error! No text of specified style in document..c, including management of frequent flows, peak flows, and construction phase hydrological impacts.</p> <p>Note – Planning scheme policy SC5 – FNQROC Regional Development Manual provides guidance on soil and water control measures to meet the requirements of the <i>Environmental Protection Act 1994</i>.</p> <p>Note – During construction phases of development, contractors and builders are to have consideration in their work methods and site preparation for their environmental duty to protect stormwater quality.</p>	
Non-tidal artificial waterways		
<p>PO6 Development involving non-tidal artificial waterways is planned, designed, constructed and operated to:</p> <ul style="list-style-type: none"> (a) protect water environmental values; (b) be compatible with the land use constraints for the site for protecting water environmental values; (c) be compatible with existing tidal and non-tidal waterways; (d) perform a function in addition to stormwater management; (e) achieve water quality objectives. 	<p>AO6.1 Development involving non-tidal artificial waterways ensures:</p> <ul style="list-style-type: none"> (a) environmental values in downstream waterways are protected; (b) any ground water recharge areas are not affected; (c) the location of the waterway incorporates low lying areas of the catchment connected to an existing waterway; (d) existing areas of ponded water are included. <p>AO6.2 Non-tidal artificial waterways are located:</p> <ul style="list-style-type: none"> (a) outside natural wetlands and any associated buffer areas; (b) to minimise disturbing soils or sediments; 	<p>N/A</p> <p>N/A</p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>(c) to avoid altering the natural hydrologic regime in acid sulfate soil and nutrient hazardous areas.</p> <p>AO6.3 Non-tidal artificial waterways located adjacent to, or connected to a tidal waterway by means of a weir, lock, pumping system or similar ensures:</p> <p>(a) there is sufficient flushing or a tidal range of >0.3 m; or</p> <p>(b) any tidal flow alteration does not adversely impact on the tidal waterway; or</p> <p>(c) there is no introduction of salt water into freshwater environments.</p> <p>AO6.4 Non-tidal artificial waterways are designed and managed for any of the following end-use purposes:</p> <p>(a) amenity (including aesthetics), landscaping or recreation; or</p> <p>(b) flood management, in accordance with a drainage catchment management plan; or</p> <p>(c) stormwater harvesting plan as part of an integrated water cycle management plan; or</p> <p>(d) aquatic habitat.</p> <p>AO6.5 The end-use purpose of the non-tidal artificial waterway is designed and operated in a way that protects water environmental values.</p> <p>AO6.6 Monitoring and maintenance programs adaptively manage water quality to achieve</p>	<p>N/A</p> <p>N/A</p> <p>N/A</p> <p>N/A</p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>relevant water quality objectives downstream of the waterway.</p> <p>AO6.7 Aquatic weeds are managed to achieve a low percentage of coverage of the water surface area, and pests and vectors are managed through design and maintenance.</p>	N/A
Wastewater discharge		
<p>PO7 Discharge of wastewater to waterways, or off site: (a) meets best practice environmental management; (b) is treated to:</p> <ul style="list-style-type: none"> (i) meet water quality objectives for its receiving waters; (ii) avoid adverse impact on ecosystem health or waterway health; (iii) maintain ecological processes, riparian vegetation and waterway integrity; (iv) offset impacts on high ecological value waters. 	<p>AO7.1 A wastewater management plan is prepared and addresses:</p> <ul style="list-style-type: none"> (a) wastewater type; (b) climatic conditions; (c) water quality objectives; (d) best practice environmental management. <p>AO7.2 The waste water management plan is managed in accordance with a waste management hierarchy that:</p> <ul style="list-style-type: none"> (a) avoids wastewater discharge to waterways; or (b) if wastewater discharge cannot practicably be avoided, minimises wastewater discharge to waterways by re-use, recycling, recovery and treatment for disposal to sewer, surface water and ground water. <p>AO7.3 Wastewater discharge is managed to avoid or minimise the release of nutrients of concern so as to minimise the occurrence, frequency and intensity of algal blooms.</p>	<p>N/A</p> <p>N/A</p> <p>N/A</p>

Performance outcomes	Acceptable outcomes	Applicant response
	<p>A07.4 Development in coastal catchments avoids or minimises and appropriately manages soil disturbance or altering natural hydrology and:</p> <ul style="list-style-type: none"> (a) avoids lowering ground water levels where potential or actual acid sulfate soils are present; (b) manages wastewater so that: <ul style="list-style-type: none"> (i) the pH of any wastewater discharges is maintained between 6.5 and 8.5 to avoid mobilisation of acid, iron, aluminium and other metals; (ii) holding times of neutralised wastewater ensures the flocculation and removal of any dissolved iron prior to release; (iii) visible iron floc is not present in any discharge; (iv) precipitated iron floc is contained and disposed of; (v) wastewater and precipitates that cannot be contained and treated for discharge on site are removed and disposed of through trade waste or another lawful method. 	N/A
Electricity supply		
<p>PO8 Development is provided with a source of power that will meet its energy needs.</p>	<p>A08.1 A connection is provided from the premises to the electricity distribution network;</p> <p>or</p> <p>A08.2 The premises is connected to the electricity distribution network in accordance with the Design Guidelines set out in Section D8 of the</p>	Proposal is capable of complying.

Performance outcomes	Acceptable outcomes	Applicant response
	<p>Planning scheme policy SC5 – FNQROC Regional Development Manual.</p> <p>Note - Areas north of the Daintree River have a different standard.</p>	
<p>PO9 Development incorporating pad-mount electricity infrastructure does not cause an adverse impact on amenity.</p>	<p>AO9.1 Pad-mount electricity infrastructure is: (a) not located in land for open space or sport and recreation purposes; (b) screened from view by landscaping or fencing; (c) accessible for maintenance.</p> <p>AO9.2 Pad-mount electricity infrastructure within a building, in a Town Centre is designed and located to enable an active street frontage.</p> <p>Note – Pad-mounts in buildings in activity centres should not be located on the street frontage.</p>	<p>Proposal is capable of complying.</p> <p>N/A</p>
Telecommunications		
<p>PO10 Development is connected to a telecommunications service approved by the relevant telecommunication regulatory authority.</p>	<p>AO10 The development is connected to telecommunications infrastructure in accordance with the standards of the relevant regulatory authority.</p>	<p>Proposal is capable of complying.</p>
<p>PO11 Provision is made for future telecommunications services (e.g. fibre optic cable).</p>	<p>AO11 Conduits are provided in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual.</p>	<p>Proposal is capable of complying.</p>
Road construction		
<p>PO12</p>	<p>AO12.1</p>	<p>Proposal is capable of complying.</p>

Performance outcomes	Acceptable outcomes	Applicant response
<p>The road to the frontage of the premises is constructed to provide for the safe and efficient movement of:</p> <ul style="list-style-type: none"> (a) pedestrians and cyclists to and from the site; (b) pedestrians and cyclists adjacent to the site; (c) vehicles on the road adjacent to the site; (d) vehicles to and from the site; (e) emergency vehicles. 	<p>The road to the frontage of the site is constructed in accordance with the Design Guidelines set out in Sections D1 and D3 of the Planning scheme policy SC5 – FNQROC Regional Development Manual, for the particular class of road, as identified in the road hierarchy.</p> <p>AO12.2 There is existing road, kerb and channel for the full road frontage of the site.</p> <p>AO12.3 Road access minimum clearances of 3.5 metres wide and 4.8 metres high are provided for the safe passage of emergency vehicles.</p>	<p>N/A</p> <p>Proposal is capable of complying.</p>
Alterations and repairs to public utility services		
<p>PO13 Infrastructure is integrated with, and efficiently extends, existing networks.</p>	<p>AO13 Development is designed to allow for efficient connection to existing infrastructure networks.</p>	<p>Proposal is capable of complying.</p>
<p>PO14 Development and works do not affect the efficient functioning of public utility mains, services or installations.</p>	<p>AO14.1 Public utility mains, services and installations are not required to be altered or repaired as a result of the development;</p> <p>or</p> <p>AO14.2 Public utility mains, services and installations are altered or repaired in association with the works so that they continue to function and satisfy the relevant Design Guidelines set out in Section D8 of the Planning scheme policy</p>	<p>Proposal is capable of complying.</p> <p>Proposal is capable of complying.</p>

Performance outcomes	Acceptable outcomes	Applicant response
	SC5 – FNQROC Regional Development Manual.	
Construction management		
PO15 Work is undertaken in a manner which minimises adverse impacts on vegetation that is to be retained.	AO15 Works include, at a minimum: (a) installation of protective fencing around retained vegetation during construction; (b) erection of advisory signage; (c) no disturbance, due to earthworks or storage of plant, materials and equipment, of ground level and soils below the canopy of any retained vegetation; (d) removal from the site of all declared noxious weeds.	Proposal is capable of complying.
PO16 Existing infrastructure is not damaged by construction activities.	AO16 Construction, alterations and any repairs to infrastructure is undertaken in accordance with the Planning scheme policy SC5 – FNQROC Regional Development Manual. Note - Construction, alterations and any repairs to State-controlled roads and rail corridors are undertaken in accordance with the Transport Infrastructure Act 1994.	Proposal is capable of complying.
For assessable development		
High speed telecommunication infrastructure		
PO17 Development provides infrastructure to facilitate the roll out of high speed telecommunications infrastructure.	AO17 No acceptable outcomes are prescribed.	Proposal is capable of complying.
Trade waste		
PO18	AO18	N/A

Table Error! No text of specified style in document..b – Stormwater management design objectives (Construction phase).

Issue	Design objectives
Drainage control (Temporary drainage works)	(a) Design life and design storm for temporary drainage works: (vi) Disturbed open area for <12 months – 1 in 2 year ARI event; (vii) Disturbed open area for 12-24 months – 1 in 5 year ARI event; (viii) Disturbed open area for >24 months – 1 in 10 year ARI event. (b) Design capacity excludes minimum 150mm freeboard. (c) Temporary culvert crossing – minimum of 1 in 1-year ARI hydraulic capacity.
Erosion control (Erosion control measures)	(a) Minimise exposure of disturbed soils at any time. (b) Divert water run-off from undisturbed areas around disturbed areas. (c) Determine erosion risk rating using local rainfall erosivity, rainfall depth, soil loss rate or other acceptable methods. (d) Implement erosion control methods corresponding to identified erosion risk rating.
Sediment control measures (sediment control measures, design storm for sediment control basins, Sediment basin dewatering)	(a) Determine appropriate sediment control measures using: (i) potential soil loss rate; or (ii) monthly erosivity; or (iii) average monthly rainfall. (b) Collect and drain stormwater from disturbed soils to sediment basin for design storm event: (i) design storm for sediment basin sizing is 80th% five-day event or similar. (c) Site discharge during sediment basin dewatering: (i) TSS < 50mg/L TSS; (ii) Turbidity not > 10% receiving water's turbidity; (iii) pH 6.5-8.5.
Water quality (Litter and other waste, hydrocarbons and other contaminants)	(a) Avoid wind-blown litter; remove grass pollutants. (b) Ensure there is no visible oil or grease sheen on released waters. (c) Dispose of waste containing contaminants at authorised facilities.
Waterway stability and flood flow management (Changes to the natural hydraulics and hydrology)	(a) For peak flow for the 100% AEP event and 1% AEP event, use constructed sediment basins to attenuate the discharge rate of stormwater from the site.

Table Error! No text of specified style in document..c – Stormwater management design objectives (post-construction phase)

Design objectives				Application
Minimum reductions in mean annual load from unmitigated development (%)				
Total suspended solids (TSS)	Total phosphorus (TP)	Total nitrogen (TN)	Gross pollutants >5mm	
80	60	40	90	<p>Development for urban purposes</p> <p>Excludes development that is less than 25% pervious.</p> <p>In lieu of modelling, the default bio-retention treatment area to comply with load reduction targets of 1.5% of contributing catchment area.</p>
<p>Water stability management</p> <p>(2) Limit peak 100% AEP event discharge within the receiving waterway to the pre-development peak 100% AEP event discharge.</p>				<p>Catchments contributing to un-lined receiving waterway. Degraded waterways may seek alternative discharge management objectives to achieve waterway stability.</p> <p>For peak flow for the 100% AEP event, use co-located storages to attenuate site discharge rate of stormwater.</p>

Figure Error! No text of specified style in document..a – New footpath sections

