

DA Form 1 – Development application details

Approved form (version 1.3 effective 28 September 2020) made under section 282 of the Planning Act 2016.

This form **must** be used to make a development application **involving code assessment or impact assessment**, except when applying for development involving only building work.

For a development application involving **building work only**, use *DA Form 2 – Building work details*.

For a development application involving **building work associated with any other type of assessable development (i.e. material change of use, operational work or reconfiguring a lot)**, use this form (*DA Form 1*) and parts 4 to 6 of *DA Form 2 – Building work details*.

Unless stated otherwise, all parts of this form **must** be completed in full and all required supporting information **must** accompany the development application.

One or more additional pages may be attached as a schedule to this development application if there is insufficient space on the form to include all the necessary information.

This form and any other form relevant to the development application must be used to make a development application relating to strategic port land and Brisbane core port land under the *Transport Infrastructure Act 1994*, and airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*. For the purpose of assessing a development application relating to strategic port land and Brisbane core port land, any reference to a planning scheme is taken to mean a land use plan for the strategic port land, Brisbane port land use plan for Brisbane core port land, or a land use plan for airport land.

Note: All terms used in this form have the meaning given under the Planning Act 2016, the Planning Regulation 2017, or the Development Assessment Rules (DA Rules).

PART 1 – APPLICANT DETAILS

1) Applicant details	
Applicant name(s) (individual or company full name)	Angela Whittaker c/- Scope Town Planning
Contact name (only applicable for companies)	Johnathan Burns
Postal address (P.O. Box or street address)	38 Kowa Street
Suburb	Mareeba
State	Qld.
Postcode	4880
Country	Aust.
Contact number	0450 781 841
Email address (non-mandatory)	jburns@scopetownplanning.com.au
Mobile number (non-mandatory)	
Fax number (non-mandatory)	
Applicant's reference number(s) (if applicable)	23006

2) Owner's consent	
2.1) Is written consent of the owner required for this development application?	
<input checked="" type="checkbox"/> Yes – the written consent of the owner(s) is attached to this development application	
<input type="checkbox"/> No – proceed to 3)	

PART 2 – LOCATION DETAILS

3) Location of the premises (complete 3.1) or 3.2), and 3.3) as applicable)

Note: Provide details below and attach a site plan for any or all premises part of the development application. For further information, see [DA Forms Guide: Relevant plans](#).

3.1) Street address and lot on plan

- ☒ Street address **AND** lot on plan (all lots must be listed), **or**
☐ Street address **AND** lot on plan for an adjoining or adjacent property of the premises (appropriate for development in water but adjoining or adjacent to land e.g. jetty, pontoon. All lots must be listed).

a)	Unit No.	Street No.	Street Name and Type	Suburb
		6133	Captain Cook Hwy.	Craiglie
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)
	4877	3	RP743352	Douglas Shire Council
b)	Unit No.	Street No.	Street Name and Type	Suburb
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)

3.2) Coordinates of premises (appropriate for development in remote areas, over part of a lot or in water not adjoining or adjacent to land e.g. channel dredging in Moreton Bay)

Note: Place each set of coordinates in a separate row.

- ☐ Coordinates of premises by longitude and latitude

Longitude(s)	Latitude(s)	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

- ☐ Coordinates of premises by easting and northing

Easting(s)	Northing(s)	Zone Ref.	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> 54 <input type="checkbox"/> 55 <input type="checkbox"/> 56	<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

3.3) Additional premises

- ☐ Additional premises are relevant to this development application and the details of these premises have been attached in a schedule to this development application
☒ Not required

4) Identify any of the following that apply to the premises and provide any relevant details

- ☒ In or adjacent to a water body or watercourse or in or above an aquifer

Name of water body, watercourse or aquifer: unknown

- ☐ On strategic port land under the *Transport Infrastructure Act 1994*

Lot on plan description of strategic port land:

Name of port authority for the lot:

- ☐ In a tidal area

Name of local government for the tidal area (if applicable):

Name of port authority for tidal area (if applicable):

- ☐ On airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*

Name of airport:

<input type="checkbox"/> Listed on the Environmental Management Register (EMR) under the <i>Environmental Protection Act 1994</i>
EMR site identification: <input type="text"/>
<input type="checkbox"/> Listed on the Contaminated Land Register (CLR) under the <i>Environmental Protection Act 1994</i>
CLR site identification: <input type="text"/>

5) Are there any existing easements over the premises?

Note: Easement uses vary throughout Queensland and are to be identified correctly and accurately. For further information on easements and how they may affect the proposed development, see [DA Forms Guide](#).

- ☐ Yes – All easement locations, types and dimensions are included in plans submitted with this development application
- ☒ No

PART 3 – DEVELOPMENT DETAILS

Section 1 – Aspects of development

6.1) Provide details about the first development aspect

a) What is the type of development? *(tick only one box)*

- ☒ Material change of use ☐ Reconfiguring a lot ☐ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☒ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☐ Code assessment ☒ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

Low Impact Industry

e) Relevant plans

Note: *Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms guide: Relevant plans](#).*

- ☒ Relevant plans of the proposed development are attached to the development application

6.2) Provide details about the second development aspect

a) What is the type of development? *(tick only one box)*

- ☐ Material change of use ☐ Reconfiguring a lot ☐ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☐ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☐ Code assessment ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

e) Relevant plans

Note: *Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).*

- ☐ Relevant plans of the proposed development are attached to the development application

6.3) Additional aspects of development

- ☐ Additional aspects of development are relevant to this development application and the details for these aspects that would be required under Part 3 Section 1 of this form have been attached to this development application

- ☒ Not required

Section 2 – Further development details

7) Does the proposed development application involve any of the following?	
Material change of use	<input checked="" type="checkbox"/> Yes – complete division 1 if assessable against a local planning instrument
Reconfiguring a lot	<input type="checkbox"/> Yes – complete division 2
Operational work	<input type="checkbox"/> Yes – complete division 3
Building work	<input type="checkbox"/> Yes – complete <i>DA Form 2 – Building work details</i>

Division 1 – Material change of use

Note: This division is only required to be completed if any part of the development application involves a material change of use assessable against a local planning instrument.

8.1) Describe the proposed material change of use			
Provide a general description of the proposed use	Provide the planning scheme definition (include each definition in a new row)	Number of dwelling units (if applicable)	Gross floor area (m ²) (if applicable)
Small fabrication business	Low Impact Industry		288
8.2) Does the proposed use involve the use of existing buildings on the premises?			
<input type="checkbox"/> Yes			
<input checked="" type="checkbox"/> No			

Division 2 – Reconfiguring a lot

Note: This division is only required to be completed if any part of the development application involves reconfiguring a lot.

9.1) What is the total number of existing lots making up the premises?	
9.2) What is the nature of the lot reconfiguration? (tick all applicable boxes)	
<input type="checkbox"/> Subdivision (complete 10))	<input type="checkbox"/> Dividing land into parts by agreement (complete 11))
<input type="checkbox"/> Boundary realignment (complete 12))	<input type="checkbox"/> Creating or changing an easement giving access to a lot from a constructed road (complete 13))

10) Subdivision				
10.1) For this development, how many lots are being created and what is the intended use of those lots:				
Intended use of lots created	Residential	Commercial	Industrial	Other, please specify:
Number of lots created				
10.2) Will the subdivision be staged?				
<input type="checkbox"/> Yes – provide additional details below				
<input type="checkbox"/> No				
How many stages will the works include?				
What stage(s) will this development application apply to?				

11) Dividing land into parts by agreement – how many parts are being created and what is the intended use of the parts?				
Intended use of parts created	Residential	Commercial	Industrial	Other, please specify:
Number of parts created				

12) Boundary realignment			
12.1) What are the current and proposed areas for each lot comprising the premises?			
Current lot		Proposed lot	
Lot on plan description	Area (m ²)	Lot on plan description	Area (m ²)
12.2) What is the reason for the boundary realignment?			

13) What are the dimensions and nature of any existing easements being changed and/or any proposed easement? (attach schedule if there are more than two easements)				
Existing or proposed?	Width (m)	Length (m)	Purpose of the easement? (e.g. pedestrian access)	Identify the land/lot(s) benefitted by the easement

Division 3 – Operational work

Note: This division is only required to be completed if any part of the development application involves operational work.

14.1) What is the nature of the operational work?	
<input type="checkbox"/> Road work <input type="checkbox"/> Drainage work <input type="checkbox"/> Landscaping <input type="checkbox"/> Other – please specify:	<input type="checkbox"/> Stormwater <input type="checkbox"/> Earthworks <input type="checkbox"/> Signage <input type="checkbox"/> Water infrastructure <input type="checkbox"/> Sewage infrastructure <input type="checkbox"/> Clearing vegetation
14.2) Is the operational work necessary to facilitate the creation of new lots? (e.g. subdivision)	
<input type="checkbox"/> Yes – specify number of new lots:	
<input type="checkbox"/> No	
14.3) What is the monetary value of the proposed operational work? (include GST, materials and labour)	
\$	

PART 4 – ASSESSMENT MANAGER DETAILS

15) Identify the assessment manager(s) who will be assessing this development application
Douglas Shire Council
16) Has the local government agreed to apply a superseded planning scheme for this development application?
<input type="checkbox"/> Yes – a copy of the decision notice is attached to this development application <input type="checkbox"/> The local government is taken to have agreed to the superseded planning scheme request – relevant documents attached <input checked="" type="checkbox"/> No

PART 5 – REFERRAL DETAILS

17) Does this development application include any aspects that have any referral requirements?

Note: A development application will require referral if prescribed by the Planning Regulation 2017.

- ☐ No, there are no referral requirements relevant to any development aspects identified in this development application – proceed to Part 6

Matters requiring referral to the **Chief Executive of the Planning Act 2016:**

- ☐ Clearing native vegetation
- ☐ Contaminated land (*unexploded ordnance*)
- ☐ Environmentally relevant activities (ERA) (*only if the ERA has not been devolved to a local government*)
- ☐ Fisheries – aquaculture
- ☐ Fisheries – declared fish habitat area
- ☐ Fisheries – marine plants
- ☐ Fisheries – waterway barrier works
- ☐ Hazardous chemical facilities
- ☐ Heritage places – Queensland heritage place (*on or near a Queensland heritage place*)
- ☐ Infrastructure-related referrals – designated premises
- ☐ Infrastructure-related referrals – state transport infrastructure
- ☒ Infrastructure-related referrals – State transport corridor and future State transport corridor
- ☐ Infrastructure-related referrals – State-controlled transport tunnels and future state-controlled transport tunnels
- ☐ Infrastructure-related referrals – near a state-controlled road intersection
- ☐ Koala habitat in SEQ region – interfering with koala habitat in koala habitat areas outside koala priority areas
- ☐ Koala habitat in SEQ region – key resource areas
- ☐ Ports – Brisbane core port land – near a State transport corridor or future State transport corridor
- ☐ Ports – Brisbane core port land – environmentally relevant activity (ERA)
- ☐ Ports – Brisbane core port land – tidal works or work in a coastal management district
- ☐ Ports – Brisbane core port land – hazardous chemical facility
- ☐ Ports – Brisbane core port land – taking or interfering with water
- ☐ Ports – Brisbane core port land – referable dams
- ☐ Ports – Brisbane core port land – fisheries
- ☐ Ports – Land within Port of Brisbane's port limits (*below high-water mark*)
- ☐ SEQ development area
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – tourist activity or sport and recreation activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – community activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – indoor recreation
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – urban activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – combined use
- ☐ Tidal works or works in a coastal management district
- ☐ Reconfiguring a lot in a coastal management district or for a canal
- ☐ Erosion prone area in a coastal management district
- ☐ Urban design
- ☐ Water-related development – taking or interfering with water
- ☐ Water-related development – removing quarry material (*from a watercourse or lake*)
- ☐ Water-related development – referable dams
- ☐ Water-related development – levees (*category 3 levees only*)
- ☐ Wetland protection area

Matters requiring referral to the **local government:**

- ☐ Airport land
- ☐ Environmentally relevant activities (ERA) (*only if the ERA has been devolved to local government*)

<input type="checkbox"/> Heritage places – Local heritage places
Matters requiring referral to the Chief Executive of the distribution entity or transmission entity:
<input type="checkbox"/> Infrastructure-related referrals – Electricity infrastructure
Matters requiring referral to:
<ul style="list-style-type: none"> • The Chief Executive of the holder of the licence, if not an individual • The holder of the licence, if the holder of the licence is an individual
<input type="checkbox"/> Infrastructure-related referrals – Oil and gas infrastructure
Matters requiring referral to the Brisbane City Council:
<input type="checkbox"/> Ports – Brisbane core port land
Matters requiring referral to the Minister responsible for administering the <i>Transport Infrastructure Act 1994</i>:
<input type="checkbox"/> Ports – Brisbane core port land <i>(where inconsistent with the Brisbane port LUP for transport reasons)</i>
<input type="checkbox"/> Ports – Strategic port land
Matters requiring referral to the relevant port operator , if applicant is not port operator:
<input type="checkbox"/> Ports – Land within Port of Brisbane's port limits <i>(below high-water mark)</i>
Matters requiring referral to the Chief Executive of the relevant port authority:
<input type="checkbox"/> Ports – Land within limits of another port <i>(below high-water mark)</i>
Matters requiring referral to the Gold Coast Waterways Authority:
<input type="checkbox"/> Tidal works or work in a coastal management district <i>(in Gold Coast waters)</i>
Matters requiring referral to the Queensland Fire and Emergency Service:
<input type="checkbox"/> Tidal works or work in a coastal management district <i>(involving a marina (more than six vessel berths))</i>

18) Has any referral agency provided a referral response for this development application?		
<input type="checkbox"/> Yes – referral response(s) received and listed below are attached to this development application		
<input checked="" type="checkbox"/> No		
Referral requirement	Referral agency	Date of referral response
Identify and describe any changes made to the proposed development application that was the subject of the referral response and this development application, or include details in a schedule to this development application <i>(if applicable)</i> .		

PART 6 – INFORMATION REQUEST

19) Information request under Part 3 of the DA Rules
<input checked="" type="checkbox"/> I agree to receive an information request if determined necessary for this development application
<input type="checkbox"/> I do not agree to accept an information request for this development application
<p>Note: By not agreeing to accept an information request I, the applicant, acknowledge:</p> <ul style="list-style-type: none"> • that this development application will be assessed and decided based on the information provided when making this development application and the assessment manager and any referral agencies relevant to the development application are not obligated under the DA Rules to accept any additional information provided by the applicant for the development application unless agreed to by the relevant parties • Part 3 of the DA Rules will still apply if the application is an application listed under section 11.3 of the DA Rules. <p>Further advice about information requests is contained in the DA Forms Guide.</p>

PART 7 – FURTHER DETAILS

20) Are there any associated development applications or current approvals? (e.g. a preliminary approval)

- ☐ Yes – provide details below or include details in a schedule to this development application
☒ No

List of approval/development application references	Reference number	Date	Assessment manager
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			

21) Has the portable long service leave levy been paid? (only applicable to development applications involving building work or operational work)

- ☐ Yes – a copy of the receipted QLeave form is attached to this development application
☐ No – I, the applicant will provide evidence that the portable long service leave levy has been paid before the assessment manager decides the development application. I acknowledge that the assessment manager may give a development approval only if I provide evidence that the portable long service leave levy has been paid
☒ Not applicable (e.g. building and construction work is less than \$150,000 excluding GST)

Amount paid	Date paid (dd/mm/yy)	QLeave levy number (A, B or E)
\$		

22) Is this development application in response to a show cause notice or required as a result of an enforcement notice?

- ☐ Yes – show cause or enforcement notice is attached
☒ No

23) Further legislative requirements

Environmentally relevant activities

23.1) Is this development application also taken to be an application for an environmental authority for an **Environmentally Relevant Activity (ERA)** under section 115 of the *Environmental Protection Act 1994*?

- ☐ Yes – the required attachment (form ESR/2015/1791) for an application for an environmental authority accompanies this development application, and details are provided in the table below
☒ No

Note: Application for an environmental authority can be found by searching “ESR/2015/1791” as a search term at www.qld.gov.au. An ERA requires an environmental authority to operate. See www.business.qld.gov.au for further information.

Proposed ERA number:		Proposed ERA threshold:	
Proposed ERA name:			

- ☐ Multiple ERAs are applicable to this development application and the details have been attached in a schedule to this development application.

Hazardous chemical facilities

23.2) Is this development application for a **hazardous chemical facility**?

- ☐ Yes – Form 69: Notification of a facility exceeding 10% of schedule 15 threshold is attached to this development application
☒ No

Note: See www.business.qld.gov.au for further information about hazardous chemical notifications.

Clearing native vegetation

23.3) Does this development application involve **clearing native vegetation** that requires written confirmation that the chief executive of the *Vegetation Management Act 1999* is satisfied the clearing is for a relevant purpose under section 22A of the *Vegetation Management Act 1999*?

☐ Yes – this development application includes written confirmation from the chief executive of the *Vegetation Management Act 1999* (s22A determination)

☒ No

Note: 1. Where a development application for operational work or material change of use requires a s22A determination and this is not included, the development application is prohibited development.
2. See <https://www.qld.gov.au/environment/land/vegetation/applying> for further information on how to obtain a s22A determination.

Environmental offsets

23.4) Is this development application taken to be a prescribed activity that may have a significant residual impact on a **prescribed environmental matter** under the *Environmental Offsets Act 2014*?

☐ Yes – I acknowledge that an environmental offset must be provided for any prescribed activity assessed as having a significant residual impact on a prescribed environmental matter

☒ No

Note: The environmental offset section of the Queensland Government's website can be accessed at www.qld.gov.au for further information on environmental offsets.

Koala habitat in SEQ Region

23.5) Does this development application involve a material change of use, reconfiguring a lot or operational work which is assessable development under Schedule 10, Part 10 of the Planning Regulation 2017?

☐ Yes – the development application involves premises in the koala habitat area in the koala priority area

☐ Yes – the development application involves premises in the koala habitat area outside the koala priority area

☒ No

Note: If a koala habitat area determination has been obtained for this premises and is current over the land, it should be provided as part of this development application. See koala habitat area guidance materials at www.des.qld.gov.au for further information.

Water resources

23.6) Does this development application involve **taking or interfering with underground water through an artesian or subartesian bore, taking or interfering with water in a watercourse, lake or spring, or taking overland flow water under the *Water Act 2000***?

☐ Yes – the relevant template is completed and attached to this development application and I acknowledge that a relevant authorisation or licence under the *Water Act 2000* may be required prior to commencing development

☒ No

Note: Contact the Department of Natural Resources, Mines and Energy at www.dnrme.qld.gov.au for further information.

DA templates are available from <https://planning.dsdmp.qld.gov.au/>. If the development application involves:

- Taking or interfering with underground water through an artesian or subartesian bore: complete DA Form 1 Template 1
- Taking or interfering with water in a watercourse, lake or spring: complete DA Form 1 Template 2
- Taking overland flow water: complete DA Form 1 Template 3.

Waterway barrier works

23.7) Does this application involve **waterway barrier works**?

☐ Yes – the relevant template is completed and attached to this development application

☒ No

DA templates are available from <https://planning.dsdmp.qld.gov.au/>. For a development application involving waterway barrier works, complete DA Form 1 Template 4.

Marine activities

23.8) Does this development application involve **aquaculture, works within a declared fish habitat area or removal, disturbance or destruction of marine plants**?

☐ Yes – an associated resource allocation authority is attached to this development application, if required under the *Fisheries Act 1994*

☒ No

Note: See guidance materials at www.daf.qld.gov.au for further information.

Quarry materials from a watercourse or lake

23.9) Does this development application involve the **removal of quarry materials from a watercourse or lake** under the *Water Act 2000*?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
☒ No

Note: Contact the Department of Natural Resources, Mines and Energy at www.dnrme.qld.gov.au and www.business.qld.gov.au for further information.

Quarry materials from land under tidal waters

23.10) Does this development application involve the **removal of quarry materials from land under tidal water** under the *Coastal Protection and Management Act 1995*?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
☒ No

Note: Contact the Department of Environment and Science at www.des.qld.gov.au for further information.

Referable dams

23.11) Does this development application involve a **referable dam** required to be failure impact assessed under section 343 of the *Water Supply (Safety and Reliability) Act 2008* (the *Water Supply Act*)?

- ☐ Yes – the 'Notice Accepting a Failure Impact Assessment' from the chief executive administering the *Water Supply Act* is attached to this development application
☒ No

Note: See guidance materials at www.dnrme.qld.gov.au for further information.

Tidal work or development within a coastal management district

23.12) Does this development application involve **tidal work or development in a coastal management district**?

- ☐ Yes – the following is included with this development application:
- ☐ Evidence the proposal meets the code for assessable development that is prescribed tidal work (*only required if application involves prescribed tidal work*)
 - ☐ A certificate of title
- ☒ No

Note: See guidance materials at www.des.qld.gov.au for further information.

Queensland and local heritage places

23.13) Does this development application propose development on or adjoining a place entered in the **Queensland heritage register** or on a place entered in a local government's **Local Heritage Register**?

- ☐ Yes – details of the heritage place are provided in the table below
☒ No

Note: See guidance materials at www.des.qld.gov.au for information requirements regarding development of Queensland heritage places.

Name of the heritage place:		Place ID:	
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Brothels

23.14) Does this development application involve a **material change of use for a brothel**?

- ☐ Yes – this development application demonstrates how the proposal meets the code for a development application for a brothel under Schedule 3 of the *Prostitution Regulation 2014*
☒ No

Decision under section 62 of the Transport Infrastructure Act 1994

23.15) Does this development application involve new or changed access to a state-controlled road?

- ☐ Yes – this application will be taken to be an application for a decision under section 62 of the *Transport Infrastructure Act 1994* (subject to the conditions in section 75 of the *Transport Infrastructure Act 1994* being satisfied)
☒ No

Walkable neighbourhoods assessment benchmarks under Schedule 12A of the Planning Regulation

23.16) Does this development application involve reconfiguring a lot into 2 or more lots in certain residential zones (except rural residential zones), where at least one road is created or extended?

☐ Yes – Schedule 12A is applicable to the development application and the assessment benchmarks contained in schedule 12A have been considered

☒ No

Note: See guidance materials at www.planning.dsdmip.qld.gov.au for further information.

PART 8 – CHECKLIST AND APPLICANT DECLARATION

24) Development application checklist

I have identified the assessment manager in question 15 and all relevant referral requirement(s) in question 17

☒ Yes

Note: See the Planning Regulation 2017 for referral requirements

If building work is associated with the proposed development, Parts 4 to 6 of [DA Form 2 – Building work details](#) have been completed and attached to this development application

☐ Yes

☒ Not applicable

Supporting information addressing any applicable assessment benchmarks is with the development application

Note: This is a mandatory requirement and includes any relevant templates under question 23, a planning report and any technical reports required by the relevant categorising instruments (e.g. local government planning schemes, State Planning Policy, State Development Assessment Provisions). For further information, see [DA Forms Guide: Planning Report Template](#).

☒ Yes

Relevant plans of the development are attached to this development application

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).

☒ Yes

The portable long service leave levy for QLeave has been paid, or will be paid before a development permit is issued (see 21)

☐ Yes

☒ Not applicable

25) Applicant declaration

☒ By making this development application, I declare that all information in this development application is true and correct

☒ Where an email address is provided in Part 1 of this form, I consent to receive future electronic communications from the assessment manager and any referral agency for the development application where written information is required or permitted pursuant to sections 11 and 12 of the *Electronic Transactions Act 2001*

Note: It is unlawful to intentionally provide false or misleading information.

Privacy – Personal information collected in this form will be used by the assessment manager and/or chosen assessment manager, any relevant referral agency and/or building certifier (including any professional advisers which may be engaged by those entities) while processing, assessing and deciding the development application. All information relating to this development application may be available for inspection and purchase, and/or published on the assessment manager's and/or referral agency's website.

Personal information will not be disclosed for a purpose unrelated to the *Planning Act 2016*, Planning Regulation 2017 and the DA Rules except where:

- such disclosure is in accordance with the provisions about public access to documents contained in the *Planning Act 2016* and the Planning Regulation 2017, and the access rules made under the *Planning Act 2016* and Planning Regulation 2017; or
- required by other legislation (including the *Right to Information Act 2009*); or
- otherwise required by law.

This information may be stored in relevant databases. The information collected will be retained as required by the *Public Records Act 2002*.

PART 9 – FOR COMPLETION OF THE ASSESSMENT MANAGER – FOR OFFICE USE ONLY

Date received: Reference number(s):

Notification of engagement of alternative assessment manager

Prescribed assessment manager	
Name of chosen assessment manager	
Date chosen assessment manager engaged	
Contact number of chosen assessment manager	
Relevant licence number(s) of chosen assessment manager	

QLeave notification and payment

Note: For completion by assessment manager if applicable

Description of the work	
QLeave project number	
Amount paid (\$)	Date paid (dd/mm/yy)
Date receipted form sighted by assessment manager	
Name of officer who sighted the form	

DEVELOPMENT APPLICATION

DEVELOPMENT PERMIT:

Material Change of Use
Low Impact Industry

6133 Captain Cook Highway, Craiglie Qld. 4877
Lot 3 on RP743352

PREPARED BY: SCOPE TOWN PLANNING

March 2023



PLANNING FOR LOCALS

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APPLICATION SUMMARY	
DEVELOPMENT APPLICATION	Material Change of Use
PROPOSED USE	Low Impact Industry
ASSESSMENT LEVEL	Impact
STREET ADDRESS	6133 Captain Cook Highway, Craiglie Qld. 4877
REAL PROPERTY ADDRESS	Lot 3 on RP743352
LAND AREA	17,650m²
APPLICANT	Scope Town Planning c/- Land Owner
LAND OWNER	Angela Whittaker
LOCAL GOVERNMENT AREA	Douglas Shire Council
PLANNING SCHEME	Douglas Shire Planning Scheme 2018
ZONE	Rural Zone
PRECINCT	n/a
LOCAL PLAN	Port Douglas - Craiglie
EASEMENTS	Nil
IMPROVEMENTS	Dwelling, Outbuildings
APPLICABLE PLANNING CODES	Rural Zone Code
	Port Douglas – Craiglie Local Plan Code
	Acid Sulfate Soils Overlay Code
	Bushfire Hazard Overlay Code
	Flood and Storm Tide Hazard Overlay Code
	Landscape Values Overlay Code
	Natural Areas Overlay Code
	Transport Network Overlay Code
	Industry Activities Code
	Access, Parking and Servicing Code
	Environmental Performance Code
	Infrastructure Works Code
	Landscaping Code
	Vegetation Management Code
APPLICABLE REFERRALS	SARA (State Controlled Road)

1 Proposal

1.1 Introduction

This application is for a Development Permit for a Material Change of Use over land at 6133 Captain Cook Highway, Craiglie Qld. 4877 formally known as Lot 3 on RP743352. The site is located within the Rural Zone.

This application is classified as **Impact Assessable Development** against the relevant codes of the Douglas Shire Planning Scheme (2018) for a Material Change of Use as prescribed by Table 5.6.j.

1.2 Proposed Development

The proposed development involves the use of the existing class 10a shed structure for the operation of the established local Port Douglas Windows & Glass (QLD) Pty Ltd business on the Rural Zoned site. The proposed use is classified as a Low Impact Industry activity resulting in the Impact Level of Assessment.

The newly constructed shed has a GFA of 288m² and an adjacent concrete driveway with an area of 72m² as illustrated in Figure 1. The shed will be used by Port Douglas Windows & Glass (QLD), a residential and commercial windows and door fabrication company servicing areas from Palm Cove, Mossman, Daintree, Julatten and Cooktown.

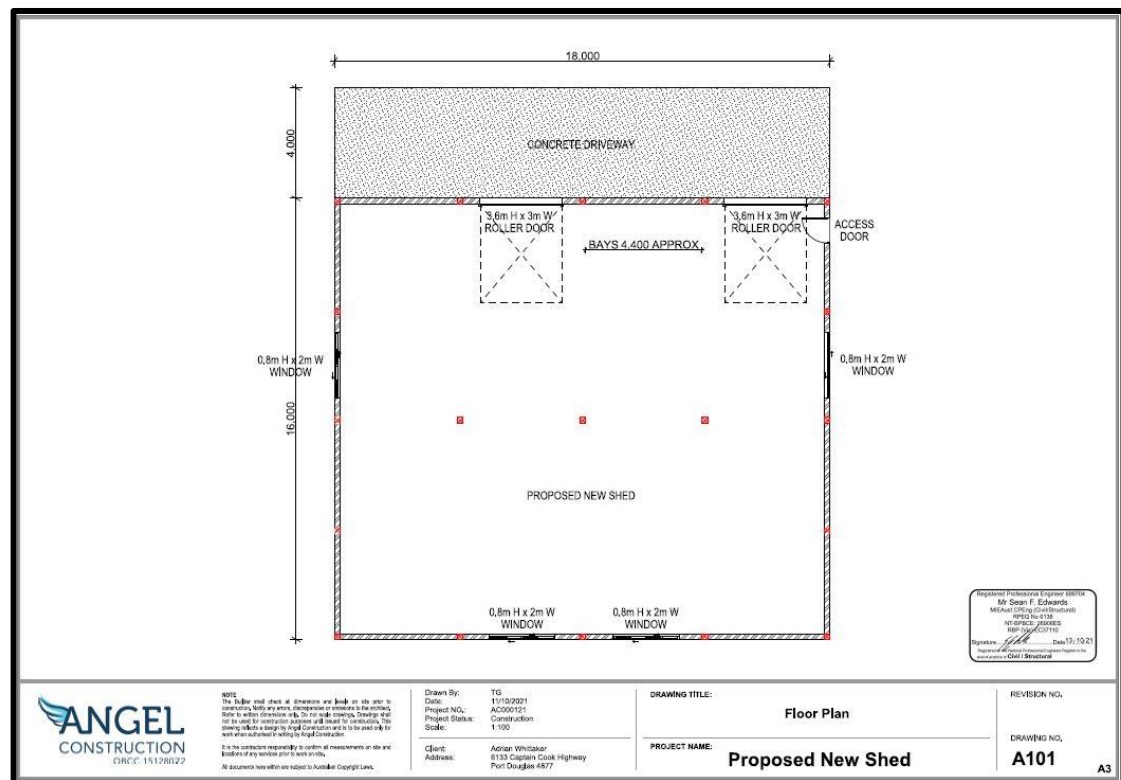


Figure 1: Building Floor Plan (Angel Construction).

1.3 Site and Locality

The Development Site is located at 6133 Captain Cook Highway, Craiglie Qld. 4877 formally known as Lot 3 on RP743352. The site is located within the Rural Zone and the Port Douglas – Craiglie Local Plan area.

The site is situated on and gains direct access to the Captain Cook Highway (a State Controlled Road). The site does not have access to any local roads.

The generally flat site is improved with a Dwelling House and Outbuildings (sheds) with retained mature vegetation throughout. The site has a 178m frontage to the Captain Cook Highway, an area of 17,650m² and is located amongst other Rural Zoned allotments containing agricultural activities.

Infrastructure required to service the site is accessible via the Captain Cook Highway frontage including site access, Reticulated Water, Electricity and Telecommunications network connections. Effluent waste is serviced via an on-site septic system located between the shed and outbuildings.

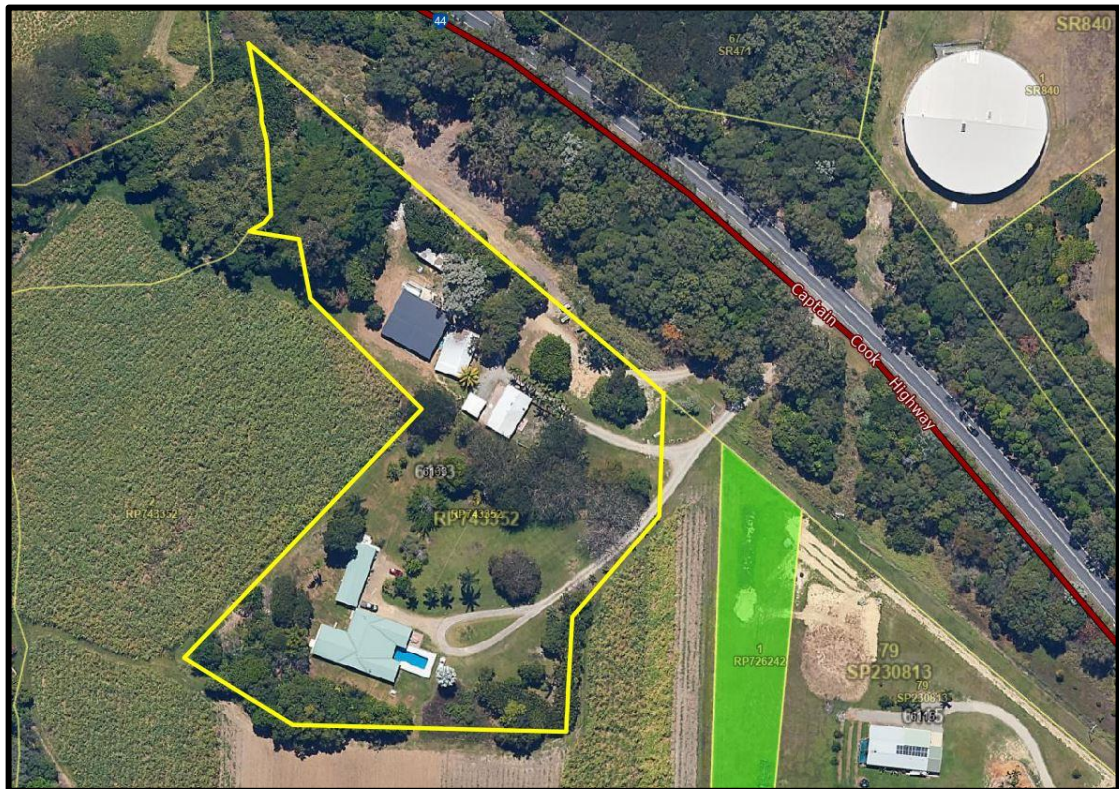


Figure 2: Development Site aerial image. (Qld. Globe)



Figure 3: Development Site location map (Whereis).



Figure 4: Development Site located in the Rural Zone (Douglas Shire Council Mapping).

1.4 Low Impact Industry Activity

The proposed Low Impact Industrial activity involves only the operation of Port Douglas Windows & Glass (QLD)'s business, in particular the fabrication of windows and doors.

SC1.1.2 INDUSTRY THRESHOLDS

Table SC 1.1.2.a – Industry thresholds

(d) Assembling or fabricating products from sheet metal or welding steel, producing less than 10 tonnes a year and not including spray painting.

Whilst the proposed activity is defined as a Low Impact Industry activity by the Douglas Shire Planning Scheme, the proposed activity is an extremely low scale operation resulting in a lower impact than some Rural Activities being fully contained within the building and managed in an environmentally responsible manner.

LOCAL DEMAND

The well-established business, Port Douglas Windows & Glass (QLD), is a highly reputable local business meeting the local demand for windows, doors, glazing and security screens in the Port Douglas, Mossman region and beyond. With no other providers in this region, Port Douglas Windows & Glass (QLD) is an in-demand, crucial supplier of window and door products to the region and, with known crime and security issues in the region, their supply of window glazing and security window and door products is an essential service to the Port Douglas, Mossman region.

OPERATIONAL ACTIVITIES

The proposed development involves the fabrication of windows and doors including window glazing and security screens. The fabrication process requires only punch machines and a drop saw which are only operated as required.

The fabrication activities involve only 2 on-site employees with fabrication to be carried out between 7:30am and 4:30pm, Monday to Friday however fabrication will not occur every day as the same employees will spend some days off-site for deliveries and installation.

The proposed facility does not include any display rooms, offices or administration activities and no visitors will be allowed on-site.

SITE SUITABILITY

Although zoned a Rural land, the site is not of a suitable area to accommodate Rural Activities of any significant scale. The land owner, also being the business owner, has sought to locate the window and door fabrication at an appropriately located industrial zoned site within the region however has been unsuccessful. As such, it is proposed that a relaxation be granted to allow the very low impact industrial activity to be carried out on the property.

The site is considered a logistically practical location for the proposed activities being located on the business owners property, centrally located in the service catchment and having good access to the road network.

SCENIC AMENITY

The Shed is partially affected by the Scenic Route Buffer of the Landscape Values Overlay (Figure 5).

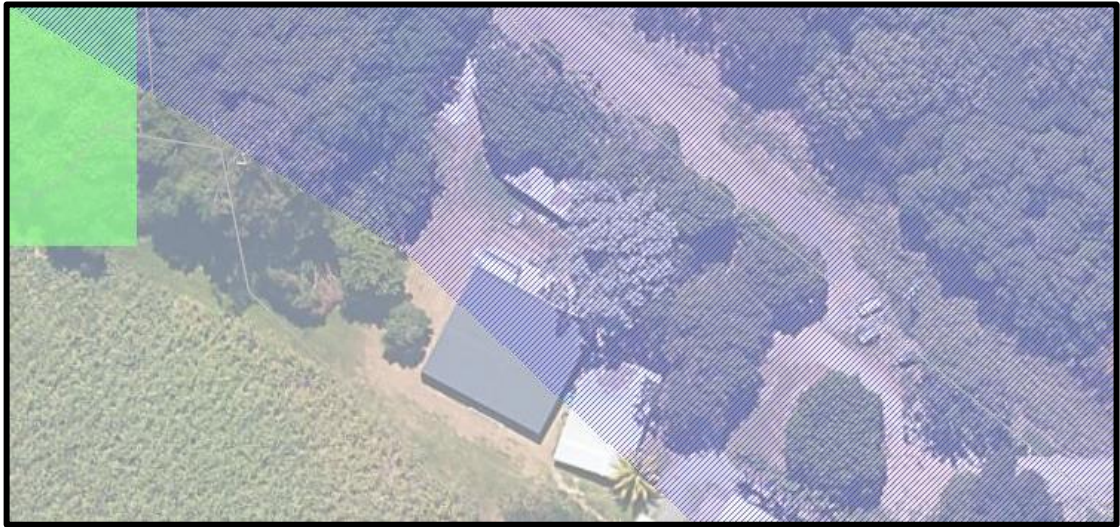


Figure 5: Scenic Route Buffer - Landscape Values Overlay (DSC Mapping).

As demonstrated by Figure 6, the Shed and other structures are not visible from the Captain Cook Highway due to the significant retained mature vegetation within the Road Reserve and retained mature vegetation on-site along the road frontage. In contrast, Lots 79SP230813 and 1RP726242 to the south east and Lots 2SP181510 and 1SP166336 to the north west are cleared of vegetation and visible from the Captain Cook Highway. The proposed development does not require any vegetation clearing and all existing vegetation is to be retained, satisfying the Acceptable Outcomes of the Landscape Values Overlay Code.

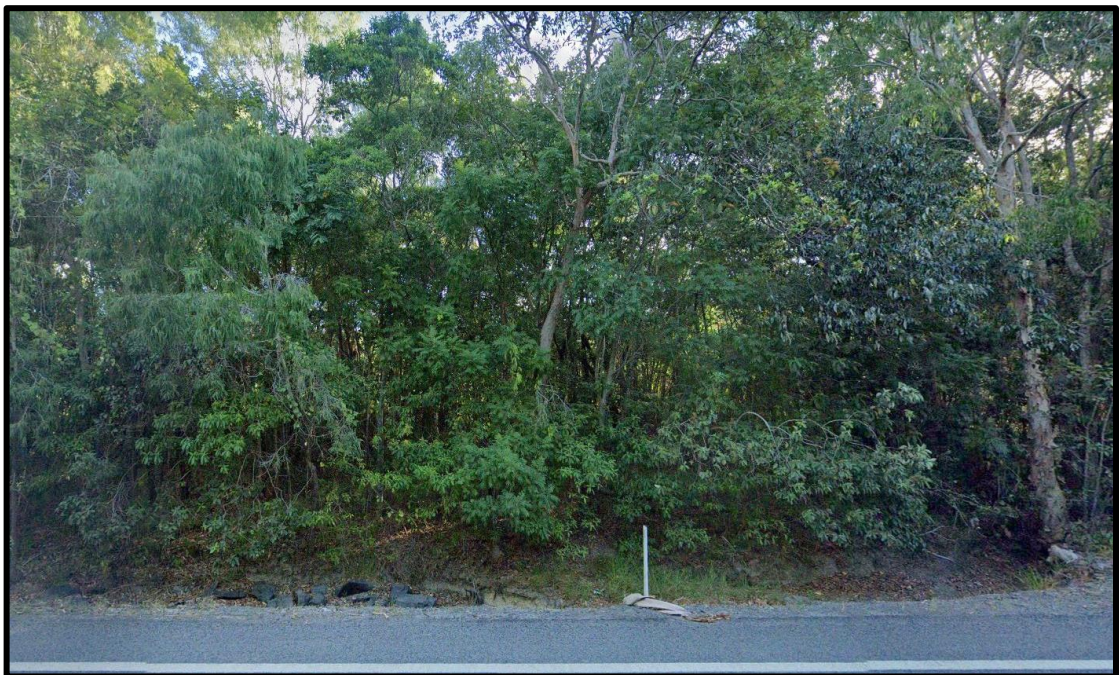


Figure 6: View toward Shed from Captain Cook Hwy. frontage (Google Maps).

ENVIRONMENTAL IMPACT

The Shed is partially affected by the MSES Regulated Vegetation area of the Natural Areas Overlay (Figure 7).

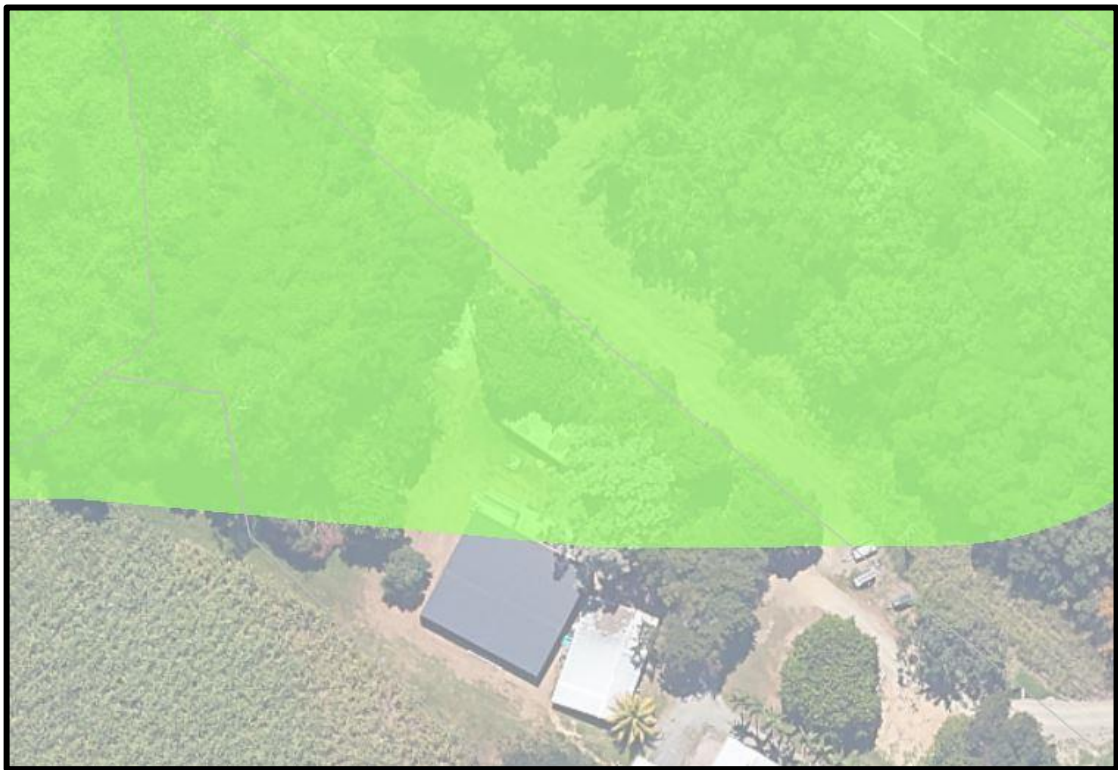


Figure 7: MSES Regulated Vegetation area - Natural Areas Overlay (DSC Mapping).

A small portion of the existing Shed is located within the MSES Regulated Vegetation area mapped over the site however the proposed development – Low Impact Industry, is to be located entirely within the structure and does not require any vegetation clearing to occur. The proposed activities will not adversely affect the local ecological corridor and no additional structures are required within the MSES area.

Operational activities will involve the fabrication of windows, glazed windows, doors and security screens and will be managed in an environmentally responsible manner. The proposed activities do not produce any significant volume of dust, odour or noise emissions with any such pollutants being contained within the building. No hazardous materials are to be stored within the Shed and the activities do not produce any hazardous waste materials.

Trade waste will be contained within a regularly service skip bin with all waste aluminium being stored in a separate bin for recycling.

INFRASTRUCTURE

No additional infrastructure is required to service the proposed use which will utilize the existing reticulated water and electrical connections and site access infrastructure. The existing Shed is serviced by an effluent disposal system in the form of a septic pit located between the Shed and adjacent outbuildings.

ACCESS AND SERVICING

The development site has frontage to and gains direct access via an existing crossover to the Captain Cook Highway, a State Controlled Road (Figure 8). The access driveway is split to service the Shed via a dedicated on-site internal driveway.

The proposed Low Impact Industrial Activity is very small in scale and intensity and does not require the use of articulated vehicles (AV) however sufficient on-site access exists to accommodate an AV class vehicle. Some deliveries may occur by rigid vehicles (RV) and trade waste will be collected by an RV class vehicle servicing an on-site skip bin.

The proposed activities involve only Utility Vehicles which are to be used for product delivery and installation which is coordinated to occur only as required during the week. These vehicle movements will not be frequenting the site on delivery/installation day as multiple jobs will be managed within the same day. As such, vehicle movements will be minimal involving only beginning and end of work day trips by the 2 employees. Ample parking areas exist on-site including the concrete apron alongside the shed.

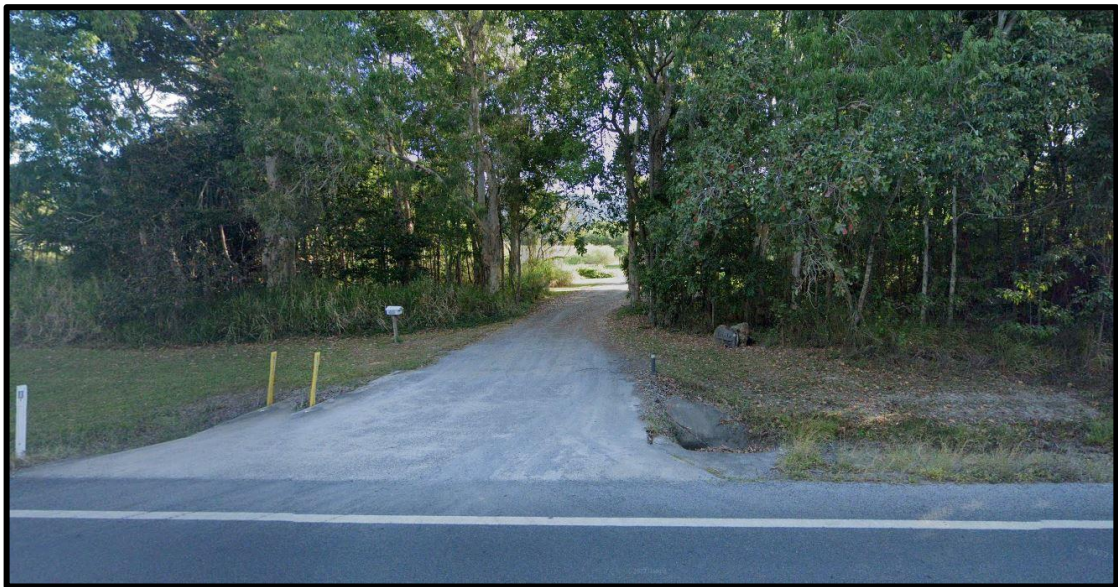


Figure 8: Existing access crossover to Captain Cook Hwy. (Google Maps).

WASTE AND WASTEWATER

The site is serviced by the reticulated water network. The existing Shed is serviced by an effluent disposal system in the form of a septic pit located between the Shed and adjacent outbuildings while all Stormwater collected from the shed roof is stored in a large rainwater tank located next to the shed. No additional Stormwater infrastructure is required or proposed.

The proposed activities do not produce any gaseous, liquid or solid hazardous waste materials. All trade waste is collected and stored in a secure skip bin stored within the shed which is serviced regularly by a waste collection service provider. The business aims to have a low environmental footprint and collects all waste aluminium materials from jobs and fabrication in a secure bin stored within the shed for regular delivery to an aluminium recycler.

2 Planning Considerations

2.1 Compliance with Planning Scheme

As prescribed by Table 5.6.j – Rural Zone, the proposed Material Change of Use is Impact Assessable development (Incompatible Use), assessable against the provisions of the relevant Codes of the Douglas Shire Planning Scheme 2018. The site is located within the Rural Zone, Port Douglas – Craiglie Local Plan Code and mapped within several Overlays as outlined in Table 1.

Table 1: Relevant planning codes.

Douglas Shire Planning Scheme 2018	
Zone Code	Rural Zone Code
Local Plan Code	Port Douglas – Craiglie Local Plan Code
Overlay Codes	Acid Sulfate Soils
	Bushfire Hazard Overlay Code
	Flood and Storm Tide Hazard Overlay Code
	Landscape Values Overlay Code
	Natural Areas Overlay Code
	Transport Network Overlay Code
Use Codes	Industry Activities Code
Other Development Codes	Access, Parking and Servicing Code
	Environmental Performance Code
	Infrastructure Works Code
	Landscaping Code
	Vegetation Management Code
	Reconfiguring a Lot Code

An assessment of the development proposal against the applicable Codes is provided in Appendix 1 – Code Assessment.

INCOMPATIBLE USE

Table 6.2.10.3.b of The Rural Zone Code identifies Low Impact Industry as an incompatible use in the Rural Zone with PO4 of the Rural Zone Code prescribing that *“The establishment of uses is consistent with the outcomes sought for the Rural Zone and protects the zone from inconsistent uses.”*

The Overall Outcomes of the Rural Zone Code, listed under 6.2.10.2 Purpose (3) are identified and addressed below;

- (a) Areas for primary production are conserved and fragmentation is avoided.

The subject site is of odd shape and contains a Dwelling house and several Sheds and outbuildings, as such it is unsuitable for use in primary production. The allotment was previously fragmented from the primary parcel of land (2RP743352) which envelopes the site and is used for cropping.

- (b) Development embraces sustainable land management practices and contributes to the amenity and landscape of the area.

The proposed use embraces sustainable practices through the collection and recycling (off-site) of waste aluminium, minimizing waste where possible. The proposed use is to be contained within an existing Shed which is screened from view from the Captain Cook Highway thus minimizing adverse impacts upon amenity and local landscape.

- (c) Adverse impacts of land use, both on-site and on adjoining areas, are avoided and any unavoidable impacts are minimized through location, design, operation and management.

The operation of the proposed use will be contained within the existing Shed with the fabrication activities being carried out by only 2 on-site employees. The management and client relations activities will be carried out at a separate location and no site visitation will occur. As such, adverse impacts both on-site and off-site will be minimized possible having a lower impact than that of some Rural activities.

- (d) Areas of remnant and riparian vegetation are retained and rehabilitated.

The proposed use will be contained within the existing Shed and no vegetation clearing is proposed or required to carry out the proposed activities. Any local remnant and riparian vegetation will be retained and protected.

STRATEGIC FRAMEWORK

It is of interest that under 6.2.10.2 Purpose (2), that the local government purpose of the Rural Zone Code is to implement Strategic Framework policy directions of Theme 5 Economy, Element 3.8.2 – Economic Growth and diversification.

Element 3.8.2 (1) states that, despite the region not containing a large industrial base, *“value adding manufacturing and processing, research and education can play an important role in the future of economic growth opportunities in the Shire. The lower operational costs for businesses and the unique lifestyle that the Shire offers should be promoted to attract new employment opportunities and economic growth.”*

Whilst the location of the proposed development is not a zone set aside for industrial activities, it is important to note the very small scale of the proposed activities and therefore the practicality of locating the operation on an industrial zones site.

While an Industrial Park is located within Craiglie, the cost of purchasing an industrial allotment and construction of a suitable shed would near \$1 million, a budget well outside of such a small yet critical local business. As such, while the Strategic Framework for Economic Growth and diversification recognizes the “important role” of “manufacturing” “in the future of economic growth opportunities in the Shire”, the Shire does not currently provide affordable industrial zoned land to support the promoted “lower operational costs for businesses”.

It is as a result of this lack of affordable and suitable industrial zoned land that the business owner has resorted to proposed the operation of the fabrication part of the business within the existing Shed located on the site.

2.2 State agency referral items

STATE CONTROLLED ROAD

The development site has frontage to, gains access via and is located within 25m proximity of the Captain Cook Highway, a State Controlled Road. As such, this development application for a Material Change of Use – Low Impact Industry triggers referral to SARA as referral agency. State Code 1 is addressed in Appendix 1.

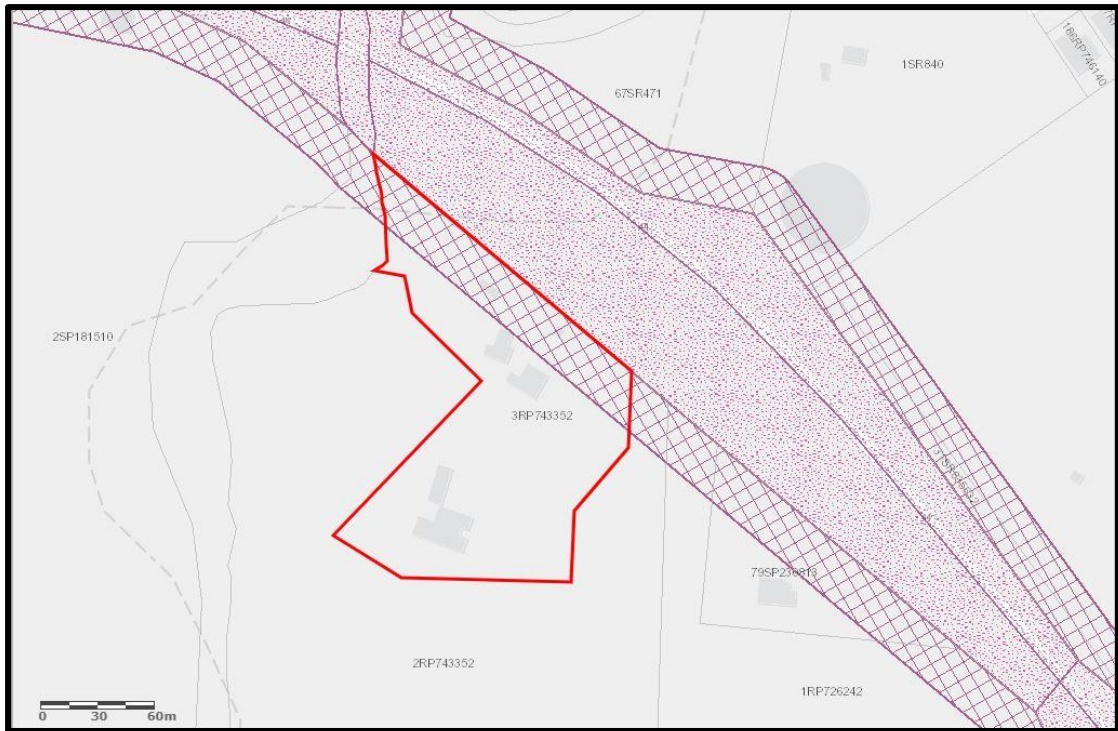


Figure 9: Development Site within 25m of the Captain Cook Hwy. (DAMS mapping).

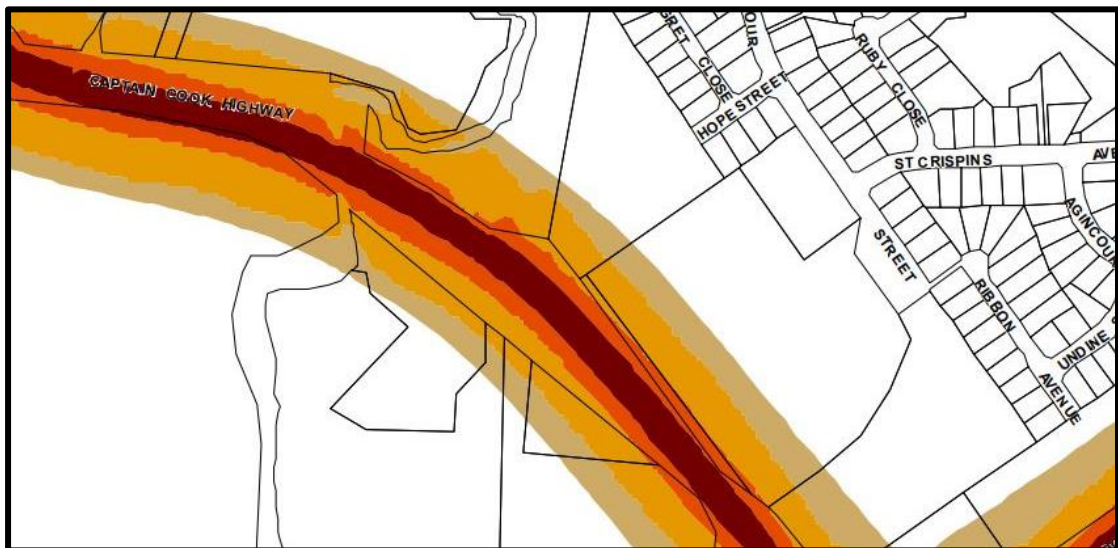


Figure 10: Development Site within the Transport Noise Corridor (DAMS mapping).

3 Planning Summary

This application is for a Development Permit for a Material Change of Use – Low Impact Industry, over land at 6133 Captain Cook Highway, Craiglie Qld. 4877 formally known as Lot 3 on RP743352. The site is located within the Rural Zone, Port Douglas – Craiglie Local Plan Code and mapped within several Overlays as outlined in Section 2.1 of this report.

As prescribed by Table 5.6.j – Rural Zone, the proposed Material Change of Use is Impact Assessable development (Incompatible Use), assessable against the provisions of the relevant Codes of the Douglas Shire Planning Scheme 2018. The site is not burdened by nor a beneficiary of any easements. No new infrastructure is required to service the proposed development.

The proposed development involves the use of the existing class 10a shed structure for the operation of the established local Port Douglas Windows & Glass (QLD) business involving the very small scale fabrication of windows and doors including glazed windows and security windows and doors. Port Douglas Windows & Glass (QLD) is a well-established, highly reputable local business and the only local provider of the essential service to the Port Douglas, Mossman region.

The development site has frontage to, gains access via and is located within 25m proximity of the Captain Cook Highway, a State Controlled Road. As such, this development application for a Material Change of Use – Low Impact Industry triggers referral to SARA as referral agency.

All applicable Planning Codes and State Code 1 are addressed in Appendix 1.

4 Recommendation

It is the professional opinion of Scope Town Planning that, whilst the proposed Material Change of Use over land at 6133 Captain Cook Highway, Craiglie is considered an Incompatible Use on the Rural zoned site, the proposed establishment of the Industrial Activity should be allowed under a relaxation approval as the business provides an essential product and service to the region and is significantly small in scale and of very low impact with no foreseeable adverse impacts related to scenic amenity, local rural activities, environmental values or the transport network.

The proposed development at large satisfies the desired outcomes and requirements of the Douglas Shire Planning Scheme (2018) and Scope Town Planning recommends that this application be fairly assessed and approved by the Douglas Shire Council with reasonable conditions.



Johnathan Burns

Senior Town Planner | **Scope Town Planning**

Appendix 1: Assessment against the Douglas Shire Planning Codes and State Code 1

APPLICATION		PREMISES	
FILE NO:	23006	ADDRESS:	6133 Captain Cook Highway, Craiglie Qld. 4877
APPLICANT:	Land Owner c-/ Scope Town Planning	RPD:	3RP743352
LODGED BY:	Scope Town Planning	AREA:	17,650m ²
DATE LODGED:	March 2023	OWNER :	Angela Whittaker
TYPE OF APPROVAL:	Development Permit		
PROPOSED DEVELOPMENT:	Material Change of Use (Low Impact Industry)		
PLANNING SCHEME:	Douglas Shire Council Planning Scheme (2018)		
ZONE:	Rural		
LEVEL OF ASSESSMENT:	Impact		
SUBMISSIONS:	N/A		

As identified in Part 5 of the Planning Scheme, this development is required to satisfy the Performance Criteria of the following Codes:

6.2.10	Rural Zone Code	9.3.12	Industry Activities Code
7.2.4	Port Douglas – Craiglie Local Plan Code	9.4.1	Access, Parking and Servicing Code
8.2.1	Acid Sulfate Soils Overlay Code	9.4.3	Environmental Performance Code
8.2.2	Bushfire Hazard Overlay Code	9.4.5	Infrastructure Works Code
8.2.4	Flood and Storm Tide Hazard Overlay Code	9.4.6	Landscaping Code
8.2.6	Landscape Values Overlay Code	9.4.9	Vegetation Management Code
8.2.7	Natural Areas Overlay Code		
8.2.10	Transport Network Overlay Code		

This development application triggers referral to SARA as referral agency for assessment against the following Codes:

State Code 1: Development in a state-controlled road environment

6.2.10 Rural Zone Code

The development site is located within the Rural Zone of the Douglas Shire Planning Scheme.



6.2.10.3 Criteria for assessment

Table 6.2.10.3.a – Rural zone code assessable development

Performance outcomes	Acceptable outcomes	Compliance
For self-assessable and assessable development		
PO1 The height of buildings is compatible with the rural character of the area and must not detrimentally impact on visual landscape amenity.	AO1.1 Dwelling houses are not more than 8.5 metres in height. Note – Height is inclusive of roof height. AO1.2 Rural farm sheds and other rural structures are not more than 10 metres in height.	Complies The existing Shed has a height of 4 metres.
PO2 Buildings and structures are setback to maintain the rural character of the area and achieve separation from buildings on adjoining properties.	AO2 Buildings are setback not less than: (a) 40 metres from the property boundary and a State-controlled road; (b) 25 metres from the property boundary adjoining Cape Tribulation Road; (c) 20 metres from the boundary with any other road; (d) 6 metres from side and rear property boundaries.	Complies The portion of the allotment within which the Shed is sited is not deep enough to allow for a 40m setback from the State Controlled Road boundary. The Shed is set back 25m from the front boundary and there are no other buildings nearby on adjoining properties. The Shed is not visible from the road due to retained dense vegetation.
PO3 Buildings/structures are designed to maintain the rural character of the area.	AO3 White and shining metallic finishes are avoided on external surfaces of buildings.	Complies The existing Shed has a dark exterior finish.
For assessable development		
PO4 The establishment of uses is consistent with the outcomes sought for the Rural zone and protects the zone from the intrusion of inconsistent uses.	AO4 Uses identified in Table 6.2.10.3.b are not established in the Rural zone.	Does not Comply The proposed development seeks a relaxation to PO4 to allow the 'inconsistent use'. The proposed Low Impact Industry use is extremely small in scale and intensity, employing only 2 people on site for the fabrication of aluminium security windows and doors and glazed windows. The site is not suitable for many Rural activities due to its odd shape and size having been previously fragmented from its parent Rural allotment. Refer to the Planning Report for further details.

<p>PO5 Uses and other development include those that:</p> <ul style="list-style-type: none"> (a) promote rural activities such as agriculture, rural enterprises and small scale industries that serve rural activities; or (b) promote low impact tourist activities based on the appreciation of the rural character, landscape and rural activities; or (c) are compatible with rural activities. 	<p>AO5 No acceptable outcomes are prescribed.</p>	<p>Does not Comply The proposed development seeks a relaxation to PO5 to allow the 'inconsistent use' which involves a 'small scale industry' which is likely to support rural activities to some degree. The proposed Low Impact Industry use is extremely small in scale and intensity. The site is not suitable for many Rural activities. Refer to the Planning Report for further details.</p>
<p>PO6 Existing native vegetation along watercourses and in, or adjacent to areas of environmental value, or areas of remnant vegetation of value is protected.</p>	<p>AO6 No acceptable outcomes are prescribed.</p>	<p>Complies Existing native vegetation along watercourses and in, or adjacent to areas of environmental value, or areas of remnant vegetation of value will be retained and protected.</p>
<p>PO7 The minimum lot size is 40 hectares,</p> <ul style="list-style-type: none"> (a) unless the lot reconfiguration results in no additional lots (e.g. amalgamation, boundary realignments to resolve encroachments); <p>or</p> <ul style="list-style-type: none"> (b) the reconfiguration is limited to one additional lot to accommodate: <ul style="list-style-type: none"> (i) Telecommunications facility; (ii) Utility installation. 	<p>AO7 No acceptable outcomes are prescribed.</p>	<p>n/a The proposed development is not for an ROL.</p>

7.2.4 Port Douglas – Craiglie Local Plan Code

The development site is located within the Port Douglas – Craiglie Local Plan Code of the Douglas Shire Planning Scheme.

7.2.4.4 Criteria for assessment

Table 7.2.4.4.a – Port Douglas / Craiglie local plan – assessable development

Performance outcomes	Acceptable outcomes	Compliance
For self-assessable and assessable development		
Development in the Port Douglas / Craiglie local plan area generally		
PO1 Pedestrians, cyclists, motorists and public transport users can easily move into and through the precinct along planned connectivity routes, identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.	AO1 A pedestrian and cycle movement network is integrated and delivered through development.	n/a The development site does not intersect a connectivity point.
PO2 Development retains and enhances key landscape elements including character trees and areas of significant vegetation contributing to the character and quality of the local plan area and significant views and vistas and other landmarks important to the context of Port Douglas / Craiglie (as identified on the Port Douglas/ Craiglie Townscape Plan map contained in Schedule 2).	AO2.1 Development provides for the retention and enhancement of existing mature trees and character vegetation that contribute to the lush tropical character of the town, including: <ul style="list-style-type: none"> (a) the tree covered backdrop of Flagstaff Hill; (b) natural vegetation along watercourses, in particular the Mowbray River, Beor Creek and Dickson Inlet; (c) the tidal vegetation along the foreshore; (d) beachfront vegetation along Four Mile Beach, including the fringe of Coconut Palms; (e) the oil palm avenues along the major roads; (f) the lush landscaping within major roundabouts at key nodes; (g) Macrossan Street and Warner Street; (h) Port Douglas waterfront. 	Complies All existing vegetation is to be retained.

	<p>AO2.2 Development protects and does not intrude into important views and vistas as identified on the Port Douglas Townscape Plan map contained in Schedule 2, in particular:</p> <ul style="list-style-type: none"> (a) Flagstaff Hill; (b) Four Mile Beach; (c) Across to the ranges over Dickson Inlet; (d) Mowbray Valley. <p>AO2.3 Important landmarks, memorials and monuments are retained.</p>	
<p>PO3 Development contributes to the protection, reinforcement and where necessary enhancement of gateways and key intersections identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.</p>	<p>AO3 Development adjacent to the gateways and nodes as identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 incorporates architectural features and landscaping treatments and design elements that enhance the sense of arrival and way finding within the town.</p>	<p>n/a The development site is not situated adjacent to the gateways or nodes as identified on the Port Douglas / Craiglie local plan maps.</p>
<p>PO4 Landscaping of development sites complements the existing tropical character of Port Douglas and Craiglie.</p>	<p>AO4 Landscaping incorporates the requirements of Planning scheme policy SC6.7 – Landscaping, in particular landscaping should be capable of achieving a 60% screening of development within 5 years and predominantly consists of endemic vegetation.</p>	<p>Complies The existing mature vegetation along the road frontage and within the road reserve provides close to 100% screening of the development site. All existing landscaping and vegetation will be retained. No additional landscaping is proposed.</p>
<p>PO5 Development does not compromise the safety and efficiency of the State-controlled road network.</p>	<p>AO5 Direct access is not provided to a State-controlled road where legal and practical access from another road is available.</p>	<p>Complies The site gains access to the Captain Cook Highway which is the only legal and practical site access point.</p> <p>The proposed development does not compromise the safety and efficiency of the State-controlled road network.</p> <p>Refer to Section 1.4 of the Planning Report for more details.</p>

For assessable development		
Additional requirements in Precinct 1 – Port Douglas precinct		
PO6 The views and vistas identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 are maintained.	AO6.1 Development does not impede continued views to scenic vistas and key streetscapes within the local plan area. AO6.2 Unless otherwise specified within this Local Plan, buildings are set back not less than 6 metres from the primary street frontage.	n/a The site is not located within a precinct.
PO7 Vehicle access, parking and service areas: (a) do not undermine the relationship between buildings and street or dominate the streetscape; (b) are designed to minimise pedestrian vehicle conflict; (c) are clearly identified and maintain ease of access at all times.	AO7.1 For all buildings, parking is: (a) to the side of buildings and recessed behind the main building line; or (b) behind buildings; or (c) wrapped by the building façade, and not visible from the street. AO7.2 Ground level parking incorporates clearly defined pedestrian routes. AO7.3 Any porte-cocheres, disabled and pedestrian accesses are accommodated within the boundary of new or refurbished development. AO7.4 Where the development is an integrated mixed-use development incorporating short term accommodation or multiple dwellings and either food and drink outlet or hotel or shop or shopping centre or office, on-site parking spaces are provided as per the number prescribed in the Parking and access code with a relaxation of 30% of spaces required for the non-residential uses. AO7.5 On-site car parking available for public use is clearly signed at the site frontage.	n/a The site is not located within a precinct.

	AO7.6 Boom gates, pay machines or other regulatory devices to control access to a publicly available car parking area are not constructed or installed.	
PO8 Precinct 1 – Port Douglas precinct is not characterised by a proliferation of advertising signs.	AO8 No acceptable outcomes are prescribed.	n/a The site is not located within a precinct.
Additional requirements for Sub-precinct 1a – Town Centre sub-precinct		
PO9 Building heights: (a) do not overwhelm or dominate the town centre; (b) respect the desired streetscape; (c) ensure a high quality appearance when viewed from both within the town centre sub-precinct and external to the town centre sub-precinct; (d) remain subservient to the natural environment and the backdrop of Flagstaff Hill. (e) do not exceed 3 storeys.	AO9 Buildings and structures are not more than 3 storeys and 13.5 metres in height, with a roof height of not less than 3 metres. Note – Height is inclusive of the roof height.	n/a The site is not located within a sub-precinct.
PO10 Building design, the streetscape, pedestrian paths and street front spaces promote integration with the surrounding area and the rest of Precinct 1 – Port Douglas Precinct.	AO10 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.
PO11 Buildings: (a) address street frontages; (b) ensure main entrances front the street or public spaces; (c) do not focus principally on internal spaces or parking areas.	AO11 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.

<p>PO12 Setbacks at ground level provide for:</p> <ul style="list-style-type: none"> (a) connection between pedestrian paths and public places; (b) areas for convenient movement of pedestrians; (c) changes in gradient of the street. 	<p>AO12 Setbacks at ground level:</p> <ul style="list-style-type: none"> (a) are clear of columns and other obstructions; (b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths. 	<p>n/a The site is not located within a sub-precinct.</p>
<p>AO13 Buildings do not result in a reduction of views and vistas from public places to:</p> <ul style="list-style-type: none"> (a) Flagstaff Hill; (b) Dickson Inlet; (c) public open space; (d) places of significance. 	<p>AO13 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO14 Development enhances the distinctive tropical resort town and identity of Port Douglas and encourages pedestrian activity at street level including shade protection across the footpath for the length of the building.</p>	<p>AO14 Development is built up to the street frontage/s at the street level and incorporates a light frame awning, a minimum of 3 metres in width for the length of the street frontage/s; or If a development includes an outdoor dining area at ground/footpath level, the dining area has a maximum setback of 3 metres and the required awning is still maintained along the length of the street frontage/s.</p> <p>Note – PO24 provides more detail on awning design.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO15 Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street- level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.</p>	<p>AO15.1 Centre activities establish: at street level on active street frontages; a maximum of one level above street level.</p> <p>AO15.2 Any residential development activities or short term accommodation is located above street level of the active frontage, but not on or up to the street frontage in any development, including mixed use development.</p>	<p>n/a The site is not located within a sub-precinct.</p>

<p>PO16 Detailed building design:</p> <ul style="list-style-type: none"> (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements; (c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the lush, vegetated character of the Town Centre sub-precinct is maintained. 	<p>AO16 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO17 Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through:</p> <ul style="list-style-type: none"> (a) surface decoration; (b) wall recesses and projections; (c) a variation in wall finishes; windows, balconies, awnings and other visible structural elements. (d) differentiating between the lower, middle and upper parts of the building by varying the façade and/or the shape of the built form, where comprised of more than two storeys. 	<p>AO17 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO18 Roofs are not characterised by a cluttered display of plant and equipment, in particular:</p> <ul style="list-style-type: none"> (a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Town Centre sub-precinct; (b) service structures, lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view; (c) rooftops are not used for advertising. 	<p>AO18 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>

<p>P019 Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to:</p> <ul style="list-style-type: none"> (a) shade windows; (b) reduce glare; (c) assist in maintaining comfortable indoor temperatures; (d) minimising heat loads; (e) enrich the North Queensland tropical character of the Town Centre sub-precinct; (f) provide architectural interest to building façades. 	<p>AO19 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>P020 Buildings are finished with high quality materials, selected for:</p> <ul style="list-style-type: none"> (a) their ability to contribute the character of Town Centre sub-precinct; (b) easy maintenance, durability and an ability not to readily stain, discolour or deteriorate. 	<p>AO20 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>P021 Buildings do not incorporate any type of glass or other materials that are likely to reflect the sun's rays in a manner that may create a nuisance, discomfort or a hazard.</p>	<p>AO21 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>P022 Façades and elevations do not include large blank walls. Openings and setbacks are used to articulate vertical building surfaces.</p>	<p>AO22.1 Development has a maximum length of unbroken building facade of 20 metres and a maximum extent of overall development in the same style/design along the street frontage/s of 40 metres.</p> <p>AO22.2 Any break in the building façade varies the alignment by a 1 metre minimum deviation.</p> <p>AO22.3 A minimum of three of the following building design features and architectural elements detailed below are incorporated to break the extended facade of a development:</p>	<p>n/a The site is not located within a sub-precinct.</p>

	<ul style="list-style-type: none"> (a) a change in roof profile; (b) a change in parapet coping; (c) a change in awning design; (d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development. 	
PO23 Building facades that face public spaces at ground level: <ul style="list-style-type: none"> (a) complement the appearance of the development and surrounding streetscape; (b) enhance the visual amenity of the public place; (c) include a variety of human scale architectural elements and details; (d) provide an opportunity for the casual and convenient surveillance of public space from within the development. 	AO23 Building facades at the ground floor of development that face public space are designed to ensure: <ul style="list-style-type: none"> (a) a minimum of 70% of the façade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development; (b) a visually prominent main entrance that faces the principal public place; (c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the façade. 	n/a The site is not located within a sub-precinct.
PO24 Awnings for pedestrian shelter are consistent with the character setting of the Town Centre sub-precinct and: <ul style="list-style-type: none"> (a) extend and cover the footpath to provide protection from the sun and rain; (b) include lighting under the awning; (c) are continuous across the frontage of the site; (d) align to provide continuity with existing or future awnings on adjoining sites; (e) are a minimum of 3.0 metres in width and generally not more than 3.5 metres above pavement height; (f) do not extend past a vertical plane, 1.2 metres inside the kerb-line to enable street trees to be planted and grow; (g) are cantilevered from the main building with any posts within the footpath being non load-bearing. 	AO24 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.

PO25 Development integrates with the streetscape and landscaping improvements for Port Douglas.	AO25 Development fronting Davidson Street, Macrossan Street, Wharf Street, Mowbray Street and Warner Street is designed to integrate with the on-street landscaping and design improvements as outlined within the Port Douglas landscape master plan contained within Planning scheme policy SC6.7 – Landscaping. Note - Planning scheme policy SC6.7 - Landscaping provides guidance on meeting the Performance Outcome.	n/a The site is not located within a sub-precinct.
Additional requirements for Sub-precinct 1b – Waterfront North sub-precinct		
PO26 The establishment of uses is consistent with the outcomes sought for sub-precinct 1b – Waterfront North.	AO26 Uses identified as inconsistent uses in Table 7.2.4.4.b — Inconsistent uses in sub-precinct 1b - Waterfront North sub-precinct are not established in sub-precinct 1b - Waterfront North.	n/a The site is not located within a sub-precinct.
PO27 The bulk and scale of buildings is consistent with surrounding development and steps down to complement the open space areas in the adjoining limited development sub-precinct.	AO27 Buildings and structures are not more than: (a) 3 storeys and 13.5 metres in height , with a roof height of not less than 3 metres, in those parts of the precinct south of Inlet Street; (b) 2 storeys and 8.5 metres in height, with a roof height of not less than 3 metres, in those parts of the precinct north of Inlet Street. Note – Height is inclusive of roof height.	n/a The site is not located within a sub-precinct.
PO28 Building design, streetscape, pedestrian paths and street front spaces promote integration with the surrounding area and the rest of Precinct 1 – Port Douglas Precinct.	AO28 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.
PO29 Public pedestrian access along the water's edge is maximised.	AO29.1 Public pedestrian access is provided along the frontage of the water's edge consisting of a boardwalk of a minimum width of 4 metres that is available of 24-hour use. AO29.2 A public plaza is incorporated into the design generally reflecting the requirements of the Port	n/a The site is not located within a sub-precinct.

	<p>Douglas Waterfront Master Plan, focusing in the vicinity of the 'Duck Pond'.</p> <p>AO29.3 Built envelopes are setback a minimum of 3.0 metres from the board walk, with a shelter/shade zone between the building envelopes and the boardwalk consisting of shade structure, canopies, verandahs and the like.</p>	
<p>PO30 Buildings: (a) address street frontages; (b) ensure main entrances front the street or public spaces.</p>	<p>AO30 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO31 Setbacks at ground level provide for: (a) connection between pedestrian paths and public places; (b) areas for convenient movement of pedestrians; (c) changes in gradient.</p>	<p>AO31 Setbacks at ground level: (a) are clear of columns and other obstructions; (b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO32 Buildings do not result in a reduction of views and vistas from public places to: (a) Dickson Inlet; (b) public open space; (c) places of significance.</p>	<p>AO32 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO33 Development enhances the distinctive tropical resort town and identity of Port Douglas and encourages pedestrian activity at ground level including shade protection across the footpath and open space areas.</p>	<p>AO33 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>

<p>PO34 Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street- level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.</p>	<p>AO34.1 Centre activities establish: (a) at street level on active street frontages; (b) a maximum of one level above street level.</p> <p>AO34.2 Residential development activities or short term accommodation is located above street /ground floor level of the active frontage, but not on or up to the street / public frontage in any development, including mixed use development.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO35 Detailed building design: (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements; (c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the lush, vegetated character of the Waterfront North sub-precinct is maintained.</p>	<p>AO35 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO36 Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through: (a) surface decoration; (b) wall recesses and projections; (c) a variation in wall finishes; windows, balconies, awnings and other visible structural elements. (d) differentiating between the lower, middle and upper parts of the building by varying the façade and/or the shape of the built form, where comprised of more than two storeys.</p>	<p>AO36 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>

PO37 Roofs are not characterised by a cluttered display of plant and equipment, in particular: <ul style="list-style-type: none"> (a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Waterfront North sub-precinct; (b) service structures, lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view; (c) rooftops are not used for advertising. 	AO37 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.
PO38 Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to: <ul style="list-style-type: none"> (a) shade windows; (b) reduce glare; (c) assist in maintaining comfortable indoor temperatures; (d) minimising heat loads; (e) enriching the North Queensland tropical character of the Waterfront North sub-precinct; (f) architectural interest to building façades. 	AO38 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.
PO39 Buildings are finished with high quality materials, selected for: <ul style="list-style-type: none"> (a) their ability to contribute the character of Waterfront North sub-precinct; (b) easy maintenance, durability and an ability not to readily stain, discolour or deteriorate. 	AO39 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.
PO40 Buildings do not incorporate any type of glass or other materials that are likely to reflect the sun's rays in a manner that may create a nuisance, discomfort or a hazard.	AO40 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.
PO41 Façades and elevations do not include large blank walls and openings and setbacks are used to	AO41.1 Development has a maximum length of unbroken building facade of 20 metres and a maximum	n/a The site is not located within a sub-precinct.

<p>articulate vertical building surfaces.</p>	<p>extent of overall development in the same style/design along the street frontage/s of 40 metres.</p> <p>AO41.2 Any break in the building façade varies the alignment by a 1 metre minimum deviation.</p> <p>AO41.3 A minimum of three of the following building design features and architectural elements detailed below are incorporated to break the extended facade of a development:</p> <ul style="list-style-type: none"> (a) a change in roof profile; (b) a change in parapet coping; (c) a change in awning design; (d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development. 	
<p>PO42 Building facades that face public spaces at ground level:</p> <ul style="list-style-type: none"> (a) complement the appearance of the development and surrounding streetscape; (b) enhance the visual amenity of the public place; (c) include a variety of human scale architectural elements and details; (d) provide an opportunity for the casual and convenient surveillance of public space from within the development. 	<p>AO42 Building facades at the ground floor of development that face public space are designed to ensure:</p> <ul style="list-style-type: none"> (a) a minimum of 70% of the façade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development; (b) a visually prominent main entrance that faces the principal public place; (c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the façade. 	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO43 Awnings for pedestrian shelter are consistent with the character setting of the Waterfront North sub-precinct and:</p> <ul style="list-style-type: none"> (a) extend and cover the footpath to provide protection from the sun and rain; (b) include lighting under the awning; (c) are continuous across pedestrian circulation areas; 	<p>AO43 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>

<p>(d) align to provide continuity with existing or future awnings on adjoining sites;</p> <p>(e) are a minimum of 3 metres in width and generally not more than 3.5 metres above pavement height;</p> <p>(f) do not extend past a vertical plane, 1.2 metres inside the street kerb-line to enable street trees to be planted and grow;</p> <p>(g) are cantilevered from the main building with any posts within the footpath being non load-bearing.</p>		
<p>PO44 The Balley Hooley rail line and turn-table is retained and incorporated into development and maintains its functionality.</p>	<p>AO44.1 Bally Hooley rail line and turn-table is retained and incorporated into development to maintain its functionality.</p> <p>AO44.2 Where development provides floor area for the Bally Hooley rail station, the gross floor area of the rail line and station does not generate a requirement for additional vehicle parking.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO45 Development recognises the importance of and relationship between the marina, commercial and residential development in the Waterfront North sub-precinct, and includes measures to mitigate the impact of:</p> <p>(a) noise;</p> <p>(b) odour;</p> <p>(c) hazardous materials;</p> <p>(d) waste and recyclable material storage.</p>	<p>AO45 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO46 Formalised public spaces and pedestrian paths/areas on freehold land are made accessible to the public.</p>	<p>AO46 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO47 Buildings, civic spaces, roads and pedestrian links are enhanced by:</p> <p>(a) appropriate landscape design and planting;</p> <p>(b) themed planting that defines entry points, and creates strong 'entry corridors' into the</p>	<p>AO47 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>

<p>waterfront;</p> <p>(c) lighting and well-considered discrete signage that complements building and landscape design;</p> <p>(d) public artwork and other similar features that reflect the heritage and character of the Port Douglas Waterfront.</p>		
<p>PO48</p> <p>Buildings are designed and sited to provide vistas along shared pedestrian/open space and movement areas in suitable locations.</p>	<p>AO48</p> <p>No acceptable outcomes are prescribed.</p>	<p>n/a</p> <p>The site is not located within a sub-precinct.</p>
<p>PO49</p> <p>Development does not diminish the viability of marine-based industrial uses that directly serve the Port Douglas tourist and fishing operators and private boat owners, particularly with respect to the slipway operation.</p>	<p>AO49</p> <p>No acceptable outcomes are prescribed.</p>	<p>n/a</p> <p>The site is not located within a sub-precinct.</p>
<p>PO50</p> <p>Marine infrastructure to service the tourism, fishing and private boating community is provided.</p>	<p>AO50</p> <p>No acceptable outcomes are prescribed.</p>	<p>n/a</p> <p>The site is not located within a sub-precinct.</p>
<p>PO51</p> <p>Changes to the Port Douglas Waterfront quay- line do not cause adverse impacts to the environmentally sensitive Dickson Inlet.</p>	<p>AO51</p> <p>Development that results in changes to the Port Douglas Waterfront quay-line is only established where an Ecological assessment report provides support to the changes.</p> <p>Note - Planning scheme policy SC6.8 – Natural environment provides guidance on preparing an ecological assessment report.</p>	<p>n/a</p> <p>The site is not located within a sub-precinct.</p>
<p>Additional requirements for Sub-precinct 1c – Waterfront South sub-precinct</p>		
<p>PO52</p> <p>The establishment of uses is consistent with the outcomes sought for Precinct 1c – Waterfront South.</p>	<p>AO52</p> <p>Uses identified as inconsistent uses Table 7.2.4.4.c are not established in Precinct 1c – Waterfront South.</p>	<p>n/a</p> <p>The site is not located within a sub-precinct.</p>

<p>PO53 Development does not adversely impact on the natural environment, natural vegetation or watercourses.</p>	<p>AO53.1 An Ecological assessment report is prepared identifying the environmental qualities of the surrounding natural and built features which are to be managed.</p> <p>Note - Planning scheme policy SC6.8 – Natural environment provides guidance on preparing an ecological assessment report.</p> <p>AO53.2 An Environmental Management Plan is prepared to manage potential impacts of the operation of the development on surrounding natural areas.</p> <p>Note - Planning scheme policy SC6.4 – Environmental management plans contains information to demonstrate compliance and guidance on preparing an Environmental Management Plan.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO54 Development of land at the end of Port Street adjacent to Dickson Inlet incorporates a slipway, or an alternative functioning facility, with capacity to service the Port Douglas marine and tourism industry.</p>	<p>AO54 A master plan for the development is provided and implemented to demonstrate the integration of the slipway, or an alternative functioning facility, with other supporting service industry activities that service the marine and tourism industry of Port Douglas.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO55 Buildings and structures are of a height, and are set back from side boundaries and other sensitive areas to ensure the scenic amenity and environmental qualities of the adjacent area are not adversely affected.</p>	<p>AO55.1 Development has a height of not more than 10 metres.</p> <p>AO55.2 Development is setback from all property boundaries not less than 3 metres.</p>	<p>n/a The site is not located within a sub-precinct.</p>

<p>PO56 The site coverage of all buildings and structures ensures development:</p> <ul style="list-style-type: none"> (a) is sited in an existing cleared area or in an area approved for clearing; (b) has sufficient area for the provision of services; (c) development does not have an adverse effect on the environmental, habitat, conservation or landscape values of the on-site and surrounding sensitive areas. 	<p>AO56 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO57 Premises include adequate provision for service vehicles, to cater for generated demand. Loading areas for service vehicles are designed to:</p> <ul style="list-style-type: none"> (a) be accommodated on-site; (b) maximise safety and efficiency of loading; (c) protect the visual and acoustic amenity of sensitive land use activities; (d) minimise adverse impacts on natural characteristics of adjacent areas. 	<p>AO57.1 Sufficient manoeuvring area is provided on-site to allow a Medium Rigid Vehicle to enter and leave the site in a forward gear.</p> <p>AO57.2 Development is designed to ensure all service vehicles are contained within the site when being loaded/unloaded.</p> <p>AO57.3 Driveways, parking and manoeuvring areas are constructed and maintained to:</p> <ul style="list-style-type: none"> (a) minimise erosion from storm water runoff; (b) retain all existing vegetation. 	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO58 Development ensures adverse impacts from service vehicles on the road network, external to the site, are minimised.</p>	<p>AO58 No acceptable outcomes are prescribed.</p>	<p>n/a The site is not located within a sub-precinct.</p>
<p>PO59 Entry to the site is landscaped to enhance the amenity of the area and provide a pleasant working environment.</p>	<p>AO59 Areas used for loading and unloading, storage, utilities and car parking are screened from public view:</p> <ul style="list-style-type: none"> (a) by a combination of landscaping and screen fencing; (b) dense planting along any road frontage is a minimum width of 3 metres. 	<p>n/a The site is not located within a sub-precinct.</p>

PO60 Landscaping is informal in character and complementary to the existing natural environment, provides screening and enhances the visual appearance of the development.	AO60 For any development landscaping is in accordance with the Plant species schedule in Planning scheme policy SC6.7– Landscaping.	n/a The site is not located within a sub-precinct.
Additional requirements for Sub-precinct 1d – Limited Development sub-precinct		
PO61 The height of buildings and structures contributes to the desired form and outcomes for the sub-precinct and are limited to a single storey.	AO61 Buildings and structures are not more than one storey and 4 metres in height. Note - Height is inclusive of the roof height.	n/a The site is not located within a sub-precinct.
Additional requirements for Sub-precinct 1e – Community and recreation sub-precinct		
PO62 The precinct is developed for organised sporting activities and other community uses.	AO62 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.
Additional requirements for Sub-precinct 1f – Flagstaff Hill sub-precinct		
PO63 Flagstaff Hill is protected from inappropriate development to protect the hill as an important natural landmark feature of Port Douglas and as a vegetated backdrop to the Town centre.	AO63 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.
PO64 All development on Flagstaff Hill is designed to minimise the visibility of the development and to ensure development is subservient to the natural landscape and topography of the site, including through: (a) building design which minimises excavation and filling; (b) buildings being designed to step down the site and incorporate foundations and footings on piers or poles; (c) buildings being visually unobtrusive and incorporating exterior finishes and muted colours which are non-reflective and complement the colours of the surrounding vegetation and view-shed; (d) protection of the views from public viewing points in the Port Douglas precinct.	AO64 No acceptable outcomes are prescribed.	n/a The site is not located within a sub-precinct.

Additional requirements for Precinct 3 – Craiglie Commercial and Light Industry precinct		
PO65 Development supports the tourism and marine industries in Port Douglas, along with the small-scale commercial and light industry land uses that support the local economy that would otherwise be better suited to a location outside the Port Douglas Town Centre Precinct.	AO65 Development consists of service and light industries and associated small scale commercial activities.	n/a The site is not located within a precinct.
PO66 Development on lots adjacent to the Captain Cook Highway is sited, designed and landscaped to provide an attractive visual approach to Port Douglas with all buildings, structures and car parking areas setback a sufficient distance from the frontage to enable landscaping to soften or screen the appearance of the development.	AO66.1 Buildings and structures are setback 8 metres from the Captain Cook Highway frontage, or no closer to the Captain Cook Highway frontage than buildings and structures on adjoining sites (averaged), whichever is the greater. AO66.2 The setback area to the Captain Cook Highway frontage is landscaped with advanced dense planting including tree species (100 litre bag stock), which will, at maturity, exceed the height of the building(s) on the site. AO66.3 Advertising signs are discreet in appearance with no large advertising signs, including tenancy signs, located on or near the Captain Cook Highway frontage, or within any landscaped setback area. AO66.4 Car parking areas, loading and other service areas are designed to be screened from the Captain Cook Highway and are located so as to not be visually prominent from the Captain Cook Highway.	n/a The site is not located within a precinct.
Additional requirements for Precinct 6 – Very Low Residential Density / Low Scale Recreation / Low Scale Educational / Low Scale Entertainment Uses precinct		
PO67 No additional lots are created within the precinct.	AO67 No acceptable outcomes are prescribed.	n/a The site is not located within a precinct.
PO68 Reconfigured lots have a minimum lot size of 2 hectares, unless the lot reconfiguration transfers lots to the higher parts of the land, to avoid the need to fill existing lots to accommodate dwelling houses.	AO68 No acceptable outcomes are prescribed.	n/a The site is not located within a precinct.

8.2.1 Acid Sulfate Soils Overlay Code

The development site is located within the Acid Sulfate Soils Overlay area of the Douglas Shire Planning Scheme.



8.2.1.3 Criteria for assessment

Table 8.2.1.3.a – Acid sulfate soils overlay code – assessable development

Performance outcomes	Acceptable outcomes	Compliance
For assessable development		
PO1 The extent and location of potential or actual acid sulfate soils is accurately identified.	AO1.1 No excavation or filling occurs on the site. or AO1.2 An acid sulfate soils investigation is undertaken. Note - Planning scheme policy SC 6.12– Potential and actual acid sulfate soils provides guidance on preparing an acid sulfate soils investigation.	Complies No excavation is proposed with this development.
PO2 Development avoids disturbing potential acid sulfate soils or actual acid sulfate soils, or is managed to avoid or minimise the release of acid and metal contaminants.	AO2.1 The disturbance of potential acid sulfate soils or actual acid sulfate soils is avoided by: (a) not excavating, or otherwise removing, soil or sediment identified as containing potential or actual acid sulfate soils; (b) not permanently or temporarily extracting groundwater that results in the aeration of previously saturated acid sulfate soils; (c) not undertaking filling that results in: (d) actual acid sulfate soils being moved below the water table; (e) previously saturated acid sulfate soils being aerated. or AO2.2 The disturbance of potential acid sulfate soils or actual acid sulfate soils is undertaken in accordance with an acid sulfate soils management plan and avoids the release of metal contaminants by: (a) neutralising existing acidity and preventing the generation of acid and metal contaminants;	Complies No excavation is proposed with this development.

	<ul style="list-style-type: none"> (b) preventing the release of surface or groundwater flows containing acid and metal contaminants into the environment; (c) preventing the in situ oxidation of potential acid sulfate soils and actual acid sulfate soils through ground water level management; (d) appropriately treating acid sulfate soils before disposal occurs on or off site; (e) documenting strategies and reporting requirements in an acid sulfate soils environmental management plan. <p>Note - Planning scheme policy SC 6.12 – Acid sulfate soils provides guidance on preparing an acid sulfate soils management plan.</p>	
PO3 No environmental harm is caused as a result of exposure to potential acid sulfate soils or actual acid sulfate soils.	AO3 No acceptable outcomes are prescribed.	Complies No exposure to potential acid sulfate soils or actual acid sulfate soils is likely.

8.2.2 Bushfire Hazard Overlay Code

The development site is located within the Bushfire Hazard Overlay area of the Douglas Shire Planning Scheme.



8.2.2.3 Criteria for assessment

Table 8.2.2.3.a – Bushfire hazard overlay code –assessable development

Performance outcomes	Acceptable outcomes	Compliance
For self-assessable and assessable development		
Compatible development		
PO1 A vulnerable use is not established or materially intensified within a bushfire hazard area (bushfire prone area) unless there is an overriding need or other exceptional circumstances. Note - See the end of this code for examples of vulnerable uses.	AO1 Vulnerable uses are not established or expanded. Note – Where, following site inspection and consultation with Council, it is clear that the mapping is in error in identifying a premises as being subject to a medium, high, very high bushfire hazard or potential impact buffer sub-category, Council may supply a letter exempting the need for a Bushfire Management Plan. Note – Where the assessment manager has not previously approved a Bushfire Management Plan (either by condition of a previous development approval), the development proponent will be expected to prepare such a plan. Note – Planning scheme policy SC6.9 - Natural hazards, provides a guide to the preparation of a Bushfire Management Plan.	Complies The proposed development is not a vulnerable use.
PO2 Emergency services and uses providing community support services are able to function effectively during and immediately after a bushfire hazard event.	AO2 Emergency Services and uses providing community support services are not located in a bushfire hazard sub-category and have direct access to low hazard evacuation routes.	Complies The proposed development is not an emergency service of community support service.
PO3 Development involving hazardous materials manufactured or stored in bulk is not located in bushfire hazard sub-category.	AO3 The manufacture or storage of hazardous material in bulk does not occur within bushfire hazard sub-category.	Complies The proposed development does not involve the manufacture or storage of hazardous material in bulk.

Development design and separation from bushfire hazard – reconfiguration of lots		
<p>PO4.1 Where reconfiguration is undertaken in an urban area or is for urban purposes or smaller scale rural residential purposes, a separation distance from hazardous vegetation is provided to achieve a radiant heat flux level of 29kW/m^2 at the edge of the proposed lot(s).</p> <p>Note - "Urban purposes" and "urban area" are defined in the <i>Sustainable Planning Regulations 2009</i>. Reconfiguration will be taken to be for rural residential purposes where proposed lots are between 2000m^2 and 2ha in area. "Smaller scale" rural residential purposes will be taken to be where the average proposed lot size is 6000m^2 or less.</p> <p>Note - The radiant heat levels and separation distances are to be established in accordance with method 2 set out in AS3959-2009.</p> <p>PO4.2 Where reconfiguration is undertaken for other purposes, a building envelope of reasonable dimensions is provided on each lot which achieves radiant heat flux level of 29kW/m^2 at any point.</p>	<p>AO4.1 No new lots are created within a bushfire hazard sub-category.</p> <p>or</p> <p>AO4.2 Lots are separated from hazardous vegetation by a distance that:</p> <ul style="list-style-type: none"> (a) achieves radiant heat flux level of 29kW/m^2 at all boundaries; and (b) is contained wholly within the development site. <p>Note - Where a separation distance is proposed to be achieved by utilising existing cleared developed areas external to the site, certainty must be established (through tenure or other means) that the land will remain cleared of hazardous vegetation. For staged developments, temporary separation distances, perimeter roads or fire trails may be absorbed as part of subsequent stages. Note - The achievement of a cleared separation distance may not be achievable where other provisions within the planning scheme require protection of certain ecological, slope, visual or character features or functions.</p>	<p>n/a The proposed development is not an ROL.</p>

<p>PO5 Where reconfiguration is undertaken in an urban area or is for urban purposes, a constructed perimeter road with reticulated water supply is established between the lots and the hazardous vegetation and is readily accessible at all times for urban fire fighting vehicles.</p> <p>The access is available for both fire fighting and maintenance/defensive works.</p>	<p>AO5.1 Lot boundaries are separated from hazardous vegetation by a public road which:</p> <ul style="list-style-type: none"> (a) has a two lane sealed carriageway; (b) contains a reticulated water supply; (c) is connected to other public roads at both ends and at intervals of no more than 500m; (d) accommodates geometry and turning radii in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; (e) has a minimum of 4.8m vertical clearance above the road; (f) is designed to ensure hydrants and water access points are not located within parking bay allocations; and (g) incorporates roll-over kerbing. <p>AO5.2 Fire hydrants are designed and installed in accordance with AS2419.1 2005, unless otherwise specified by the relevant water entity.</p> <p>Note - Applicants should have regard to the relevant standards set out in the reconfiguration of a lot code and works codes in this planning scheme.</p>	<p>n/a The proposed development is not an ROL.</p>
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<p>PO6</p> <p>Where reconfiguration is undertaken for smaller scale rural residential purposes, either a constructed perimeter road or a formed, all weather fire trail is established between the lots and the hazardous vegetation and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>The access is available for both fire fighting and maintenance/hazard reduction works.</p>	<p>AO6</p> <p>Lot boundaries are separated from hazardous vegetation by a public road or fire trail which has:</p> <ul style="list-style-type: none"> (a) a reserve or easement width of at least 20m; (b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15tonne vehicle and which is at least 6m clear of vegetation; (c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path; (d) a minimum of 4.8m vertical clearance; (e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; (f) a maximum gradient of 12.5%; (g) a cross fall of no greater than 10 degrees; (h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy; (i) vehicular access at each end which is connected to the public road network at intervals of no more than 500m; (j) designated fire trail signage; (k) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and (l) if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services. 	<p>n/a</p> <p>The proposed development is not an ROL.</p>
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<p>P07 Where reconfiguration is undertaken for other purposes, a formed, all weather fire trail is provided between the hazardous vegetation and either the lot boundary or building envelope, and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>However, a fire trail will not be required where it would not serve a practical fire management purpose.</p>	<p>A07 Lot boundaries are separated from hazardous vegetation by a public road or fire trail which has:</p> <ul style="list-style-type: none"> (a) a reserve or easement width of at least 20m; (b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15tonne vehicle and which is at least 6m clear of vegetation; (c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path; (d) a minimum of 4.8m vertical clearance; (e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; (f) a maximum gradient of 12.5%; (g) a cross fall of no greater than 10 degrees; (h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy; (i) vehicular access at each end which is connected to the public road network; (j) designated fire trail signage; (k) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and (l) if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services. 	<p>n/a The proposed development is not an ROL.</p>
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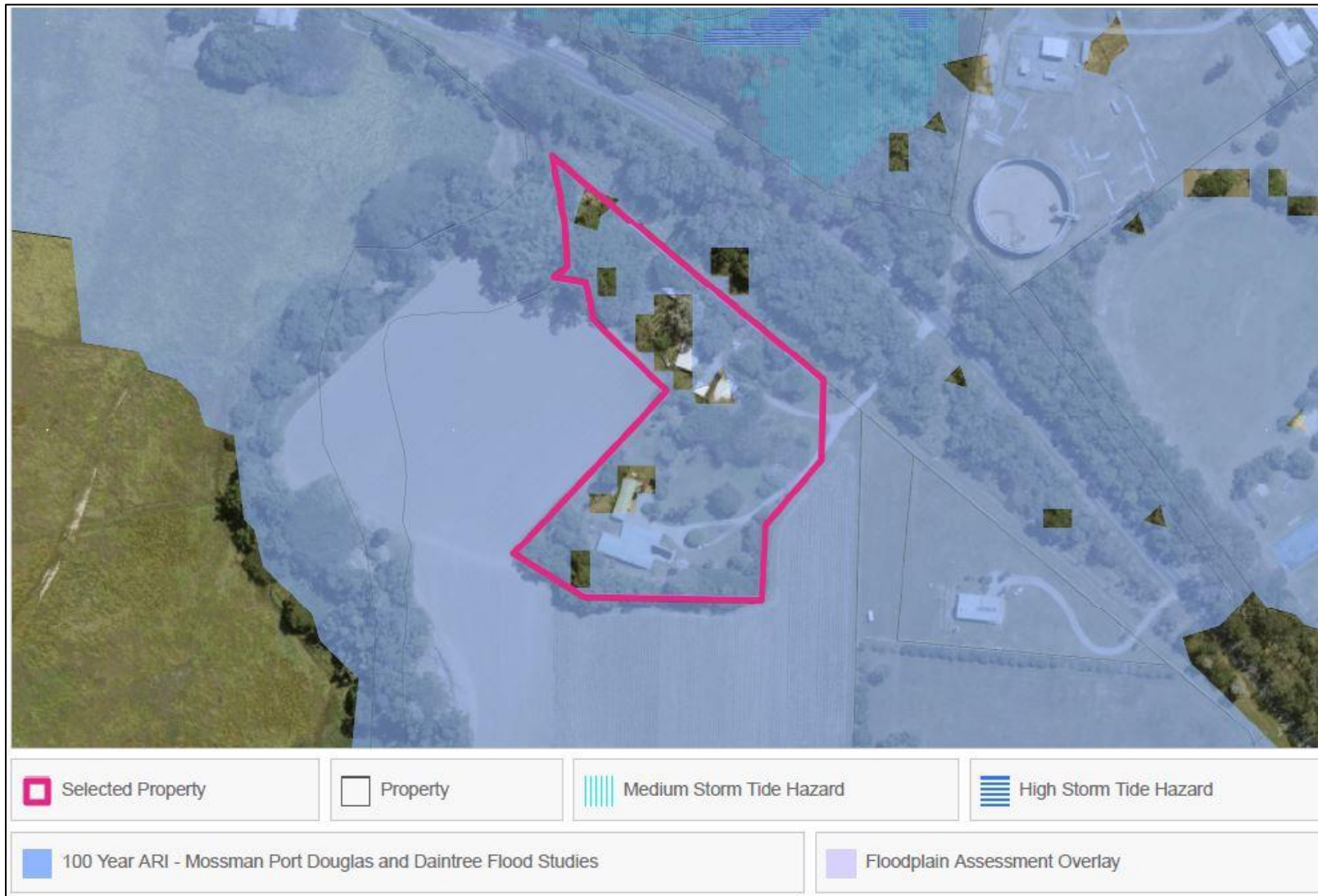
<p>PO8</p> <p>The development design responds to the potential threat of bushfire and establishes clear evacuation routes which demonstrate an acceptable or tolerable risk to people.</p>	<p>AO8</p> <p>The lot layout:</p> <ul style="list-style-type: none"> (a) minimises the length of the development perimeter exposed to, or adjoining hazardous vegetation; (b) avoids the creation of potential bottle-neckpoints in the movement network; (c) establishes direct access to a safe assembly /evacuation area in the event of an approaching bushfire; and (d) ensures roads likely to be used in the event of a fire are designed to minimise traffic congestion. <p>Note - For example, developments should avoid finger-like or hour-glass subdivision patterns or substantive vegetated corridors between lots. In order to demonstrate compliance with the performance outcome, a bushfire management plan prepared by a suitably qualified person may be required. The bushfire management plan should be developed in accordance with the Public Safety Business Agency (PSBA) guideline entitled "Undertaking a Bushfire Protection Plan. Advice from the Queensland Fire and Emergency Services (QFES) should be sought as appropriate</p>	<p>n/a</p> <p>The proposed development is not an ROL.</p>
<p>PO9</p> <p>Critical infrastructure does not increase the potential bushfire hazard.</p>	<p>AO9</p> <p>Critical or potentially hazardous infrastructures such as water supply, electricity, gas and telecommunications are placed underground.</p>	<p>n/a</p> <p>No new infrastructure is proposed.</p>
<p>Development design and separation from bushfire hazard – material change of use</p>		
<p>PO10</p> <p>Development is located and designed to ensure proposed buildings or building envelopes achieve a radiant heat flux level at any point on the building or envelope respectively, of:</p> <ul style="list-style-type: none"> (a) 10kW/m² where involving a vulnerable use; or (b) 29kW/m² otherwise. <p>The radiant heat flux level is achieved by separation unless this is not practically achievable.</p> <p>Note - The radiant heat levels and separation distances are to be established in accordance with method 2 set out in AS3959-2009.</p>	<p>AO10</p> <p>Buildings or building envelopes are separated from hazardous vegetation by a distance that:</p> <ul style="list-style-type: none"> (a) achieves a radiant heat flux level of at any point on the building or envelope respectively, of 10kW/m² for a vulnerable use or 29kW/m² otherwise; and (b) is contained wholly within the development site. <p>Note - Where a separation distance is proposed to be achieved by utilising existing cleared developed areas external to the site, certainty must be established (through tenure or other means) that the land will remain cleared of hazardous vegetation.</p> <p>For staged developments, temporary separation distances, perimeter roads or fire trails may be absorbed as part of subsequent stages.</p> <p>Note - The achievement of a cleared separation distance may not be achievable where other provisions within the planning scheme require protection of certain ecological, slope, visual or character features or functions.</p>	<p>n/a</p> <p>The proposed use is located within an existing building.</p> <p>No new buildings or building envelopes are proposed.</p>

<p>PO11</p> <p>A formed, all weather fire trail is provided between the hazardous vegetation and the site boundary or building envelope, and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>However, a fire trail will not be required where it would not serve a practical fire management purpose.</p> <p>Note - Fire trails are unlikely to be required where a development site involves less than 2.5ha</p>	<p>AO11</p> <p>Development sites are separated from hazardous vegetation by a public road or fire trail which has:</p> <ul style="list-style-type: none"> (a) a reserve or easement width of at least 20m; (b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15tonne vehicle and which is at least 6m clear of vegetation; (c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path; (d) a minimum of 4.8m vertical clearance; (e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; (f) a maximum gradient of 12.5%; (g) a cross fall of no greater than 10 degrees; (h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy; (i) vehicular access at each end which is connected to the public road network which is connected to the public road network at intervals of no more than 500m; (j) designated fire trail signage; (k) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and (l) if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services. 	<p>Complies</p> <p>The development is easily accessed via the Captain Cook Highway and contains level, 6m wide formed internal driveways capable of accommodating firefighting and emergency vehicles.</p> <p>The Captain Cook Highway road reserve contains a 15m wide cleared buffer along the site frontage boundary.</p>
<p>All development</p>		
<p>PO12</p> <p>All premises are provided with vehicular access that enables safe evacuation for occupants and easy access by fire fighting appliances.</p>	<p>AO12</p> <p>Private driveways:</p> <ul style="list-style-type: none"> (a) do not exceed a length of 60m from the street to the building; (b) do not exceed a gradient of 12.5%; (c) have a minimum width of 3.5m; (d) have a minimum of 4.8m vertical clearance; (e) accommodate turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; and (f) serve no more than 3 dwellings or buildings. 	<p>Complies</p> <p>The development site has vehicular access that enables safe evacuation for occupants and easy access by firefighting appliances.</p>

<p>PO13 Development outside reticulated water supply areas includes a dedicated static supply that is available solely for fire fighting purposes and can be accessed by fire fighting appliances.</p>	<p>AO13 A water tank is provided within 10m of each building (other than a class 10 building) which:</p> <ul style="list-style-type: none"> (a) is either below ground level or of non-flammable construction; (b) has a take off connection at a level that allows the following dedicated, static watersupply to be left available for access by firefighters: <ul style="list-style-type: none"> (i) 10,000l for residential buildings <p>Note – A minimum of 7,500l is required in a tank and the extra2,500l may be in the form of accessible swimming pools or dams.</p> <ul style="list-style-type: none"> (ii) 45,000l for industrial buildings; and (iii) 20,000l for other buildings; (c) includes shielding of tanks and pumps inaccordance with the relevant standards; (d) includes a hardstand area allowing medium rigid vehicle (15 tonne fire appliance) accesswithin 6m of the tank; (e) is provided with fire brigade tank fittings – 50mm ball valve and male camlock couplingand, if underground, an access hole of 200mm (minimum) to accommodate suctionlines; and (f) is clearly identified by directional signageprovided at the street frontage. 	<p>n/a The development site is connected to the reticulated water supply network.</p>
<p>PO14 Landscaping does not increase the potential bushfire risk.</p>	<p>AO14 Landscaping uses species that are less likely to exacerbate a bushfire event, and does not increase fuel loads within separation areas.</p>	<p>Complies The proposed development does not include any new landscaping.</p>
<p>PO15 The risk of bushfire and the need to mitigate that risk is balanced against other factors (such as but not limited to, biodiversity or scenic amenity).</p>	<p>AO15 Bushfire risk mitigation treatments do not have a significant impact on the natural environment or landscape character of the locality where this hasvalue.</p>	<p>Complies The proposed development bushfire risk mitigation treatments do not have any adverse impact on the natural environment or landscape character of the locality.</p>

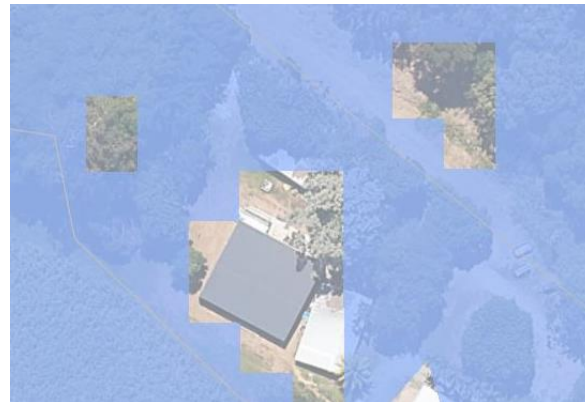
8.2.4 Flood and Storm Tide Hazard Overlay Code

The development site is located within the Flood and Storm Tide Hazard Overlay area of the Douglas Shire Planning Scheme.



8.2.4.3 Criteria for assessment

Table 8.2.4.3.a – Flood and storm tide hazards overlay code –assessable development

Performance outcomes	Acceptable outcomes	Compliance
For assessable and self-assessable development		
<p>PO1 Development is located and designed to:</p> <ul style="list-style-type: none"> (a) ensure the safety of all persons; (b) minimise damage to the development and contents of buildings; (c) provide suitable amenity; (d) minimise disruption to residents, recovery time, and rebuilding or restoration costs after inundation events. <p>Note – For assessable development within the flood plain assessment sub-category, a flood study by a suitably qualified professional is required to identify compliance with the intent of the acceptable outcome.</p>	<p>AO1.1 Development is sited on parts of the land that is not within the Flood and Storm tide hazards overlay maps contained in Schedule 2;</p> <p>or</p> <p>For dwelling houses,</p> <p>AO1.2 Development within the Flood and Storm Tide hazards overlay maps (excluding the Flood plain assessment sub-category) is designed to provide immunity to the Defined Inundation Event as outlined within Table 8.2.4.3.b plus a freeboard of 300mm.</p> <p>AO1.3 New buildings are:</p> <ul style="list-style-type: none"> (a) not located within the overlay area; (b) located on the highest part of the site to minimise entrance of flood waters; (c) provided with clear and direct pedestrian and vehicle evacuation routes off the site. <p>AO1.4 In non urban areas, buildings and infrastructure are set back 50 metres from natural riparian corridors to maintain their natural function of reducing velocity of floodwaters.</p>	<p>Complies The proposed use is located within the existing Shed that is located on parts of the land that are not within the Flood and Storm tide hazards overlay mapping.</p> 

For assessable development		
P02 The development is compatible with the level of risk associated with the natural hazard.	AO2 The following uses are not located in land inundated by the Defined Flood Event (DFE)/ Storm tide: (a) Retirement facility; (b) Community care facility; (c) Child care centre.	n/a The proposed development does not involve a Retirement facility, Community care facility or Child care centre. The proposed development is compatible with the level of risk associated with the natural hazard.
P03 Development siting and layout responds to flooding potential and maintains personal safety.	For Material change of use AO3.1 New buildings are: (a) not located within the overlay area; (b) located on the highest part of the site to minimise entrance of flood waters; (c) provided with clear and direct pedestrian and vehicle evacuation routes off the site. or AO3.2 The development incorporates an area on site that is at least 300mm above the highest known flood inundation level with sufficient space to accommodate the likely population of the development safely for a relatively short time until flash flooding subsides or people can be evacuated. or AO3.3 Where involving an extension to an existing dwelling house that is situated below DFE /Storm tide, the maximum size of the extension does not exceed 70m ² gross floor area. Note – If part of the site is outside the Hazard Overlay area, this is the preferred location of all buildings.	Complies The proposed development is located within the existing Shed which is not located within the overlay area, is located on the highest part of the site to minimise entrance of flood waters and is provided with clear and direct pedestrian and vehicle evacuation routes off the site.

	<p>For Reconfiguring a lot</p> <p>AO3.4 Additional lots: (a) are not located in the hazard overlay area; or (b) are demonstrated to be above the flood level identified for the site.</p> <p>Note - If part of the site is outside the Hazard Overlay area, this is the preferred location for all lots (excluding park or other open space and recreation lots).</p> <p>Note – Buildings subsequently developed on the lots will need to comply with the relevant building assessment provisions under the Building Act 1975.</p> <p>AO3.5 Road and/or pathway layout ensures residents are not physically isolated from adjacent flood free urban areas and provides a safe and clear evacuation route path: (a) by locating entry points into the reconfiguration above the flood level and avoiding culs-de-sac or other non-permeable layouts; and (b) by direct and simple routes to main carriageways.</p> <p>AO3.6 Signage is provided on site (regardless of whether the land is in public or private ownership) indicating the position and path of all safe evacuation routes off the site and if the site contains, or is within 100m of a floodable waterway, hazard warning signage and depth indicators are also provided at key hazard points, such as at floodway crossings or entrances to low-lying reserves.</p> <p>or</p>	<p>n/a The proposed development is not an ROL.</p>
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	<p>AO3.7 There is no intensification of residential uses within the flood affected areas on land situated below the DFE/Storm tide.</p> <p>For Material change of use (Residential uses)</p> <p>AO3.8 The design and layout of buildings used for residential purposes minimise risk from flooding by providing:</p> <p>(a) parking and other low intensive, non-habitable uses at ground level;</p> <p>Note - The high-set 'Queenslander' style house is a resilient low-density housing solution in floodplain areas. Higher density residential development should ensure only non- habitable rooms (e.g. garages, laundries) are located on the ground floor.</p>	
<p>PO4 Development is resilient to flood events by ensuring design and built form account for the potential risks of flooding.</p>	<p>For Material change of use (Non-residential uses)</p> <p>AO4.2 Nonresidential buildings and structures allow for the flow through of flood waters on the ground floor.</p> <p>Note - Businesses should ensure that they have the necessary contingency plans in place to account for the potential need to relocate property prior to a flood event (e.g. allow enough time to transfer stock to the upstairs level of a building or off site).</p> <p>Note - The relevant building assessment provisions under the Building Act 1975 apply to all building work within the Hazard Area and need to take into account the flood potential within the area.</p> <p>AO4.3 Materials are stored on-site:</p> <p>(a) are those that are readily able to be moved in a flood event;</p> <p>(b) where capable of creating a safety hazard by being shifted by flood waters, are contained in order to minimise movement in times of flood.</p> <p>Notes -</p> <p>(a) Businesses should ensure that they have the necessary contingency plans in place to account for the potential need to relocate property prior to a flood event (e.g.</p>	<p>Complies The proposed use involves the existing Shed located on the highest portion of the site and the structure is built in accordance with all applicable building codes.</p> <p>All materials stored within the building will be readily able to be moved in the event of flooding and will be contained within the building.</p>

	<p>allow enough time to transfer stock to the upstairs level of a building or off site).</p> <p>(b) Queensland Government Fact Sheet 'Repairing your House after a Flood' provides information about water resilient products and building techniques.</p>	
<p>PO5</p> <p>Development directly, indirectly and cumulatively avoids any increase in water flow velocity or flood level and does not increase the potential flood damage either on site or on other properties.</p> <p>Note – Berms and mounds are considered to be an undesirable built form outcome and are not supported.</p>	<p>For Operational works</p> <p>AO5.1</p> <p>Works in urban areas associated with the proposed development do not involve:</p> <p>(a) any physical alteration to a watercourse or floodway including vegetation clearing; or</p> <p>(b) a net increase in filling (including berms and mounds).</p> <p>AO5.2</p> <p>Works (including buildings and earthworks) in non-urban areas either:</p> <p>(a) do not involve a net increase in filling greater than 50m³; or</p> <p>(b) do not result in any reductions of on-site flood storage capacity and contain within the subject site any changes to depth/duration/velocity of flood waters; or</p> <p>(c) do not change flood characteristics outside the subject site in ways that result in:</p> <p>(i) loss of flood storage;</p> <p>(ii) loss of/changes to flow paths;</p> <p>(iii) acceleration or retardation of flows or any reduction in flood warning times elsewhere on the flood plain.</p>	<p>n/a</p> <p>The proposed development is not for Operational Works.</p>

	<p>For Material change of use</p> <p>AO5.3 Where development is located in an area affected by DFE/Storm tide, a hydraulic and hydrology report, prepared by a suitably qualified professional, demonstrates that the development maintains the flood storage capacity on the subject site; and</p> <p>(a) does not increase the volume, velocity, concentration of flow path alignment of stormwater flow across sites upstream, downstream or in the general vicinity of the subject site; and</p> <p>(b) does not increase ponding on sites upstream, downstream or in the general vicinity of the subject site.</p> <p>For Material change of use and Reconfiguring a lot</p> <p>AO5.4 In non urban areas, buildings and infrastructure are set back 50 metres from natural riparian corridors to maintain their natural function of reducing velocity of floodwaters.</p> <p>Note – Fences and irrigation infrastructure (e.g. irrigation tape) in rural areas should be managed to minimise adverse the impacts that they may have on downstream properties in the event of a flood.</p>	<p>Complies The proposed use is located within the existing Shed that is located on parts of the land that are not within the Flood and Storm tide hazards overlay mapping.</p> <p>The existing Shed is not located near any natural riparian corridors.</p>
<p>PO6 Development avoids the release of hazardous materials into floodwaters.</p>	<p>For Material change of use</p> <p>AO6.1 Materials manufactured or stored on site are not hazardous or noxious, or comprise materials that may cause a detrimental effect on the environment if discharged in a flood event;</p> <p>or</p> <p>AO6.2 If a DFE level is adopted, structures used for the manufacture or storage of hazardous materials are:</p> <p>(a) located above the DFE level; or</p> <p>(b) designed to prevent the intrusion of floodwaters.</p>	<p>Complies The proposed use does not involve the manufacture or storage of hazardous materials nor comprise materials that may cause a detrimental effect on the environment if discharged in a flood event.</p>

	<p>AO6.3 Infrastructure is designed and constructed to resist hydrostatic and hydrodynamic forces as a result of inundation by the DFE.</p> <p>AO6.4 If a flood level is not adopted, hazardous materials and their manufacturing equipment are located on the highest part of the site to enhance flood immunity and designed to prevent the intrusion of floodwaters.</p> <p>Note – Refer to Work Health and Safety Act 2011 and associated Regulation and Guidelines, the Environmental Protection Act 1994 and the relevant building assessment provisions under the Building Act 1975 for requirements related to the manufacture and storage of hazardous materials.</p>	
<p>P07 The development supports, and does not unduly burden, disaster management response or recovery capacity and capabilities.</p>	<p>AO7 Development does not:</p> <ul style="list-style-type: none"> (a) increase the number of people calculated to be at risk of flooding; (b) increase the number of people likely to need evacuation; (c) shorten flood warning times; and (d) impact on the ability of traffic to use evacuation routes, or unreasonably increase traffic volumes on evacuation routes. 	<p>Complies The development involves only 2 on-site employees and supports and does not unduly burden disaster management response or recovery capacity and capabilities.</p>
<p>P08 Development involving community infrastructure:</p> <ul style="list-style-type: none"> (a) remains functional to serve community need during and immediately after a flood event; (b) is designed, sited and operated to avoid adverse impacts on the community or environment due to impacts of flooding on infrastructure, facilities or access and egress routes; (c) retains essential site access during a flood event; (d) is able to remain functional even when other infrastructure or services may be compromised in a flood event. 	<p>AO8.1 The following uses are not located on land inundated during a DFE/Storm tide:</p> <ul style="list-style-type: none"> (a) community residence; and (b) emergency services; and (c) residential care facility; and (d) utility installations involving water and sewerage treatment plants; and (e) storage of valuable records or items of historic or cultural significance (e.g. archives, museums, galleries, libraries). 	<p>Complies The proposed development does not involve community infrastructure or uses listed in AO8.1.</p> <p>No new infrastructure is proposed or required.</p>

	<p>or</p> <p>AO8.2</p> <p>The following uses are not located on land inundated during a 1% AEP flood event:</p> <ul style="list-style-type: none"> (a) community and cultural facilities, including facilities where an education and care service under the Education and care Services National law (Queensland) is operated or child care service under the Child Care Act 2002 is conducted, (b) community centres; (c) meeting halls; (d) galleries; (e) libraries. <p>The following uses are not located on land inundated during a 0.5% AEP flood event.</p> <ul style="list-style-type: none"> (a) emergency shelters; (b) police facilities; (c) sub stations; (d) water treatment plant <p>The following uses are not located on land inundated during a 0.2% AEP flood event:</p> <ul style="list-style-type: none"> (a) correctional facilities; (b) emergency services; (c) power stations; (d) major switch yards. <p>and/or</p> <p>AO8.3</p> <p>The following uses have direct access to low hazard evacuation routes as defined in Table 8.2.4.3.c:</p> <ul style="list-style-type: none"> (a) community residence; and (b) emergency services; and (c) hospitals; and (d) residential care facility; and (e) sub stations; and (f) utility installations involving water and sewerage treatment plants. 	
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	<p>AO8.4 Any components of infrastructure that are likely to fail to function or may result in contamination when inundated by flood, such as electrical switch gear and motors, telecommunications connections, or water supply pipeline air valves are:</p> <ul style="list-style-type: none">(a) located above DFE/Storm tide or the highest known flood level for the site;(b) designed and constructed to exclude floodwater intrusion / infiltration. <p>AO8.5 Infrastructure is designed and constructed to resist hydrostatic and hydrodynamic forces as a result of inundation by a flood.</p>	
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8.2.6 Landscape Values Overlay Code

The development site is located within the Landscape Values Overlay area of the Douglas Shire Planning Scheme.



8.2.6.3 Criteria for assessment

Table 8.2.6.3.a – Landscape values overlay code – assessable development

Performance outcomes	Acceptable outcomes	Compliance
For assessable development		
Development in a High landscape value area		
<p>PO1 Development within High landscape value areas identified on the Landscape values overlay maps contained in Schedule 2:</p> <ul style="list-style-type: none"> (a) avoids detrimental impacts on the landscape values of forested skylines, visible hillslopes, ridgelines, the coastal foreshore or the shoreline of other water bodies through the loss of vegetation; (b) is effectively screened from view from a road, lookout or other public place by an existing natural landform or native vegetation, or will be effectively screened by native vegetation within 3 years of construction; (c) retains existing vegetation and incorporates new landscaping to enhance existing vegetation and visually soften built form elements; (d) incorporates development of a scale, design, height, position on site, construction materials and external finishes that are compatible with the landscape values of the locality; (e) avoids detrimental impacts on landscape values and excessive changes to the natural landform as a result of the location, position on site, scale, design, extent and alignment of earthworks, roads, driveways, retaining walls and other on-ground or in-ground infrastructure; (f) avoids detrimental impacts on landscape values and views as a result of the location, position on site, scale, design and alignment of telecommunications facilities, electricity 	<p>AO1.1 Buildings and structures are not more than 8.5 metres and two storeys in height.</p> <p>Note - Height is inclusive of roof height.</p> <p>AO1.2 Buildings and structures are setback not less than 50 metres from ridgelines or peaks.</p> <p>AO1.3 Development is screened from view from roads or other public places by an existing natural landform or an existing native vegetation buffer.</p> <p>AO1.4 Where development on land steeper than 1 in 6 (16.6%) cannot be avoided:</p> <ul style="list-style-type: none"> (a) development follows the natural; contours of the site; (b) buildings are split level or suspended floor construction, or a combination of the two; (c) lightweight materials are used to areas with suspended floors. <p>Note - Examples of suitable lightweight materials include timber or fibre cement boards or sheeting for walls and factory treated metal sheeting for walls and roofs.</p> <p>AO1.5 The external features, walls and roofs of buildings and structures have a subdued and non-reflective palette.</p> <p>Note - Examples of suitable colours include shades of green, olive green, blue green, grey green, green blue, indigo, brown, blue grey, and green yellow.</p>	<p>n/a The development site is not located within a high landscape value area.</p>

<p>towers, poles and lines and other tall infrastructure;</p> <p>(g) extractive industry operations are avoided.</p> <p>Note - A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in order to satisfy performance outcomes.</p>	<p>AO1.6 No clearing of native vegetation occurs on land with a slope greater than 1 in 6 (16.5%).</p> <p>AO1.7 Where for accommodation activities or reconfiguration of a lot in a High landscape value area, development demonstrates that the height, design, scale, positioning on-site, proposed construction materials and external finishes are compatible with the landscape values.</p> <p>Note - A visual impact assessment undertaken in accordance with Planning scheme policy SC6.6 – Landscape values may be required.</p> <p>AO1.8 Advertising devices do not occur.</p>	
<p>Development within the Medium landscape value area</p>		
<p>PO2 Development within Medium landscape value areas identified on the Landscape values overlay maps contained in Schedule 2:</p> <p>(a) avoids detrimental impacts on the landscape values of forested skylines, visible hillslopes, ridgelines, the coastal foreshore or the shoreline of other water bodies through the loss of vegetation;</p> <p>(b) is effectively screened from view from a road, lookout or other public place by an existing natural landform or native vegetation, or will be effectively screened by native vegetation within 5 years of construction;</p> <p>(c) retains existing vegetation and incorporates new landscaping to enhance existing vegetation and visually soften built form elements;</p> <p>(d) incorporates development of a scale, design, height, position on site, construction materials and external finishes that are compatible with the landscape values of the locality;</p> <p>(e) avoids detrimental impacts on landscape values and excessive changes to the natural</p>	<p>AO2.1 Buildings and structures are not more than 8.5 metres and two storeys in height.</p> <p>Note - Height is inclusive of the roof height.</p> <p>AO2.2 Development is screened from view from roads or other public places by an existing natural landform or an existing native vegetation buffer.</p> <p>AO2.3 Where development on land steeper than 1 in 6 (16.6%) cannot be avoided:</p> <p>(a) development follows the natural; contours of the site;</p> <p>(b) buildings are split level or suspended floor construction, or a combination of the two;</p> <p>(c) lightweight materials are used to areas with suspended floors.</p> <p>Note - Examples of suitable lightweight materials include timber or fibre cement boards or sheeting for walls and factory treated metal sheeting for walls and roofs.</p>	<p>n/a The development site is not located within a medium landscape value area.</p>

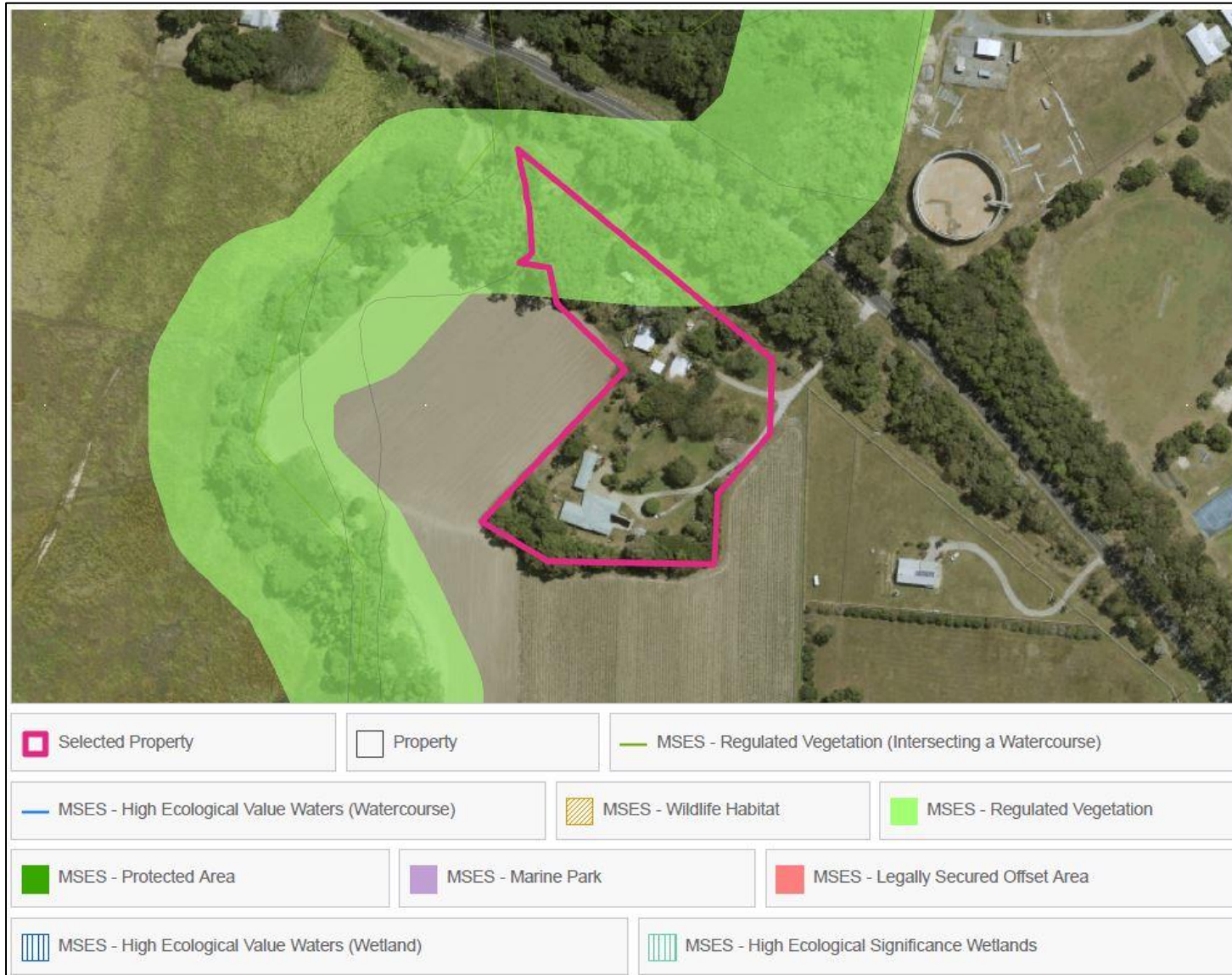
<p>landform as a result of the location, position on site, scale, design and alignment of earthworks, roads, driveways, retaining walls and other on-ground or in-ground infrastructure;</p> <p>(f) avoids detrimental impacts on landscape values and views as a result of the location, position on site, scale, design and alignment of telecommunications facilities, electricity towers, poles and lines and other tall infrastructure;</p> <p>(g) extractive industry operations are avoided, or where they cannot be avoided, are screened from view.</p> <p>Note - A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in order to satisfy performance outcomes.</p>	<p>AO2.4 The external features, walls and roofs of buildings and structures have a subdued and non-reflective palette.</p> <p>Note - Examples of suitable colours include shades of green, olive green, blue green, grey green, green blue, indigo, brown, blue grey, and green yellow.</p> <p>AO2.5 No clearing of native vegetation occurs on land with a slope greater than 1 in 6 (16.6%).</p> <p>AO2.6 Advertising devices do not occur.</p>	
Development within a Scenic route buffer / view corridor area		
<p>PO3 Development within a Scenic route buffer / view corridor area as identified on the Landscape values overlay maps contained in Schedule 2:</p> <p>(a) retains visual access to views of the surrounding landscape, the sea and other water bodies;</p> <p>(b) retains existing vegetation and incorporates landscaping to visually screen and soften built form elements whilst not impeding distant views or view corridors;</p> <p>(c) incorporates building materials and external finishes that are compatible with the visual amenity and the landscape character;</p> <p>(d) minimises visual impacts on the setting and views in terms of:</p> <p>(i) the scale, height and setback of buildings;</p> <p>(ii) the extent of earthworks and impacts on the landform including the location and configuration of access roads and driveways;</p> <p>(iii) the scale, extent and visual prominence of advertising devices.</p>	<p>AO3.1 Where within a Scenic route buffer / view corridor area, the height of buildings and structures is not more than identified within the acceptable outcomes of the applicable zone code.</p> <p>AO3.2 No clearing of native vegetation is undertaken within a Scenic route buffer area.</p> <p>AO3.3 Where within a Scenic route buffer / view corridor area development is set back and screened from view from a scenic route by existing native vegetation with a width of at least 10 metres and landscaped in accordance with the requirements of the landscaping code.</p> <p>AO3.4 Development does not result in the replacement of, or creation of new, additional, or enlarged advertising devices.</p>	<p>Complies The development site is located within a Scenic route buffer area.</p> <p>The proposed development does not involve the construction of any new buildings</p> <p>No clearing of native vegetation is proposed.</p> <p>The development site is screened by both on-site and road reserve native vegetation. No additional landscaping is proposed.</p> <p>The proposed development does not result in the replacement or creation of new, additional, or enlarged advertising devices.</p>

<p>Note - A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in order to satisfy performance outcomes.</p>		
<p>Development within the Coastal scenery area</p>		
<p>PO4 The landscape values of the Coastal scenery zone as identified on the Landscape values overlay maps contained in Schedule 2 are managed to integrated and limit the visual impact of development.</p> <p>Note - A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in order to satisfy performance outcomes.</p>	<p>AO4.1 The dominance of the natural character of the coast is maintained or enhanced when viewed from the foreshore.</p> <p>AO4.2 Where located adjacent to the foreshore buildings and structures are setback: (a) Where no adjoining development, a minimum of 50 metres from the coastal high water mark and the setback area is landscaped with a native vegetation buffer that has a minimum width of 25 metres; or (b) Where there is adjoining development, setbacks will be consistent with that of adjoining buildings and structures, but not less than 10 metres from the coastal high water mark. The setback area is landscaped in accordance with the requirements of the Landscaping code.</p> <p>AO4.3 Where separated from the foreshore by land contained within public ownership (e.g. unallocated State land, esplanade or other public open space), buildings and structures area setback: (a) where no adjoining development, a minimum of 6 metres from the coastward property boundary. The setback area is landscaped in accordance with the requirements of the Landscaping code; or (b) where there is adjoining development, setbacks will be consistent with that of adjoining buildings and structures. The setback area is landscaped in accordance with the requirements of the Landscaping code.</p>	<p>n/a The development site is not located within the coastal scenery zone.</p>

<p>P05</p> <p>Development is to maximise opportunities to maintain and/or enhance natural landscape values through the maintenance and restoration of vegetated buffers between development and coastal waters, where practical.</p> <p>Note – A visual impact assessment is undertaken in accordance with Planning scheme policy SC6.6 – Landscape values in satisfaction of a performance outcome.</p>	<p>A05</p> <p>No clearing of native vegetation is undertaken within a Coastal scenery area zone, except for exempt vegetation damage undertaken in accordance with the Vegetation management code</p>	<p>n/a</p> <p>The development site is not located within the coastal scenery zone.</p>
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8.2.7 Natural Areas Overlay Code

The development site is located within the Natural Areas Overlay area of the Douglas Shire Planning Scheme.



8.2.7.3 Criteria for assessment

Table 8.2.7.3.a – Natural areas overlay code – assessable development

Performance outcomes	Acceptable outcomes	Compliance
For self-assessable and assessable development		
Protection of matters of environmental significance		
PO1 Development protects matters of environmental significance.	AO1.1 Development avoids significant impact on the relevant environmental values. or AO1.2 A report is prepared by an appropriately qualified person demonstrating to the satisfaction of the assessment manager, that the development site does not contain any matters of state and local environmental significance. or AO1.3 Development is located, designed and operated to mitigate significant impacts on environmental values. For example, a report certified by an appropriately qualified person demonstrating to the satisfaction of the assessment manager, how the proposed development mitigates impacts, including on water quality, hydrology and biological processes.	Complies The proposed use is of very small scale and very low impact in nature and all appropriate workplace environmental protection measures will be maintained. The low impact activities will be contained within the building and do not produce any significant volume of dust, odour or noise emissions. No hazardous materials are to be stored within the Shed and the activities do not produce any hazardous waste materials. Trade waste will be contained within a regularly service skip bin with all waste aluminium being stored in a separate bin for recycling. Refer to the Planning Report for details.

Management of impacts on matters of environmental significance		
PO2 Development is located, designed and constructed to avoid significant impacts on matters of environmental significance.	AO2 The design and layout of development minimises adverse impacts on ecologically important areas by: <ul style="list-style-type: none"> (a) focusing development in cleared areas to protect existing habitat; (b) utilising design to consolidate density and preserve existing habitat and native vegetation; (c) aligning new property boundaries to maintain ecologically important areas; (d) ensuring that alterations to natural landforms, hydrology and drainage patterns on the development site do not negatively affect ecologically important areas; (e) ensuring that significant fauna habitats are protected in their environmental context; and (f) incorporating measures that allow for the safe movement of fauna through the site. 	Complies The proposed use is to be located within the existing Shed. No vegetation clearing is proposed or required and the existing local habitat and native vegetation is to be retained.
PO3 An adequate buffer to areas of state environmental significance is provided and maintained.	AO3.1 A buffer for an area of state environmental significance (Wetland protection area) has a minimum width of: <ul style="list-style-type: none"> (a) 100 metres where the area is located outside Urban areas; or (b) 50 metres where the area is located within a Urban areas. or AO3.2 A buffer for an area of state environmental significance is applied and maintained, the width of which is supported by an evaluation of environmental values, including the function and threats to matters of environmental significance.	n/a The development site is not located near any areas of state environmental significance.

<p>P04 Wetland and wetland buffer areas are maintained, protected and restored.</p> <p>Note – Wetland buffer areas are identified in AO3.1.</p>	<p>AO4.1 Native vegetation within wetlands and wetland buffer areas is retained.</p> <p>AO4.2 Degraded sections of wetlands and wetland buffer areas are revegetated with endemic native plants in patterns and densities which emulate the relevant regional ecosystem.</p>	<p>n/a The development site is not located near any wetlands or wetland buffer areas.</p>
<p>P05 Development avoids the introduction of non- native pest species (plant or animal), that pose a risk to ecological integrity.</p>	<p>AO5.1 Development avoids the introduction of non-native pest species.</p> <p>AO5.2 The threat of existing pest species is controlled by adopting pest management practices for long-term ecological integrity.</p>	<p>Complies The proposed development does not introduce any non- native pest species (plant or animal), that pose a risk to ecological integrity.</p>
<p>Ecological connectivity</p>		
<p>P06 Development protects and enhances ecological connectivity and/or habitat extent.</p>	<p>AO6.1 Development retains native vegetation in areas large enough to maintain ecological values, functions and processes.</p> <p>and</p> <p>AO6.2 Development within an ecological corridor rehabilitates native vegetation.</p> <p>and</p> <p>AO6.3 Development within a conservation corridor mitigates adverse impacts on native fauna, feeding, nesting, breeding and roosting sites and native fauna movements.</p>	<p>Complies The proposed use is to be located within the existing Shed. No vegetation clearing is proposed or required and the existing local habitat and native vegetation is to be retained.</p>

<p>P07 Development minimises disturbance to matters of state environmental significance (including existing ecological corridors).</p>	<p>AO7.1 Development avoids shading of vegetation by setting back buildings by a distance equivalent to the height of the native vegetation.</p> <p>and</p> <p>AO7.2 Development does not encroach within 10 metres of existing riparian vegetation and watercourses.</p>	<p>n/a The development site is not located near any areas of state environmental significance. No vegetation clearing is proposed or required and the existing local habitat and native vegetation is to be retained.</p>
<p>Waterways in an urban area</p>		
<p>P08 Development is set back from waterways to protect and maintain:</p> <ul style="list-style-type: none"> (a) water quality; (b) hydrological functions; (c) ecological processes; (d) biodiversity values; (e) riparian and in-stream habitat values and connectivity; (f) in-stream migration. 	<p>AO8.1 Where a waterway is contained within an easement or a reserve required for that purpose, development does not occur within the easement or reserve;</p> <p>or</p> <p>AO8.2 Development does not occur on the part of the site affected by the waterway corridor.</p> <p>Note – Waterway corridors are identified within Table 8.2.7.3.b.</p>	<p>n/a The development site is not located near a watercourse in an urban area.</p>
<p>Waterways in a non-urban area</p>		
<p>P09 Development is set back from waterways to protect and maintain:</p> <ul style="list-style-type: none"> (a) water quality; (b) hydrological functions; (c) ecological processes; (d) biodiversity values; (e) riparian and in-stream habitat values and connectivity; (f) in-stream migration. 	<p>AO9 Development does not occur on that part of the site affected by a waterway corridor.</p> <p>Note – Waterway corridors are identified within Table 8.2.7.3.b.</p>	<p>n/a The development site is not located near a watercourse.</p>

8.2.10 Transport Network Overlay Code

The development site is located within the Transport Network Overlay area of the Douglas Shire Planning Scheme.



8.2.10.3 Criteria for assessment

Table 8.2.10.3.a – Transport network overlay code – assessable development

Performance outcomes	Acceptable outcomes	Compliance
For assessable development		
PO1 Development supports the road hierarchy for the region. Note -A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.	AO1.1 Development is compatible with the intended role and function of the transport network as identified on the Transport network overlay maps contained in Schedule 2. AO1.2 Development does not compromise the safety and efficiency of the transport network. AO1.3 Development is designed to provide access via the lowest order road, where legal and practicable access can be provided to that road.	Complies The proposed development is compatible with the intended role and function of the transport network. The proposed development does not compromise the safety and efficiency of the transport network. The proposed development is accessed via the lowest order road available.
PO2 Transport infrastructure is provided in an integrated and timely manner. Note - A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.	AO2 Development provides infrastructure (including improvements to existing infrastructure) in accordance with: (e) the Transport network overlay maps contained in Schedule 2; (a) any relevant Local Plan. Note – The Translink Public Transport Infrastructure Manual provides guidance on the design of public transport facilities.	n/a No additional transport infrastructure or upgrades to existing transport infrastructure is required or proposed.
PO3 Development involving sensitive land uses within a major transport corridor buffer area is located, designed and maintained to avoid or mitigate adverse impacts on amenity for the sensitive land use.	AO3 No acceptable outcomes are prescribed. Note – Part 4.4 of the Queensland Development Code provides requirements for residential building design in a designated transport noise corridor.	Complies The proposed development will be designed and maintained to avoid or mitigate adverse impacts on amenity.

<p>PO4 Development does not compromise the intended role and function or safety and efficiency of major transport corridors.</p> <p>Note - A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.</p>	<p>AO4.1 Development is compatible with the role and function (including the future role and function) of major transport corridors.</p> <p>AO4.2 Direct access is not provided to a major transport corridor where legal and practical access from another road is available.</p> <p>AO4.3 Intersection and access points associated with major transport corridors are located in accordance with: (a) the Transport network overlay maps contained in Schedule 2; and (b) any relevant Local Plan.</p> <p>AO4.4 The layout of development and the design of the associated access is compatible with existing and future boundaries of the major transport corridor or major transport facility.</p>	<p>Complies The proposed development is compatible with the role and function of major transport corridors.</p> <p>The proposed development is accessed via the lowest order road available.</p> <p>No new intersections or access points associated with major transport corridors are proposed.</p> <p>The layout of development and the design of the associated access is compatible with existing and future boundaries of the major transport corridor.</p>
<p>PO5 Development retains and enhances existing vegetation between a development and a major transport corridor, so as to provide screening to potential noise, dust, odour and visual impacts emanating from the corridor.</p>	<p>AO5 No acceptable outcomes are prescribed.</p>	<p>Complies The proposed development retains all existing vegetation between the development site and the major transport corridor.</p>
<p>Pedestrian and cycle network</p>		
<p>PO6 Lot reconfiguration assists in the implementation of the pedestrian and cycle movement network to achieve safe, attractive and efficient pedestrian and cycle networks.</p>	<p>AO6.1 Where a lot is subject to, or adjacent to an element of the pedestrian and cycle Movement network (identified on the Transport network overlay maps contained in Schedule 2) the specific location of this element of the pedestrian and cycle network is incorporated in the design of the lot layout.</p>	<p>n/a The proposed development is not an ROL.</p>

	AO6.2 The element of the pedestrian and cycle network is constructed in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme policy SC6.5 – FNQROC Regional Development Manual.	n/a No new pedestrian or cycle routes are proposed or required.
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9.3.12 Industry Activities Code

The proposed development is subject to assessment against the provisions of the Industry Activities Code of the Douglas Shire Planning Scheme.

9.3.12.3 Criteria for assessment

Table 9.3.12.3.a – Industry activities code –assessable development

Performance outcomes	Acceptable outcomes	Compliance
For self-assessable and assessable development		
PO1 The site coverage of buildings ensures that there is sufficient area for the provision of services and landscaping and caters for flood storage in areas affected by flooding.	AO1 The site coverage is not more than 80%.	Complies The site coverage is not more than 80%.
PO2 Setbacks: (a) contribute to an attractive and consistent streetscape appearance; (b) provide for visible employee and customer car parking; (c) allow for landscape planting along street frontages; (d) minimise unusable spaces between buildings / boundaries.	AO2 Buildings, display areas and storage areas are set back: (a) 6 metres from the main road frontage; (b) 3 metres from any secondary road frontage; (c) where the site has a common boundary with land in an industry zone, the building is setback either: (i) zero metres from the side or rear boundary; or (ii) not less than 3 metres from the side or rear boundary. (d) where a site adjoins land other than an industry zone, the building is setback not less than 3 metres from the side or rear boundary.	Complies No new buildings are proposed. The existing Shed has a setback of 25m from the frontage boundary.

Amenity		
<p>PO3 The appearance of development provides a quality, legible appearance and workplace.</p>	<p>AO3.1 Pedestrian entrances to buildings are: (a) easily identifiable from the street and directly accessible from the car parking areas; (b) provided with sun and rain shelter a minimum of 900mm width immediately above the entryway.</p> <p>AO3.2 Ancillary office or sales space is orientated toward the street frontage and is provided with human scale elements (such as windows, doors, shading devices and variation of construction materials and colours).</p> <p>AO3.3 Customer car parking is located to the front or side of premises with clear and direct pedestrian access to the main customer building entry.</p> <p>AO3.4 Outdoor storage areas are not located forward of the building line.</p> <p>AO3.5 Illumination is provided within parking and pedestrian areas during night time hours of operation.</p> <p>AO3.6 Development provides clear and legible street numbering for the benefit of motorists.</p> <p>AO3.7 Gates to a road frontage are sliding or open inwardly into the site.</p> <p>AO3.8 Development on a site greater than 2,500m² incorporates on-site amenity areas for staff that: (a) provide seating and tables; (b) incorporates weather protection.</p>	<p>Complies The proposed development does not involve on-site customer or visitor activity and no ancillary office is proposed.</p> <p>The proposed use utilizes the existing Shed and involves only 2 on-site employees who will use existing on-site amenities.</p>

Landscaping		
<p>PO4 Landscaping is provided to:</p> <ul style="list-style-type: none"> (a) enhance the appearance and amenity of the development; (b) contribute positively to the appearance of the streetscape. 	<p>AO4.1 At least 5% of the site is landscaped.</p> <p>AO4.2 A landscape strip not less than 2 metres is provided within the site along the road frontage.</p> <p>AO4.3 Landscaped areas adjoining parking and manoeuvring areas are protected from vehicular encroachment by a 150mm high vertical concrete kerb or similar obstruction.</p> <p>AO4.4 Planting is to consist of a combination of hardy tropical tree and spreading ground cover species in accordance with Planning scheme policy SC6.7 - Landscaping that will complement the scale of proposed development, without interfering with casual surveillance and sightlines.</p> <p>AO4.5 Hardy tropical shrubs are provided in accordance with Planning scheme policy SC6.7 - Landscaping to screen bin storage and service areas.</p> <p>AO4.6 Fencing along street frontages is more than 50% transparent.</p>	<p>Complies All existing on-site vegetation is to be retained and no vegetation clearing is proposed. Sufficient on-site landscaping exists and will be retained. As the site is not visible from the road and no visitors or customers will be allowed on-site, no additional landscaping is proposed.</p>
Services		
<p>PO5 Development adequately takes into account the functional requirements of infrastructure needs and service of the use.</p>	<p>AO5 Design takes into account the potential need to provide:</p> <ul style="list-style-type: none"> (a) space and access for trade waste connections to the sewer network; (b) waste and recyclable material storage areas; (c) storage tanks; (d) fire fighting booster pumps; (e) electrical infrastructure; (f) car parking, manoeuvring areas including loading facilities. 	<p>Complies The proposed development will utilize the existing infrastructure which is sufficient for the proposed use.</p>

Access and loading/unloading of goods		
<p>PO6 The transport of goods and materials to and from sites does not adversely affect the movement of traffic on roads adjacent to the site.</p>	<p>AO6.1 All vehicles are contained within the site when loading and unloading.</p> <p>AO6.2 Manoeuvring area is provided on site to allow a Medium rigid vehicle to enter and exit the site in a forward gear.</p> <p>AO6.3 Site access is limited to one access point to each frontage.</p> <p>AO6.4 Where a site has a frontage greater than 40 metres, two access points to the street frontage can be provided they are separated by a distance of not less than 10 metres.</p>	<p>Complies All vehicles will be contained within the site when loading and unloading.</p> <p>The site contains ample vehicle maneuvering areas for vehicles to enter and exit the site in forward gear. The proposed use will involve 2 utility vehicles and be serviced by a medium rigid vehicle.</p> <p>The site has 1 access point only.</p>

9.4.1 Access, Parking and Servicing Code

The proposed development is subject to assessment against the provisions of the Access, Parking and Servicing Code of the Douglas Shire Planning Scheme.

9.4.1.3 Criteria for assessment

Table 9.4.1.3.a – Access, parking and servicing code – assessable development

Performance outcomes	Acceptable outcomes	Compliance
For self-assessable and assessable development		
PO1 Sufficient on-site car parking is provided to cater for the amount and type of vehicle traffic expected to be generated by the use or uses of the site, having particular regard to: <ul style="list-style-type: none"> (a) the desired character of the area; (b) the nature of the particular use and its specific characteristics and scale; (c) the number of employees and the likely number of visitors to the site; (d) the level of local accessibility; (e) the nature and frequency of any public transport serving the area; (f) whether or not the use involves the retention of an existing building and the previous requirements for car parking for the building (g) whether or not the use involves a heritage building or place of local significance; (h) whether or not the proposed use involves the retention of significant vegetation. 	AO1.1 The minimum number of on-site vehicle parking spaces is not less than the number prescribed in Table 9.4.1.3.b for that particular use or uses. Note - Where the number of spaces calculated from the table is not a whole number, the number of spaces provided is the next highest whole number. AO1.2 Car parking spaces are freely available for the parking of vehicles at all times and are not used for external storage purposes, the display of products or rented/sub-leased. AO1.3 Parking for motorcycles is substituted for ordinary vehicle parking to a maximum level of 2% of total ordinary vehicle parking. AO1.4 For parking areas exceeding 50 spaces parking, is provided for recreational vehicles as a substitute for ordinary vehicle parking to a maximum of 5% of total ordinary vehicle parking rate.	Complies The proposed development will provide 4 vehicle parking spaces as demanded by the use which will have only 2 employees utilizing 2 company vehicles. There will be no visitors or customers attending the site.
PO2 Vehicle parking areas are designed and constructed in accordance with relevant standards.	AO2 Vehicle parking areas are designed and constructed in accordance with Australian Standard: <ul style="list-style-type: none"> (a) AS2890.1; (b) AS2890.3; (c) AS2890.6. 	Will Comply Vehicle parking areas are designed and constructed in accordance with relevant standards.

<p>PO3 Access points are designed and constructed:</p> <ul style="list-style-type: none"> (a) to operate safely and efficiently; (b) to accommodate the anticipated type and volume of vehicles (c) to provide for shared vehicle (including cyclists) and pedestrian use, where appropriate; (d) so that they do not impede traffic or pedestrian movement on the adjacent road area; (e) so that they do not adversely impact upon existing intersections or future road or intersection improvements; (f) so that they do not adversely impact current and future on-street parking arrangements; (g) so that they do not adversely impact on existing services within the road reserve adjacent to the site; (h) so that they do not involve ramping, cutting of the adjoining road reserve or any built structures (other than what may be necessary to cross over a stormwater channel). 	<p>AO3.1 Access is limited to one access cross over per site and is an access point located, designed and constructed in accordance with:</p> <ul style="list-style-type: none"> (a) Australian Standard AS2890.1; (b) Planning scheme policy SC6.5 – FNQROC Regional Development Manual - access crossovers. <p>AO3.2 Access, including driveways or access crossovers:</p> <ul style="list-style-type: none"> (a) are not placed over an existing: <ul style="list-style-type: none"> (i) telecommunications pit; (ii) stormwater kerb inlet; (iii) sewer utility hole; (iv) water valve or hydrant. (b) are designed to accommodate any adjacent footpath; (c) adhere to minimum sight distance requirements in accordance with AS2980.1. <p>AO3.3 Driveways are:</p> <ul style="list-style-type: none"> (a) designed to follow as closely as possible to the existing contours, but are no steeper than the gradients outlined in Planning scheme policy SC6.5 – FNQROC Regional Development Manual; (b) constructed such that where there is a grade shift to 1 in 4 (25%), there is an area with a grade of no more than 1 in 6 (16.6%) prior to this area, for a distance of at least 5 metres; (c) on gradients greater than 1 in 6 (16.6%) driveways are constructed to ensure the cross-fall of the driveway is one way and directed into the hill, for vehicle safety and drainage purposes; (d) constructed such that the transitional change in grade from the road to the lot is fully contained within the lot and not within the road reserve; (e) designed to include all necessary associated drainage that intercepts and directs storm water runoff to the storm water drainage system. 	<p>Complies The proposed use will utilize the existing site access cross.</p> <p>The proposed development does not involve the construction of new driveways or crossovers.</p>
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	AO3.4 Surface construction materials are consistent with the current or intended future streetscape or character of the area and contrast with the surface construction materials of any adjacent footpath.	
P04 Sufficient on-site wheel chair accessible car parking spaces are provided and are identified and reserved for such purposes.	AO4 The number of on-site wheel chair accessible car parking spaces complies with the rates specified in AS2890 Parking Facilities.	Complies The proposed development does not propose any accessibility spaces but can accommodate one if required.
P05 Access for people with disabilities is provided to the building from the parking area and from the street.	AO5 Access for people with disabilities is provided in accordance with the relevant Australian Standard.	Complies Access for people with disabilities is achievable to the building from the parking area.
P06 Sufficient on-site bicycle parking is provided to cater for the anticipated demand generated by the development.	AO6 The number of on-site bicycle parking spaces complies with the rates specified in Table 9.4.1.3.b.	Complies The proposed development can accommodate any required bicycle parking requirements if required.
P07 Development provides secure and convenient bicycle parking which: (a) for visitors is obvious and located close to the building's main entrance; (b) for employees is conveniently located to provide secure and convenient access between the bicycle storage area, end-of-trip facilities and the main area of the building; (c) is easily and safely accessible from outside the site.	AO7.1 Development provides bicycle parking spaces for employees which are co-located with end-of-trip facilities (shower cubicles and lockers); AO7.2 Development ensures that the location of visitor bicycle parking is discernible either by direct view or using signs from the street. AO7.3 Development provides visitor bicycle parking which does not impede pedestrian movement.	Complies The proposed development can accommodate any required bicycle parking requirements if required.
P08 Development provides walking and cycle routes through the site which: (a) link to the external network and pedestrian and cyclist destinations such as schools, shopping centres, open space, public transport stations, shops and local activity centres along the safest, most direct and convenient routes; (b) encourage walking and cycling; (c) ensure pedestrian and cyclist safety.	AO8 Development provides walking and cycle routes which are constructed on the carriageway or through the site to: (a) create a walking or cycle route along the full frontage of the site; (b) connect to public transport and existing cycle and walking routes at the frontage or boundary of the site.	n/a The proposed development does not propose or require any walking or cycle routes.

<p>PO9 Access, internal circulation and on-site parking for service vehicles are designed and constructed:</p> <ul style="list-style-type: none"> (a) in accordance with relevant standards; (b) so that they do not interfere with the amenity of the surrounding area; (c) so that they allow for the safe and convenient movement of pedestrians, cyclists and other vehicles. 	<p>AO9.1 Access driveways, vehicle manoeuvring and on-site parking for service vehicles are designed and constructed in accordance with AS2890.1 and AS2890.2.</p> <p>AO9.2 Service and loading areas are contained fully within the site.</p> <p>AO9.3 The movement of service vehicles and service operations are designed so they:</p> <ul style="list-style-type: none"> (a) do not impede access to parking spaces; (b) do not impede vehicle or pedestrian traffic movement. 	<p>Will Comply Site access, internal circulation and on-site parking for service vehicles will be designed and constructed in accordance with relevant standards.</p>
<p>PO10 Sufficient queuing and set down areas are provided to accommodate the demand generated by the development.</p>	<p>AO10.1 Development provides adequate area on-site for vehicle queuing to accommodate the demand generated by the development where drive through facilities or drop-off/pick-up services are proposed as part of the use, including, but not limited to, the following land uses:</p> <ul style="list-style-type: none"> (a) car wash; (b) child care centre; (c) educational establishment where for a school; (d) food and drink outlet, where including a drive-through facility; (e) hardware and trade supplies, where including a drive-through facility; (f) hotel, where including a drive-through facility; (g) service station. <p>AO10.2 Queuing and set-down areas are designed and constructed in accordance with AS2890.1.</p>	<p>n/a The proposed development does not propose or require any queuing or set down areas.</p>

9.4.3 Environmental Performance Code

The proposed development is subject to assessment against the provisions of the Environmental Performance Code of the Douglas Shire Planning Scheme.

9.4.3.3 Criteria for assessment

Table 9.4.3.3.a – Environmental performance code - assessable development

Performance outcomes	Acceptable outcomes	Compliance
Lighting		
PO1 Lighting incorporated within development does not cause an adverse impact on the amenity of adjacent uses and nearby sensitive land uses.	AO1.1 Technical parameters, design, installation, operation and maintenance of outdoor lighting comply with the requirements of Australian standard AS4282-1997 Control of the obtrusive effects of outdoor lighting. AO1.2 Development that involves flood lighting is restricted to a type that gives no upward component of light where mounted horizontally. AO1.3 Access, car parking and manoeuvring areas are designed to shield nearby residential premises from impacts of vehicle headlights.	Will Comply The proposed development will incorporate lighting that does not cause an adverse impact on the amenity of adjacent uses and nearby sensitive land uses.
Noise		
PO2 Potential noise generated from the development is avoided through design, location and operation of the activity. Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.	AO2.1 Development does not involve activities that would cause noise related environmental harm or nuisance; or AO2.2 Development ensures noise does not emanate from the site through the use of materials, structures and architectural features to not cause an adverse noise impact on adjacent uses. AO2.3 The design and layout of development ensures car parking areas avoid noise impacting directly	Complies The proposed development does not involve activities that would cause noise related environmental harm or nuisance. No sensitive land uses are located close enough to be impacted by any noise generated by the proposed use.

	<p>on adjacent sensitive land uses through one or more of the following:</p> <ul style="list-style-type: none"> (a) car parking is located away from adjacent sensitive land uses; (b) car parking is enclosed within a building; (c) a noise ameliorating fence or structure is established adjacent to car parking areas where the fence or structure will not have a visual amenity impact on the adjoining premises; (d) buffered with dense landscaping. <p>Editor's note - The Environmental Protection (Noise) Policy 2008, Schedule 1 provides guidance on acoustic quality objectives to ensure environmental harm (including nuisance) is avoided.</p>	
Airborne particles and other emissions		
<p>PO3 Potential airborne particles and emissions generated from the development are avoided through design, location and operation of the activity.</p> <p>Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p>AO3.1 Development does not involve activities that will result in airborne particles or emissions being generated;</p> <p>or</p> <p>AO3.2 The design, layout and operation of the development activity ensures that no airborne particles or emissions cause environmental harm or nuisance.</p> <p>Note - examples of activities which generally cause airborne particles include spray painting, abrasive blasting, manufacturing activities and car wash facilities.</p> <p>Examples of emissions include exhaust ventilation from basement or enclosed parking structures, air conditioning/refrigeration ventilation and exhaustion.</p> <p>The <i>Environmental Protection (Air) Policy 2008</i>, Schedule 1 provides guidance on air quality objectives to ensure environmental harm (including nuisance) is avoided.</p>	<p>Complies The proposed development does not involve activities that will result in airborne particles or emissions being generated. All activities will be contained within the building.</p>

Odours		
<p>PO4 Potential odour causing activities associated with the development are avoided through design, location and operation of the activity.</p> <p>Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p>AO4.1 The development does not involve activities that create odorous emissions; or AO4.2 The use does not result in odour that causes environmental harm or nuisance with respect to surrounding land uses.</p>	<p>Complies The proposed development does not involve activities that create odorous emissions.</p>
Waste and recyclable material storage		
<p>PO5 Waste and recyclable material storage facilities are located and maintained to not cause adverse impacts on adjacent uses.</p> <p>Note – Planning Scheme Policy SC6.4 – Environmental management plans provides guidance on preparing a report to demonstrate compliance with the purpose and outcomes of the code.</p>	<p>AO5.1 The use ensures that all putrescent waste is stored in a manner that prevents odour nuisance and is disposed of at regular intervals. AO5.2 Waste and recyclable material storage facilities are located, designed and maintained to not cause an adverse impact on users of the premises and adjacent uses through consideration of:</p> <ul style="list-style-type: none"> (a) the location of the waste and recyclable material storage areas in relation to the noise and odour generated; (b) the number of receptacles provided in relation to the collection, maintenance and use of the receptacles; (c) the durability of the receptacles, sheltering and potential impacts of local climatic conditions; (d) the ability to mitigate spillage, seepage or leakage from receptacles into adjacent areas and sensitive receiving waters and environments. <p>Editor's note - the <i>Environmental Protection (Waste Management) Policy 2008</i> provides guidance on the design of waste containers (receptacles) to ensure environmental harm (including nuisance) is avoided.</p>	<p>Complies The proposed development includes waste and recyclable material storage facilities which will be stored in a manner that prevents odour nuisance and is disposed of at regular intervals.</p>

Sensitive land use activities		
PO6 Sensitive land use activities are not established in areas which will receive potentially incompatible impacts on amenity from surrounding, existing development activities and land uses.	AO6.1 Sensitive land use activities are not established in areas that will be adversely impacted upon by existing land uses, activities and potential development possible in an area; or AO6.2 Sensitive land activities are located in areas where potential adverse amenity impacts mitigate all potential impacts through layout, design, operation and maintenance.	Complies The proposed development is not a sensitive land use activity.
Stormwater quality		
PO7 The quality of stormwater flowing over, through or being discharged from development activities into watercourses and drainage lines is of adequate quality for downstream environments, with respect to: (a) the amount and type of pollutants borne from the activity; (b) maintaining natural stream flows; (c) the amount and type of site disturbance; (d) site management and control measures.	AO7.1 Development activities are designed to ensure stormwater over roofed and hard stand areas is directed to a lawful point of discharge. AO7.2 Development ensures movement of stormwater over the site is not impeded or directed through potentially polluting activities. AO7.3 Soil and water control measures are incorporated into the activity's design and operation to control sediment and erosion potentially entering watercourses, drainage lines and downstream receiving waters. Note - Planning scheme policy - FNQROC Regional Development Manual provides guidance on soil and water control measures to meet the requirements of the Environmental Protection Act 1994. During construction phases of development, contractors and builders are to have consideration in their work methods and site preparation for their environmental duty to protect stormwater quality.	Complies The proposed development will be designed and constructed to ensure stormwater over roofed and hard stand areas is directed to a lawful point of discharge. The movement of stormwater over the site will not be impeded or directed through potentially polluting activities. The proposed use will utilize the existing Shed structure which includes a rainwater tank for Stormwater retention. No new construction activities are proposed.

Pest plants (for material change of use on vacant land over 1,000m ²)		
<p>PO8 Development activities and sites provide for the removal of all pest plants and implement ongoing measures to ensure that pest plants do not reinfest the site or nearby sites.</p> <p>Editor's note - This does not remove or replace all land owner's obligations or responsibilities under the <i>Land Protection (Pest and Stock Route Management) Act 2002</i>.</p>	<p>AO8.1 The land is free of declared pest plants before development establishes new buildings, structures and practices; or AO8.2 Pest plants detected on a development site are removed in accordance with a management plan prepared by an appropriately qualified person prior to construction of buildings and structures or earthworks.</p> <p>Note - A declaration from an appropriately qualified person validates the land being free from pest plants.</p> <p>Declared pest plants include locally declared and State declared pest plants.</p>	<p>n/a The development is not for material change of use on vacant land over 1,000m².</p>

9.4.5 Infrastructure Works Code

The proposed development is subject to assessment against the provisions of the Infrastructure Works Code of the Douglas Shire Planning Scheme.

9.4.5.3 Criteria for assessment

Table 9.4.5.3.a – Infrastructure works code –assessable development

Performance outcomes	Acceptable outcomes	Compliance
For self-assessable and assessable development		
Works on a local government road		
PO1 Works on a local government road do not adversely impact on footpaths or existing infrastructure within the road verge and maintain the flow, safety and efficiency of pedestrians, cyclists and vehicles.	AO1.1 Footpaths/pathways are located in the road verge and are provided for the hierarchy of the road and located and designed and constructed in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual. AO1.2 Kerb ramp crossovers are constructed in accordance with Planning scheme policy SC 5 – FNQROC Regional Development Manual. AO1.3 New pipes, cables, conduits or other similar infrastructure required to cross existing footpaths: (a) are installed via trenchless methods; or (b) where footpath infrastructure is removed to install infrastructure, the new section of footpath is installed to the standard detailed in the Planning scheme policy SC5 – FNQROC Regional Development Manual, and is not less than a 1.2 metre section.	n/a There is no footpath in the site frontage road reserve. No new footpaths are proposed. No new crossovers are proposed. The proposed development does not involve any structures located in the road reserve.

	<p>AO1.4 Where existing footpaths are damaged as a result of development, footpaths are reinstated ensuring:</p> <ul style="list-style-type: none"> (a) similar surface finishes are used; (b) there is no change in level at joins of new and existing sections; (c) new sections are matched to existing in terms of dimension and reinforcement. <p>Note – Figure 9.4.5.3.a provides guidance on meeting the outcomes.</p> <p>AO1.5 Decks, verandahs, stairs, posts and other structures located in the road reserve do not restrict or impede pedestrian movement on footpaths or change the level of the road verges.</p>	
Accessibility structures		
<p>PO2 Development is designed to ensure it is accessible for people of all abilities and accessibility features do not impact on the efficient and safe use of footpaths.</p> <p>Note – Accessibility features are those features required to ensure access to premises is provided for people of all abilities and include ramps and lifts.</p>	<p>AO2.1 Accessibility structures are not located within the road reserve.</p> <p>AO2.2 Accessibility structures are designed in accordance with AS1428.3.</p> <p>AO2.3 When retrofitting accessibility features in existing buildings, all structures and changes in grade are contained within the boundaries of the lot and not within the road reserve.</p>	<p>n/a There is no footpath in the site frontage road reserve.</p>
Water supply		
<p>PO3 An adequate, safe and reliable supply of potable, fire fighting and general use water is provided.</p>	<p>AO3.1 The premises is connected to Council's reticulated water supply system in accordance with the Design Guidelines set out in Section D6 of the Planning scheme policy SC5 – FNQROC Regional Development Manual; or</p>	<p>Complies The development site is connected to Council's reticulated water supply network.</p>

	<p>AO3.2 Where a reticulated water supply system is not available to the premises, on site water storage tank/s with a minimum capacity of 10,000 litres of stored water, with a minimum 7,500 litre tank, with the balance from other sources (e.g. accessible swimming pool, dam etc.) and access to the tank/s for fire trucks is provided for each new house or other development. Tank/s are to be fitted with a 50mm ball valve with a camlock fitting and installed and connected prior to occupation of the house and sited to be visually unobtrusive.</p>	
Treatment and disposal of effluent		
<p>PO4 Provision is made for the treatment and disposal of effluent to ensure that there are no adverse impacts on water quality and no adverse ecological impacts as a result of the system or as a result of increasing the cumulative effect of systems in the locality.</p>	<p>AO4.1 The site is connected to Council's sewerage system and the extension of or connection to the sewerage system is designed and constructed in accordance with the Design Guidelines set out in Section D7 of the Planning scheme policy SC5 – FNQROC Regional Development Manual; or AO4.2 Where not in a sewerage scheme area, the proposed disposal system meets the requirements of Section 33 of the Environmental Protection Policy (Water) 1997 and the proposed on site effluent disposal system is designed in accordance with the Plumbing and Drainage Act (2002).</p>	<p>Complies The proposed development will utilize the existing on-site septic system.</p>
Stormwater quality		
<p>PO5 Development is planned, designed, constructed and operated to avoid or minimise adverse impacts on stormwater quality in natural and developed catchments by: (a) achieving stormwater quality objectives; (b) protecting water environmental values;</p>	<p>AO5.1 A connection is provided from the premises to Council's drainage system; or AO5.2 An underground drainage system is constructed to convey stormwater from the premises to</p>	<p>Complies The proposed development will be designed and constructed to ensure stormwater over roofed and hard stand areas is directed to a lawful point of discharge.</p>

<p>(c) maintaining waterway hydrology.</p>	<p>Council's drainage system in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p> <p>AO5.3 A stormwater quality management plan is prepared, and provides for achievable stormwater quality treatment measures meeting design objectives listed in Table 9.4.5.3.b and Table 9.4.5.3.c, reflecting land use constraints, such as:</p> <ul style="list-style-type: none"> (a) erosive, dispersive and/or saline soil types; (b) landscape features (including landform); (c) acid sulfate soil and management of nutrients of concern; (d) rainfall erosivity. <p>AO5.4 Erosion and sediment control practices are designed, installed, constructed, monitored, maintained, and carried out in accordance with an erosion and sediment control plan.</p> <p>AO5.5 Development incorporates stormwater flow control measures to achieve the design objectives set out in Table 9.4.5.3.b and Table 9.4.5.3.c, including management of frequent flows, peak flows, and construction phase hydrological impacts.</p> <p>Note – Planning scheme policy SC5 – FNQROC Regional Development Manual provides guidance on soil and water control measures to meet the requirements of the <i>Environmental Protection Act 1994</i>.</p> <p>Note – During construction phases of development, contractors and builders are to have consideration in their work methods and site preparation for their environmental duty to protect stormwater quality.</p>	<p>The proposed use will utilize the existing Shed structure which includes a rainwater tank for Stormwater retention.</p> <p>The proposed development does not involve any new construction.</p>
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Non-tidal artificial waterways		
<p>P06 Development involving non-tidal artificial waterways is planned, designed, constructed and operated to:</p> <ul style="list-style-type: none"> (a) protect water environmental values; (b) be compatible with the land use constraints for the site for protecting water environmental values; (c) be compatible with existing tidal and non-tidal waterways; (d) perform a function in addition to stormwater management; (e) achieve water quality objectives. 	<p>AO6.1 Development involving non-tidal artificial waterways ensures:</p> <ul style="list-style-type: none"> (a) environmental values in downstream waterways are protected; (b) any ground water recharge areas are not affected; (c) the location of the waterway incorporates lowlying areas of the catchment connected to an existing waterway; (d) existing areas of ponded water are included. <p>AO6.2 Non-tidal artificial waterways are located:</p> <ul style="list-style-type: none"> (a) outside natural wetlands and any associated buffer areas; (b) to minimise disturbing soils or sediments; (c) to avoid altering the natural hydrologic regime in acid sulfate soil and nutrient hazardous areas. <p>AO6.3 Non-tidal artificial waterways located adjacent to, or connected to a tidal waterway by means of a weir, lock, pumping system or similar ensures:</p> <ul style="list-style-type: none"> (a) there is sufficient flushing or a tidal range of >0.3 m; or (b) any tidal flow alteration does not adversely impact on the tidal waterway; or (c) there is no introduction of salt water into freshwater environments. <p>AO6.4 Non-tidal artificial waterways are designed and managed for any of the following end-use purposes:</p> <ul style="list-style-type: none"> (a) amenity (including aesthetics), landscaping or recreation; or (b) flood management, in accordance with a drainage catchment management plan; or (c) stormwater harvesting plan as part of an integrated water cycle management plan; or 	<p>n/a The proposed development does not involve any non-tidal artificial waterways.</p>

	<p>(d) aquatic habitat.</p> <p>AO6.5 The end-use purpose of the non-tidal artificial waterway is designed and operated in a way that protects water environmental values.</p> <p>AO6.6 Monitoring and maintenance programs adaptively manage water quality to achieve relevant water quality objectives downstream of the waterway.</p> <p>AO6.7 Aquatic weeds are managed to achieve a low percentage of coverage of the water surface area, and pests and vectors are managed through design and maintenance.</p>	
Wastewater discharge		
<p>PO7 Discharge of wastewater to waterways, or off site:</p> <p>(a) meets best practice environmental management;</p> <p>(b) is treated to:</p> <p>(i) meet water quality objectives for its receiving waters;</p> <p>(ii) avoid adverse impact on ecosystem health or waterway health;</p> <p>(iii) maintain ecological processes, riparian vegetation and waterway integrity;</p> <p>(iv) offset impacts on high ecological value waters.</p>	<p>AO7.1 A wastewater management plan is prepared and addresses:</p> <p>(a) wastewater type;</p> <p>(b) climatic conditions;</p> <p>(c) water quality objectives;</p> <p>(d) best practice environmental management.</p> <p>AO7.2 The waste water management plan is managed in accordance with a waste management hierarchy that:</p> <p>(a) avoids wastewater discharge to waterways; or</p> <p>(b) if wastewater discharge cannot practicably be avoided, minimises wastewater discharge to waterways by re-use, recycling, recovery and treatment for disposal to sewer, surface water and ground water.</p> <p>AO7.3 Wastewater discharge is managed to avoid or minimise the release of nutrients of concern so as to minimise the occurrence, frequency and intensity of algal blooms.</p>	<p>n/a The proposed development does not involve any discharge of wastewater to waterways or off site.</p>

	<p>AO7.4 Development in coastal catchments avoids or minimises and appropriately manages soil disturbance or altering natural hydrology and:</p> <ul style="list-style-type: none"> (a) avoids lowering ground water levels where potential or actual acid sulfate soils are present; (b) manages wastewater so that: <ul style="list-style-type: none"> (i) the pH of any wastewater discharges is maintained between 6.5 and 8.5 to avoid mobilisation of acid, iron, aluminium and other metals; (ii) holding times of neutralised wastewater ensures the flocculation and removal of any dissolved iron prior to release; (iii) visible iron floc is not present in any discharge; (iv) precipitated iron floc is contained and disposed of; (v) wastewater and precipitates that cannot be contained and treated for discharge on site are removed and disposed of through trade waste or another lawful method. 	
Electricity supply		
<p>PO8 Development is provided with a source of power that will meet its energy needs.</p>	<p>AO8.1 A connection is provided from the premises to the electricity distribution network; or</p> <p>AO8.2 The premises is connected to the electricity distribution network in accordance with the Design Guidelines set out in Section D8 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p> <p>Note - Areas north of the Daintree River have a different standard.</p>	<p>Complies The development site is connected to Council's reticulated electricity distribution network.</p>

<p>PO9 Development incorporating pad-mount electricity infrastructure does not cause an adverse impact on amenity.</p>	<p>AO9.1 Pad-mount electricity infrastructure is: (a) not located in land for open space or sport and recreation purposes; (b) screened from view by landscaping or fencing; (c) accessible for maintenance.</p> <p>AO9.2 Pad-mount electricity infrastructure within a building, in a Town Centre is designed and located to enable an active street frontage.</p> <p>Note – Pad-mounts in buildings in activity centres should not be located on the street frontage.</p>	<p>n/a The proposed ROL does not involve any pad-mount electricity infrastructure.</p>
Telecommunications		
<p>PO10 Development is connected to a telecommunications service approved by the relevant telecommunication regulatory authority.</p>	<p>AO10 The development is connected to telecommunications infrastructure in accordance with the standards of the relevant regulatory authority.</p>	<p>Complies The development site is connected to the telecommunications service network.</p>
<p>PO11 Provision is made for future telecommunications services (e.g. fibre optic cable).</p>	<p>AO11 Conduits are provided in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual.</p>	<p>n/a No new telecommunications infrastructure is proposed or required.</p>
Road construction		
<p>PO12 The road to the frontage of the premises is constructed to provide for the safe and efficient movement of: (a) pedestrians and cyclists to and from the site; (b) pedestrians and cyclists adjacent to the site; (c) vehicles on the road adjacent to the site; (d) vehicles to and from the site; (e) emergency vehicles.</p>	<p>AO12.1 The road to the frontage of the site is constructed in accordance with the Design Guidelines set out in Sections D1 and D3 of the Planning scheme policy SC5 – FNQROC Regional Development Manual, for the particular class of road, as identified in the road hierarchy.</p>	<p>n/a No alterations are proposed or required to the road frontage of the site. Existing site access satisfies the minimum clearances for emergency vehicle access.</p>

	<p>AO12.2 There is existing road, kerb and channel for the full road frontage of the site.</p> <p>AO12.3 Road access minimum clearances of 3.5 metres wide and 4.8 metres high are provided for the safe passage of emergency vehicles.</p>	
Alterations and repairs to public utility services		
<p>PO13 Infrastructure is integrated with, and efficiently extends, existing networks.</p>	<p>AO13 Development is designed to allow for efficient connection to existing infrastructure networks.</p>	<p>n/a The proposed development does not involve any alterations to any existing public utility services.</p>
<p>PO14 Development and works do not affect the efficient functioning of public utility mains, services or installations.</p>	<p>AO14.1 Public utility mains, services and installations are not required to be altered or repaired as a result of the development; or AO14.2 Public utility mains, services and installations are altered or repaired in association with the works so that they continue to function and satisfy the relevant Design Guidelines set out in Section D8 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.</p>	<p>n/a The proposed development does not involve any alterations to any existing public utility services.</p>
Construction management		
<p>PO15 Work is undertaken in a manner which minimises adverse impacts on vegetation that is to be retained.</p>	<p>AO15 Works include, at a minimum: (a) installation of protective fencing around retained vegetation during construction; (b) erection of advisory signage; (c) no disturbance, due to earthworks or storage of plant, materials and equipment, of ground level and soils below the canopy of any retained vegetation; (d) removal from the site of all declared noxious weeds.</p>	<p>n/a No new construction is proposed.</p>

PO16 Existing infrastructure is not damaged by construction activities.	AO16 Construction, alterations and any repairs to infrastructure is undertaken in accordance with the Planning scheme policy SC5 – FNQROC Regional Development Manual. Note - Construction, alterations and any repairs to State-controlled roads and rail corridors are undertaken in accordance with the Transport Infrastructure Act 1994.	n/a No new construction is proposed.
For assessable development		
High speed telecommunication infrastructure		
PO17 Development provides infrastructure to facilitate the roll out of high speed telecommunications infrastructure.	AO17 No acceptable outcomes are prescribed.	n/a The development site is connected to the telecommunications service network. No new telecommunications infrastructure is proposed or required.
Trade waste		
PO18 Where relevant, the development is capable of providing for the storage, collection treatment and disposal of trade waste such that: (a) off-site releases of contaminants do not occur; (b) the health and safety of people and the environment are protected; (c) the performance of the wastewater system is not put at risk.	AO18 No acceptable outcomes are prescribed.	Complies The proposed development incorporated environmentally responsible storage, collection treatment and disposal of trade waste.
Fire services in developments accessed by common private title		
PO19 Hydrants are located in positions that will enable fire services to access water safely, effectively and efficiently.	AO19.1 Residential streets and common access ways within a common private title places hydrants at intervals of no more than 120 metres and at each intersection. Hydrants may have a single outlet and be situated above or below ground.	Complies The development site is accessible to firefighting services.

	AO19.2 Commercial and industrial streets and access ways within a common private title serving commercial properties such as factories and warehouses and offices are provided with above or below ground fire hydrants located at not more than 90 metre intervals and at each intersection. Above ground fire hydrants have dual-valved outlets.	n/a The development does not involve any commercial or industrial streets or access ways within a common private title serving commercial properties.
PO20 Hydrants are suitable identified so that fire services can locate them at all hours. Note – Hydrants are identified as specified in the Department of Transport and Main Roads Technical Note: 'Identification of street hydrants for fire fighting purposes' available under 'Publications'.	AO20 No acceptable outcomes are prescribed.	Complies The development site is accessible to firefighting services.

9.4.6 Landscaping Code

The proposed development is subject to assessment against the provisions of the Landscaping Code of the Douglas Shire Planning Scheme.

9.4.6.3 Criteria for assessment

Table 9.4.6.3.a – Landscaping code –assessable development

Performance outcomes	Acceptable outcomes
For self-assessable and assessable development	
Landscape design	
<p>PO1 Development provides landscaping that contributes to and creates a high quality landscape character for the site, street and local areas of the Shire by:</p> <ul style="list-style-type: none"> (a) promoting the Shire's character as a tropical environment; (b) softening the built form of development; (c) enhancing the appearance of the development from within and outside the development and makes a positive contribution to the streetscape; (d) screening the view of buildings, structures, open storage areas, service equipment, machinery plant and the like from public places, residences and other sensitive development; where necessary, ensuring the privacy of habitable rooms and private outdoor recreation areas; (f) contributing to a comfortable living environment and improved energy efficiency, by providing shade to reduce glare and heat absorption and re-radiation from buildings, parking areas and other hard surfaces; (g) ensuring private outdoor recreation space is useable; (h) providing long term soil erosion protection; (i) providing a safe environment; (j) integrating existing vegetation and other natural features of the premises into the development; (e) not adversely affecting vehicular and pedestrian sightlines and road safety. 	<p>AO1 Development provides landscaping:</p> <ul style="list-style-type: none"> (a) in accordance with the minimum area, dimensions and other requirements of applicable development codes; (b) that is designed and planned in a way that meets the guidelines for landscaping outlined in Planning Scheme Policy SC6.7 – Landscaping; (c) that is carried out and maintained in accordance with a landscaping plan that meets the guidelines for landscaping outlined in Planning Scheme Policy SC6.7 – Landscaping. <p>Note - Planning scheme policy SC6.7 – Landscaping provides guidance on meeting the outcomes of this code. A landscape plan submitted for approval in accordance with the Planning policy is one way to achieve this outcome.</p>
	<p>Complies No landscaping is proposed as part of the development however all existing vegetation will be retained.</p> <p>The existing on-site mature vegetation and road reserve mature vegetation provides a visual and noise attenuating buffer between the road and on-site buildings and activities.</p>

For assessable development		
<p>PO2 Landscaping contributes to a sense of place, is functional to the surroundings and enhances the streetscape and visual appearance of the development.</p>	<p>AO2.1 No acceptable outcomes are specified. Note - Landscaping is in accordance with the requirements specified in Planning scheme policy SC6.7 – Landscaping.</p> <p>AO2.2 Tropical urbanism is incorporated into building design. Note – ‘Tropical urbanism’ includes many things such as green walls, green roofs, podium planting and vegetation incorporated into the design of a building.</p>	<p>Complies The existing on-site mature vegetation and road reserve mature vegetation will be retained and provides a visual and noise attenuating buffer between the road and on-site buildings and activities.</p>
<p>PO3 Development provides landscaping that is, as far as practical, consistent with the existing desirable landscape character of the area and protects trees, vegetation and other features of ecological, recreational, aesthetic and cultural value.</p>	<p>AO3.1 Existing vegetation on site is retained and incorporated into the site design, wherever possible, utilising the methodologies and principles outline in AS4970-2009 Protection of Trees on Development Sites.</p> <p>AO3.2 Mature vegetation on the site that is removed or damaged during development is replaced with advanced species.</p> <p>AO3.3 Where there is an existing landscape character in a street or locality which results from existing vegetation, similar species are incorporated into new development.</p> <p>AO3.4 Street trees are species which enhance the landscape character of the streetscape, with species chosen from the Planning scheme policy SC6.7 – Landscaping.</p>	<p>Complies No additional landscaping is proposed as part of the development however all existing vegetation will be retained.</p>
<p>PO4 Plant species are selected with consideration to the scale and form of development, screening, buffering, streetscape, shading and the locality of the area.</p>	<p>AO4 Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.</p>	<p>n/a No additional landscaping is proposed as part of the development.</p>

PO5 Shade planting is provided in car parking areas where uncovered or open, and adjacent to driveways and internal roadways.	AO5 Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.	n/a No exposed car parking areas are proposed as part of the development.
PO6 Landscaped areas are designed in order to allow for efficient maintenance.	AO6.1 A maintenance program is undertaken in accordance with Planning scheme policy SC6.7 – Landscaping. AO6.2 Tree maintenance is to have regard to the 'Safe Useful Life Expectancy of Trees (SULE). Note – It may be more appropriate to replace trees with a SULE of less than 20 years (as an example), and replant with younger healthy species.	n/a No additional landscaping is proposed as part of the development.
PO7 Podium planting is provided with appropriate species for long term survival and ease of maintenance, with beds capable of proper drainage.	AO7.1 Podium planting beds are provided with irrigation and are connected to stormwater infrastructure to permit flush out. AO7.2 Species of plants are selected for long term performance designed to suit the degree of access to podiums and roof tops for maintenance.	n/a No podium planting is proposed as part of the development.
PO8 Development provides for the removal of all weed and invasive species and implement on-going measures to ensure that weeds and invasive species do not reinfest the site and nearby premises.	AO8 Weed and invasive species detected on a development site are removed in accordance with a management plan prepared by an appropriately qualified person.	Complies The site contains no known invasive species however, should weeds or invasive species be detected on the site, they will be removed in accordance with a management plan prepared by an appropriately qualified person.
PO9 The landscape design enhances personal safety and reduces the potential for crime and vandalism.	AO9 No acceptable outcomes are specified. Note - Planning scheme policy SC6.3 – Crime prevention through environmental design (CPTED) provides guidance on meeting this outcome.	n/a No additional landscaping is proposed as part of the development.
PO10 The location and type of plant species does not adversely affect the function and accessibility of services and facilities and service areas.	AO10 Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.	n/a No additional landscaping is proposed as part of the development.

9.4.9 Vegetation Management Code

The proposed development is subject to assessment against the provisions of the Vegetation Management Code of the Douglas Shire Planning Scheme.

9.4.9.3 Criteria for assessment

Table 9.4.9.3.a – Vegetation management –assessable development

Performance outcomes	Acceptable outcomes	Compliance
For self-assessable and assessable development		
PO1 Vegetation is protected to ensure that: (a) the character and amenity of the local area is maintained; (b) vegetation damage does not result in fragmentation of habitats; (c) vegetation damage is undertaken in a sustainable manner; (d) the Shire's biodiversity and ecological values are maintained and protected; (e) vegetation of historical, cultural and / or visual significance is retained; (f) vegetation is retained for erosion prevention and slope stabilisation.	AO1.1 Vegetation damage is undertaken by a statutory authority on land other than freehold land that the statutory authority has control over; or AO1.2 Vegetation damage is undertaken by or on behalf of the local government on land controlled, owned or operated by the local government; or AO1.3 Vegetation damage, other than referenced in AO1.1 or AO1.2 is the damage of: (a) vegetation declared as a pest pursuant to the <i>Land Protection (Pest and Stock Route Management) Act 2002</i> ; or (b) vegetation identified within the local government's register of declared plants pursuant to the local government's local laws; or (c) vegetation is located within a Rural zone and the trunk is located within ten metres of an existing building; or (d) vegetation is located within the Conservation zone or Environmental management zone	Complies The existing on-site mature vegetation and road reserve mature vegetation will be retained and provides a visual and noise attenuating buffer between the road and on-site buildings and activities.

and the trunk is located within three metres of an existing or approved structure, not including a boundary fence;.

or

AO1.4

Vegetation damage that is reasonably necessary for carrying out work that is:

- (a) authorised or required under legislation or a local law;
- (b) specified in a notice served by the local government or another regulatory authority;

or

AO1.5

Vegetation damage for development where the damage is on land the subject of a valid development approval and is necessary to give effect to the development approval;

or

AO1.6

Vegetation damage is in accordance with an approved Property Map of Assessable Vegetation issued under the *Vegetation Management Act 1999*;

or

AO1.7

Vegetation damage is essential to the maintenance of an existing fire break;

or

AO1.8

Vegetation damage is essential to prevent interference to overhead service cabling;

or

AO1.9

Vegetation damage is for an approved Forest practice, where the lot is subject to a scheme approved under the *Vegetation Management Act 1999*;

or

AO1.10

Vegetation damage is undertaken in accordance with section 584 of the *Sustainable Planning Act*

	<p>2009.</p> <p>AO1.11 Vegetation damage where it is necessary to remove one tree in order to protect an adjacent more significant tree (where they are growing close to one another).</p> <p>AO1.12 Private property owners may only remove dead, dying, structurally unsound vegetation following receipt of written advice from, at minimum, a fully qualified Certificate V Arborist. A copy of the written advice is to be submitted to Council for its records, a minimum of seven business days prior to the vegetation damage work commencing.</p>	
<p>PO2 Vegetation damaged on a lot does not result in a nuisance.</p>	<p>AO2.1 Damaged vegetation is removed and disposed of at an approved site; or AO2.2 Damaged vegetation is mulched or chipped if used onsite.</p>	<p>Complies All on-site vegetation is to be retained however any damaged vegetation will be removed and disposed of at an approved site and/or mulched or chipped if used onsite.</p>
For assessable development		
<p>PO3 Vegetation damage identified on the Places of significance overlay lot does not result in a negative impact on the site's heritage values.</p>	<p>AO3 No acceptable outcomes are prescribed.</p>	<p>n/a The development site does not contain any vegetation damage identified on the Places of significance overlay mapping.</p>

State code 1: Development in a state-controlled road environment

The proposed development is subject to assessment against the provisions of State code 1: Development in a state-controlled road environment.

Table 1.1 *Development in general*

Performance outcomes	Acceptable outcomes	Response
Buildings, structures, infrastructure, services and utilities		
PO1 The location of the development does not create a safety hazard for users of the state-controlled road .	AO1.1 Development is not located in a state-controlled road . AND AO1.2 Development can be maintained without requiring access to a state-controlled road .	Complies The development site is located on and gains access via the Captain Cook Highway, a state-controlled road. The location of the development does not create a safety hazard for users of the state-controlled road.
PO2 The design and construction of the development does not adversely impact the structural integrity or physical condition of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies The design and construction of the development does not adversely impact the structural integrity or physical condition of the state-controlled road or road transport infrastructure.
PO3 The location of the development does not obstruct road transport infrastructure or adversely impact the operating performance of the state-controlled road .	No acceptable outcome is prescribed.	Complies The location of the development does not obstruct road transport infrastructure or adversely impact the operating performance of the state-controlled road.
PO4 The location, placement, design and operation of advertising devices, visible from the state-controlled road , do not create a safety hazard for users of the state-controlled road .	No acceptable outcome is prescribed.	Complies The proposed development does not involve any advertising devices which might create a safety hazard for users of the state-controlled road.

<p>PO5 The design and construction of buildings and structures does not create a safety hazard by distracting users of the state-controlled road.</p>	<p>A05.1 Facades of buildings and structures fronting the state-controlled road are made of non-reflective materials. AND A05.2 Facades of buildings and structures do not direct or reflect point light sources into the face of oncoming traffic on the state-controlled road. AND A05.3 External lighting of buildings and structures is not directed into the face of oncoming traffic on the state-controlled road. AND A05.4 External lighting of buildings and structures does not involve flashing or laser lights.</p>	<p>n/a The proposed development does not involve the design or construction of any new buildings or structures.</p>
<p>PO6 Road, pedestrian and bikeway bridges over a state-controlled road are designed and constructed to prevent projectiles from being thrown onto the state-controlled road.</p>	<p>A06.1 Road, pedestrian and bikeway bridges over the state-controlled road include throw protection screens in accordance with section 4.11 of the Design Criteria for Bridges and Other Structures Manual, Department of Transport and Main Roads, 2020.</p>	<p>n/a The proposed development does not involve any road, pedestrian or bikeway bridges over a state-controlled road.</p>
<p>Landscaping</p>		
<p>PO7 The location of landscaping does not create a safety hazard for users of the state-controlled road.</p>	<p>A07.1 Landscaping is not located in a state-controlled road. AND A07.2 Landscaping can be maintained without requiring access to a state-controlled road. AND A07.3 Landscaping does not block or obscure the sight lines for vehicular access to a state-controlled road.</p>	<p>n/a The proposed development does not involve any new landscaping.</p>

Stormwater and overland flow		
PO8 Stormwater run-off or overland flow from the development site does not create or exacerbate a safety hazard for users of the state-controlled road .	No acceptable outcome is prescribed.	Complies All stormwater run-off or overland flow from the development site will maintained to the existing lawful point of discharge.
PO9 Stormwater run-off or overland flow from the development site does not result in a material worsening of the operating performance of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies All stormwater run-off or overland flow from the development site will not result in a material worsening of the operating performance of the state-controlled road or road transport infrastructure.
PO10 Stormwater run-off or overland flow from the development site does not adversely impact the structural integrity or physical condition of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies Stormwater run-off or overland flow from the development site will not adversely impact the structural integrity or physical condition of the state-controlled road or road transport infrastructure.
PO11 Development ensures that stormwater is lawfully discharged.	AO11.1 Development does not create any new points of discharge to a state-controlled road . AND AO11.2 Development does not concentrate flows to a state-controlled road . AND AO11.3 Stormwater run-off is discharged to a lawful point of discharge . AND AO11.4 Development does not worsen the condition of an existing lawful point of discharge to the state-controlled road .	Complies All stormwater run-off or overland flow from the development site will maintained to the existing lawful point of discharge.

Flooding		
PO12 Development does not result in a material worsening of flooding impacts within a state-controlled road .	AO12.1 For all flood events up to 1% annual exceedance probability , development results in negligible impacts (within +/- 10mm) to existing flood levels within a state-controlled road . AND AO12.2 For all flood events up to 1% annual exceedance probability , development results in negligible impacts (up to a 10% increase) to existing peak velocities within a state-controlled road . AND AO12.3 For all flood events up to 1% annual exceedance probability , development results in negligible impacts (up to a 10% increase) to existing time of submergence of a state-controlled road .	Complies Development does not result in a material worsening of flooding impacts within a state-controlled road.
Drainage Infrastructure		
PO13 Drainage infrastructure does not create a safety hazard for users in the state-controlled road .	AO13.1 Drainage infrastructure is wholly contained within the development site, except at the lawful point of discharge . AND AO13.2 Drainage infrastructure can be maintained without requiring access to a state-controlled road .	Complies All drainage infrastructure is wholly contained within the development site, except at the lawful point of discharge and can be maintained without requiring access to a state-controlled road.
PO14 Drainage infrastructure associated with, or within, a state-controlled road is constructed, and designed to ensure the structural integrity and physical condition of existing drainage infrastructure and the surrounding drainage network.	No acceptable outcome is prescribed.	n/a No additional drainage infrastructure is proposed.

Table 1.2 Vehicular access, road layout and local roads

Performance outcomes	Acceptable outcomes	Response
Vehicular access to a state-controlled road or within 100 metres of a state-controlled road intersection		
PO15 The location, design and operation of a new or changed access to a state-controlled road does not compromise the safety of users of the state-controlled road .	No acceptable outcome is prescribed.	n/a No new or changed access points are proposed.
PO16 The location, design and operation of a new or changed access does not adversely impact the functional requirements of the state-controlled road .	No acceptable outcome is prescribed.	n/a No new or changed access points are proposed.
PO17 The location, design and operation of a new or changed access is consistent with the future intent of the state-controlled road .	No acceptable outcome is prescribed.	n/a No new or changed access points are proposed.
PO18 New or changed access is consistent with the access for the relevant limited access road policy : 1. LAR 1 where direct access is prohibited; or 2. LAR 2 where access may be permitted, subject to assessment.	No acceptable outcome is prescribed.	n/a No new or changed access points are proposed.
PO19 New or changed access to a local road within 100 metres of an intersection with a state-controlled road does not compromise the safety of users of the state-controlled road .	No acceptable outcome is prescribed.	n/a No new or changed access points are proposed.
PO20 New or changed access to a local road within 100 metres of an intersection with a state-controlled road does not adversely impact on the operating performance of the intersection.	No acceptable outcome is prescribed.	n/a No new or changed access points are proposed.

Public passenger transport and active transport		
PO21 Development does not compromise the safety of users of public passenger transport infrastructure, public passenger services and active transport infrastructure .	No acceptable outcome is prescribed.	Complies The proposed development does not compromise the safety of users of public passenger transport infrastructure, public passenger services or active transport infrastructure.
PO22 Development maintains the ability for people to access public passenger transport infrastructure, public passenger services and active transport infrastructure .	No acceptable outcome is prescribed.	Complies The proposed development maintains the ability for people to access public passenger transport infrastructure, public passenger services and active transport infrastructure.
PO23 Development does not adversely impact the operating performance of public passenger transport infrastructure, public passenger services and active transport infrastructure .	No acceptable outcome is prescribed.	Complies The proposed development does not adversely impact the operating performance of public passenger transport infrastructure, public passenger services and active transport infrastructure.
PO24 Development does not adversely impact the structural integrity or physical condition of public passenger transport infrastructure and active transport infrastructure .	No acceptable outcome is prescribed.	Complies The proposed development does not adversely impact the structural integrity or physical condition of public passenger transport infrastructure and active transport infrastructure.

Table 1.3 **Network impacts**

Performance outcomes	Acceptable outcomes	Response
PO25 Development does not compromise the safety of users of the state-controlled road network.	No acceptable outcome is prescribed.	Complies The proposed development does not compromise the safety of users of the state-controlled road network.
PO26 Development ensures no net worsening of the operating performance of the state-controlled road network.	No acceptable outcome is prescribed.	Complies The proposed development will have no net worsening effect on the operating performance of the state-controlled road network.
PO27 Traffic movements are not directed onto a state-controlled road where they can be accommodated on the local road network.	No acceptable outcome is prescribed.	Complies Traffic movements cannot be accommodated on the local road network. The site has direct access only to the Captain Cook Highway, a State Controlled Road.

PO28 Development involving haulage exceeding 10,000 tonnes per year does not adversely impact the pavement of a state-controlled road .	No acceptable outcome is prescribed.	n/a Development does not involve haulage.
PO29 Development does not impede delivery of planned upgrades of state-controlled roads .	No acceptable outcome is prescribed.	Complies Development does not impede delivery of planned upgrades of state-controlled roads.
PO30 Development does not impede delivery of corridor improvements located entirely within the state-controlled road corridor .	No acceptable outcome is prescribed.	Complies Development does not impede delivery of corridor improvements located entirely within the state-controlled road corridor.

Table 1.4 *Filling, excavation, building foundations and retaining structures*

Performance outcomes	Acceptable outcomes	Response
PO31 Development does not create a safety hazard for users of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies Development does not create a safety hazard for users of the state-controlled road or road transport infrastructure.
PO32 Development does not adversely impact the operating performance of the state-controlled road .	No acceptable outcome is prescribed.	Complies Development does not adversely impact the operating performance of the state-controlled road.
PO33 Development does not undermine, damage or cause subsidence of a state-controlled road .	No acceptable outcome is prescribed.	Complies Development does not undermine damage or cause subsidence of a state-controlled road.
PO34 Development does not cause ground water disturbance in a state-controlled road .	No acceptable outcome is prescribed.	Complies Development does not cause ground water disturbance in a state-controlled road.
PO35 Excavation, boring, piling, blasting and fill compaction do not adversely impact the physical condition or structural integrity of a state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	n/a Development does not involve excavation, boring, piling, blasting or fill compaction.
PO36 Filling and excavation associated with the construction of new or changed access do not compromise the operation or capacity of existing drainage infrastructure for a state-controlled road .	No acceptable outcome is prescribed.	n/a Development does not involve any filling or excavation.

Table 1.5 **Environmental emissions**

Performance outcomes	Acceptable outcomes	Response
Reconfiguring a lot		
Involving the creation of 5 or fewer new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor		
PO37 Development minimises free field noise intrusion from a state-controlled road .	AO37.1 Development provides a noise barrier or earth mound which is designed, sited and constructed: <ol style="list-style-type: none"> to achieve the maximum free field acoustic levels in reference table 2 (item 2.1); in accordance with: <ol style="list-style-type: none"> Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. OR AO37.2 Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound. OR AO37.3 Development provides a solid gap-free fence or other solid gap-free structure along the full extent of the boundary closest to the state-controlled road .	n/a The proposed development is not an ROL.
Involving the creation of 6 or more new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor		
PO38 Reconfiguring a lot minimises free field noise intrusion from a state-controlled road .	AO38.1 Development provides noise barrier or earth mound which is designed, sited and constructed: <ol style="list-style-type: none"> to achieve the maximum free field acoustic levels in reference table 2 (item 2.1); 	n/a The proposed development is not an ROL. Development does not create 6 or more new lots.

	<p>2. in accordance with:</p> <ul style="list-style-type: none"> a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. <p>OR</p> <p>AO38.2 Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.</p>	
Material change of use (accommodation activity)		
Ground floor level requirements adjacent to a state-controlled road or type 1 multi-modal corridor		
<p>PO39 Development minimises noise intrusion from a state-controlled road in private open space.</p>	<p>AO39.1 Development provides a noise barrier or earth mound which is designed, sited and constructed:</p> <ul style="list-style-type: none"> 1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.2) for private open space at the ground floor level; 2. in accordance with: <ul style="list-style-type: none"> a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. <p>OR</p> <p>AO39.2 Development achieves the maximum free field acoustic level in reference table 2 (item</p>	<p>n/a The proposed development is not an accommodation activity.</p>

	2.2) for private open space by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.	
PO40 Development (excluding a relevant residential building or relocated building) minimises noise intrusion from a state-controlled road in habitable rooms at the facade.	AO40.1 Development (excluding a relevant residential building or relocated building) provides a noise barrier or earth mound which is designed, sited and constructed: 1. to achieve the maximum building façade acoustic level in reference table 1 (item 1.1) for habitable rooms ; 2. in accordance with: a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. OR AO40.2 Development (excluding a relevant residential building or relocated building) achieves the maximum building façade acoustic level in reference table 1 (item 1.1) for habitable rooms by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.	n/a The proposed development is not an accommodation activity.
PO41 Habitable rooms (excluding a relevant residential building or relocated building) are designed and constructed using materials to achieve the maximum internal acoustic level in reference table 3 (item 3.1).	No acceptable outcome is provided.	n/a The proposed development is not an accommodation activity.
Above ground floor level requirements (accommodation activity) adjacent to a state-controlled road or type 1 multi-modal corridor		
PO42 Balconies, podiums, and roof decks include: 1. a continuous solid gap-free structure or	No acceptable outcome is provided.	n/a The proposed development is not an accommodation activity.

<p>balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia);</p> <p>2. highly acoustically absorbent material treatment for the total area of the soffit above balconies, podiums, and roof decks.</p>		
<p>PO43 Habitable rooms (excluding a relevant residential building or relocated building) are designed and constructed using materials to achieve the maximum internal acoustic level in reference table 3 (item 3.1).</p>	<p>No acceptable outcome is provided.</p>	<p>n/a The proposed development is not an accommodation activity.</p>
<p>Material change of use (other uses)</p>		
<p>Ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor</p>		
<p>PO44 Development:</p> <p>1. provides a noise barrier or earth mound that is designed, sited and constructed:</p> <ol style="list-style-type: none"> to achieve the maximum free field acoustic level in reference table 2 (item 2.3) for all outdoor education areas and outdoor play areas; in accordance with: <ol style="list-style-type: none"> Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or <p>2. achieves the maximum free field acoustic level in reference table 2 (item 2.3) for all outdoor education areas and outdoor play areas by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.</p>	<p>No acceptable outcome is provided.</p>	<p>n/a The proposed development is not for a childcare centre, educational establishment or hospital activity.</p>

<p>PO45 Development involving a childcare centre or educational establishment:</p> <ol style="list-style-type: none"> 1. provides a noise barrier or earth mound that is designed, sited and constructed; 2. to achieve the maximum building facade acoustic level in reference table 1 (item 1.2); 3. in accordance with: <ol style="list-style-type: none"> a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or 4. achieves the maximum building facade acoustic level in reference table 1 (item 1.2) by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound. 	<p>No acceptable outcome is provided.</p>	<p>n/a The proposed development is not for a childcare centre, educational establishment or hospital activity.</p>
<p>PO46 Development involving:</p> <ol style="list-style-type: none"> 1. indoor education areas and indoor play areas; or 2. sleeping rooms in a childcare centre; or 3. patient care areas in a hospital achieves the maximum internal acoustic level in reference table 3 (items 3.2-3.4). 	<p>No acceptable outcome is provided.</p>	<p>n/a The proposed development is not for a childcare centre, educational establishment or hospital activity.</p>
<p>Above ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor</p>		
<p>PO47 Development involving a childcare centre or educational establishment which have balconies, podiums or elevated outdoor play areas predicted to exceed the maximum free field acoustic level in reference table 2 (item 2.3) due to noise from a state-controlled road are</p>	<p>No acceptable outcome is provided.</p>	<p>n/a The proposed development is not for a childcare centre, educational establishment or hospital activity.</p>

provided with: 1. a continuous solid gap-free structure or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia); 2. highly acoustically absorbent material treatment for the total area of the soffit above balconies or elevated outdoor play areas .		
PO48 Development including: 1. indoor education areas and indoor play areas in a childcare centre or educational establishment ; or 2. sleeping rooms in a childcare centre ; or 3. patient care areas in a hospital located above ground level, is designed and constructed to achieve the maximum internal acoustic level in reference table 3 (items 3.2-3.4).	No acceptable outcome is provided.	n/a The proposed development is not for a childcare centre, educational establishment or hospital activity.
Air, light and vibration		
PO49 Private open space, outdoor education areas and outdoor play areas are protected from air quality impacts from a state-controlled road .	AO49.1 Each dwelling or unit has access to a private open space which is shielded from a state-controlled road by a building, solid gap-free fence , or other solid gap-free structure . OR AO49.2 Each outdoor education area and outdoor play area is shielded from a state-controlled road by a building, solid gap-free fence , or other solid gap-free structure .	n/a The proposed development is not for a childcare centre, educational establishment or hospital activity.
PO50 Patient care areas within hospitals are protected from vibration impacts from a state-controlled road or type 1 multi-modal corridor .	AO50.1 Hospitals are designed and constructed to ensure vibration in the patient treatment area does not exceed a vibration dose value of $0.1\text{m/s}^{1.75}$. AND AO50.2 Hospitals are designed and constructed to ensure vibration in the ward of a patient care area does not exceed a vibration dose value of $0.4\text{m/s}^{1.75}$.	n/a The proposed development is not for a childcare centre, educational establishment or hospital activity.

PO51 Development is designed and sited to ensure light from infrastructure within, and from users of, a state-controlled road or type 1 multi-modal corridor , does not: <ol style="list-style-type: none"> intrude into buildings during night hours (10pm to 6am); create unreasonable disturbance during evening hours (6pm to 10pm). 	No acceptable outcomes are prescribed.	n/a The proposed development is not for a childcare centre, educational establishment or hospital activity.
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Table 1.6: Development in a future state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
PO52 Development does not impede delivery of a future state-controlled road .	AO52.1 Development is not located in a future state-controlled road . OR ALL OF THE FOLLOWING APPLY: AO52.2 Development does not involve filling and excavation of, or material changes to, a future state-controlled road . AND AO52.3 The intensification of lots does not occur within a future state-controlled road . AND AO52.4 Development does not result in the landlocking of parcels once a future state-controlled road is delivered.	Complies Development does not impede delivery of a future state-controlled road.
PO53 The location and design of new or changed access does not create a safety hazard for users of a future state-controlled road .	AO53.1 Development does not include new or changed access to a future state-controlled road .	n/a No new or changed access points are proposed.
PO54 Filling, excavation, building foundations and retaining structures do not undermine, damage or cause subsidence of a future state-controlled road .	No acceptable outcome is prescribed.	n/a The proposed development does not involve any filling, excavation, building foundations or retaining structures.

<p>PO55 Development does not result in a material worsening of stormwater, flooding, overland flow or drainage impacts in a future state-controlled road or road transport infrastructure.</p>	<p>No acceptable outcome is prescribed.</p>	<p>Complies The proposed development does not alter or propose any new drainage infrastructure and will not result in a material worsening of stormwater, flooding, overland flow or drainage impacts in a future state-controlled road or road transport infrastructure.</p>
<p>PO56 Development ensures that stormwater is lawfully discharged.</p>	<p>AO56.1 Development does not create any new points of discharge to a future state-controlled road. AND AO56.2 Development does not concentrate flows to a future state-controlled road. AND AO56.3 Stormwater run-off is discharged to a lawful point of discharge. AND AO56.4 Development does not worsen the condition of an existing lawful point of discharge to the future state-controlled road.</p>	<p>Complies All stormwater run-off or overland flow from the development site will maintained to the existing lawful point of discharge. The proposed development does not create any new points of discharge to a future state-controlled road, concentrate flows to a future state-controlled road or worsen the condition of an existing lawful point of discharge to the future state-controlled road.</p>

State Development Assessment Provisions v3.0

State code 1: Development in a state-controlled road environment

6133 CAPTAIN COOK HIGHWAY

PROPOSED NEW SHED



NOTES
CONSTRUCTION AND MATERIALS SHALL COMPLY WITH CURRENT EDITIONS OF RELEVANT SAA CODES.

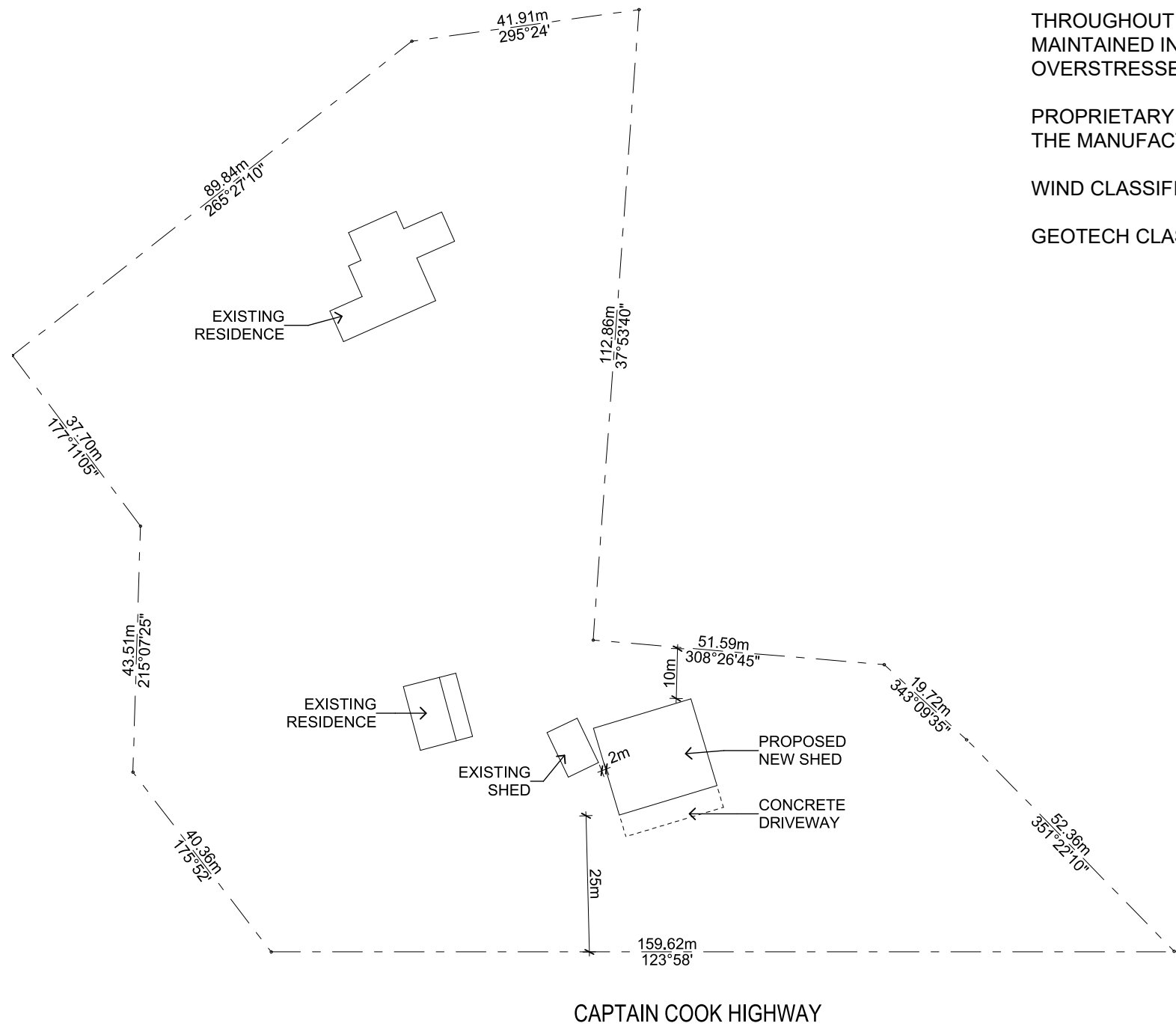
PLAN SET-OUT DIMENSIONS ARE NOMINAL ONLY, CONFIRM ON SITE. DIMENSIONS SHALL BE VERIFIED BEFORE COMMENCING CONSTRUCTION. DO NOT SCALE DRAWINGS TO OBTAIN DIMENSIONS.

THROUGHOUT CONSTRUCTION, THE STRUCTURE SHALL BE MAINTAINED IN A STABLE CONDITION AND NO PART SHALL BE OVERSTRESSED.

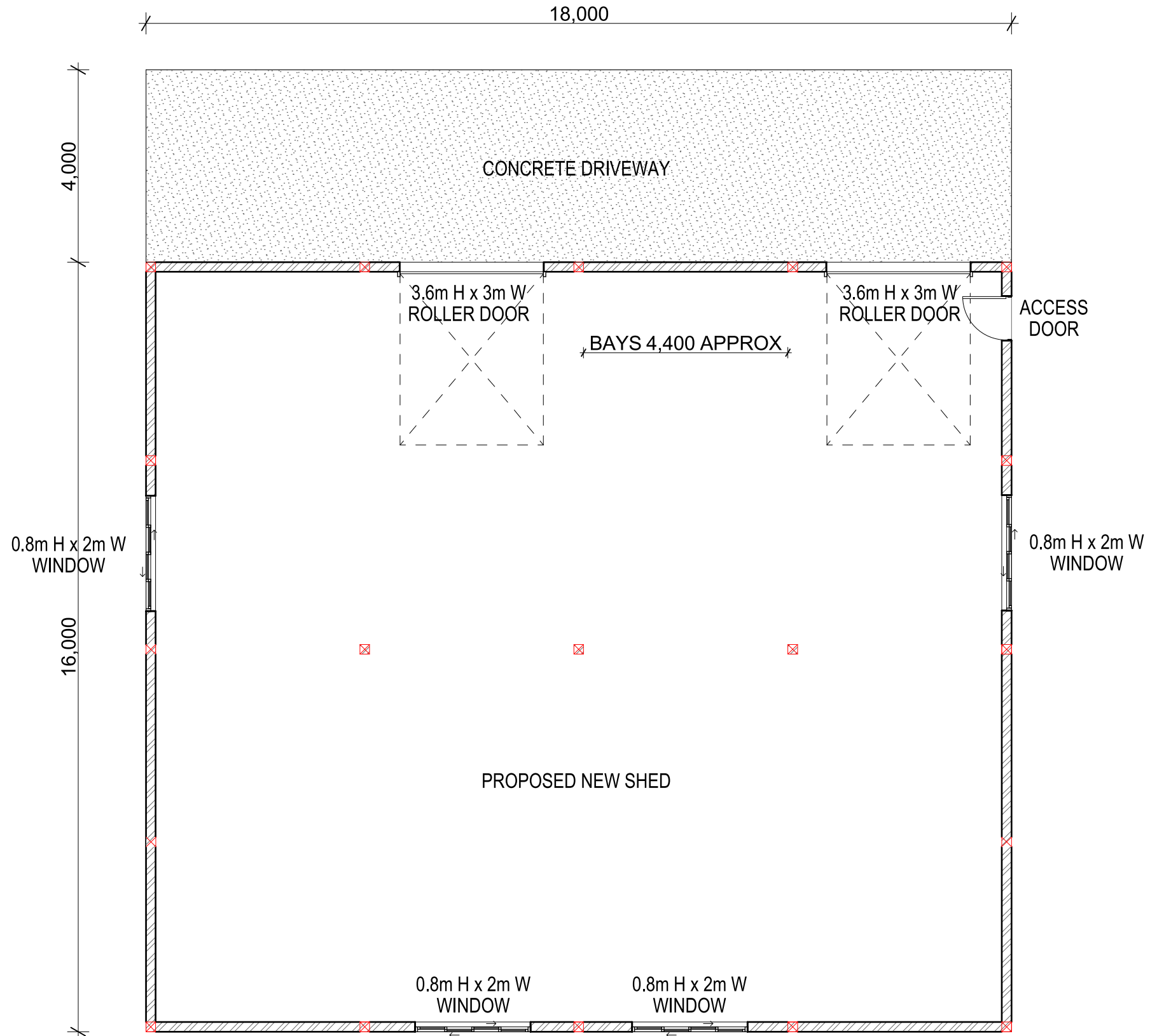
PROPRIETARY ITEMS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATION.

WIND CLASSIFICATION TO AS4055 - C2

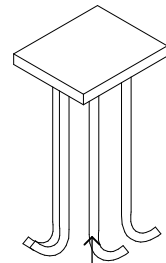
GEOTECH CLASSIFICATION - M ADOPTED



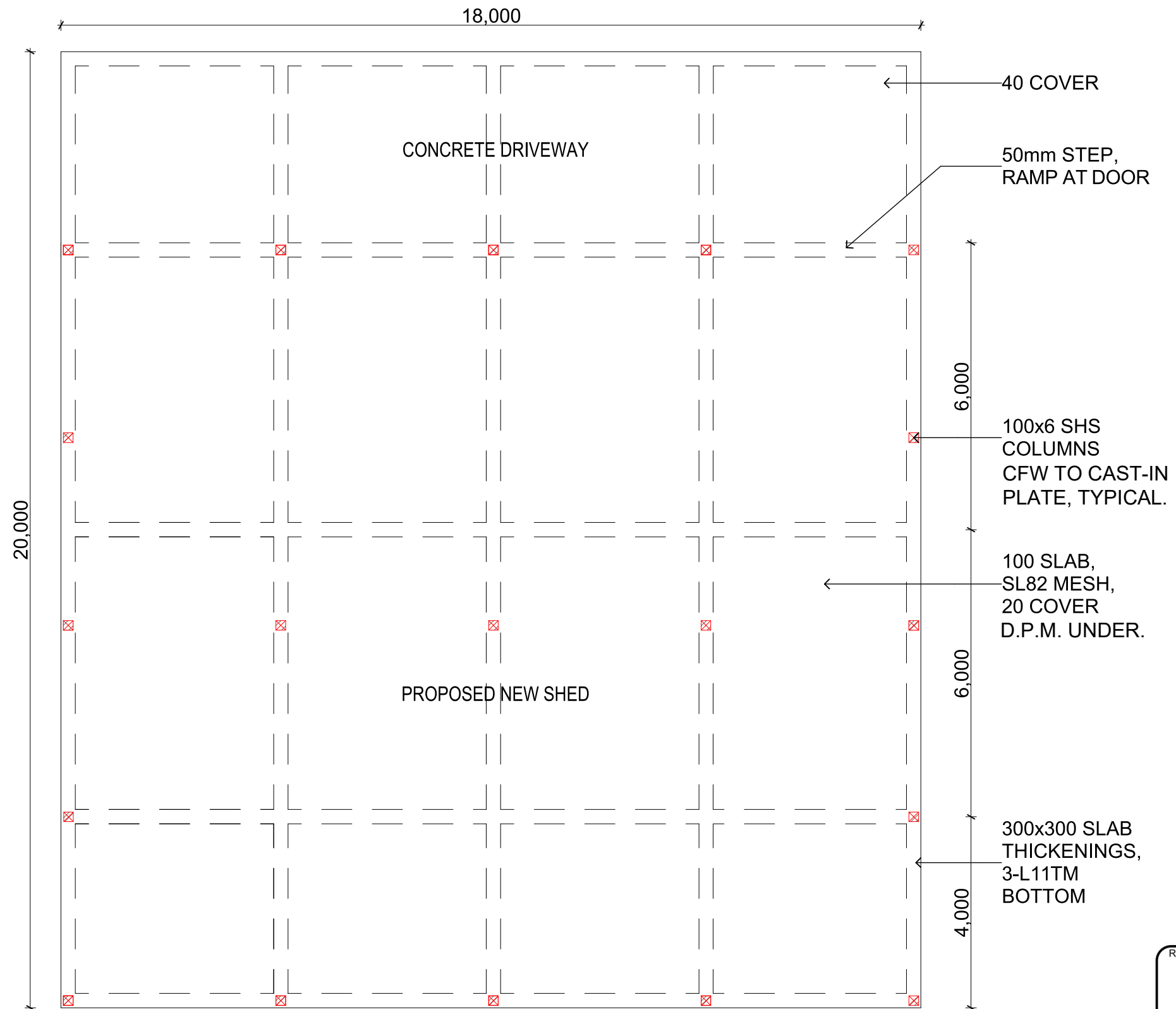
Registered Professional Engineer 699704
Mr Sean F. Edwards
MIEAust CPEng (Civil/Structural)
RPEQ No 6138
NT-BPBCE: 26906ES
RBP (Vic) EC37110
Signature.....Date 13/10/21
Registered in the National Professional Engineers Register in the
area of practice of Civil / Structural



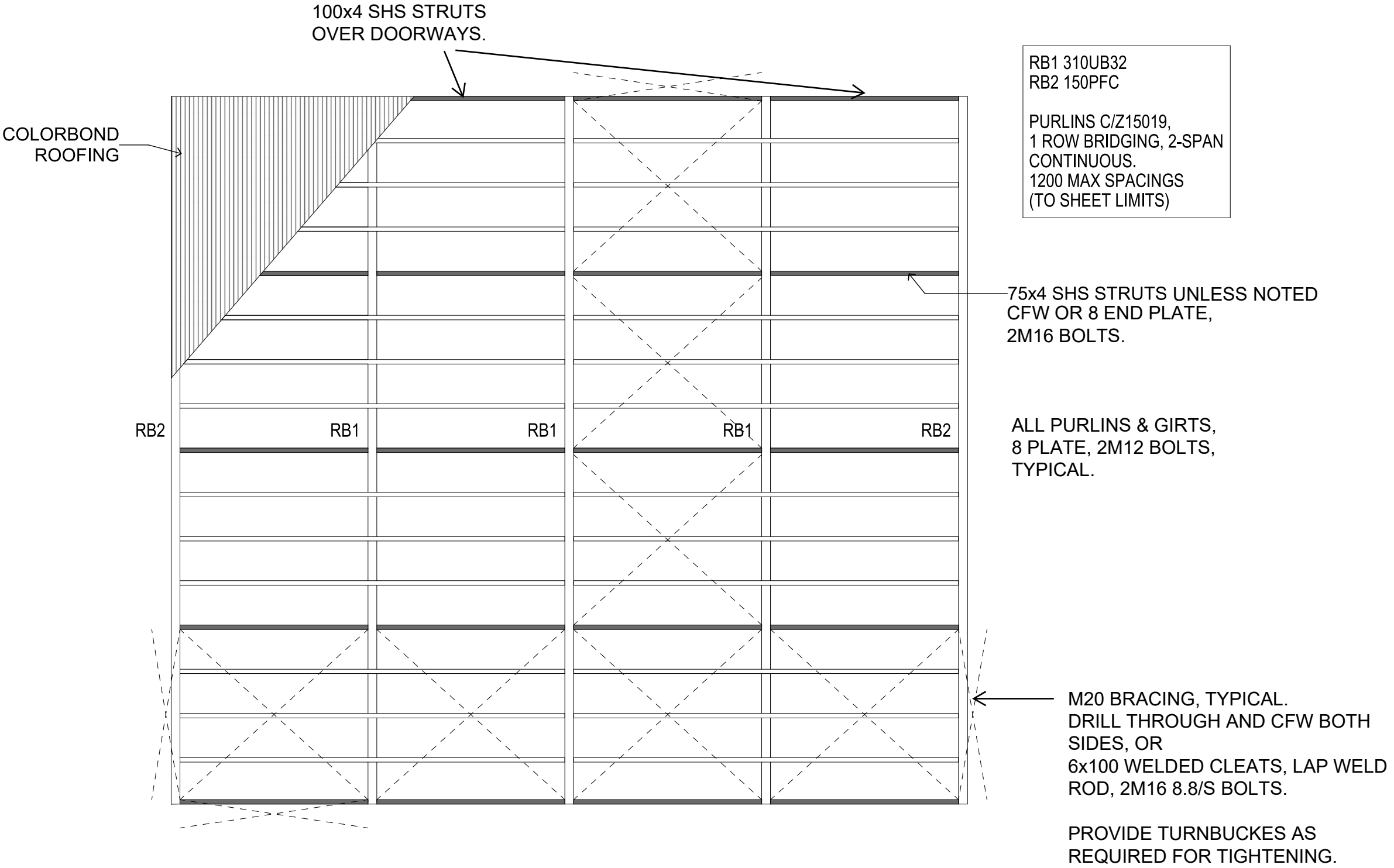
Registered Professional Engineer 699704
Mr Sean F. Edwards
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RBP (Vic) EC37110
Signature: *[Signature]* Date: 13/10/21
Registered on the National Professional Engineers Register in the
area of practice of **Civil / Structural**



200x10 GALVANIZED
PLATE 4N12 COGGED
BARS AT 150CRS
CFW TO PLATE.
250 EMBEDMENT



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NOTE
The Builder shall check all dimensions and levels on site prior to construction. Notify any errors, discrepancies or omissions to the architect. Refer to written dimensions only. Do not scale drawings. Drawings shall not be used for construction purposes until issued for construction. This drawing reflects a design by Angel Construction and is to be used only for work when authorised in writing by Angel Construction.

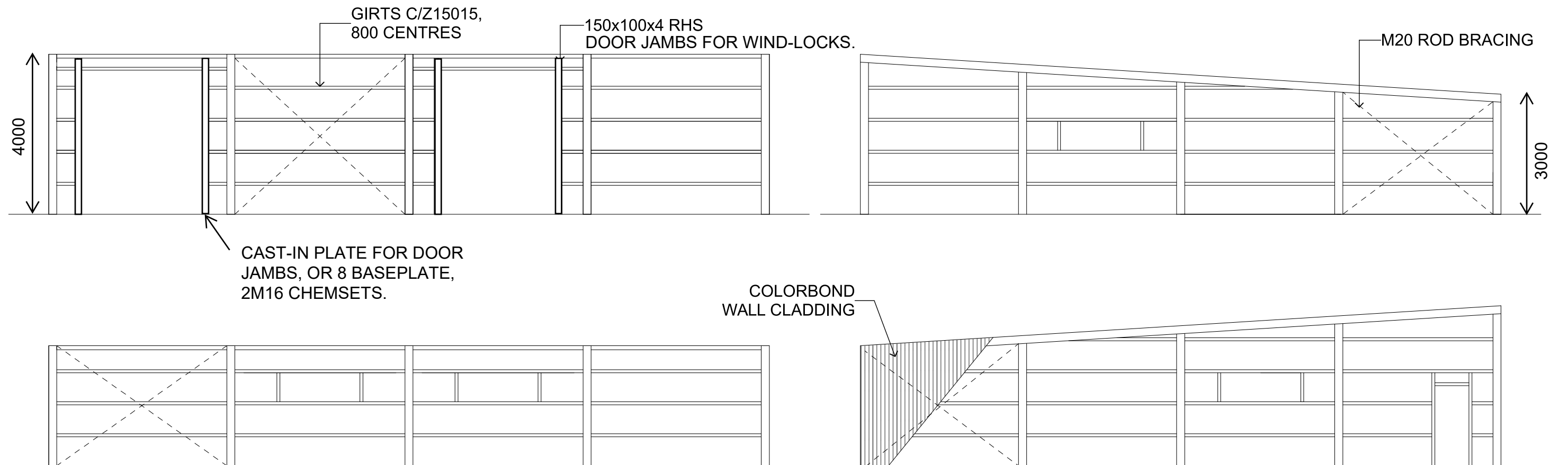
It is the contractors responsibility to confirm all measurements on site and locations of any services prior to work on site.

All documents here within are subject to Australian Copyright Laws.

Drawn By:	TG
Date:	11/10/2021
Project NO.:	AC000121
Project Status:	Construction
Scale:	1:100
Client:	Adrian Whittaker
Address:	6133 Captain Cook Highway Port Douglas 4877

DRAWING TITLE:	Roof Framing Plan
PROJECT NAME:	Proposed New Shed

REVISION NO.	
DRAWING NO.	A103



Registered Professional Engineer 699704
Mr Sean F. Edwards
MIEAust CPEng (Civil/Structural)
RPEQ No 6138
NT-BPBCE: 26906ES
RBP (Vic) EC37110
Signature: *[Signature]* Date: 13/10/21
Registered on the National Professional Engineers Register in the
area of practice of **Civil / Structural**

Individual owner's consent for making a development application under the *Planning Act 2016*

I, Angela Whittaker

[Insert full name.]

as owner of the premises identified as follows:

6133 Captain Cook Highway, Craiglie Qld. 4877
3RP743352

consent to the making of a development application under the *Planning Act 2016* by:

Scope Town Planning

on the premises described above for:

Material Change of Use (Light Industry)



16/03/2023

[signature of owner and date signed]