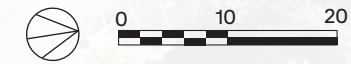


Landscape Architecture

Landscape
Upper & Lower
Ground
Plan

LEGEND

- 1. ENTRY DRIVEWAY
- 2. PORTE COCHERE
- 3. FEATURE ARBOUR
- 4. BASEMENT RAMP
- 5. LOBBY
- 6. BOH
- 7. DECK GRILL
- 8. LOBBY LOUNGE
- 9. ALL DAY DINING LOWER GROUND
- 10. GUESTROOMS
- 11. ALFRESCO DINING DECK
- 12. POOL LOUNGES / CABANAS
- 13. BEACH LOUNGES
- 14. WATER LOUNGE / SHALLOW POOL AREA
- 15. POOL BAR
- 16. BRIDGE / WALKWAY
- 17. PRIVATE PLUNGE POOLS
- 18. LAGOON POOLS
- 19. SWIM OUT TERRACES
- 20. GREEN ROOF ARBOURS OVER BREEZEWAYS
- 21. ATRIUM
- 22. LOUNGE SPILL OUT
- 23. KIDS' CLUB LOWER GROUND
- 24. KIDS' CLUB OUTDOOR PLAY SPACE
- 25. WELLNESS CENTRE & DAY SPA AT LOWER GROUND WITH GUESTROOMS ABOVE
- 26. DAY SPA TREATMENT PODS
- 27. UPPER GROUND PRIVATE PLUNGE POOLS
- 28. PEDESTRIAN ACCESS TO PORTE COCHERE



Landscape
Levels 1 & 2 Plan

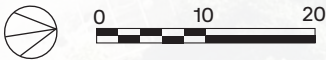
- LEVEL 1
- 1. SPECIALTY RESTAURANT
 - 2. FUNCTION / PREFUNCTION
 - 3. GRAND COMMONS BAR
 - 4. BREAKOUT BALCONY
- LEVEL 2
- 5. 'WILDFIRE' SEASONAL DINING
 - 6. OUTDOOR SEATING
 - 7. 'SUNDOWNERS' POOL BAR
 - 8. ROOFTOP POOL
 - 9. ROOFTOP POOL DECK
 - 10. SWIM UP BAR / DJ STATION
- 11. GUESTROOMS
 - 12. GREEN ROOF ARBOURS OVER BREEZEWAYS
 - 13. ATRIUM
 - 14. BRIDGE CONNECTIONS
 - 15. PLUNGE POOLS



Landscape
Combined
Levels /
Masterplan

LEGEND

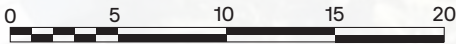
- 1. ENTRY DRIVEWAY
- 2. PORTE COCHERE
- 3. FEATURE ARBOUR
- 4. BASEMENT RAMP
- 5. LOBBY BELOW L1
- 6. BOH
- 7. ROOFTOP L2 BAR/RESTAURANT
- 8. LOUNGE BELOW L1
- 9. ALL DAY DINING BELOW L1
- 10. GUESTROOMS
- 11. ALFRESCO DINING DECK
- 12. POOL LOUNGES / CABANAS
- 13. LAGOON BEACH
- 14. WATER LOUNGE / SHALLOW POOL AREA
- 15. POOL BAR
- 16. BRIDGE / WALKWAY AT L2
- 17. PRIVATE PLUNGE POOLS
- 18. LAGOON POOLS
- 19. SWIM OUT TERRACES
- 20. GREEN ROOF ARBOURS OVER BREEZEWAYS
- 21. ATRIUM
- 22. LOUNGE SPILL OUT TERRACE
- 23. ROOFTOP FACILITIES L2
- 24. WELLNESS CENTRE & DAY SPA L1
- 25. KIDS' CLUB LOWER GROUND
- 26. DAY SPA TREATMENT PODS
- 27. KIDS' CLUB OUTDOOR PLAY SPACE
- 28. DECK GRILL
- 29. ALL WEATHER WALKWAYS AT GROUND
- 30. PEDESTRIAN ACCESS TO PORTE COCHERE



Landscape
South-West /
Zoom-in

LEGEND

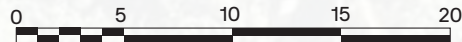
- 1. PORTE COCHERE WITH
FEATURE GREEN ROOF
AWNING ABOVE
- 2. LOBBY BELOW
- 3. LOUNGE TERRACE
- 4. ALL DAY DINING TERRACE
- 5. HOTEL SUITES
- 6. POOL LOUNGES / CABANAS
- 7. POOL LOUNGES
- 8. WATER LOUNGE / SHALLOW
POOL AREA
- 9. POOL PAVILION
- 10. BRIDGE / WALKWAY
- 11. PRIVATE PLUNGE POOLS
- 12. LAGOON POOLS
- 13. SWIM OUT TERRACES
- 14. GREEN ROOF ARBOURS OVER
BREEZEWAYS
- 15. ATRIUM
- 16. ROOFTOP FACILITIES L2
- 17. LANDSCAPE BUFFERS
- 18. FIRE PIT
- 19. INDIGENOUS SCULPTURAL
ART FOCAL POINT
- 20. ALL WEATHER WALKWAYS



Landscape
South-East /
Zoom-in

LEGEND

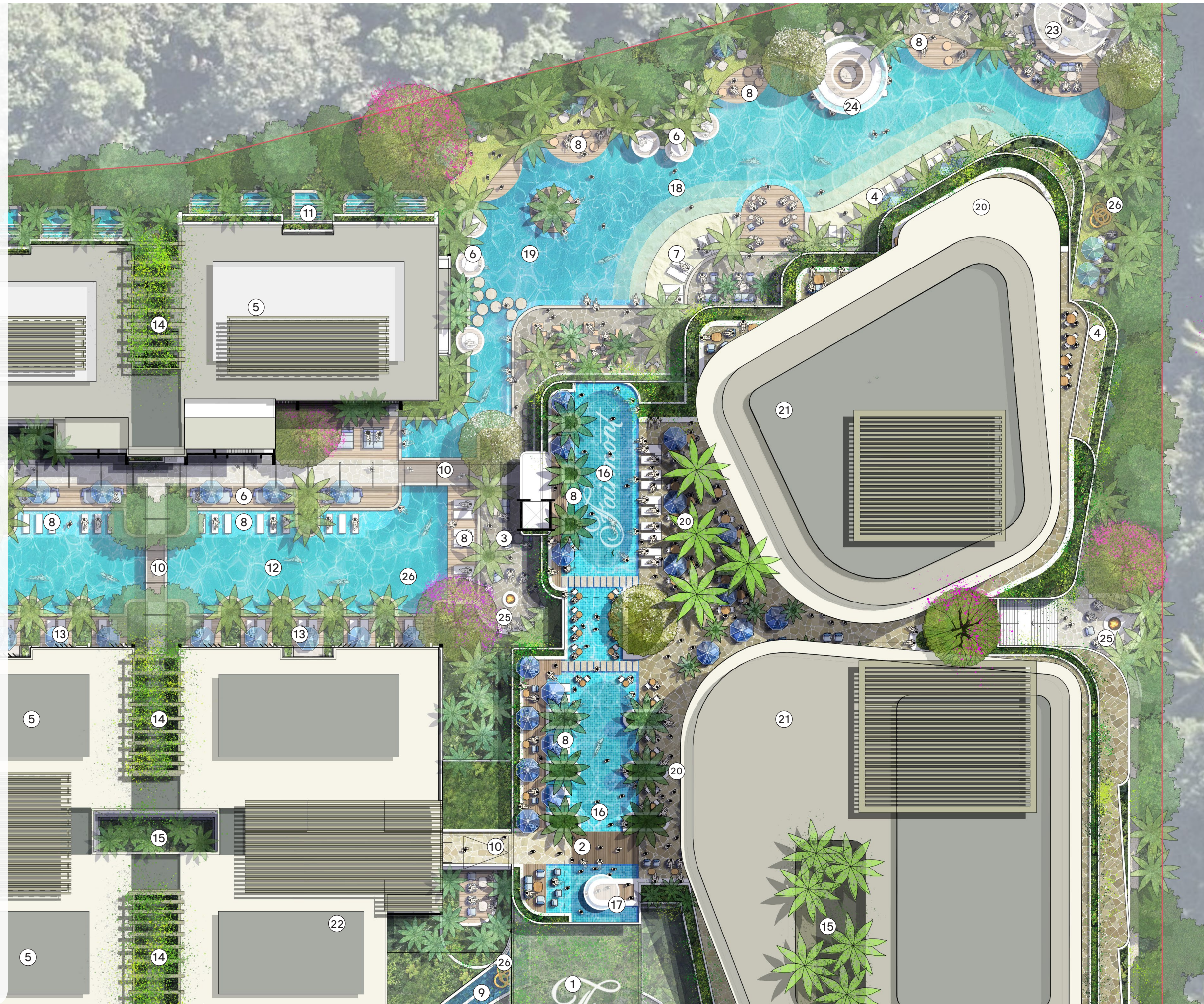
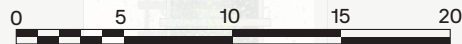
- 1. PORTE COCHERE
- 2. LOBBY BELOW
- 3. VEHICLE ARRIVAL EXPERIENCE
- 4. ARRIVAL SIGNAGE AND BRANDING
- 5. HOTEL SUITES
- 6. POOL LOUNGES / CABANAS
- 7. LAGOON BEACH
- 8. WATER LOUNGE / SHALLOW POOL AREA
- 9. POOL PAVILION
- 10. BRIDGE / WALKWAY
- 11. PRIVATE PLUNGE POOLS
- 12. LAGOON POOLS
- 13. SWIM OUT TERRACES
- 14. GREEN ROOF OVER BREEZEWAYS
- 15. ATRIUM
- 16. ROOFTOP FACILITIES L2
- 17. LANDSCAPE BUFFER
- 18. CONCEALED BASEMENT ACCESS - GREEN ARBOR ABOVE
- 19. PEDESTRIAN ENTRY
- 20. WELLNESS CENTRE & DAY SPA L1
- 21. DAY SPA TREATMENT PODS
- 22. KIDS' CLUB PLAY SPACE AND POOL
- 23. INDIGENOUS SCULPTURAL ART FOCAL POINT



Landscape
North /
Zoom-in

LEGEND

- 1. PORTE COCHERE UG
- 2. LOBBY BELOW UG
- 3. LOUNGE TERRACE
- 4. ALL DAY DINING TERRACE LG
- 5. HOTEL SUITES
- 6. POOL LOUNGES / CABANAS
- 7. BEACH LOUNGES
- 8. WATER LOUNGE / SHALLOW POOL AREA
- 9. WATER FEATURE / SIGNAGE
- 10. BRIDGE / WALKWAY
- 11. PRIVATE PLUNGE POOLS
- 12. LAGOON POOLS
- 13. SWIM OUT TERRACES
- 14. GREEN ROOF ARBOURS OVER BREEZEWAYS
- 15. ATRIUM
- 16. L2 ADULTS POOL
- 17. SWIM-UP BAR/DJ STATION
- 18. FAMILY LAGOON POOL
- 19. KIDS' POOL
- 20. ALFRESCO TERRACE L2
- 21. BARS AND RESTAURANTS L2
- 22. KIDS' CLUB LOWER GROUND
- 23. DECK GRILL
- 24. POOL BAR
- 25. FIRE PIT
- 26. INDIGENOUS SCULPTURAL ART FOCAL POINT



Water



Softscape



Hardscape



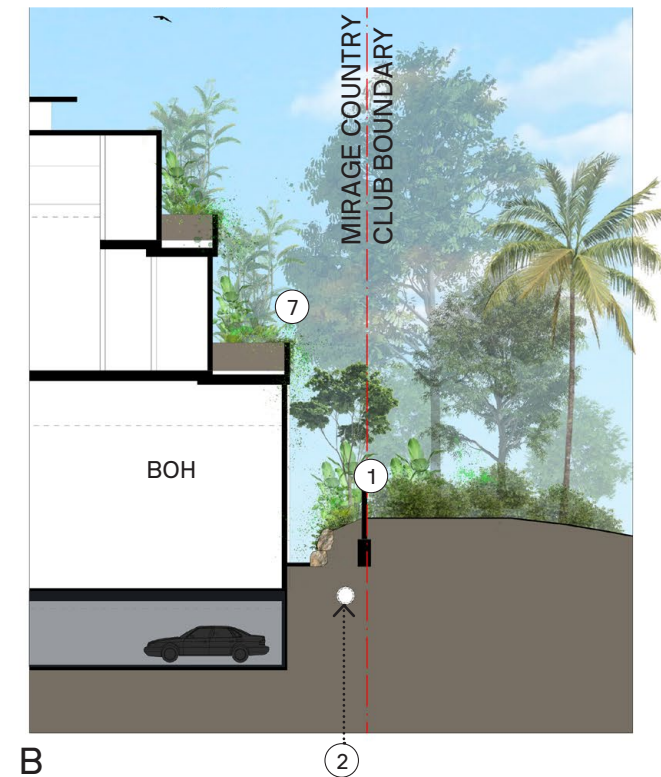
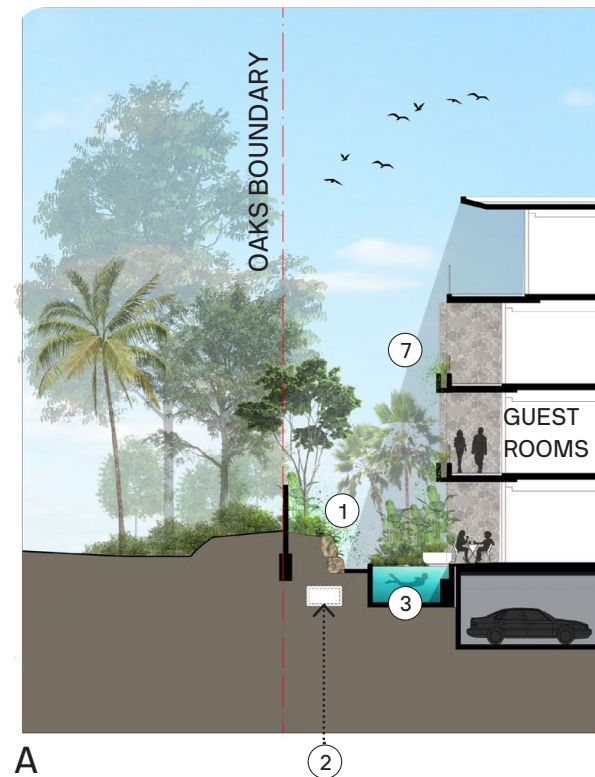
Areas

<i>Landscape Space</i>	
<i>@ Ground Level</i>	<i>11,000m²</i>
<i>@ Roof Areas</i>	<i>1,500m²</i>
<i>Total</i>	<i>12,500m²</i>
<i>Landscape Site Percentage</i>	<i>60%+</i>
<i>Including over 2,500m² of Pool Areas and over 5,000m² of Gardens.</i>	

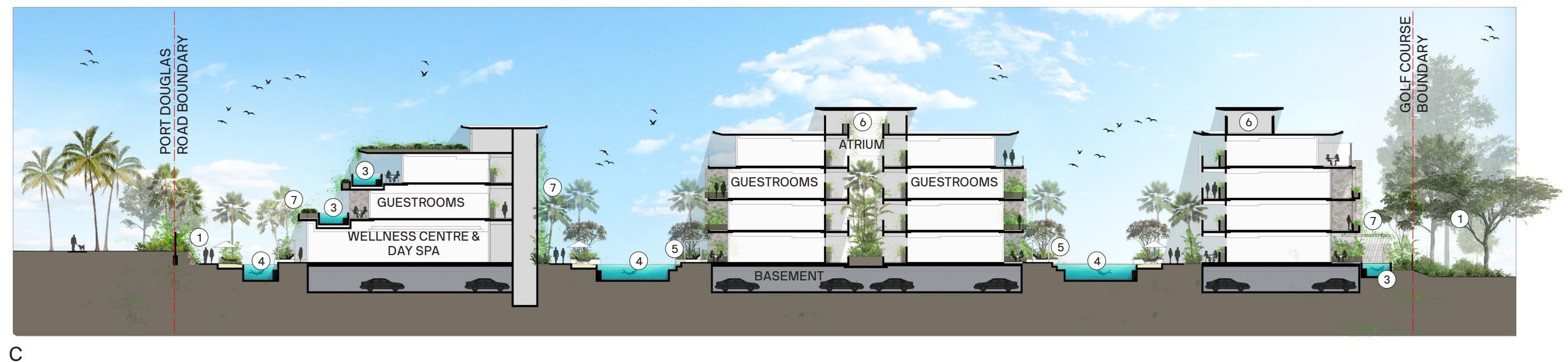
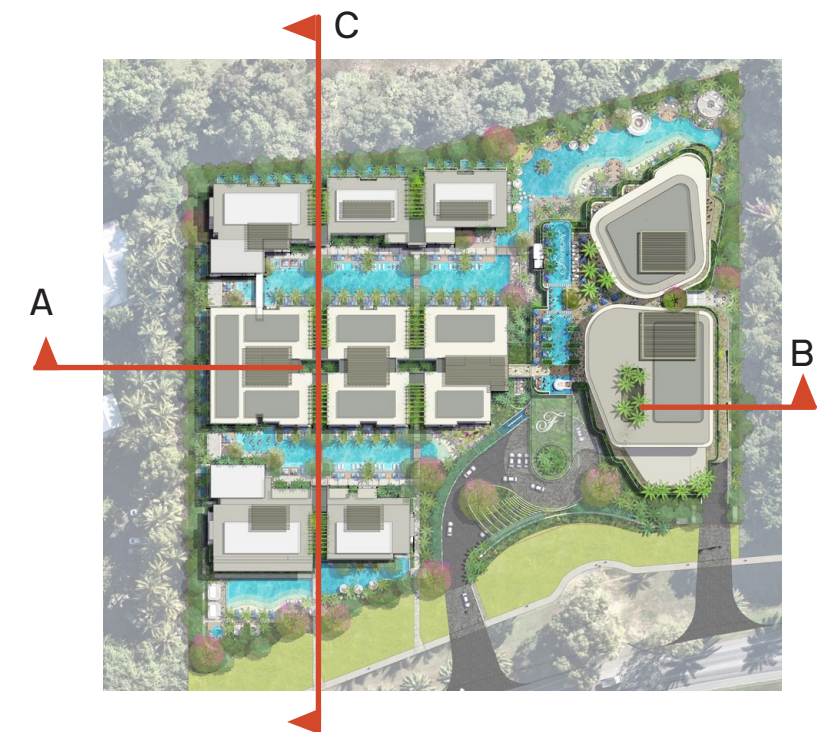
Basement Extents
Plan



Landscape / Sections



1. LANDSCAPE FENCING & BUFFER
2. STORMWATER INFRASTRUCTURE AND OVERLAND FLOW ZONE
3. PRIVATE PLUNGE POOL
4. LAGOON POOL
5. SWIM OUTS
6. LIFT OVERRUNS AND SERVICES PROJECTIONS ONLY
7. GREEN WALLS AND CASCADING PLANTS



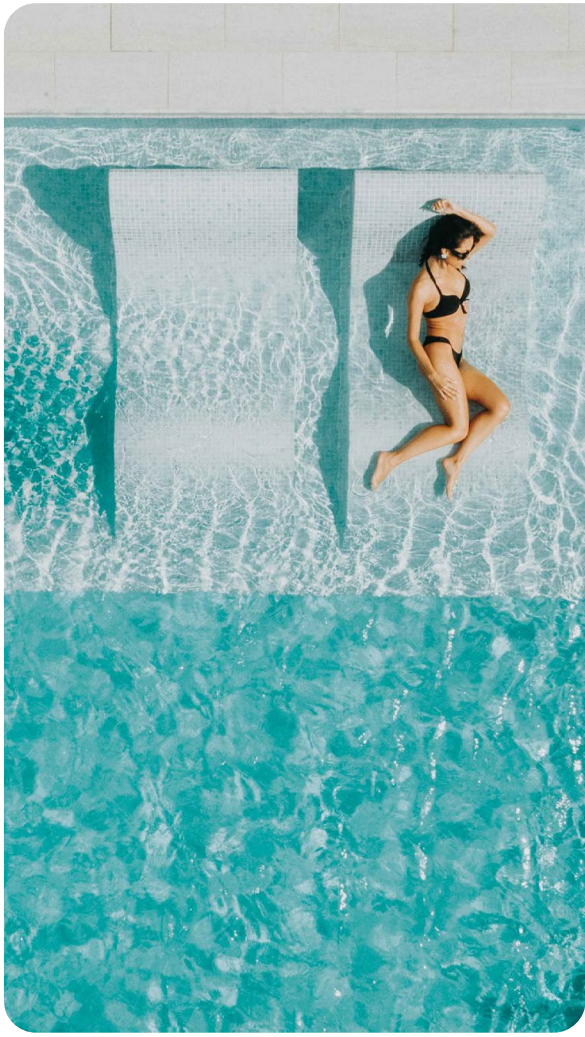
*Landscape /
Mood Imagery*

Lower Ground

- WHISKEY CAVE
- ALL DAY DINING AND OUTDOOR DINING - DECK GRILL AND POOL BAR
- POOL LOUNGES / CABANAS - SWIM OUT TERRACES - FAMILY POOL - LAGOON POOLS - WATER LOUNGES
- WELLNESS CENTRE & DAY SPA - KIDS' CLUB - HOTEL SUITES

Upper Ground

- HOTEL BAR - CAFE / COFFEE SHOP - LOBBY LOUNGE



*Landscape /
Mood Imagery*

Level 2 - Rooftop

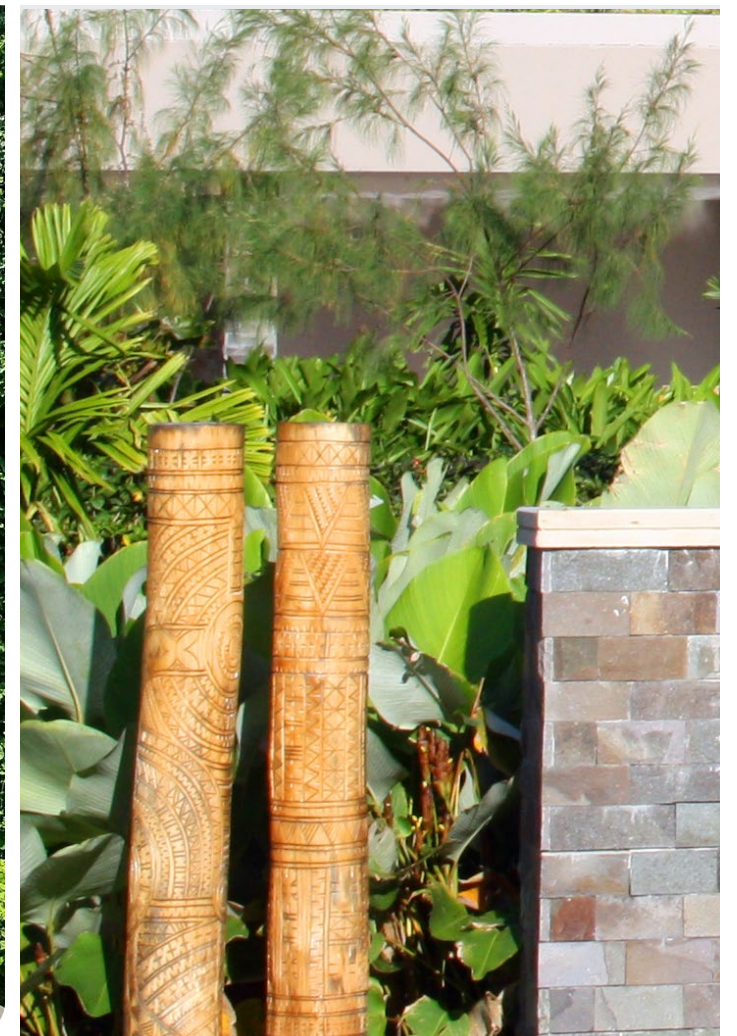
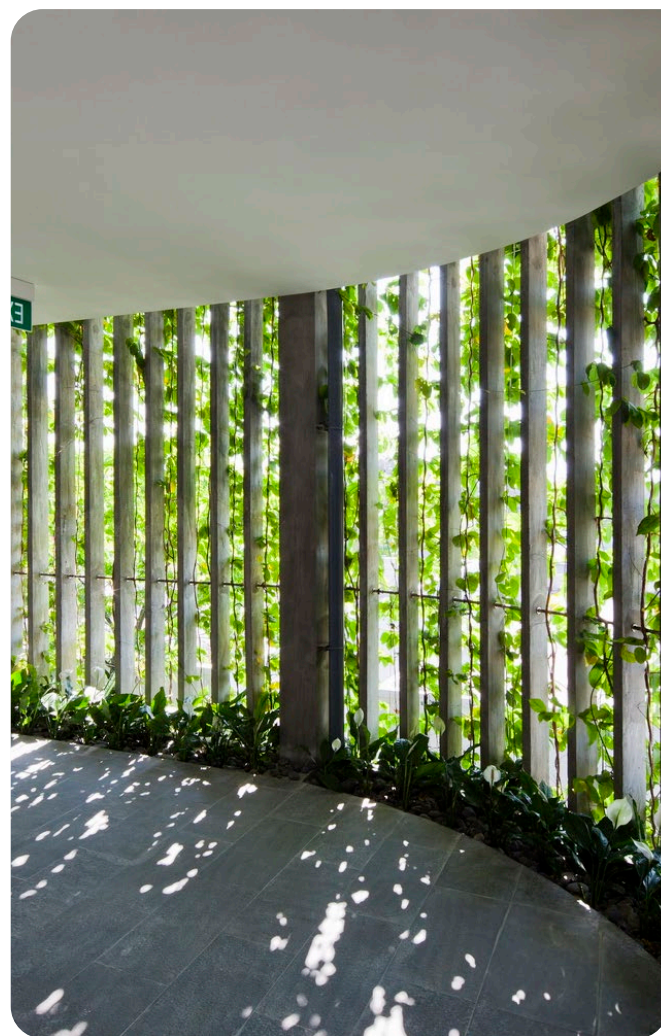
- 'WILDFIRE' SEASONAL DINING - 'SUNDOWNERS' ROOF POOL BAR
- ADULTS / FAMILY POOL - SWIM UP BAR / DJ STATION



Landscape / Mood Imagery

Green Spaces

- GREEN ROOF ARBOURS OVER BREEZEWAYS
- FEATURE ARBOUR
- WALKWAYS



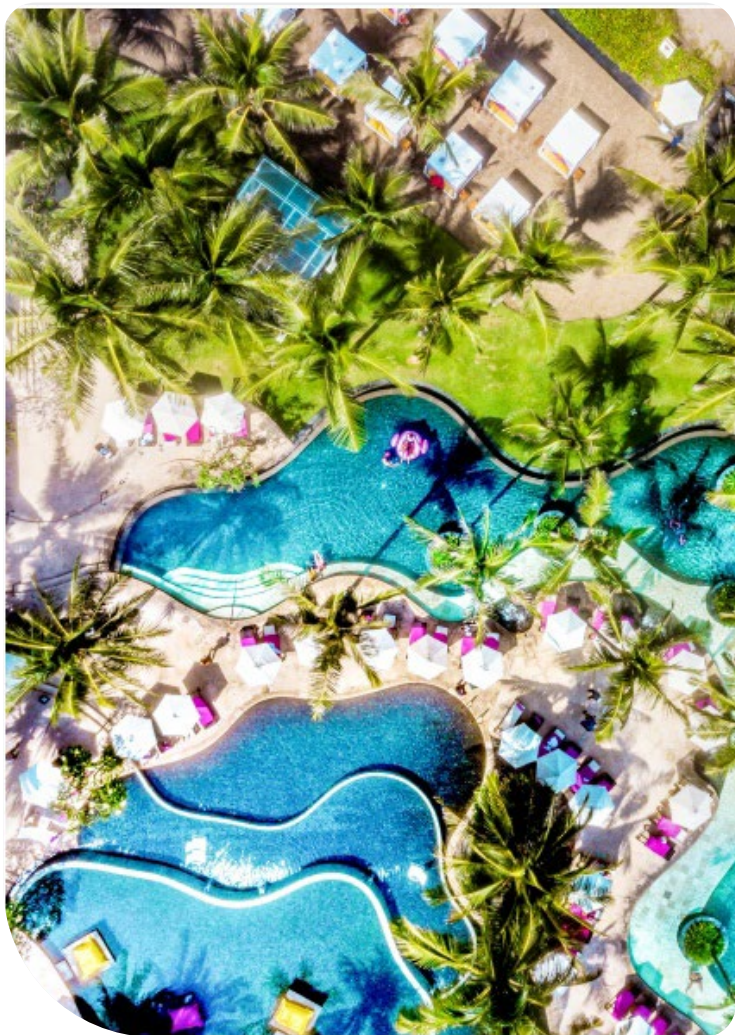
*Landscape /
Mood Imagery*

Resort Ambience



*Landscape /
Mood Imagery*

Resort Ambience



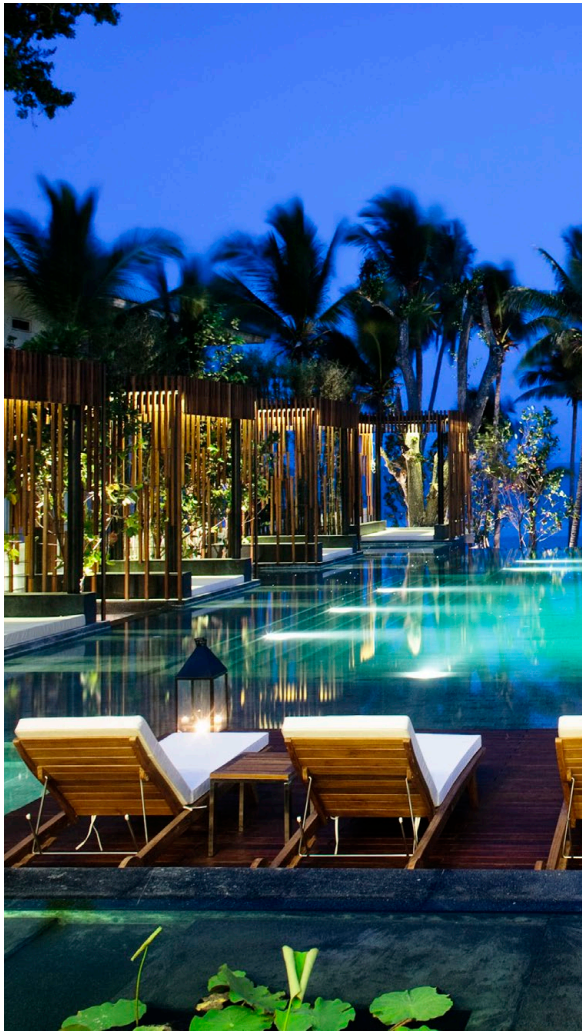
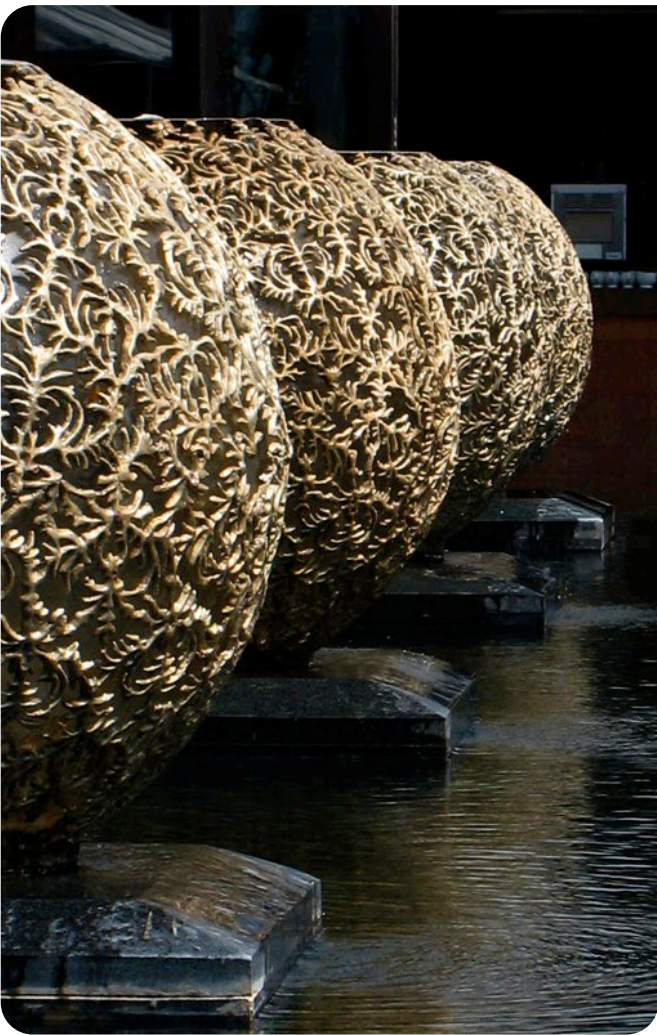
*Landscape /
Mood Imagery*

Resort Ambience



*Landscape /
Mood Imagery*

Resort Ambience



*Landscape /
Hardscape Palette*



GENERAL PAVING
NATURAL STONE



GENERAL PAVING
NATURAL STONE



GENERAL PAVING
NATURAL STONE



DRIVEWAY
COBBLESTONE



GENERAL PAVING
COBBLESTONE



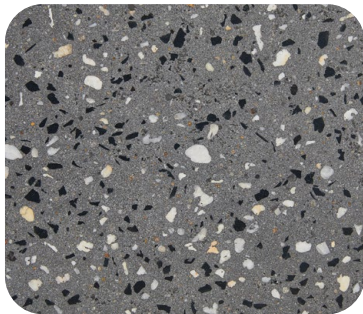
DECKING
COMPOSITE



THEMATIC/BRANDED
SIGNAGE SUITE
PC ALUMINIUM



GENERAL PAVING
EXPOSED CONCRETE



DRIVEWAY
EXPOSED CONCRETE



FEATURE WALL
NATURAL STONE



BENCHES
INSITU CONCRETE



ART AND SCULPTURE
TBC



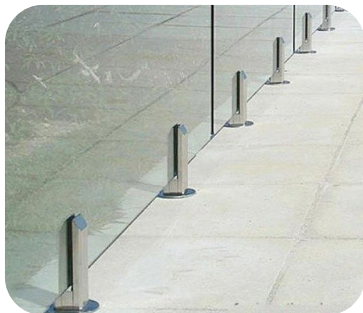
FENCING & SCREENING
ALUMINIUM BATTENS



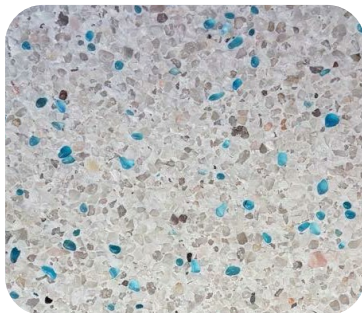
PLANTERS
GRC



PLAYGROUND
RUBBER SOFTFALL



POOL FENCING
GLASS



POOL FINISH
PEBBLECRETE



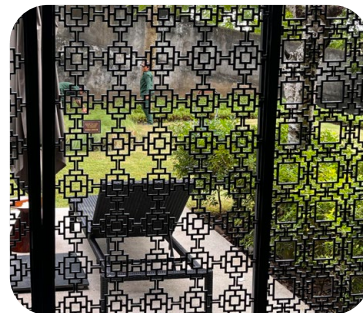
POTS
GRC



PEBBLES
NATURAL



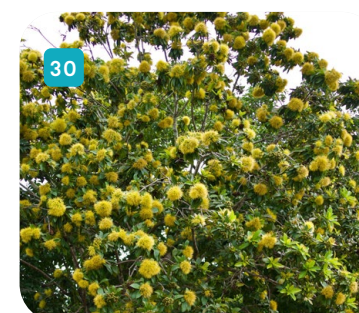
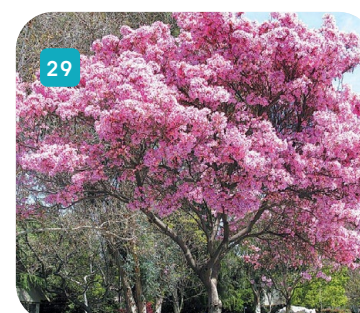
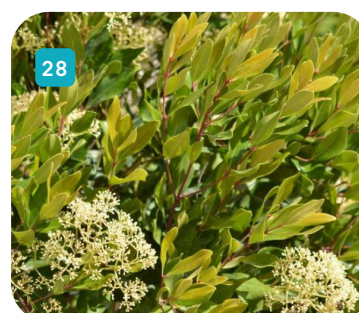
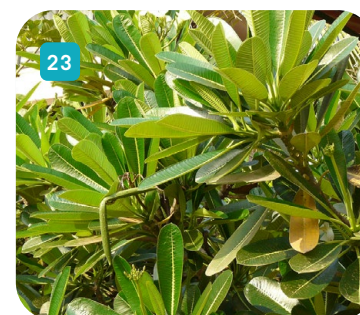
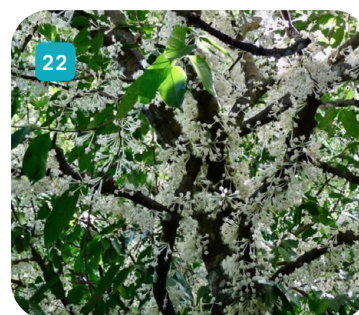
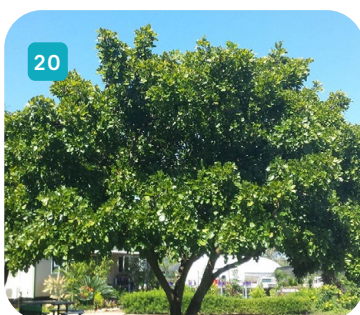
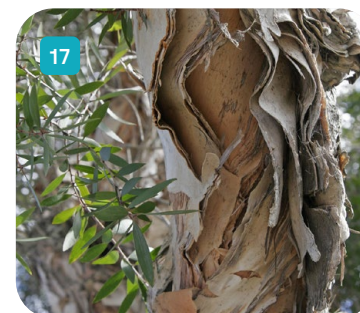
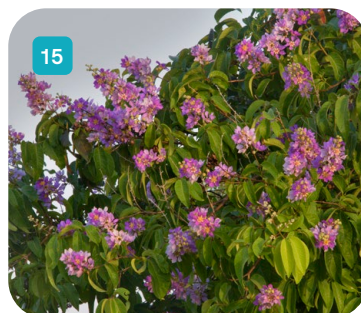
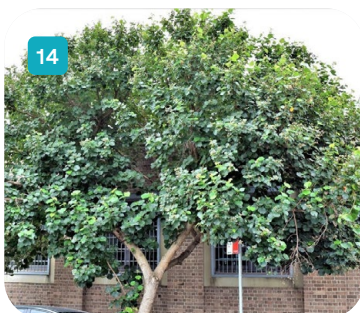
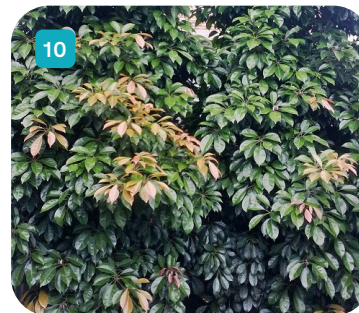
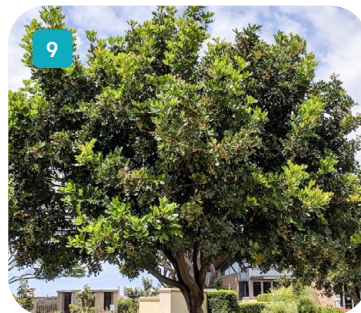
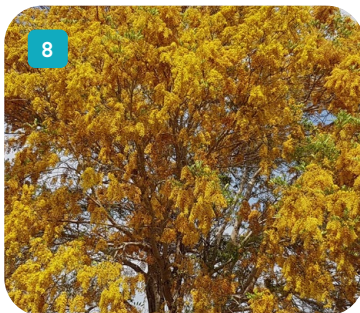
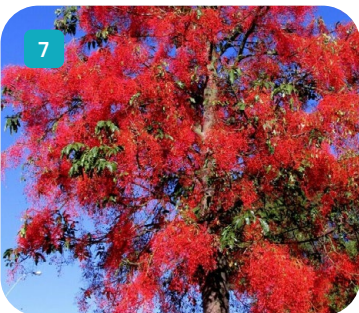
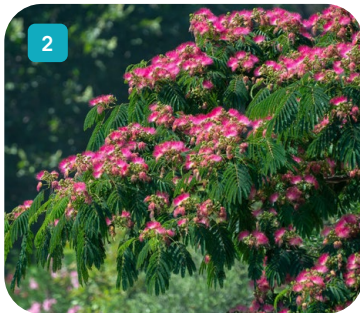
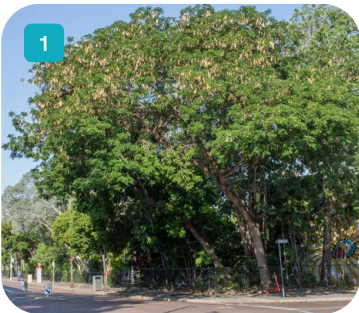
FEATURE LIGHTING
SUITES



FEATURE SCREEN
ELEMENTS
PC ALUMINIUM

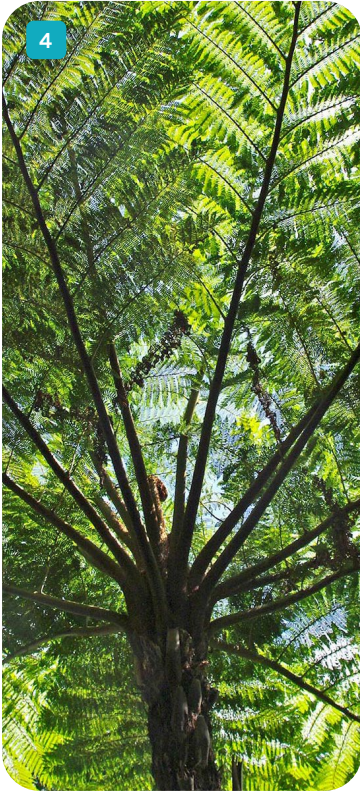
Landscape /
Softscape Palette - Trees

- 1. ALBIZIA lebbeck
- 2. ALBIZIA julibrissin
- 3. BARRINGTONIA acutangula
- 4. BARRINGTONIA calyptrata
- 5. BAUHINIA alba
- 6. BAUHINIA variagata
- 7. BRACHYCHITON acerifolius
- 8. CASSIA brewsteri
- 9. CUPANIOPSIS anacardioides
- 10. ELAEOCARPUS eumundii
- 11. EUPOMATIA laurina
- 12. HARPULLIA pendula
- 13. HIBISCUS tiliaceus rubra
- 14. HIBISCUS tiliaceus
- 15. LAGERSTROEMIA speciosa
- 16. MANILTOA lenticellata
- 17. MELALEUCA quinquenervia
- 18. MELALEUCA viridiflora
- 19. MELICOPE elleryana
- 20. MILLETTIA pinnata
- 21. PANDANUS utilis
- 22. PHALERIA clerodendron
- 23. PLUMERIA obtusa
- 24. PLUMERIA rubra
- 25. RANDIA fitzalanii
- 26. STENOCARPUS sinatus
- 27. SYZYGIVM tierneyanum
- 28. SYZYGIVM hemilampra
- 29. TABEBUIA palmeri
- 30. XANTHOSTEMON Chrysanthus



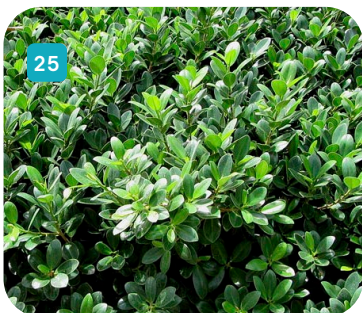
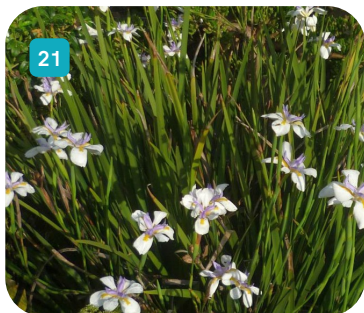
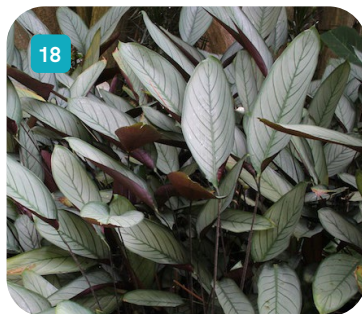
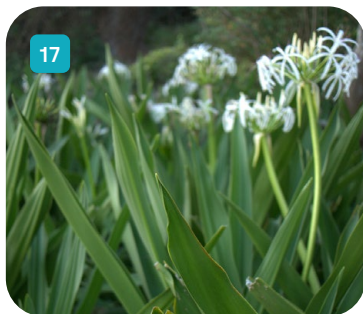
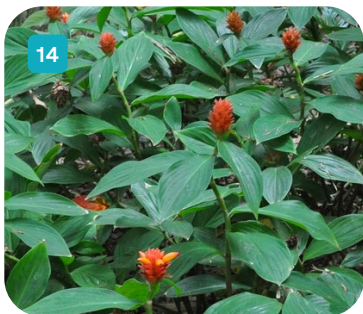
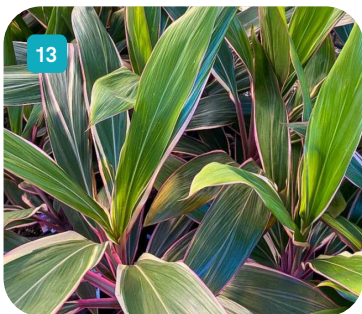
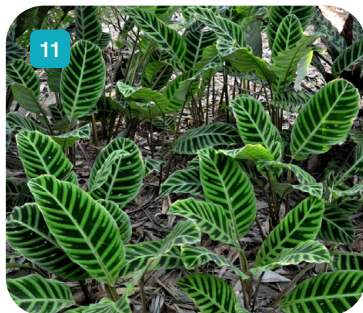
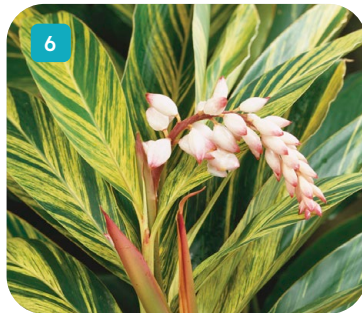
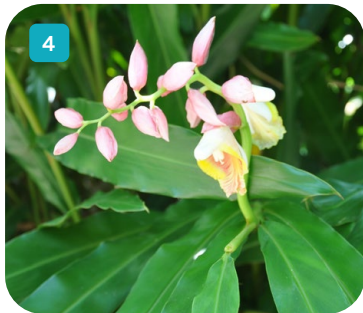
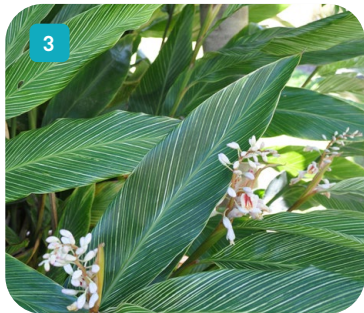
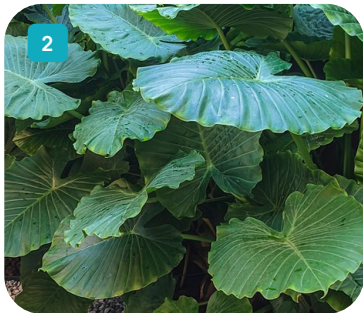
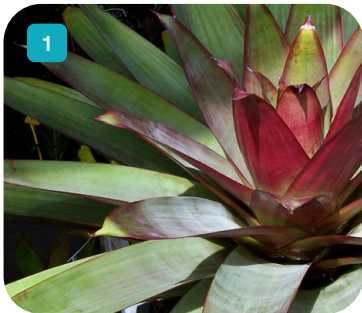
Landscape /
Softscape Palette - Palms

- 1. ARCHONTOPHOENIX alexandrae
- 2. BISMARKIA nobilis
- 3. CHAMAEDOREA seifrizii
- 4. COCOS nucifera
- 5. CYATHEA cooperi
- 6. HOWEA forsteriana
- 7. LICUALA grandis
- 8. LICUALA ramsayi
- 9. LIVISTONA muelleri
- 10. PTYCHOSPERMA macarthurii
- 11. RHAPIS excelsa
- 12. WODYETIA bifurcata



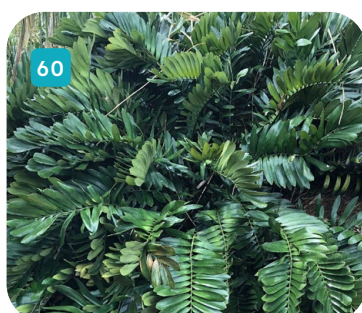
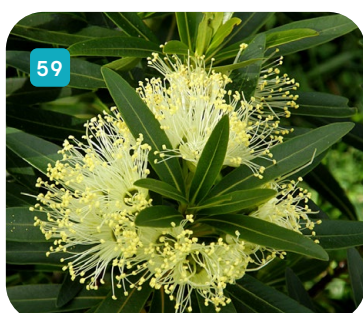
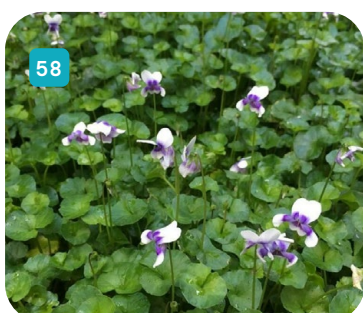
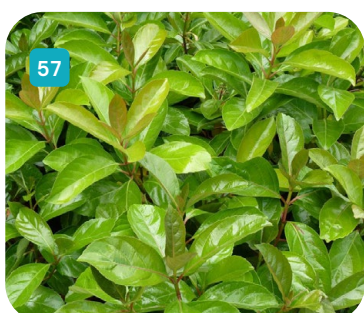
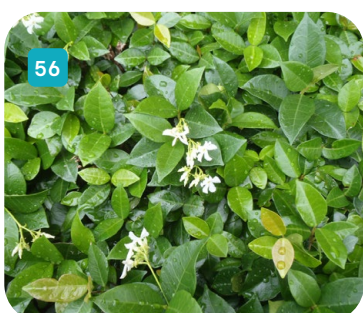
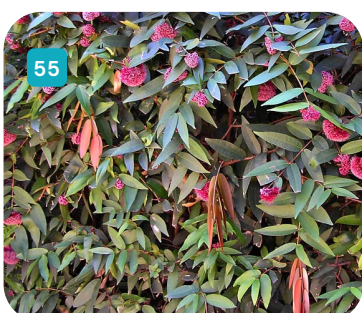
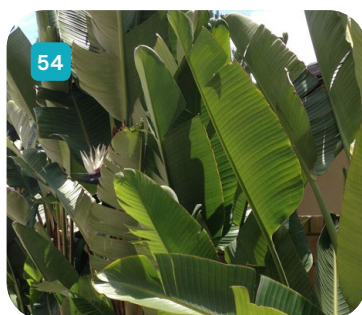
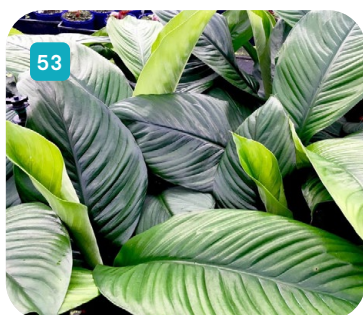
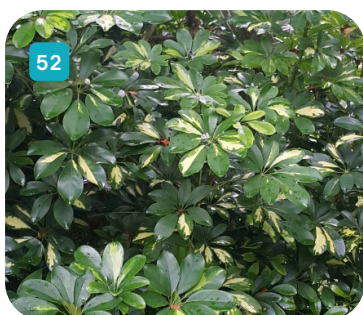
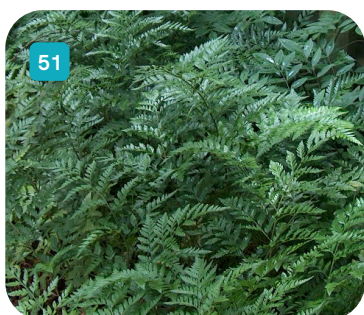
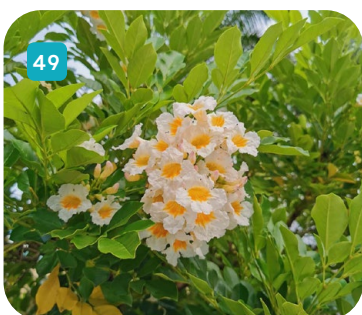
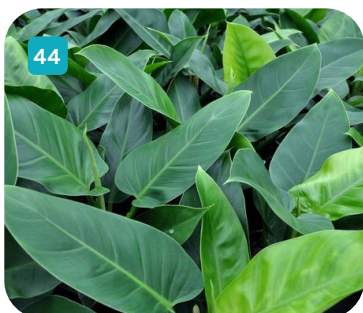
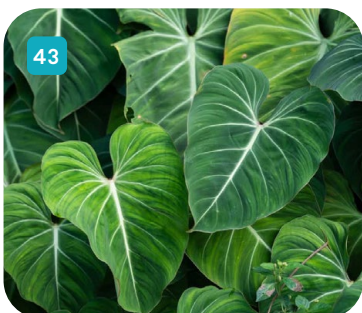
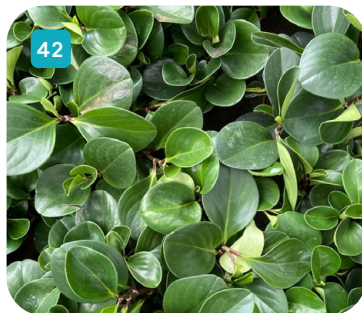
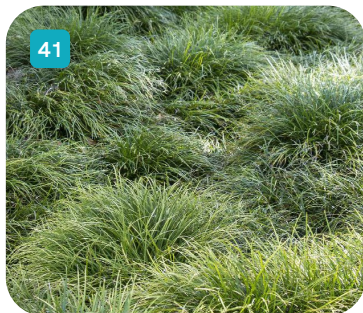
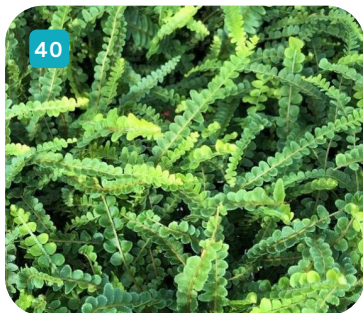
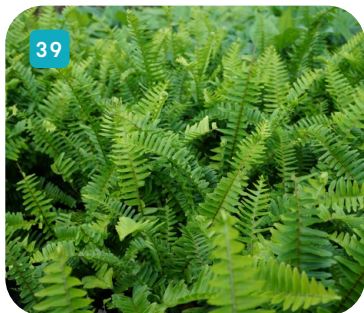
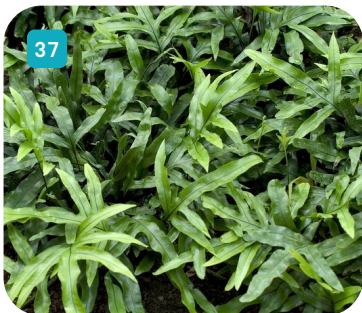
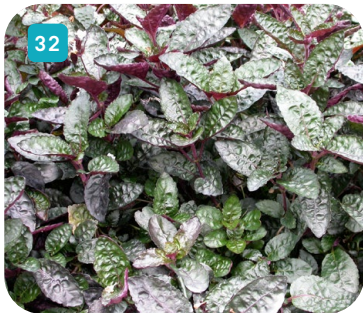
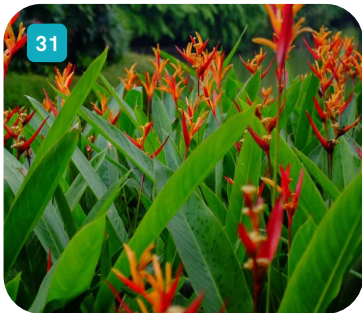
*Landscape /
Softscape Palette - Shrubs & Groundcovers*

- 1. ALCANTAREA imperialis 'Rubra'
- 2. ALOCASIA brisbanensis
- 3. ALPINIA formosana
- 4. ALPINIA henryi
- 5. ALPINIA nutans
- 6. ALPINIA zerumbet
- 7. ALPINIA caerulea Red
- 8. ASPIDISTRA elatior
- 9. ASPLENIUM australasicum
- 10. CALATHEA lutea
- 11. CALATHEA zebrina
- 12. CORDYLINE manners-suttoniae
- 13. CORDYLINE fruticosa sp.
- 14. COSTUS productus
- 15. COSTUS spiralus
- 16. CRINUM menehune
- 17. CRINUM pedunculatum
- 18. CTENANTHE setosa 'Grey Star'
- 19. CYCAS revoluta
- 20. DICHORISANDRA thyrsiflora
- 21. DIETES grandiflora
- 22. DIFFENBACHIA sp.
- 23. DIOON spinulosum
- 24. DRACAENA deremensis 'Janet Craig'
- 25. FICUS microcarpa 'Green Island'
- 26. GARDENIA 'Glennie River'
- 27. GARDENIA radicans
- 28. GRAPTOPHYLLUM excelsum
- 29. HELICONIA bihai
- 30. HELICONIA caribaea



*Landscape /
Softscape Palette - Shrubs & Groundcovers*

- 31. HELICONIA psittacorum
- 32. HEMIGRAPHIS alternata 'Exotica'
- 33. HYMENOCALLIS speciosa
- 34. IXORA sp
- 35. LIRIOPE 'Evergreen Giant'
- 36. LOMANDRA hystrix
- 37. MICROSORUM diversifolium
- 38. MICROSORUM punctatum
- 39. NEPHROLEPIS 'Kimberly Queen'
- 40. NEPHROLEPIS cordifolia 'Duffii'
- 41. OPHIOPOGON japonicus
- 42. PEPEROMIA obtusifolia
- 43. PHILODENDRON gloriosum
- 44. PHILODENDRON 'Imperial Green'
- 45. PHILODENDRON 'Rojo Congo'
- 46. PHILODENDRON selloum
- 47. PHILODENDRON xanadu
- 48. PLEIOSTACHYA pruinosa
- 49. RADERMACHERA 'Summerscent'
- 50. RHODODENDRON lochiaie
- 51. RUMOHRA adiantiformis
- 52. SCHEFFLERA arboricola 'Jacqueline'
- 53. SPATHIPHYLLUM sensation
- 54. STRELITZIA nicolai
- 55. SYZYGIUM wilsonii
- 56. TRACHELOSPERMUM asiaticum
- 57. VIBURNUM odoratissimum 'Emerald Lustre'
- 58. VIOLA hederacea
- 59. XANTHOSTEMON verticulatus
- 60. ZAMIA furfuracea



Landscape / Softscape Palette - Climbers & Cascading Plants

1. ABELMOSCHUS moschatus
2. ACALYPHA reptans
3. ADIANTUM atroviride
4. ADIANTUM hispidulum
5. APTENIA cordifolia
6. ARISTOLOCHIA tagala
7. BOUGAINVILLEA sp.
8. CARPOBROTUS glaucescens
9. CHONEMORPHA fragrans
10. CISSUS antarctica
11. DICHONDRA Silver Falls
12. EPIPREMNUM pinnatum
13. EVOLVULUS pilosus 'White'
14. HARDENBERGIA violacea
15. HIBBERTIA scandens
16. HOYA australis
17. IPOMOEA horsfalliae
18. IPOMOEA pes-caprae
19. JASMINUM elongatum
20. MANDEVILLA sp.
21. PANDOREA jasminoides
22. PIPER novae-hollandiae
23. PLATYCERIUM sp.
24. RUSSELIA equisetiformis
25. SCAEVOLA aemula
26. STEPHANOTIS floribunda
27. TECOMANTHE burungu
28. TECOMANTHE dendrophila
29. TRACHELOSPERMUM asiaticum
30. VIOLA hederacea



Development Summary

Development / Summary

FAIRMONT RESORT									
71-85 PORT DOUGLAS RD, PORT DOUGLAS, QLD, 4877									

Development /
Summary

	Event studio									92m²	
	Storage									8m²	
	Event MGMT									28m²	
	Gold reception									64m²	
	Breeze way									66m²	
	Units									3,477m²	
	Circulation									961m²	
	Service + BOH										167m²
	Lifts + Cores										265m²
									Total	7,254m²	432m²
Level 1	Specialty-restaurant									466m²	
	Kitchen									192m²	
	Female & Male									152m²	
	Meeting 3									56m²	
	Meeting 4									56m²	
	Meeting 5									59m²	
	Lift lobby									27m²	
	Grand commons gallery seating + Commons bar + Circulation									679m²	
	Banquet finishing kitchen									184m²	
	BOH access + BOH lift lobby + Service corridor									118m²	
	Store									33m²	
	Furniture Store									78m²	
	Ballroom									387m²	
	Sound Locks + Av Access									45m²	
	OPW store									13m²	
	Pre-function									185m²	
	Meeting 1									48m²	
	Meeting 2									46m²	
	Bridal Suite / VIP									53m²	
	Units									3,426m²	
	Circulation									957m²	
	Service + BOH										265m²
	Lifts + Cores										80m²
									Total	7,260m²	345m²
Level 2	Wildfire - seasonal dining									447m²	
	BOH kitchen									141m²	
	Pool 4 - pump room									49m²	
	Female & Male Amenities									100m²	
	BOH									15m²	
	Sundowners - Roof pool bar									425m²	
	Lift lobby									24m²	
	Finishing kitchen									126m²	
	BOH lift lobby									26m²	
	AV									31m²	
	Corridor									26m²	
	Female & Male Amenities									43m²	
	Gold Lift lobby									24m²	
	Gold Lounge									259m²	
	Gold Lounge BOH									40m²	
	Units									2,147m²	
	Circulation									674m²	
	Service + BOH										207m²
	Lifts + Cores										106m²
	Service mechanical plant										152m²
									Total	4,597m²	465m²
Roof	Roof Cores & Plant										1,094m²
									Total		1,094m²
									GRAND TOTAL	28,969m²	4,575m²
Carparking: Required	(Note: Refer Traffic Report for car parking rates)										
Accommodation											
	202 rooms	0.75	Per unit				151.5				
	Visitors	3	Total				3				
	Service/Staff	2	For the first 10 Units				2				
	Service/Staff	0.50	Per 10 units above the first 10				9.6				
Other											
	Food and Drink	1	Space per 25sqm	3,900m²	70.2	55% Deduction applied. Refer Traffic Report.					
	Prefunction	1	Space per 15sqm	500m²	0	100% Deduction applied. Refer Traffic Report.					
	Function Rooms	1	Space per 15sqm	1,150m²	34.5	55% Deduction applied. Refer Traffic Report.					
	Gym	1	Space per 15sqm	86m²	0	It is assumed that the Gym is for Hotel Guests only. 100% Deduction.					
	Spa	1	Space per 20sqm	450m²	11.3	50% Deduction applied. Refer Traffic Report.					
				TOTAL	283						
Provided											
Basement Level 1							TOTAL	301.0			
Bicycles: Required	(Note: Refer Traffic Report for bicycle parking rates)										
	202 rooms	1	Per 10 Rooms		20.2						
	Food and Drink	1	Per 100sqm	3,900m²	17.6	55% Deduction applied. Refer Traffic Report.					
	Prefunction	1	Per 100sqm	500m²	0	100% Deduction applied. Refer Traffic Report.					
	Function Rooms	1	Per 100sqm	1,150m²	5.2	55% Deduction applied. Refer Traffic Report.					
	Gym	1	Per 4 employees	86m²	0	It is assumed that the Gym is for Hotel Guests only. 100% Deduction.					
	Spa	1	Per 100sqm	450m²	2.3	50% Deduction applied. Refer Traffic Report.					
				TOTAL	46						



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DBI References

Note: Reference images included in this presentation are intended for in-house discussions only.
DBI does not claim authorship or intend to adopt architectural character. These images have been sourced from various publications.

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ABU DHABI . BRISBANE . CANBERRA . GOLD COAST . SYDNEY

Attachment 5

Traffic Engineering Technical Note,
prepared by PTT Traffic & Transport
Engineering

FAIRMONT PORT DOUGLAS (27/06/2024) TRAFFIC ENGINEERING ADVICE

INTRODUCTION

This technical note has been prepared by PTT, as requested by Chiodo Corporation, to provide high level traffic engineering advice for the proposed Fairmont Resort development in Port Douglas.

PROPOSED DEVELOPMENT

We have reviewed the proposed plan of development, from which we have surmised that the proposal comprises:

- 202 short term accommodation units
- 3,900m² GFA of food and drink uses
- 500m² GFA pre-function area
- , 50m² GFA function room
- 86m² GFA gymnasium
- 450m² GFA spa

The proposal is supported by:

- 30 car parking spaces located in a single basement car park level
- a separate service dock

Two driveways are proposed from Port Douglas Road, with one serving the porte-cochere / basement parking area and the other serving the BOH / loading dock.

Separate pedestrian access is proposed to the existing pedestrian path running north-south along the site's eastern frontage.

PORT DOUGLAS ROAD

The subject gains access from Port Douglas Road, a two-lane undivided State controlled road. Accordingly, the key authority with respect to the location and design of the accesses to Port Douglas Road will be the Department of Transport and Main Roads (DTMR).

Port Douglas Road has a signed speed limit of 60km/h and on-road cycle lanes.

Traffic volumes on this road are estimated to be about 1,500 vehicles per day, growing at about 0.3% per annum.

There have been no reported crashes on Port Douglas Road in the vicinity of the subject site over the last 3 years.

VEHICULAR ACCESS

The subject site is served by an existing cross-over on Port Douglas Road, located about 50m south of the Lakeland Avenue / Avenue of Palms roundabout. The existing access does not incorporate any auxiliary left/right turn treatments.

The subject proposal would see the existing access closed and replaced with two new accesses to Port Douglas Road, one to the south of the existing access (serving the porte-cochere and basement car parking) and the other to the north of the existing access, serving the BOH / loading dock.

ACCESS DESIGN

The available sight distance at the proposed sight access appears to be adequate for the local speed limit (ie 60km/h).

It is likely that the main vehicular access will need to incorporate separate left and right turn lanes, much like the existing access to the Oaks Resort, located further south on Port Douglas Drive (refer Figure). In broad terms, this will require a 6-7m widening of the pavement in Port Douglas Road across the site access, tapering down to the existing carriageway over about 100- 20m either side of the access.

Figure : OAKS RESORT ACCESS



A similar, albeit smaller scale upgrade, may be necessary for the proposed second access serving the BOH / loading dock.

PEDESTRIAN ACCESS

The plan of development incorporates two pedestrian accesses from the accommodation component of the development to the existing pedestrian path running north-south along the site's eastern frontage.

It is recommended that an additional pedestrian connection be provide to/from the port-cochere / FOH, as this is expected to be a major origin / destination for resort guests.

CARPARKING

We have reviewed the local planning scheme and believe that it requires car parking to be provided at the following rates:

- short term accommodation: 0.75 spaces / unit + 3 spaces for visitors + 2 staff parking spaces for the first 10 units + 0.5 staff parking spaces per 10 units above that
- food and drink: 1 space / 25m² GFA
- pre-function: 1 space / 5m² GFA
- function: 1 space / 5m² GFA
- gym: 1 space / 5m² GFA
- spa: 1 space / 20m² GFA

Assuming a nominal 100m² GFA for each of the gym and spa uses, the strict application of the above rates suggests a requirement for 46 car parking spaces. This calculation is shown in Table 1.

Table 1 : ACCEPTABLE OUTCOME CAR PARKING REQUIREMENT

Use	Scale	Rate	Spaces
STA	202 units	0.75 / unit	151.5
		3 visitors	3.0
		2 staff for first 10 units	2.0
		0.5 staff / 10 units above that	9.6
F&D	3900 m ²	1/25m ²	156.0
Pre-Functi	500 m ²	1/15m ²	33.3
Function	1150 m ²	1/15m ²	76.7
Gym	86 m ²	1/15m ²	5.7
Spa	450 m ²	1/20m ²	22.5
Total			461 spaces

However, a significant proportion of patrons of the non-residential uses are likely to be guests of the resort, which means that the net car parking demand is likely to be significantly lower than that shown in Table 1. In particular:

- the gym is unlikely to generate any additional parking demand, given that it is likely to be reserved for guest use only - a 100% deduction for this use is considered reasonable
- similarly, the spa is unlikely to generate any additional guest parking demand, although it is likely to require some parking for staff - a 50% deduction for this use is considered reasonable

- by definition, the pre-function area is unlikely to be used at the same time as the function area - a 00% deduction for this use is considered reasonable
- data published by DTMR suggests that up to 70% of patrons of food and drink outlets in resorts are guests of the resort – after accounting for staff a 55% deduction for this use is considered reasonable

Accordingly, a performance based approach estimate of the peak car parking demand generated by the proposed development is 283 spaces. This calculation is shown in Table 2.

Table 2: PERFORMANCE OUTCOME CAR PARKING REQUIREMENT

Use	Scale	Rate	Deduction	Spaces
STA	202 units	0.75 / unit	0%	151.5
		3 visitors	0%	3.0
		2 staff for first 10 units	0%	2.0
		0.5 staff / 10 units above that	0%	9.6
F&D	3900 m ²	1/25m ²	55%	70.2
Pre-Function	500 m ²	1/15m ²	100%	0.0
Function	1150 m ²	1/15m ²	55%	34.5
Gym	86 m ²	1/15m ²	100%	0.0
Spa	450 m ²	1/20m ²	50%	11.3
Total				283 spaces

This suggests that the 30 parking spaces shown on the proposed plan of development should be sufficient to cater for the peak car parking demand generated by the proposed development.

CARPARKING DIMENSIONS

According to the local planning scheme, the on-site car parking area needs to be designed in accordance with AS2890. . This document nominates the following key design criteria:

- staff parking spaces: 2.4m wide x 5.4m long
- guest parking spaces: 2.5m wide x 5.4m long
- minimum vertical height clearance: 2.3m
- parking aisles: 6.2m wide (wall-to-wall)
- circulation roads and ramps (ie no parking): 6.5m wide (wall-to-wall)
- maximum ramp gradient: :5
- grade transitions: 2m long @ :8

BICYCLE PARKING

We have reviewed the local planning scheme and believe that it requires bicycle parking to be provided at the following rates:

- short term accommodation: space / 0 units
- food and drink: space / 00m² GFA
- pre-function: space / 00m² GFA

- function: space / 100m² GFA
- gym: space / 4 employees
- spa: space / 100m² GFA

As noted above, a significant proportion of patrons of the non-residential uses are likely to be guests of the resort. This means that the net bicycle parking demand is likely to be significantly less than the strict application of the above rates would suggest. Adopting the same deductions detailed above, we estimate the peak bicycle parking demand generated by the proposed development to be 46 spaces. This calculation is shown in Table 3.

Table 3: PERFORMANCE OUTCOME BICYCLE PARKING REQUIREMENT

Use	Scale	Rate	Deduction	Spaces
STA	202 units	1 / 10 units	0%	20.2
F&D	3900 m ²	1/100m ²	55%	17.6
Pre-Function	500 m ²	1/100m ²	100%	0.0
Function	1150 m ²	1/100m ²	55%	5.2
Gym	86 m ²	1/4 employees	100%	0.0
Spa	450 m ²	1/100m ²	50%	2.3
Total				46 spaces

The proposed plan of development does not include any dedicated bicycle parking.

It is recommended that the proposed plan of development be amended to incorporate 46 bicycle parking spaces.

SERVICE VEHICLE PARKING

We have reviewed the local planning scheme and believe that it requires the following loading bays to be provided:

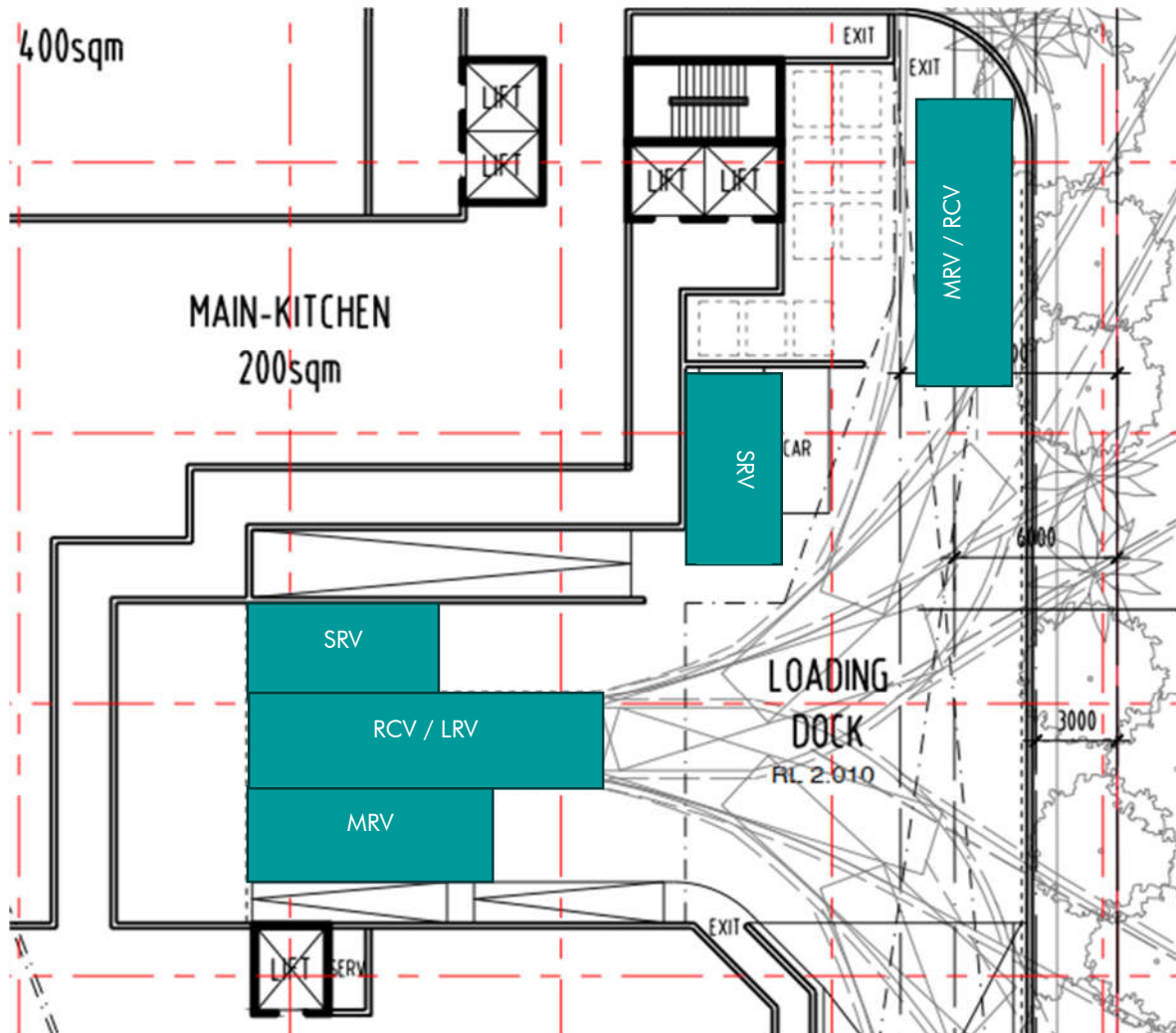
- short term accommodation: SRV
- food and drink: 2 x VAN, 2 x SRV, 2 x MRV, 1 x LRV
- pre-function: RCV
- function: RCV
- gym: RCV
- spa: VAN

The proposed plan of development incorporates a BOH / loading area. There would be some economies of scale with this arrangement because a given loading bay could reasonably be shared amongst various uses. On this basis, we recommend the following minimum provision for on-site loading:

- 2 x VAN bays (5.4m long x 3.0m wide x 2.3m vertical clearance) – in basement car park
- 2 x SRV bays (7.0m long x 3.5m wide x 3.5m vertical clearance)
- 1 x MRV bay (9.0m long x 3.5m wide x 4.5m vertical clearance)
- 1 x MRV / RCV bay (10.5m long x 3.5m wide x 6.5m vertical clearance)
- 1 x LRV bay (3.0m long x 3.5m wide x 4.5m vertical clearance)

We have reviewed the proposed plan of development and recommend that all of the above bays, except for the two VAN bays, be located as shown in Figure 2. We recommend that two VAN bays be provided in the basement car park, close to one of the lift wells or BOH area.

Figure 2: RECOMMENDED LOADING ZONES



CONCLUSIONS

If you have any questions regarding the issues discussed above, please do not hesitate to contact us.

Yours sincerely,

Adam Pekol
Director (RPEQ 5286)
27/06/2024

Attachment 6

Strategic Framework Response

3.2 Strategic Intent

3.2.1.1 Douglas Shires Regional Context

Figure 3.1.a and Figure 3.1.b show Douglas Shire in its regional context. The Shire is the northern-most local government area in Queensland's Far North Region. Its southern boundary is just north of Ellis Beach, approximately 25 kilometres north of the Cairns City centre: the major urban centre of the Far North Region. It extends about 95 kilometres along the Coral Sea coastline, adjacent to the Great Barrier Reef Marine Park, to the mouth of the Bloomfield River. In total, the Shire covers approximately 2427 square kilometres.

The Shire abuts Cook Shire and the small Wujal Wujal Aboriginal Shire located on the Bloomfield River to its north, Mareeba Shire to its west and Cairns Regional Council to its south.

Mossman, the administrative centre of the Shire is located 65 kilometres from Cairns City Centre and about 150 kilometres from Cooktown to the north by road. Port Douglas, one of Australia's premium tourism destinations is located on the coast approximately 10 kilometres to the east-south-east of Mossman.

A major feature of the Shire is its mountainous terrain which defines the northern, western and south boundaries and extends right to the Coral Sea coastline in numerous locations. The broad Daintree River effectively divides the Shire into northern and southern parts. To the south are the major coastal plain, farming and settlement areas and to the north is a predominately mountainous, forested and relatively isolated environment

3.2.1.2 Physical Setting

The Shire contains rich and diverse environmental, cultural and scenic landscapes including extensive areas of mountainous forested terrain, in the north, west and south defining limited areas of alluvial coastal plain, river valleys, estuarine flats and coastal headlands and sand ridges. The mountainous areas retain their natural vegetation whereas most of the coastal plain has been cleared for cultivation, grazing and settlement. Much of the Shire's foreshore, riverine and estuarine vegetation remains intact, although some areas have been cleared, as a result of agricultural or urban development. Numerous peaks in the Shire exceed 1,000 metres above sea level.

The Shire's main coastal plain is between Yule Point in the south and Rocky Point in the north, with the mountain ranges meeting the coastline at each of these two points. This area contains most of the Shire's cultivated land and the main urban settlements of Mossman and Port Douglas. This coastal plain is drained by the Mowbray, Mossman and, South Mossman Rivers and Saltwater Creek.

To the south of Yule Point, the Shire is relatively narrow in width and principally comprises the steep slopes of the McAlister Range. Small flatter areas at Wangetti, Oak Beach and Pebbly Beach contain small coastal and rural residential communities and some limited cropping areas. To the north of Rocky Point, is the Dagmar Range, the Wonga foreshore dune formation and the Daintree / Stewart Creek River valley. The Daintree River is the Shire's major watercourse, draining an extensive area of mountainous terrain. It is periodically subject to significant and extensive flooding and represents a significant geographical barrier between north and the south of the Shire, particularly at its lower reaches.

To the north of the Daintree River valley, the Shire consists almost entirely of mountainous terrain with small pockets of flat areas adjacent to the coast. The most significant of these flatter areas is immediately to the north of the Alexandra Range. Major creeks draining this area include Hutchinson, Buchanan, Bailey, Mackenzie and Cooper Creeks. Another area of flat land occurs between Noah Head and Cape Tribulation, with smaller areas around Noah Creek and just to the north of Cape Tribulation. Significant creeks include Myall and Mason Creeks.

To the north of Cape Tribulation, other areas of flat land are located to the south of Cowie Point and Donovan Point. A number of fast flowing creeks drain directly to the Coral Sea including Emmagen, Tachalbadga, Donovan, Melissa Creeks, while Meelele Creek and the Woobadda Creek drain northward into the Bloomfield River.

The far northern part of the shire in Buru-China Camp and the surrounding areas, the landscape is vastly different to the coastal regions north of the Daintree. In the Buru region, the landscape is mostly undulating to hilly, with steeper areas along the McDowall Range in the south and around Mount Owen to the east. Central and western areas are within the Bloomfield River catchments (Granite, Watermelon, Keating, Baird and Woobadda Creek sub-catchments). Eastern and southern areas are within the Daintree River catchment (Boolbun, Adastra, Kobi, Devil Devil and Hilda Creeks). The region is dominated by open eucalypt woodland and areas of grasslands with regrowth occurring in previously disturbed former mining, timber harvesting and grazing country.

The Shire also includes a number of offshore islands including Snapper Island, Low Isles and Struck Island.

The topography of Douglas Shire acts as substantial point of difference to other parts of the Far North Queensland region creating a quite distinct community of interest within the Shire. Residents identify closely with the Shire's physical setting as defining the boundaries to their community. This contributes significantly to the Shire's heightened sense of identity and interest in local issues.

The Shire's physical setting, located in a wet tropical climate, framed by lush World Heritage listed forests and fringed by the Coral Sea and associated World Heritage listed Barrier Reef Marine Park to the east, makes the Shire a desirable location for its residents and for tourists alike.

3.2.1.3 Environment

The Shire's natural beauty attracts residents and tourists from around the world. Two World Heritage Areas (the Wet Tropics World Heritage Area and the Great Barrier Reef World Heritage Area), cover 80% of the Shire, and the interface between the two is unique to the wet tropics. This characteristic is invaluable from both an environmental conservation and tourism perspective. The general predominance of the natural environment throughout the shire and the unique value of forests growing down to the shoreline should be recognised.

The Wet Tropics also presents an unparalleled record of the ecological and evolutionary processes that shaped the flora and fauna of Australia, containing the relics of the great Gondwanan forest that covered Australia and part of Antarctica, 50 to 100 million years ago. This area is mostly tropical rainforest and is extremely important for its rich and unique biodiversity

Douglas Shire is home to a diverse range of native and endemic plants and animals, including habitat for rare and endangered species, some of which are found nowhere else on the planet. The Shire's high biodiversity values are reflected in its natural environments, ranging from rainforest and wet sclerophyll woodlands to mangrove forests and wetlands, to vegetated sand dunes/swale systems, reefs, foreshore areas, intertidal seagrass beds and estuarine creek and river systems. Some of these areas are located outside the World Heritage Areas and are exposed to development pressure.

Douglas Shire adjoins the State Great Barrier Reef Coast Marine Park. The Commonwealth Great Barrier Reef Marine Park is adjacent to the low water mark. The Great Barrier Reef is internationally recognised for its outstanding biodiversity. The World Heritage status of the reef acknowledges its diversity of species and habitats. The reef's extraordinary biodiversity and the interconnectedness of species and habitats make the Great Barrier Reef and surrounding areas one of the most complex natural systems on Earth. Maintaining a healthy and diverse Great Barrier Reef ecosystem is important so it is better able to withstand, recover and adapt to any negative impacts and stresses. A productive and healthy reef environment also contributes significantly to successful local fishing and prawn production industries.

The zoning plans which are in force over the marine parks control the use of the tidal lands and tidal waters within the parks. However, they do not place any direct control over the use of adjacent land, except where such use involves works within the park area, or directly discharges into the park area. Areas of important remnant/riparian habitat and areas of considerable biodiversity value which occur along the coastal plain are also subject to development pressure

3.2.1.4 Historical Context

Douglas Shire has a rich and diverse cultural heritage. The land was originally occupied by aboriginal people from distinct groups with different dialects in different parts of the Shire. The Buru-China Camp region has a long history of settlement, both by Indigenous and non- Indigenous people. Large aboriginal camps existed throughout the Buru area long before European settlement and many families remained after the land was fenced and divided into leases for cattle grazing. Although many Indigenous families were forced to move away, there has always been an aboriginal presence in the area with dwellings constructed using available materials, some of which were left following mining activities.

Timber harvesting, tin and gold mining, droving and cattle grazing were predominant in the Buru-China Camp region. In the early 1900s, the township of China Camp included a store, school, butcher and market gardens. The name China Camp was conceived when large numbers of Chinese camped in the area en-route to the Palmer River goldfields having landed at in Bloomfield.

Elsewhere, the post-contact cultural heritage places in Douglas reflect the area's development and can be described through a series of influential phases which provide an historical explanation of the Shire's existing settlement pattern. The majority of post-contact places reflect these phases:

(a) Gold discovery and port settlement:

- (i) Gold discovery in the 1870s on the Hodgkinson River led to the development of a suitable route to the coast for access to and from the goldfields, resulting in the opening of a track down the southern range of the Mowbray River valley (the Bump Road). This track connected to the newly declared Town of Port Douglas, established as the main port servicing the goldfields. In the late 1870s and early 1880s, Port Douglas was the preferred location for banks and government offices to service the trade from the mining fields. Craiglie, to the south of Port Douglas, was originally established as a packers and teamsters village, being the closest place to Port Douglas along the track to graze horses and cattle.

(b) Timber harvesting and the development of the sugar industry:

- (i) In the late 1870s timber getters were initially harvesting cedar along the Daintree and Mossman Rivers. In the 1880s, cane growing was established in the Mossman River Valley, ultimately leading to the establishment of the Mossman sugar mill on its current site in 1897.
- (ii) Land north of the Daintree River was settled in the early 1880's with the intention of developing tropical agriculture. However such schemes were limited in success due to the areas isolation, protracted wet weather and cyclones.

(c) The rise of Mossman as the Shire's administrative centre:

- (i) Following the establishment of rail to Cairns (1891) and the establishment of the port of Cairns, the importance of Port Douglas declined. In 1900 a tramway link was established between the Mossman mill and Port Douglas for the transport of sugar, along with the establishment of cane rail to cane fields surrounding the town. By the 1920s, many businesses had relocated to be near the mill in Mossman from Port Douglas, a process that was hastened by a severe cyclone in 1911 that destroyed and damaged many of Port Douglas's buildings.

(d) Development of road connections and pastoralism:

- (i) The opening of the Captain Cook Highway to Cairns along the coast in 1933 led to the decline in the use of the Bump Road and the introduction of bulk sugar handling resulted in the cessation of sugar export via Port Douglas. The town reverted primarily to a fishing village and low key holiday destination, mainly used by nearby locals
- (ii) Dairying was established close to the Daintree township in the 1930's. However, this was later superseded by beef cattle raising. A road was also established between Mossman and Daintree at this time

(e) Tourism expansion and the Daintree blockade:

- (i) In the 1980's, the natural attractions of the Shire drew increasing numbers of visitors and an associated surge in the demand for accommodation, particularly in Port Douglas. Visitation was significantly boosted in 1984 with the opening of the Cairns Airport with direct international flights. An integrated resort development was established between Port Douglas and Four Mile Beach, along with a major tourist development at Dickson's Inlet in 1988, giving impetus for the development of further tourist accommodation and facilities. In addition, both Port Douglas and Mossman expanded its residential communities to cater for those seeking to live within the Shire.
- (ii) In 1983-84, a controversial plan to connect Cairns to Cooktown resulted in a road being pushed through the forest via a coastal route over the steep Cowie and Donovan Ranges, connecting Cape Tribulation to the Bloomfield River. The construction of the road resulted in a large scale environmental protest known as the 'Daintree Blockade' which generated nationwide and international publicity, drawing attention to the plight of North Queensland's rainforests. In addition, in the late 1980's, the State Government over-ruled a Council decision to refuse a proposal to subdivide land into 958 one-hectare housing lots in the Daintree rainforests of Cow Bay, Diwan and Alexandra Bay. These actions refocused attention on the Daintree area which is now recognised by all levels of government for its natural attributes and landscape values, resulting in measures to curb further incompatible development in this part of the Shire.

These phases of history can be demonstrated through the variety of communities represented in the Douglas Shire, its pattern of settlement, a variety of cultural heritage places, public and community buildings, such as halls, churches, schools, cane rail infrastructure and other commercial and industrial buildings, including the Mossman sugar mill and the wharves at Port Douglas

3.2.1.5 People and Settlement

In 2011 Douglas Shire's population was 10 829 (ABS 2011 Census Quick Stats). At this time, the population of its main settlements included Port Douglas/Craigie (4237), Mossman (1732), Wonga Beach (994) and Cooya Beach (855).

In comparison to other parts of Far North Queensland, the Shire has:

- (a) Higher proportions of older age groups 55-64 years (13.9%) and 65-74 years (8.3%).
- (b) Higher proportion of townhouses/flats/apartments (25%), concentrated in Port Douglas
- (c) Higher proportion of residents in long term caravan accommodation (3.7%).
- (d) Higher household vacancy rates, particularly in Port Douglas, Wonga Beach and areas north of the Daintree

The key demographic characteristics for Douglas are as follows:

- (a) Population density is greatest in Port Douglas
- (b) There is noticeable seasonal variation in population in Port Douglas due to the influx of tourists during certain times of the year.
- (c) A significant proportion of the population lives outside the Shire's urban areas.
- (d) The proportion of the population in the older age groups is increasing, consistent with State-wide trends
- (e) Of the families in Douglas, 37.0% were couple families with children, 47.3% were couple families without children and 14.6% were one parent families. Household sizes are decreasing, consistent with State-wide trends
- (f) Of the employed people in Douglas, 14.2% worked in Accommodation. Other major industries of employment included Cafes, Restaurants and Takeaway Food Services 7.8%, School Education 4.4%, Scenic and Sightseeing Transport 4.4% and Supermarket and Grocery Stores 3.3%
- (g) On the day of the Census, the methods of travel to work for employed people were Car, as driver 54.5%, Walked only 8.5% and Car, as passenger 6.0%. Other common responses were Bicycle 5.0% and Other 1.4%. On the day, 1.7% of employed people travelled to work on public transport and 60.7% by car (either as driver or as passenger).

3.2.1.6 Built Environment and Heritage

Douglas Shire is characterised by a variety of buildings and places of heritage significance which contribute to the sense of place and have informed the character and style of the settlement areas in the Shire.

A distinct tropical vernacular is established in the Shire, representing a wide variety of different eras of Shire development and progress. This distinctive tropical vernacular contributes significantly to the identity of the Shire, making the place special for visitors and residents alike.

The historical gridded street pattern in the Port Douglas town centre, the dominance of vegetation over built form, and the open space between the Boat Harbour and Wharf Street are important elements in the townscape of Port Douglas.

The rural setting of Mossman as the Shire's administrative centre and mill town, framed by the western mountains, Mount Beaufort and the Mossman River, and its characteristically traditional north Queensland on-the-front-alignment buildings and wide main street, and the Rain-trees are important qualities of the town.

A particular characteristic of built environment in Douglas Shire is that each of the Shire's communities are contained within well-defined urban boundaries, designated towns, villages and other settlement areas. There is clear sense of town and country, where the settlement edges are not blurred by expanding suburbia. For example, the remote rural setting of Daintree Village is a very distinctive, tourism-focussed community, contained and surrounded by agricultural land and the Daintree River.

From the Mossman sugar mill to the Blockade site at Cape Tribulation, to the wooden bridges that span many creeks and rivers in the Shire, to the more formal monuments such as the Bombing monument in Miallo, the Carstens Memorial and the War Memorial in Macrossan Street and Wharf Street, Port Douglas, respectively, each helps describe the story of the Shire.

Elsewhere, other built heritage places include rural homesteads, public and community buildings such as halls, churches, cemeteries, schools and tramway infrastructure

3.2.1.7 Economy

The tourism and sugar industries are the principal determinants of economic activity, employment and population growth in Douglas Shire. The development and construction industry and the retail service sectors are dependent on these important income generating industries. Other primary production activities, such as fisheries, although important, have a relatively minor role to play.

The interdependent relationship between the Mossman sugar mill and canegrowers makes the sugar industry that much more important to the economic well-being of the Shire. One cannot survive without the other, making the retention of cane lands very important. The cane land areas are not only important for agriculture but are

increasingly valued in terms of their contribution to scenic amenity, the Shire's tourism and recreational value and to the overall liveability of the Shire. However, while the sugar industry is still a significant contributor to the economy, it employs a relatively small proportion of the Shire's workforce.

Both domestic and international tourist visitation continues to be vital to the Shire's economy. The tourism industry relies primarily on the internationally renowned natural attractions of the Great Barrier Reef and the World Heritage Area rainforests, as well as the Shire's outstanding scenic setting. Port Douglas will continue to be one of Australia's premier tourism destinations. Great care is needed to ensure that development not only protects the Shire's natural assets, but promotes attractions that are relevant to, and appropriate for the Shire, so that the long-term unique attractiveness of the Shire to tourists is sustained.

Port Douglas is home to a small fishing fleet and is expected to continue to function as a small, but significant port. However, aquaculture expansion is likely to be limited by environmental constraints.

Beef cattle production is a relatively small industry in the Shire, located mainly in, and around, the Stewart Creek Valley.

Other crops, principally tropical fruit crops including tea and cocoa, are of little direct economic significance. However they are a valuable asset to the local economy and tourist industry.

The unique community, north of the Daintree River, has the opportunity to showcase sustainable living and legitimate caretaking of the local environment. Small scale developments that occur in existing cleared areas contribute to this area's local economy without compromising the environmental values of the area.

3.2.1.8 Transport

The transport system in Douglas Shire consists of a network of sealed and unsealed roads, tramways and sidings, a port, public boat ramps, marinas and wharves, a vehicular ferry across the Daintree River and private airstrips and helicopter landing sites. Most of this network is located south of the Daintree River servicing the settlement and farming areas. A local network of pedestrian and cycle paths is also available in some of the urban areas of the Shire.

The Captain Cook Highway is the principal access route into Douglas Shire, linking the area to Cairns. The Captain Cook Highway between Buchan Point (in Cairns Regional Council) and Yule Point is of great scenic importance. For the most, it is a winding two-lane road. Upgrading this road to a higher standard has the potential to significantly detract from its scenic appeal. Accordingly the vehicular capacity of the Captain Cook Highway has implications on the ultimate development and population capacity of the Shire.

In the long term, Council may need to consider a by-pass around Craiglie depending on the ultimate pattern of development planned for the Port Douglas-Craiglie area.

Other significant roads in the Shire include the Mossman-Mount Molloy Road providing direct access to the Tablelands, the Mossman-Daintree Road providing the main route north of Mossman to the Daintree River and Village, Bailey Creek and Cape Tribulation Roads, north of the Daintree River and the 4 wheel-drive Bloomfield Track, between Cape Tribulation and Degarra.

The Cairns Regional Electricity Board constructed the CREB Track from Daintree to Bloomfield in 1965 for the supply of electricity through to Cooktown. The CREB track is 4 wheel-drive only and becomes inaccessible particularly during the wet season. The CREB track is the main route for residents in the Buru-China Camp area to access services and neighbouring communities.

The Daintree River and its vehicular ferry represents a significant break between movement north and south of the river and also represents a significant threshold on development and population capacity on the northern side of the river. The Far North Queensland Regional Plan does not envisage any alternative to the ferry, and as such, areas north of the Daintree River will remain sparsely populated and maintained as a conservation area and a remote recreation experience.

Port Douglas is a significant port for fishing boats, reef tours and holiday charter vessels and this role is expected to continue. Land available for the water access is limited and the waterfront area has been the subject of a number of studies that seek to coordinate land use at the waterfront

3.2.1.9 Infrastructure

Service infrastructure in Douglas Shire has always represented a constraint on the extent and configuration of development within the Shire. Adequate services can be provided to a variety of development forms, at a cost. However, traditionally Douglas Shire has sought to minimise that cost, economically and environmentally. The protection of ecological and scenic landscape values are a significant factor for consideration in the implementation of any infrastructure scheme

Water Supply

- (a) Douglas Shire extracts raw water from Daintree in the north to Mossman in the south. Some of these extraction locations are pristine rainforest and untouched by human activity. Once extracted raw water is treated at Council's treatment plants and then stored in one of seventeen reservoirs so that it is on tap when required throughout the region.
- (b) Mossman, North Mossman, Port Douglas, Newell, Cooya Beach, Cassowary and Mowbray communities are supplied with water from Rex Creek, a tributary of the Mossman River within the Mossman Gorge section of the Daintree National Park. The water is treated at the Mossman Water Treatment Plant located at Gorge View Crescent, Mossman

- (c) Miallo, Rocky Point and Wonga and surrounding communities are supplied with water from Little Falls Creek in the Whyanbeel Valley. Water is supplied to most surrounding areas, with the exceptions being the upper parts of Whyanbeel, Saltwater Creek, Bamboo Valley Creek and north of Vixies Road in Wonga. This water is treated at the Whyanbeel Water Treatment Plant located at Karnak Road, Karnak.
- (d) The Daintree and surrounding communities are supplied from Intake Creek, a tributary of the Daintree River upstream of the township and Stewart Creek. This water is treated at the Daintree Water Treatment Plant located at Stewart Creek Road, Daintree Village. The existing infrastructure is of limited capacity and probably not able to cater for much more development than currently exists in the area
- (e) Elsewhere in the Shire, development is reliant on its own water supply provisions, including the extraction of bore water
- (f) Upgrading of water supply capacity represents a significant financial investment to the Shire and can be considered to be a threshold for the capacity of future development in the Shire.

Waste Water Treatment

- (a) There are two wastewater treatment plants in Douglas Shire serving the main township communities, as follows:
 - (i) The Mossman Wastewater Treatment Plant located at Junction Road, Mossman, services the Mossman township, the Mossman Gorge Community and parts of Cooya Beach. The plant treats wastewater to a secondary standard and has a capacity of 4529 people or 1.15 mega-litres per day. The liquid stream treatment is an oxidation ditch with clarifier. Effluent is chlorinated and discharged to the Mossman River. Sludge is dewatered via sludge lagoon and belt filter press.
 - (ii) The Port Douglas Waste Water Treatment Plant located at Port Road, Port Douglas, services Port Douglas and Craiglie. The plant treats waste water to a tertiary standard and has a capacity of 20,000 people or 4.9 mega-litres per day. The liquid stream treatment is via a sequencing batch reactor. Effluent is treated with UV and discharged to Dickson's Inlet or reused. Sludge is dewatered from an aerobic digestion system using a belt filter press. Recycled water is reused at Mirage and the Links Golf Courses.
 - (iii) Due to the relatively low-lying and flat nature of the terrain, the Port Douglas system includes a number of small catchments which drain by gravity to pump stations. These pump the sewage via rising mains to the sewage treatment plant. The capacity of individual pump stations represents a significant consideration in respect of any higher density development which may occur in the Port Douglas area
- (b) Elsewhere in the Shire, other methods of wastewater treatment are employed including package plants for isolated developments or individual septic tanks and absorption trenches or transpiration areas. The latter can be problematic during the wet season, particularly in areas with a high water table

Refusal Disposal

- (a) The disposal of waste by land fill no longer occurs in Douglas Shire. Waste in the Shire is collected at transfer stations and disposed of outside the Shire. There are four transfer stations that receive local residents' domestic and self-haul waste and recycling materials, located at Killaloe Dump Road, Killaloe; Rankine Street, Newell; Quandong Road, Cow Bay and Mossman-Daintree Road, Daintree.
- (b) All general waste that is collected kerbside is taken to the Advanced Resource Recovery facility, operated by Sita Environmental Solutions at Port smith (Cairns). All general waste is processed into compost. Any residual waste (non-organic waste that cannot be processed) is transported to landfill on the Atherton Tablelands

3.2.1.10 Community Services

Social infrastructure is the framework of community services and facilities that provide for the quality of life and well-being of the Shire's residents. The distinctive character and unique sense of place of each town and village represented in the Shire contributes to strong community pride and well-being. A range of community facilities and services are available to meet the needs of the community. A hospital is located in Mossman along with family, maternal and child health care services provided elsewhere in Mossman and in Port Douglas. A need has been identified for a well-located, aged care facility in the Shire.

A range of other facilities and organisations are provided throughout the Shire, including childcare centres, schools, libraries, public parks and reserves, community halls, sporting clubs, youth groups and emergency services. (3) The region also offers a range of formal outdoor sporting spaces, interconnected and accessible open space corridors and opportunities for self- directed recreation. An outdoor healthy lifestyle is integral to the character of the Shire, including in particular, the provision of well-designed and highly shaded park and open space areas in the Mossman and Port Douglas town centres.

3.2.2 Shaping the Future

3.2.2.1 Planning Stewardship

The planning scheme has a very important role to play in ensuring that growth in Douglas Shire is aimed at minimising human impact on the environment while continuing to support economic progress and social well-being. Essentially this entails strong stewardship and the pursuit of planning strategies that:

- (a) conserve the Shire's natural environment and outstanding biodiversity values;

- (b) utilise the region's natural resources efficiently, in particular water and energy, while reducing waste;
- (c) ensure the region's atmosphere remains clear and clean;
- (d) contain urban growth to a defined footprint and promote the best use of land that is allocated for that purpose;
- (e) support development of a diverse thriving economy that complements the region's green and environmentally responsible image.
- (f) provide opportunities that ensure everyone is able to participate, live, work and benefit in the healthy cohesive environment that the Shire promotes.

This planning scheme promotes environmental responsibility in Douglas Shire as being fundamental to the community's future prosperity and is consequently a common theme that will direct land use decision making.

3.2.2.2 Reinforcing Douglas Shire's sense of place and identity

Sense of place is a dual characteristic that derives from certain distinctive features of a place and the feelings and perceptions that people hold with regard to that place. In planning, it is usually used to promote positive places that foster community attachment.

At the Shire-wide scale, the topography, creeks and rivers, and the coastline contribute significantly to the Shire's sense of identity. Residents identify closely with the Shire's physical setting, and this in part, contributes to a heightened sense of place and interest in local issues. This physical definition creates a range of smaller communities within the Shire's boundaries, many with their own unique sense of identity, character, history and culture. It is important to protect this sense of place as it enhances the quality of life for its residents and is of enormous value for tourism.

During the life of this planning scheme, the Shire will continue to grow while retaining the unique characteristics of individual communities and building upon these qualities through local variation in development requirements. This will be done through the Local Plans which are tailor-made to suit local needs. Amongst other things, each local plan will:

- (a) identify the qualities that make the individual place special, including environmental and built form characteristics;
- (b) promote gateway treatments, nodes of activity, viewing corridors and landmark sites;
- (c) define an urban / rural interface boundary to reinforce the sense of town and country.

In addition, tourism activity will need to be Shire-appropriate with a discerning preference for nature-based tourism that has a direct association with the environmental and rural characteristics of the Shire, as opposed to large-scale attractions that have little connection to local context, particularly those that can be found anywhere.

Port Douglas will retain its lively tropical tourism 'buzz' as a premier tourist destination. Development will be carefully planned to achieve sensitive incremental change, rather than instant, over-scaled development projects, that have little local context or character.

3.2.2.3 Catering for Economic Opportunity

Tourism and primary production will remain significant drivers and employers in the Shire, but other supporting industries and services will continue to emerge. While increasing economic opportunity for Douglas will involve strategies that go beyond the scope of what a planning scheme can achieve, it will involve the general diversification of the Shire's economic base to counter the negative effects of global trends. In turn, this will provide greater resilience in the local Douglas economy. Amongst other things, this will involve:

- (a) a focus on building up small business in the community;
- (b) the allocation of land for future enterprise needs within the community;
- (c) building up tourism opportunities taking advantage of the Cairns airport as an international gateway, and in particular marketing Port Douglas as a premier tourism destination;
- (d) recognition and protection of the Shire's environmental and scenic assets and realising the recreational and economic opportunity they deliver, in a well-planned and managed network of environmental parks, trails and tracks;
- (e) improving transport links into and out of the Shire, as well as within the Shire;
- (f) allowing 'start-up' business to establish as small-scale home-based businesses without excessive red tape;
- (g) a continued focus on education and training opportunities within the community;
- (h) taking advantage of digital technologies and the associated digital economy.

This planning scheme will promote Douglas Shire as a place to invest and conduct appropriately planned and environmentally responsible businesses.

3.2.2.4 A Sustainable Shire

The planning scheme has a key role to play in ensuring growth in the region is sustainable aiming to minimise human impact on the environment while, at the same time, continuing to support economic opportunity and social well-being. Essentially this entails the pursuit of planning strategies that:

- (a) conserve the Shire's natural environment and outstanding biodiversity values;

- (b) utilise the Shire's resources, including energy and water, efficiently including waste reduction;
- (c) ensure the Shire's atmosphere remains clear and clean;
- (d) contain urban growth to a defined urban footprint and promote the best use of land that is allocated for urban purposes;
- (e) support the development of a diverse and thriving economy that complements the Shire's green and environmentally responsible image;
- (f) provides opportunities that ensure everyone is able to participate, live, work and benefit in the healthy environment that the Shire promotes.

The planning scheme promotes sustainability in Douglas Shire as being fundamental to the Shire's future prosperity and is consequently a common theme that will direct future land use decision making.

3.2.2.5 Increasing House Choice

Housing needs of the Douglas Shire are varied and as such a variety in housing choice is needed to cater for those needs. While there is a concentration of high-density housing in Port Douglas, the majority of this housing type is tourist accommodation or resort style living. Elsewhere, a significant proportion of the Shire's housing stock consists of suburban detached housing. The development industry continues to construct this form of housing, which is most notable in the more recent residential estates established in Mossman, Cooya Beach and Craiglie.

The trend in the Shire has generally been toward smaller lot sizes, containing bigger houses. However, this is not necessarily producing a more affordable product. Despite this, housing is still reasonably affordable in some parts of the Shire. However, there is quite a substantial difference in affordability between individual communities in the Shire. As household sizes continue to decrease alternative accommodation choices will be need to supply beyond the standard 3 and 4 bedroom housing stock that continues as the predominant housing supplied to the market.

There is also growing need for some form of aged care facility in the Shire as the population in older age groups increases, in line with the State-wide trend

3.2.2.6

Douglas Shire has previously had sufficient space to cater for its population growth and this is likely to continue to be the case into the foreseeable future. However the amount of land in the Shire that is constrained by one or more factors is significant. Amongst other things, such constraints include:

- (a) land containing vegetation of high ecological significance, including endangered, and of concern, flora and fauna;

- (b) wetland areas;
- (c) quality agricultural land;
- (d) land subject to significant natural hazards such as flood and storm tide inundation.

The implication of the limited supply of unconstrained land makes decisions about land use and future urban growth, even more important. The Shire will cater for modest growth in housing and most of this growth will be accommodated in Mossman in the foreseeable future, with an adequate supply of land already catered for.

There is pressure in Port Douglas / Craiglie to extend urban development on to the western side of the Captain Cook Highway. Previous planning regimes resisted such a move, as being contrary to the Shire's iconic values. Additional land is still available at the southern end of Craiglie on the eastern side of the Captain Cook Highway and in-fill opportunities still exist in Port Douglas so as not to warrant an immediate need for urban residential development on the western side of the highway. However, the area is considered in the longer term to potentially represent an area that may be suitable for accommodating further residential growth.

Furthermore, the capacity for the Shire to continue to grow is also constrained by the need to provide a reliable and adequate water supply, and in the road capacity of the Captain Cook Highway to cater for increased traffic volumes. Both of these represent thresholds, beyond which significant and expensive infrastructure upgrades would be required.

3.4 Theme 1 – Settlement Pattern

The settlement pattern for the region is shown on the Strategic framework maps contained in Schedule 2.

3.4.1 Strategic Outcomes	
Strategic Outcome	Applicant Response
Strategic Outcome 1 The Shire will consist of a network of well planned, discrete settlements that integrate existing and future development while maintaining an emphasis on the natural and scenic qualities of the landscape.	The proposed development will support this outcome, as it is located within an established settlement (Port Douglas), as well as being within an area that has been appropriately zoned and earmarked for future tourist accommodation development. The proposed development aims to not only maintain, but also emphasise the natural and scenic qualities of the existing landscape and character, which comprises of lush landscaping that complements the tropical resort town atmosphere. Where possible, existing green vistas observed from neighbouring properties will be maintained throughout the site, via the imposition of appropriate landscaping. Suitable landscaping will be implemented to complement the existing character and landscape and reduce negative visual impacts. It is also noted that the proposed development has been redesigned and the bulk/scale has been significantly reduced to complement the existing character currently observed throughout Port Douglas.
Strategic Outcome 2 Coastal settlements are consolidated by conserving the natural state of coastal areas outside existing urban areas.	The subject site is located within an existing urban area.
Strategic Outcome 3 Commercial and retail activity is concentrated into defined town centres	It is noted that the proposed resort complex incorporates ancillary commercial and/or retail uses (i.e. restaurants, bars, cafes, function facility). Whilst the site is not located within the defined town centre of Port Douglas, the site is within close proximity (approx. 2.4km) to land mapped as being within the Centre Zone. Notwithstanding this, commercial and/or retail uses forming part of the proposal will be ancillary to the primary tourist accommodation component of the proposed development. These ancillary uses are intended to complement and support the Fairmont Resort development and are not expected to detract from the existing

	<p>town centre. Ancillary uses are focussed more on adding to the whole experience and providing convivence for guests staying at the resort who wish to dine and recreate on site. It is intended that ancillary land uses will be made available for use by the wider community/general public.</p> <p>Additionally, it is noted that the proposed development will ultimately support the existing township of Port Douglas, by providing additional accommodation options and in turn supporting an increase in tourist visitation to area.</p>
<p>Strategic Outcome 4</p> <p>Industrial activities are located in areas that area separate from sensitive land uses</p>	<p>The proposal does not involve an industrial activity.</p>
<p>Strategic Outcome 5</p> <p>New rural residential development areas are not established. However existing rural residential areas will be protected from further incremental subdivision and land use activities that would compromise the ambience and amenity of such areas.</p>	<p>The proposal does not involve rural residential development nor the establishment of a new rural residential area.</p>
<p>Strategic Outcome 6</p> <p>The rights and interests of native Title land holders are recognised through provision for Indigenous housing and seasonal camps (bayan) on Aboriginal Freehold land within the Return to Country local plan code.</p>	<p>The subject site is not located within the Return to Country Local Plan Area and is not Aboriginal Freehold land.</p>
<p>Strategic Outcome 7</p> <p>Natural hazards and the impacts of climate change will determine the location, scale and intensity of land use activities. Development will achieve an acceptable or tolerable level of risk based on a fit for purpose risk assessment.</p>	<p>The proposed development has been sited and designed appropriately to ensure the effects of any natural hazard events are mitigated or managed, so as to reduce or remove impacts on visitors to and occupants on site.</p>

3.4.2 Element – Urban Settlement	
Strategic Outcome	Applicant Response
<p>Strategic Outcome 1</p> <p>The urban area is intended to include residential areas, business areas, community and recreational facilities and other services and facilities necessary to provide for the Shire's urban population.</p>	<p>The subject site is located within an urban area, immediately surrounded/dominated by tourist related development. The proposed development seeks approval to construct a resort complex which is consistent with the underlying zoning of the site – being the Tourist Accommodation Zone. Proposed development is not expected to impact on/reduce opportunities to establish other facilities, services and/or land uses. It is noted that the proposed resort complex incorporates ancillary commercial and/or retail uses. It is intended that these ancillary land uses will be open and available for use by the wider community/general public.</p>
<p>Strategic Outcome 2</p> <p>The two major urban communities in Douglas Shire are Port Douglas and Mossman. Port Douglas has a distinctive tourism focus, being located close to the sea and Dickson's Inlet, while Mossman is the Shire's major administrative and rural service centre. Douglas Shire also has a range of compact townships and villages in scenic rural settings.</p>	<p>The proposal seeks approval to construct a six star internationally recognised resort complex within Port Douglas and is therefore considered to support this outcome. The proposal is expected to support and enhance tourism throughout the area, by offering a six-star experience to tourists visiting the area. It is noted that there are currently no options for six-star accommodation existing within Port Douglas.</p>
<p>Strategic Outcome 3</p> <p>Residential development is primarily detached low-set housing, with medium density development focussed in and around the Port Douglas town centre, and in the nearby tourist areas to the south of the town centre.</p>	<p>The proposal does not involve residential development.</p>
<p>Strategic Outcome 4</p> <p>Commercial development is low-profile and is predominately focussed along the main streets in Port Douglas (Macrossan Street) and Mossman (Front Street).</p>	<p>Although the proposal includes some commercial use elements, these uses will remain subordinate and ancillary to the primary tourist accommodation land use element of the proposed development .</p>

<p>Strategic Outcome 5</p> <p>Industrial development occurs in clusters in Mossman South and adjacent to the Mossman sugar mill close to the Mossman town centre. A newer industrial land subdivision is established on the western side of the Captain Cook Highway in Craiglie. Marine-orientated industry, and associated tourism activities, occurs adjacent to Dickson Inlet in Port Douglas.</p>	<p>The proposal does not involve industrial development.</p>
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3.4.2.1 Specific Outcomes	
Specific Outcome	Applicant Response
<p>Specific Outcome 1</p> <p>The settlement pattern for Douglas Shire is shown on the Strategic Framework map contained Schedule 2</p>	<p>The proposal is considered to be consistent with the settlement pattern for Douglas Shire as shown on the Strategic Framework maps contained within Schedule 2. More specifically, it is noted that the subject site is located within an identified existing urban area and comprises urban development.</p>
<p>Specific Outcome 2</p> <p>Urban development occurs within the urban area as shown on the Strategic Framework Map contained in Schedule 2. Expansion beyond this boundary is not facilitated as it does not support the efficient and orderly delivery of urban infrastructure.</p>	<p>The subject site is located within an identified existing urban area and comprises urban development. It is noted that the proposal involves infill development within an existing developed area. Tourist related developments have been established/are existing on land immediately surrounding the subject site. Due to the established nature of the site, all required urban infrastructure services are available to the site, including reticulated water, gas, sewer, stormwater, telecommunications, and electricity.</p>
<p>Specific Outcome 3</p> <p>Residential investigation areas are allocated at Wujal Wujal (reflecting the Urban Footprint nominated in the 'Regional Plan'), Mossman and Craiglie, but some of these areas are not anticipated to be required until later in the planning scheme time horizon: if not beyond it. No urban development is to occur within the</p>	<p>The subject site is not located within a Residential Investigation area.</p>

Residential investigation areas until a comprehensive structure plan has been prepared that provides for integrated, well connected residential settlements, taking into account constraints, infrastructure servicing and capacity requirements, flooding and drainage, and sequencing.	
Specific Outcome 4 The development of rural towns and coastal settlements will be confined to distinct township boundaries, with coastal settlements consolidated to conserve the natural state of coastal areas outside existing urban areas.	The subject site is located within an existing urban area.
Specific Outcome 5 Urban areas are protected from encroachment of competing and conflicting land use activities.	The subject site is located within an existing urban area. The site has been appropriately zoned and earmarked for future tourist accommodation development. The proposed land use is not considered to be in conflict with existing/surrounding land uses, which comprise primarily tourist related development.
Specific Outcome 6 Artificial lakeside and coastal canal developments are not established	Artificial lakeside and coastal canal developments do not form part of this proposal.
Specific Outcome 7 Residential developments are designed to incorporate pedestrian, cycle and road connections that offer a high degree of permeability and legibility.	The proposal does not involve residential development.
Specific Outcome 8 Development does not rely on complex, visually intrusive engineering solutions to overcome site constraints.	The subject site remains relatively unconstrained. A portion of the site has already been excavated (in part). No visually intrusive engineering solutions were required/formed part of the design proposal.

<p>Specific Outcome 9</p> <p>Local plans provide localised planning responses and desired development outcomes for communities.</p>	<p>The subject site is located within the Port Douglas / Craiglie Local Plan Area. Proposed development is considered to achieve the desired development outcomes for the Port Douglas community. Please refer to Attachment 7 for detailed code compliance responses to the Port Douglas / Craiglie Local Plan Area Code – more specifically for further details on how the proposed development meets the desired development outcomes for Port Douglas.</p>
<p>Specific Outcome 10</p> <p>Residential developments are designed and located to ensure the operational integrity of key infrastructure within the shire is not compromised.</p>	<p>The proposal does not involve residential development.</p>

3.4.3 Element – Activity Centres	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Activity centres are concentrations of activity, including amongst other things, retail, business, employment, research, education, health and community services.</p>	<p>The subject site is not located within/or near an activity centre, pursuant to the Local Planning Scheme Strategic Framework mapping. Notwithstanding this, the Far North Queensland Regional Plan identifies Port Douglas as a District Regional Activity Centre. Within the Regional Plan, Port Douglas is defined as having a strong tourism focus. In considering this, it is submitted that the proposed development will contribute to and complement the existing District Regional Activity Centre of Port Douglas and aligns with the strong tourism focus that has been noted within the Regional Plan. The proposed development seeks approval to construct a six-star luxury hotel for the purpose of providing additional/different accommodation options for tourists visiting the area.</p>
<p>Strategic Outcome 2</p>	<p>As per above.</p>

<p>Douglas Shire is part of a network of activity centres that is specifically defined within the Regional Plan. The principal activity centre for the Far North Queensland region is Cairns City, which provides the highest concentration, range and density of activities and services for the entire region. Also of planning influence to Douglas Shire is the existing Major Regional Activity Centre at Smithfield.</p> <p>Note – All of these places are part of the adjoining Cairns Regional Council local government area.</p>	
<p>Strategic Outcome 3</p> <p>Mossman and Port Douglas are both defined as District regional activity centres under the Regional Plan. District regional activity centres generally contain a reasonably large concentration of business, employment and population. They provide a range of convenience retail and urban services and may contain small district or branch offices of government. Despite being relatively close, each centre has a different role that results in different economic synergies. Mossman is the main administrative, service, agricultural and industrial centre, whereas Port Douglas has a stronger tourism focus.</p>	<p>As per above.</p>
<p>Strategic Outcome 4</p> <p>Attractive, vibrant and functional activity centres are integral to local economic development as well as servicing the needs of the business sector and the community. New commercial developments should be located in appropriate areas within the town centres to ensure that their viability is maintained. There is also a need to retain a high level of services within the Shire to meet the needs of the community, to provide employment opportunities and reduce the need to commute outside the Shire for work opportunities</p>	<p>As per above.</p>

3.4.3.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>The hierarchy of activity centres established within the region is shown on the Strategic framework maps contained in Schedule 2</p>	<p>The subject site is not located within/or near an activity centre, pursuant to the Local Planning Scheme Strategic Framework mapping.</p>
<p>Specific Outcome 2</p> <p>Activity centres are the focus of retail, employment and community services catering for the needs of the Shire's residential and tourist-oriented communities alike.</p>	<p>The subject site is not located within/or near an activity centre, pursuant to the Local Planning Scheme Strategic Framework mapping.</p>
<p>Specific Outcome 3</p> <p>Mossman and Port Douglas are the Shire's District activity centres. Mixed use development will be permitted, noting that this form of development is more likely to be achieved in Port Douglas, due to its tourism focus. Future growth of each centre will respect the centre's distinctive character and functions.</p>	<p>It is submitted that the proposed development will contribute to and complement the existing District Regional Activity Centre of Port Douglas and aligns with the strong tourism focus that has been noted within the Regional Plan. The proposed development seeks approval to construct a six-star luxury hotel for the purpose of providing additional/different accommodation options for tourists visiting the area. It is noted that the proposed resort complex incorporates ancillary commercial and/or retail uses (i.e. restaurants, bars, cafes, function facility). These ancillary uses are intended to complement and support the Fairmont Resort development and are not expected to detract from viability or vibrancy of the existing town centre. These ancillary uses will focus on adding to the whole experience and providing convivence for guests staying at the resort who wish to dine and recreate on site. It is intended that ancillary land uses will be made</p>

	available for use by the wider community/general public. Additionally, it is noted that the proposed development will ultimately support the existing township of Port Douglas by providing additional accommodation options and in turn supporting an increase in/tourist visitation to area.
Specific Outcome 4 Local activity centres will take the form of a cluster of shops and associated commercial activities catering for the needs of a cluster of smaller neighbourhoods.	The subject site is not located within/or near an activity centre, pursuant to the Local Planning Scheme Strategic Framework mapping.
Specific Outcome 5 Neighbourhood activity centres will take the form of small-scale neighbourhood facilities providing opportunities to cater for the day-to-day convenience needs of individual towns and neighbourhoods.	The subject site is not located within/or near an activity centre, pursuant to the Local Planning Scheme Strategic Framework mapping.
Specific Outcome 6 New and redeveloping activity centres will establish high quality environments drawing on the positive characteristics of the surrounding area having particular regard to creating or reinforcing security, vitality and a sense of place.	The subject site is not located within/or near an activity centre, pursuant to the Local Planning Scheme Strategic Framework mapping. The area has not been identified as a potential future activity centre.
Specific Outcome 7 Entertainment and after-hours activities are appropriately located in activity centres, mainly concentrating in and close to the Port Douglas town centre.	Whilst the site is not located within the defined town centre of Port Douglas, the site is within close proximity (approx. 2.4km) to land mapped as being within the Centre Zone. It is noted that the function facility, which forms part of the proposed development, may attract events that occur after hours (i.e. weddings). Notwithstanding this, given the site's close proximity to nearby activity centres and the town centre, the function facility component of the proposal is considered to be appropriately located and can easily rely on/utilise existing town centre activities/services, where necessary.

<p>Specific Outcome 8</p> <p>Development is designed to integrate into activity centres through mechanisms such as:</p> <ul style="list-style-type: none"> (a) mixed use development. (b) high quality footpaths and streetscapes suitable for walking and cycling. (c) awnings and shade trees along pathways. (d) active street faces with smaller active front tenancies. (e) limiting or sharing vehicular access across footpaths to encourage a continuous built form. (f) concealing parking behind, underneath and/or above tenancies in architecturally screened roof-top car parking. (g) using spaces above retail tenancies for other uses. 	<p>The subject site is not located within/or near an activity centre, pursuant to the Local Planning Scheme Strategic Framework mapping.</p>
<p>Specific Outcome 9</p> <p>Community services and facilities are provided close to the communities they serve and public transport routes.</p>	<p>The proposal does not involve community services and/or facilities.</p>

3.4.4 Element – Industry Areas and Activities	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Growth in manufacturing, property and business services and transport and storage will continue to contribute to Douglas Shire's economy. The infrastructure required to support industry and business development will continue to be planned and provided.</p>	<p>The subject site is not located within/or near an industrial area. The proposal does not involve industry land use/s.</p>
<p>Strategic Outcome 2</p>	

Any future expansion of industry is encouraged in Mossman rather than Port Douglas having regard to the high demand for land for tourism and residential purposes at Port Douglas and the role of Mossman as a commercial centre.	The subject site is not located within/or near an industrial area. The proposal does not involve industry land use/s.
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3.4.4.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 Sufficient land and infrastructure is supplied in Industry areas such as Craiglie and Mossman South to accommodate new and expanding enterprises.	The subject site is not located within/or near an industrial area. The proposal does not involve industry land use/s.
Specific Outcome 2 The Mossman Mill is of critical importance to the future prosperity of Mossman. It is intended that land, at and near, the Mossman Mill will become the consolidated industrial area in Mossman catering for low/medium industrial development. As such, an Industry investigation area is allocated on the Strategic framework maps as contained in Schedule 2 for Mossman North adjacent to the sugar mill. This investigation area will cater for the types of industry that may otherwise constitute an amenity nuisance in other industrial locations. However, prior to establishment of such uses, it will need to be demonstrated that such uses are adequately separated, and any impacts appropriately mitigated to ensure that nearby residential amenity is not compromised.	The subject site is not located within Mossman and/or near the Mossman Mill. The proposal does not involve industry land use/s.
Specific Outcome 3	The subject site is not located within/or near an industrial area. The proposal does not involve industry land use/s.

Existing higher impact industry uses are protected from encroachment by development that would compromise the ability of the land use to function safely and efficiently.	
Specific Outcome 4 Marine orientated activities will establish in Dickson's Inlet in Port Douglas, in accordance with the Port Douglas / Craigie Local Plan code.	The proposal does not involve marine orientated activities.
Specific Outcome 5 The industry area at Craigie is a mix of commercial services / service industries. However, it will not be promoted as an area suitable for land uses that rely heavily on the highway for exposure to trade/function.	The subject site is not located within/or near the Craigie industrial area. The proposal does not involve industry land use/s.

3.4.5 Element – Residential Areas and Activities	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Mossman currently contains a large supply of broad hectare land available for residential growth, whereas other areas including Port Douglas/Craigie are constrained by ecological values and infrastructure constraints.	The subject site is not located within a residential area. Notwithstanding this, it is noted that a portion of the land immediately surrounding the subject site, to the north, west and east, contains the Mirage Port Douglas development and is located within the Integrated Resort Precinct. Land use and planning provisions for the Mirage Port Douglas are managed through the Integrated Resort and Development Act 1987 (IRDA) and any subsequent scheme approved in accordance with the relevant legislation. The Mirage Port Douglas Integrated Resort Development Scheme identifies some of the land surrounding the subject site as being suitable for potential future residential land uses. The potential for future residential development surrounding the subject site was taken into consideration when siting and designing the proposed development. Appropriate measures have been implemented to ensure the proposed development is unlikely to negatively impact on any future residential development constructed on adjoining land parcel/s.

<p>Strategic Outcome 2</p> <p>The coastal settlements of Wonga Beach, Newell and Cooya Beach and various other small towns, villages and rural residential land, including the land north of the Daintree River, are not intended to grow or increase in density over and above existing zoned land and what current approvals provide. These communities generally have low levels of infrastructure and significant coastal or environmental constraints.</p>	<p>The subject site is not located within/or near the coastal settlements of Wonga Beach, Newell and Cooya Beach.</p>
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3.4.5.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Coordinated residential development will be facilitated. Development will not compromise the effective development, or continued use of adjacent land or the comprehensive development of land that is part of a local plan or structure plan.</p>	<p>The subject site is not located within a residential area nor does the proposal involve a residential land use. Notwithstanding this, it is noted that a portion of the land immediately surrounding the subject site, to the north, west and east, contains the Mirage Port Douglas development and is located within the Integrated Resort Precinct. Land use and planning provisions for the Mirage Port Douglas are managed through the Integrated Resort and Development Act 1987 (IRDA) and any subsequent scheme approved in accordance with the relevant legislation. The Mirage Port Douglas Integrated Resort Development Scheme identifies some of the land surrounding the subject site as being suitable for potential future residential land uses. The potential for future residential development surrounding the subject site was taken into consideration when siting and designing the proposed development. Appropriate measures have been implemented to ensure the proposed development is unlikely to negatively impact on any future residential development constructed on adjoining land parcel/s.</p>
<p>Specific Outcome 2</p>	<p>The proposal is not for residential development.</p>

Residential development is located and designed to provide a wide range of living options that maintain and enhance the character and identity of individual communities.	
Specific Outcome 3 Residential areas are used for residential purposes. Non-residential uses are provided, only where they support the local community, and where such uses do not detract from the residential amenity of the area.	The subject site is not located within a residential area.
Specific Outcome 4 Medium density housing is facilitated within identified areas in the relevant zones.	The proposal is not for residential development.
Specific Outcome 5 Areas identified for low density residential development provide predominantly low-rise detached dwelling houses to maintain the low-density character of the area.	The proposal is not for residential development and is not located within a residential area.
Specific Outcome 6 Residential greenfield development incorporates: <ul style="list-style-type: none"> (a) mixed lot sizes to promote a range of housing choices and densities; (b) high levels of pedestrian, cycle and road permeability and connectivity; (c) the efficient and timely provision of infrastructure and services; (d) open space that provides opportunities for sport and recreation activities; (e) elements that promote a positive identity and character. 	The proposal is not for residential development.

<p>Specific Outcome 7</p> <p>Residential development incorporates tropical design principles that are responsive to the natural environment and climate.</p>	<p>The proposal is not for residential development.</p>
<p>Specific Outcome 8</p> <p>Home based businesses are established where they are low impact in nature, subordinate to the primary residential use, and do not detract from the amenity of the area.</p>	<p>The proposal does not involve a home-based business.</p>

3.4.6 Element – Rural Residential Areas	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>The valleys and surrounding lush tropical hillslopes in Douglas Shire add significantly to the visual quality of the Shire and are seen as attractive locations for those seeking a rural residential lifestyle. However, the Shire also contains some highly productive agricultural land and there is a need to reserve rural land for commercial scale primary production and avoid the extension of rural residential activities into these areas. There is also a need to avoid the land use and servicing issues arising from the development of rural residential areas.</p>	<p>The proposal is not for rural residential development.</p>
<p>Strategic Outcome 2</p> <p>Where rural residential development is firmly established in parts of the Shire such as in the Mowbray Valley, Upper Cassowary, Shannonnvale, parts of Somerset and Whyanbeel, and to the north of Vixies Road in Wonga, such development</p>	<p>The proposal is not for rural residential development.</p>

continues to offer a high amenity residential alternative to township living and should be protected from any further urbanisation, or inappropriate subdivision.	
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3.4.6.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 New rural residential development areas are not established. Existing rural residential areas are protected from further incremental subdivision that results in lot sizes less than the minimum area to ensure that the character and amenity of these areas is maintained.	The proposal is not for rural residential development.
Specific Outcome 2 In rural residential areas any subdivision that is permitted creates lots of appropriate shape and proportions to ensure that existing rural residential amenity and character is not compromised. Rural residential areas are characterised by low-scale detached housing on large lots with sufficient area available to maintain a generous separation between each house on each lot.	The subject site is not located within/or near a rural residential area.
Specific Outcome Incompatible land uses are not established in rural residential areas	The subject site is not located within/or near a rural residential area.

3.4.7 Element – Mitigation of Hazards	
Strategic Outcomes	Applicant Response
Strategic Outcome 1	

Douglas Shire is susceptible to a variety of naturally occurring hazards including floods, cyclonic winds, bushfire, landslides, coastal hazards, storm tide and erosion prone areas. Reasonable efforts to communicate predicted hazard risks to the community are nominated in this planning scheme. However, none of the planning scheme mapping infers that parts of the Shire are entirely free from any particular form of hazard.	The proposed development has been sited and designed with respect to hazard overlays, to minimise and mitigate risk from hazard events.
Strategic Outcome 2 Some established urban areas are already located in parts of the Shire which are periodically vulnerable to a variety of hazards. A reasonable risk-tolerant approach to development will be taken in existing areas taking into account individual hazard impacts. However as a first principle, further expansion of development into areas subject to hazards should be avoided	The proposed development has been sited and designed with respect to hazard overlays, to minimise and mitigate risk from hazard events.

3.4.7.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 Development avoids natural hazard areas or mitigates risks to an acceptable or tolerable level.	The proposed development has been sited and designed with respect to hazard overlays, to minimise and mitigate risk from hazard events.
Specific Outcome 2 Development directly, indirectly and cumulatively avoids an increase in the severity of the natural hazard and the potential damage on the site or to other properties.	The proposed development has been sited and designed with respect to hazard overlays, to minimise and mitigate risk from hazard events.
Specific Outcome 3	The proposal does not involve the construction of community infrastructure.

Locate and design community infrastructure to maintain the required level of functionality during and immediately after a natural hazard event.	
<p>Specific Outcome 4</p> <p>Development does not unduly burden disaster management response or recovery capacity and capabilities.</p>	The proposed development has been sited and designed with respect to hazard overlays, to minimise and mitigate risk from hazard events. In considering this, the proposed development is not expected to unduly burden disaster management response or recovery capacity and capabilities.
<p>Specific Outcome 5</p> <p>New development takes into account the latest science with respect to climate change in addition to the avoidance of natural hazards.</p>	The subject site has been assessed in respect to hazard overlays, any conditions regarding these overlays will be complied with and managed accordingly to prevent and mitigate effects of hazards.

3.4.7.2 Land Use Strategies	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Additional modelling of the impacts of sea-level rise may be required, along with strategies to mitigate sea-level rise, including retreat strategies.</p>	The subject site has been assessed in respect to hazard overlays, any conditions regarding these overlays will be complied with and managed accordingly to prevent and mitigate effects of hazards.

3.4.8 Element – Recognition of the Rights and Interests of Native Title Land Holders	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Overview of the Clans, native title determination outcomes and land transfer – the spatial extent and history within Douglas Shire (forthcoming by Jabalbina).</p>	It is noted that the subject site is located within a current Native Title claim area (QC2024/001 – Djabugay Nation). Notwithstanding this, the subject site comprises freehold land and is therefore excluded from the Native Title claim application. Natural and culturally significant areas have still been taken into consideration and have informed the appropriate siting of development so as to avoid adverse impacts on these areas.

<p>Strategic Outcome 2</p> <p>All possess the desire to return to their Country to practice their culture and strengthen their identity.</p>	<p>The proposal is not expected to impact on the desire of native title holders to return to their Country.</p>
<p>Strategic Outcome 3</p> <p>The intent of Return to Country local plan code is to provide for a range of residential and economic land uses to reflect the aspirations of the Eastern Kuku Yalanji People. These ILUA's reflect these agreements and the planning scheme supports the Eastern Kuku Yalanji People in their return to Country</p>	<p>The subject site is not located within the Return to Country Local Plan Area.</p>

3.4.8.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>The range of agreed land uses contained within the ILUA's are reflected in the land uses that apply within the Return to Country local plan code</p>	<p>The subject site is not located within the Return to Country Local Plan Area and it is understood that there are no registered/proposed ILUAs existing/relevant to the subject site.</p>
<p>Specific Outcome 2</p> <p>Culturally appropriate housing is facilitated within the Return to Country local plan code.</p>	<p>The subject site is not located within the Return to Country Local Plan Area. The proposal does not involve residential development.</p>
<p>Specific Outcome 3</p> <p>The Return to Country Local Plan has been created to reflect the Eastern Kuku Yalanji aspirations to return to Country and derive economic benefits from their land and waters.</p>	<p>The subject site is not located within the Return to Country Local Plan Area.</p>

3.5 Theme 2 – Environmental and Landscape Values

3.5.1 Strategic Outcomes	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>The Aboriginal cultural heritage values of the Wet Tropics bioregion are acknowledged, protected and enhanced. The Aboriginal Rainforest People of the Wet Tropics have lived continuously in the rainforest environment for at least 50000 years, and this is the only place in Australia where Aboriginal people have permanently inhabited a tropical rainforest environment.</p>	<p>Proposed development is located within a significantly developed, urban area. Development has been designed and sited so as to avoid adverse impacts on natural and culturally significant areas.</p>
<p>Strategic Outcome 2</p> <p>The unique environmental values of the Shire, which result from its location within the Wet Tropics bioregion, are maintained and protected for current and future generations.</p>	<p>Proposed development is located within a significantly developed, urban area. Development has been designed and sited so as to avoid adverse impacts on natural and culturally significant areas.</p>
<p>Strategic Outcome 3</p> <p>Those parts of the Shire located within the Wet Tropics and Great Barrier Reef World Heritage Areas and other areas of environmental value and ecological significance, are preserved and protected for nature conservation, landscape/scenic quality, biodiversity and habitat values. In particular, the Southern Cassowary and its habitat are protected.</p>	<p>The subject site is not located within the Wet Tropics and Great Barrier Reef World Heritage Areas. Additionally, it is noted that the subject site involves vacant land and does not include any mapped matters of State Environmental Significance. Notwithstanding this, development has been appropriately designed and sited so as to avoid adverse impacts on natural and culturally significant areas.</p>
<p>Strategic Outcome 4</p> <p>Development takes into account matters of:</p> <p>(a) national environmental significance (MNES) and the requirements of the Environment Protection and Biodiversity Conservation Act 1999 (Cth) (EPBCA);</p> <p>(b) state environmental significance (MSES);</p>	<p>The subject site involves vacant land and does not include any mapped matters of National, State or Local Environmental Significance.</p>

(c) local environmental significance (MLES).	
<p>Strategic Outcome 5</p> <p>The integrity and system functions of the Great Barrier Reef World Heritage Area and the State Great Barrier Reef Coast Marine Park are to be protected by maintaining and restoring the connectivity, resilience and condition of marine and coastal ecosystems.</p>	<p>The subject site is not located within the Great Barrier Reef World Heritage Area and/or the State Great Barrier Reef Coast Marine Park.</p>
<p>Strategic Outcome 6</p> <p>Natural waterways such as the Daintree River, the Mossman River, the Mowbray River and Dickson Inlet; all wetlands but particularly those on the Directory of Wetlands Importance in Australia, being the Lower Daintree River, Alexandra Bay and the Hilda Creek Headwater; and all catchments located in coastal areas within the Shire, are managed to protect their ecological processes, enhance water quality, conserve riparian ecological values and landscape and scenic quality, while acknowledging nature based recreation opportunities.</p>	<p>The site is located east of Dickson Inlet. Water potentially discharged from or flowing through the site will be managed to ensure that sediment and nutrient movement is appropriately managed, avoiding impacts on ultimate receiving waters. No major adverse impacts on the ecological processes, water quality, and any riparian ecological values of Dickon Inlet are anticipated to occur as a result of this development.</p>
<p>Strategic Outcome 7</p> <p>Development north of the Daintree River is restricted to achieve low key, sustainable and self- sufficient outcomes and is carefully planned to conserve the scenic and ecological World Heritage values of the area.</p>	<p>The subject site is located south of the Daintree River.</p>
<p>Strategic Outcome 8</p> <p>Where appropriate, development maintains and enhances public access and use of natural areas, rivers, dams' creeks and foreshores.</p>	<p>The proposed development is not anticipated to impact on the public's ability to access and use natural areas, rivers, dams, creeks and foreshores. The subject site is located within a developed/urban area and comprises freehold land.</p>

	There are no existing public thoroughfares through the site that facilitate public access to the nearby natural area of Dickson Inlet.
Strategic Outcome 9 The unique environmental character of the Shire comprising internationally renowned landscapes, ecologically significant rainforest systems, sensitive coastal systems and areas of unsurpassed natural beauty, are maintained in association with sustainable development practices, which seek to minimise the effects of development on the natural environment.	<p>No major adverse impacts on the environment and biodiversity of the region are anticipated to occur as a result of this development.</p> <p>The implementation of sustainable development practices will ensure that potential or perceived impacts on the natural environment are appropriately managed and/or mitigated.</p> <p>The site comprises of vacant, cleared land. No significantly environmental matters have been identified within the subject site. The proposed development has been designed so as to ensure the bulk and scale does not significantly impact on surrounding views/vistas. Suitable/sufficient landscaping throughout the site will also ensure that the development complements and responds to the existing landscape values.</p>

3.5.2 Element – Aboriginal Cultural Heritage Values	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Jabalbina is registered as the Cultural Heritage Body for the Eastern Kuku Yalanji Peoples' traditional estate under the Aboriginal Cultural Heritage Act 2003.	<p>The subject site is not located within Eastern Kuku Yalanji People's traditional estate.</p>
Strategic Outcome 2 In November 2012, the Australian Government announced the inclusion of the national Indigenous heritage values as part of the existing National Heritage Listing for the Wet Tropics of Queensland.	<p>The subject site is not located within the Wet Tropics World Heritage Area.</p>

3.5.2.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>The cultural heritage values of the Wet Tropics bioregion are protected and enhanced through the implementation of Jabalbina's cultural heritage management plan.</p>	<p>The subject site is not located within the Wet Tropics World Heritage Area.</p>

3.5.3 Element - Biodiversity	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Douglas Shire is internationally recognised as a high biodiversity hot spot. The Shire forms part of two World Heritage listed areas: the forests of the Wet Tropics and the reefs and waters of the Great Barrier Reef. The rainforests contain some of the oldest continually surviving tracts of tropical rainforest in the world and contain many plant and animal species found nowhere else. The Great Barrier Reef is an international icon, known for its abundance and diversity of corals, fish and many other forms of marine life. In addition, the Shire contains significant freshwater biodiversity and wetlands, mangroves and coastal vegetation.</p>	<p>No major adverse impacts on the environment and biodiversity of the region are anticipated to occur as a result of this development.</p> <p>The subject site is not located within the Wet Tropics and Great Barrier Reef World Heritage Areas.</p> <p>Proposed development is located within a significantly developed, urban area. Development has been designed and sited so as to avoid adverse impacts on natural and culturally significant areas.</p> <p>No matters of National, State or Local environmental significance have been identified within the subject site.</p>
<p>Strategic Outcome 2</p> <p>Maintaining and restoring the Shire's biodiversity is important. Not only does biodiversity provide crucial ecosystem services such as clean air and water, it also provides a basis for the Shire's economic prosperity and social well-being. Accordingly, the impacts of human habitation and tourist activity require careful management.</p>	<p>Proposed development is located within a significantly developed, urban area. Development has been designed and sited so as to avoid adverse impacts on natural and culturally significant areas.</p> <p>No major adverse impacts on the environment and biodiversity of the region are anticipated to occur as a result of this development.</p>

3.5.3.1 Specific Outcomes

Specific Outcomes	Applicant Response
Specific Outcome 1 The Shire's biodiversity values and associated ecosystems, including matters of national environmental significance (MNES) and matters of state significance (MSES) , are maintained and protected from incompatible development, managed, and where appropriate enhanced, to restore ecological integrity and habitat connectivity.	<p>Proposed development is located within a significantly developed/urban area. Development has been designed and sited so as to avoid adverse impacts on natural and culturally significant areas.</p> <p>No major adverse impacts on the environment and biodiversity of the region are anticipated to occur as a result of this development.</p> <p>No matters of National, State or Local environmental significance have been identified within the subject site.</p> <p>Notwithstanding this, it is noted that there is mapped wildlife habitat (MSES – threatened (endangered or vulnerable) wildlife and special least concern animals) on land directly adjoining the western property boundary. In part, a small portion of this mapped wildlife habitat intersects with the site's western boundary. Species recorded as being present within the mapped wildlife area include the estuarine crocodile and Pacific Golden Plover. Suitable boundary setbacks and landscaping will be implemented to ensure MSES and MLES are protected.</p>



MSES - Species
Threatened (endangered or vulnerable) wildlife and
special least concern animals

▲ Towns
 --- Main Roads/Highways
 --- Secondary roads
 --- Major waterways
 [Green area with vertical lines] Wildlife habitat (special least concern)
 [Yellow area with diagonal lines] Wildlife habitat (endangered or vulnerable)
 [Red area] Special Use and Plan

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Specific Outcome 2

Development:

- (a) is located in areas that avoids significant adverse impacts on matters of state environmental significance (MSES) and matters of local environmental significance (MLES);
- (b) protects and enhances MSES and MLES; and
- (c) maintains and enhances ecological connectivity.

The proposed development has been sited and designed with respect to matters of state environmental significance (MSES) and matters of local environmental significance (MLES).

It is noted that there is mapped wildlife habitat (MSES – threatened (endangered or vulnerable) wildlife and special least concern animals) on land directly adjoining the western property boundary. In part, a small portion of this mapped wildlife habitat intersects with the site's western boundary. Species recorded as being present within the mapped wildlife area include the estuarine crocodile and Pacific Golden Plover. Suitable boundary setbacks and landscaping will be implemented to ensure MSES and MLES are protected. Given the developed nature of the area, ecological connectivity will be enhanced where possible.

<p>Specific Outcome 3</p> <p>Endangered, vulnerable, and threatened flora and fauna species and habitats are protected</p>	<p>MSES reporting identifies only one vulnerable species as being present within the subject site – the estuarine crocodile. Given that the site is vacant, predominantly clear of vegetation, and does not include any mapped waterways and/or coastal estuaries, it is submitted that the site does not include representative estuarine crocodile wildlife habitat.</p> <p>No major adverse impacts on the environment and biodiversity of the region are anticipated to occur as a result of this development.</p>
<p>Specific Outcome 4</p> <p>Aquatic biodiversity is protected through the minimisation of development impacts on waterway and wetland environments.</p>	<p>The are no waterways and/or wetlands mapped within the subject site. Notwithstanding this, it is noted that the site adjoins the State's Great Barrier Reef Coastal Marine Park, which incorporates Dickson Inlet. Water quality objectives across the site are capable of being managed to ensure that potential or perceived impacts on broader water quality outcomes are mitigated.</p>
<p>Specific Outcome 5</p> <p>The integrity of the region's areas of ecological significance and their contribution to biodiversity is maintained for present and future generations.</p>	<p>No major adverse impacts on the environment and biodiversity of the region are anticipated to occur as a result of this development.</p> <p>The proposed development seeks to avoid areas of ecological significance across the site, giving consideration to areas of ecological significance adjoining the site. Development is to occur within existing cleared and disturbed areas.</p> <p>The site is capable of accommodating management strategies and infrastructure to ensure environmental objectives are achieved.</p>
<p>Specific Outcome 6</p> <p>Weeds and pests, in particular feral pigs and purpose-bred wild hunting dogs, are a significant threat to the region's biodiversity conservation values. Development avoids exacerbating threats and eradicates invasive species.</p>	<p>The site will continue to be managed to avoid and mitigate risks associated with spread of weeds, pests and invasive species.</p>
<p>Specific Outcome 7</p>	<p>Proposed development is located within a significantly developed/urban area. Development has been designed and sited so as to avoid adverse impacts on natural and culturally significant areas.</p>

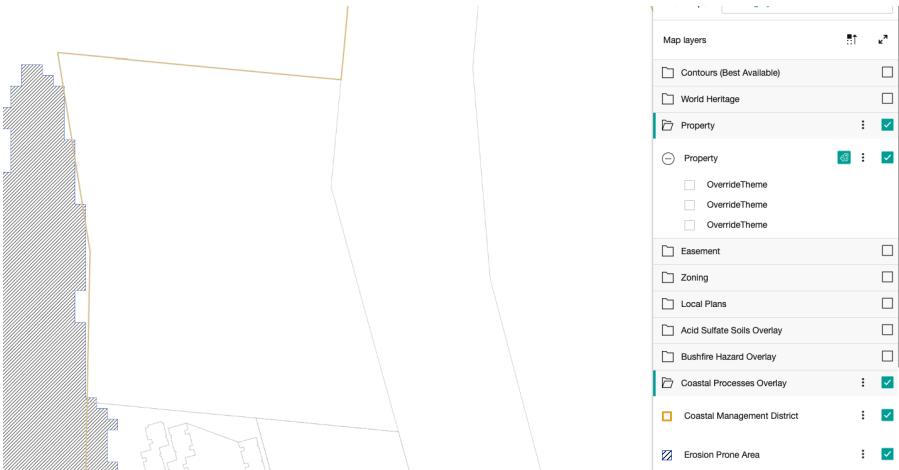
Development is designed, sited, operated and managed to avoid or minimise potential impacts on natural corridors and habitat links.	No natural corridors/habitat links are understood to exist within the site.
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3.5.3.2 Land Use Strategies	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 A Planning Scheme Policy on Environmental offsets is to be developed, in conjunction with the development of mapping for Matters of Local Environmental Significance, including an analysis to determine the best locations to develop the Strategic habitat investment areas	Proposed development is not anticipated to impact on Matters of State and/or Local Environmental Significance.

3.5.4 Element – Coastal Zones	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 With over 95 kilometres of coastline and a vast network of shoreline estuaries, the Shire plays an important role in affording protection to the Coral Sea and the Great Barrier Reef. This extensive area of natural coastline contributes significantly to the desirability and attraction of the Douglas Shire to visitors and residents alike. As is the case with many Australian communities, the coastline provides a strong sense of place. Great care needs to be taken to ensure that competing pressures for recreational use do not diminish these values.	The subject site is located within an urban area and is not located along the coastline.
Strategic Outcome 2 Beyond its natural fluctuations, the coastal environment is particularly sensitive to the effects of climate change and sea level rise. Policies are required to	The site will continue to be managed to avoid and mitigate risks associated with climate change and sea level rise.

protect life and property from the hazards of storm surge and coastal erosion incorporating climate change variables	
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3.5.4.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Coastal resources, including foreshores, coastal wetlands, mangroves, marine ecosystems and sand dunes are protected, managed, and where appropriate, rehabilitated, with no further loss of the coastal ecosystem extent and function.</p>	<p>The subject site is located within an urban area and has been appropriately zoned/earmarked for future tourist accommodation development. The proposed development will not alter coastal processes.</p>
<p>Specific Outcome 2</p> <p>Natural coastal processes such as sand dune erosion and accretion are allowed to occur in all areas without intervention, other than where a significant strategic asset requires protection.</p>	<p>The subject site is located within an urban area and is not located along the coastline. The proposed development will not alter coastal processes.</p>
<p>Specific Outcome 3</p> <p>Waterways and wetlands are managed and maintained to ensure the natural quality and quantity of water delivered to the Coral Sea is not diminished.</p>	<p>There are no waterways and/or wetlands mapped within the subject site. Notwithstanding this, it is noted that the site is located east of Dickson Inlet. Water quality objectives across the site are capable of being managed to ensure that potential or perceived impacts on broader water quality outcomes are mitigated.</p>
<p>Specific Outcome 4</p> <p>Coastal dependent development is carefully planned, constructed and managed to minimise impacts on the coastline. Coastal dependent development is expected to predominately occur with waterfront sub-precincts in Port Douglas.</p>	<p>The proposal does not involve coastal dependent development.</p>

<p>Specific Outcome 5</p> <p>Development maintains and enhances natural processes and the protective function of landforms and vegetation that can mitigate risks associated with the natural hazard;</p>	<p>The proposed development has been sited and designed with respect to natural hazard overlays, to minimise and mitigate risk from hazard events. Development is not anticipated to impact on the existing natural landform and or existing vegetation, given the site is already predominantly clear of vegetation and comprises relatively flat land.</p>
<p>Specific Outcome 6</p> <p>Maintain the erosion prone area within the coastal management district as a development free buffer zone (except for coastal dependent development, temporary or readily relocatable or able to be abandoned development).</p>	<p>The subject site is not located within the Coastal Management District. It is noted however, that the Coastal Management District adjoins the subject site along the northern and western property boundaries. Very small portions of the subject site are mapped as being erosion prone along the western property boundary. Notwithstanding this, development will be suitably setback so as to ensure erosion prone areas mapped within the site are avoided.</p> 
<p>Specific Outcome 7</p> <p>Redevelopment of existing permanent structures in the erosion prone area is required to avoid coastal erosion risks through a strategy of planned retreat, or mitigate coastal erosion risks.</p>	<p>An insignificant portion of the subject site is mapped as being erosion prone along the western property boundary. Notwithstanding this, development will be suitably setback so as to ensure erosion prone areas mapped within the site are avoided.</p>

<p>Specific Outcome 8</p> <p>Land use activity avoids the disturbance of areas of land that are prone to the release of acid sulphate or any other metal contaminant producing soils or adequately manages, treats contaminant producing soils.</p>	<p>The subject site is mapped as containing acid sulfate soils (<5mAHD). Site works will be managed accordingly to ensure that no disturbance to acid sulfate soils occurs, and/or where disturbance occurs, disturbance is managed appropriately to mitigate impacts. More detailed assessments will be undertaken to assess the risk of disturbance to acid sulfate soils.</p>
<p>Specific Outcome 9</p> <p>The community's access to the foreshore is maintained, and where appropriate, extended.</p>	<p>The proposed development will not impact on the community's ability to access to foreshore.</p>

3.5.5 Element – Scenic Amenity	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Douglas Shire is located on a narrow coastal plain between the Great Dividing Range and the Coral Sea. The lush green backdrop provides a dramatic green frame to the Shire's diverse tourist areas, townships, rural lands and rural communities. This green frame adds significantly to the Shire's much admired high quality scenic amenity. In addition, the sugar cane fields contribute significantly to the Shire's scenic amenity and sense of place on the plains below the green frame. Great care is required to protect the Shire's natural and rural settings given the vital contribution they make to the Shire's identity and to the everyday experiences of residents and visitors alike.</p>	<p>The site does not provide landscape scenic value. Notwithstanding this, it is noted that the eastern portion of the site is mapped as being within the scenic route buffer area. The scenic value will not be negatively impacted or affected by development proposed, in circumstances whereby the site:</p> <ul style="list-style-type: none"> • Will be suitably landscaped equivalent to those tourist developments north and south of the site, along Port Douglas Road. • Landscaping will enhance and complement the existing 'landscape character' experienced by motorists driving along Port Douglas Road. • The built form and scale of development has been appropriately designed so as to ensure that green vistas through the site are maintained. • Building bulk and mass, along the primary road frontage (Port Douglas Road), will be reduced and built form will be broken up by establishing the development footprint within four (4) separate buildings. <p>There are unlikely to be detrimental impacts arising from this development. Inappropriate vegetation clearing is not contemplated nor proposed.</p>

3.5.5.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Development protects, maintains and enhances the region's Landscape values as shown on the Landscape values overlay maps contained in Schedule 2</p>	<p>The site is not mapped as containing landscape scenic values.</p>
<p>Specific Outcome 2</p> <p>Major scenic routes and scenic outlooks, as shown on the Landscape values overlay maps contained in Schedule 2, are protected from both the detrimental visual impacts of development and inappropriate vegetation clearing that may detract from the scenic qualities of the scenic route, outlook or ocean-side views.</p>	<p>The eastern portion of the site is mapped as being within the scenic route buffer area. Scenic values will not be negatively impacted or affected by development proposed, in circumstances whereby the site:</p> <ul style="list-style-type: none"> • Will be suitably landscaped equivalent to those tourist developments north and south of the site, along Port Douglas Road. • Landscaping will enhance and complement the existing 'landscape character' experienced by motorists driving along Port Douglas Road. • The built form and scale of development has been appropriately designed so as to ensure that green vistas through the site are maintained. • Building bulk and mass, along the primary road frontage (Port Douglas Road), will be reduced and built form will be broken up by establishing the development footprint within four (4) separate buildings. <p>There are unlikely to be detrimental impacts arising from this development. Inappropriate vegetation clearing is not contemplated nor proposed. The proposed design will achieve a high standard of amenity.</p>

Specific Outcome 3 Rural and inter-urban breaks are protected from negative visual intrusion.	The subject site is not located in a rural and/or inter-urban break setting.
Specific Outcome 4 The hillslopes, including Flagstaff Hill, are retained as a valued scenic backdrop to the Shire and the landscape character of the hillslopes are protected from inappropriate development.	Views of Mowbray National Park (south/south-west of the site) may be enhanced with the increased height of buildings proposed on site and the creation of better vantage points.
Specific Outcome 5 The location, size and scale of advertising devices located along the Shire's road networks is reduced. Older off-premises advertising devices that have become dilapidated are removed and not replaced	Where required, advertising devices will be installed in accordance with relevant guidelines.

3.5.6 Element – Air and Acoustic Protection and Hazardous Materials	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Other than the Mossman Sugar Mill and some of the marine industries in Dickson Inlet at Port Douglas, there are no areas of land devoted to the heavier forms of industry that would generate significant air or acoustic problems. As a result, it is possible that new industrial development may present future challenges. The key management approach in planning is to separate sensitive land uses from generators of nuisance. Given the historical development of the Shire (i.e. the Mossman sugar mill and port industries), this is not always practical and mitigation measures need to be implemented as an alternative.	The proposal does not involve any industry land use.

3.5.6.1 Specific Outcomes

Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>The air and acoustic environment and hazardous materials are carefully managed to maintain the health and well-being of the community and the natural environment.</p>	<p>The proposed development has been sited and designed with respect to the amenity, form, and use of the surrounding area. Hazardous materials are unlikely to be stored on site.</p>
<p>Specific Outcome 2</p> <p>Industries that have the potential to cause greater air and acoustic impacts and/or that include hazardous materials are separated from sensitive land uses.</p>	<p>Industry uses are not proposed.</p>
<p>Specific Outcome 3</p> <p>New noisy recreational activities such as major motorsport activities are not likely to be compatible with the amenity of the Shire. Impacts on sensitive receiving environments, including environmental habitats is to be avoided.</p> <p>Note – Air and acoustic impacts are avoided and/or mitigated to comply with appropriate standards, Environmental Protection (Air) and Environmental Protection (Noise) Policy standards.</p>	<p>New noisy recreational activities are not proposed.</p>

3.6 Theme 3 – Natural Resource Management

3.6.1 Strategic Outcomes	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 The natural resources of the Shire, such as agricultural land, and in particular land suitable for sugar cultivation, forestry, water, fisheries and extractive resources are protected or managed in a sustainable manner to ensure their ecological and economic values are assured.	The proposed development will not require the use of / or interference with the Shire's natural resources. The subject site is located within an existing urban area.
Strategic Outcome 2 Natural resources are well managed through federal, state, local government, regional resource management bodies, traditional owners and the community.	The proposed development will not require the use of / or interference with the Shire's natural resources. The subject site is located within an existing urban area.
Strategic Outcome 3 Extractive activities do not unduly compromise natural landscape values.	Extractive Industry is not proposed.

3.6.2 Element – Land and Catchment Management	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Interaction with the natural environment directly and indirectly impacts on the community's lifestyle and standard of living. Land and water degradation issues facing the Shire include soil erosion, pest plants and animals and water quality. It is important to ensure that the Shire's natural resources are protected and actively promoted for the benefit of current and future generations.	The site is located east of Dickson Inlet. Water potentially discharged from or flowing through the site will be managed to ensure that sediment and nutrient movement is appropriately managed, avoiding impacts on ultimate receiving waters. It is noted that the Mirage Golf Course is located between the proposed site and the receiving waters of Dickson Inlet. No major adverse impact on the ecological processes, water quality, and any riparian ecological values of Dickson Inlet is anticipated to occur as a result of this development. Additionally, it is noted

	that the site is capable of accommodating management strategies and infrastructure to ensure water quality objectives are achieved.
<p>Strategic Outcome 2</p> <p>The Shire consists of a range of land tenures from national parks through to freehold land and leasehold properties. Improved management practices are required to prevent overuse or degradation of natural resources. A significant part of this management relies on the three levels of government as well as regional resource management bodies, traditional owners, landowners and the community using statutory and non-statutory means. This planning scheme focusses primarily on the significant role of land use planning. However, the presence of other non-planning scheme programs such as pest management and riparian rehabilitation programs is acknowledged.</p>	<p>The site is located within an existing urban environment and is not within proximity to protected area estate. The proposed development is consistent with the existing surrounding land uses.</p>

3.6.2.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Creeks, wetlands and the sea are an important resource for the community. The Shire's water is sustainably managed in accordance with total water cycle management principles to maintain the ecological health of the Shire's ecosystems. Development and land uses will not be permitted to cause pollution or other negative impacts on water quality or the other qualities of these essential environments. Development is to facilitate the protection of environmental values and achieve water quality objectives for Queensland waters.</p>	<p>The site is located east of Dickson Inlet. Water potentially discharged from or flowing through the site will be managed to ensure that sediment and nutrient movement is appropriately managed, avoiding impacts on ultimate receiving waters. It is noted that the Mirage Golf Course is located between the proposed site and the receiving waters of Dickson Inlet. No major adverse impact on the ecological processes, water quality, and any riparian ecological values of Dickon Inlet is anticipated to occur as a result of this development. Additionally, it is noted that the site is capable of accommodating management strategies and infrastructure to ensure water quality objectives are achieved.</p>

<p>Specific Outcome 2</p> <p>Water catchments and ground water aquifers are recognised and protected from inappropriate land use activity that may compromise the quality and characteristics of the water resource. In particular water needs to be managed to give priority to agricultural production and water supply to the Shire's communities, rather than for other purposes such as bottled-water extraction.</p>	<p>Water quality objectives across the site are capable of being managed to ensure that potential or perceived impacts on broader water quality outcomes are mitigated.</p> <p>The proposed development is characteristic of the intended future use and development of the area, which is zoned tourist accommodation. Development is not expected to impact on the quantity/quality of existing water resources.</p>
<p>Specific Outcome 3</p> <p>Water quality, in-stream and riparian waterway values, and the nature-based recreation values of modified waterways, and wetlands and their catchments are protected from harmful activities.</p>	<p>Water quality objectives across the site are capable of being managed to ensure that potential or perceived impacts on broader water quality outcomes are mitigated.</p> <p>The proposed development will not result in harmful activities likely to impact on water quality.</p>
<p>Specific Outcome 4</p> <p>Development is planned, designed, constructed and operated to manage stormwater in ways that help protect the environmental values of waters including the biodiversity and functioning of the aquatic ecosystem.</p>	<p>Stormwater management can and will be incorporated to ensure that increases in impact up or downstream and/or on neighbouring properties do not occur, and that important water quality objectives of aquatic ecosystems can be achieved.</p>
<p>Specific Outcome 5</p> <p>The construction of dams on waterways does not detrimentally affect environmental flows, safety, or the amount of water available to downstream users</p>	<p>Construction of a dam is not proposed.</p>
<p>Specific Outcome 6</p> <p>Water quality and its physical and chemical properties are maintained and managed to avoid and reduce the conveyance of sediment and nutrients into natural hydrological systems.</p>	<p>Water potentially discharged from or flowing through the site will be managed to ensure that sediment and nutrient movement is appropriately managed, avoiding impacts on ultimate receiving waters.</p>

<p>Specific Outcome 7</p> <p>Traditional ecological land management practices and knowledge is recognised and acknowledged</p>	<p>Where required, regard will be given to traditional ecological land management practices and knowledge.</p>
<p>Specific Outcome 8</p> <p>Weeds and pests are managed to ensure that natural resources are not degraded.</p>	<p>The site will continue to be managed to avoid and mitigate risks associated with spread of weeds, pests and invasive species.</p>

3.6.3 Element – Primary Productions, Forestry and Fisheries	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Primary production is a significant part of the natural resource and landscape value of the Shire. The Shire's townships and villages rely heavily on their surrounding sugar cultivation, tropical agriculture, horticulture and grazing industries. In particular, the primacy of the sugar cultivation will continue as a dominant rural pursuit in the Shire due to the critical importance of the sugar industry to the Shire. Continued support is necessary to ensure the continued prosperity of these townships through the exclusion of non-agricultural land use activities in primary production areas.</p>	<p>Primary production activities are not proposed.</p>
<p>Strategic Outcome 2</p> <p>Forestry is not a prevalent activity in the Shire and where it occurs; it tends to be small scale production that is compatible with the Shire's rural production area. Large scale timber production is not a compatible land use within the Shire.</p>	<p>Forestry production is not proposed.</p>

Strategic Outcome 3	
The importance of the Shire's key fisheries activities are recognised and are protected.	Proposed development is not anticipated to impact on existing nearby fisheries activities currently occurring within Dickson Inlet.

3.6.3.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 The viability of agricultural land is protected and maintained. Land uses that have the potential to conflict with on-going primary production are not established in rural areas	The subject site is located within an existing urban area and not anticipated to impact on existing agricultural land.
Specific Outcome 2 Rural areas include a range of rural activities of varying scale depending on land suitability and access to appropriate infrastructure, with development being consistent with prevailing land uses in the area.	The subject site is not located within a rural area.
Specific Outcome 3 Lot reconfiguration does not result in the further fragmentation of rural land. Boundary realignments only occur where improved agricultural production or environmental protection outcomes are demonstrated, or where they resolve encroachments.	The proposal is not for the reconfiguration of a lot.
Specific Outcome 4 Timber plantations and forestry production are small in scale and do not compromise the predominant pattern of primary production in the Shire.	Timber plantation and/or forestry production is not proposed.

<p>Specific Outcome 5</p> <p>Estuarine, marine and freshwater environments and their associated fish habitats are protected from the adverse impacts generated by new or intensified developments to ensure healthy and plentiful fish stocks.</p>	<p>The site is located east of Dickson Inlet. Water potentially discharged from or flowing through the site will be managed to ensure that sediment and nutrient movement is appropriately managed, avoiding impacts on ultimate receiving waters. It is noted that the Mirage Golf Course is located between the proposed site and the receiving waters of Dickson Inlet. No major adverse impact on the ecological processes, water quality, fish habitat values and fish stocks of Dickon Inlet is anticipated to occur as a result of this development. Additionally, it is noted that the site is capable of accommodating management strategies and infrastructure to ensure water quality objectives are achieved.</p>
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3.6.4 Element – Resource Extraction	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Douglas Shire is not, and is not likely to be, a major resource extraction area. However, where extraction does occur, it is small-scale, visually unobtrusive and managed in an environmentally responsible manner. All land disturbed by mining and extractive industries is appropriately rehabilitated.</p>	<p>Resource extraction is not proposed.</p>

3.6.4.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Coal Seam Gas ('CSG') extraction does not occur in Douglas Shire</p>	<p>Coal seam gas extraction is not proposed.</p>
<p>Specific Outcome 2</p>	<p>Extractive industries are not proposed/do not form part of this proposal.</p>

Other extractive industries do not detrimentally impact on community well-being or the Shire's ecological, landscape, scenic amenity and rural production values, and in particular the sugar industry.	
Specific Outcome 3 Extractive industries are either progressively rehabilitated to a natural state, or are rehabilitated upon completion of extractive activities, with all buildings, machinery and other associated infrastructure being removed.	Extractive industries are not proposed/do not form part of this proposal.

3.7 Theme 4 – Strong Communities and Identity

3.7.1 Strategic Outcomes	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Places of cultural and heritage significance, both Indigenous and non-Indigenous, are identified, protected and retained for their significance and importance to the history and identity of the Shire.	The subject site is not identified as having cultural and/or heritage significance.
Strategic Outcome 2 The distinctive character and unique sense of place of the Shire's towns, villages and other settlement areas are maintained, promoting community pride and well-being and community safety and prosperity.	The subject site is located within an existing urban area, within the township of Port Douglas. Port Douglas is characterised by lush landscaping that complements the tropical resort town atmosphere. The proposed development will reflect qualities of good urban design and enhance the amenity of the streetscape, whilst maintaining/contributing the existing character of the area.
Strategic Outcome 3 Residential communities, particularly communities within the major tourism areas of Port Douglas, Daintree Village and the Daintree Lowlands maintain a	Although not residential development, the proposed development is anticipated to support and promote a sense of community and harmony between residents and visitors. Residents will be provided with opportunities to access and use the ancillary features, forming part of the proposed Fairmont Resort. This will include access to the resort's pools, restaurants, bars and function facilities.

prosperous economy, a sense of community and promote harmony between residents and visitors.	
Strategic Outcome 4 Development is managed to enhance the character and identity of existing and future communities, providing services, facilities, parks, recreation areas, places to work, live and play and to celebrate culture, history and identity.	The subject site is located within an existing urban area, within the township of Port Douglas, which is characterised by lush landscaping that complements the tropical resort town atmosphere. The proposed development will be appropriately managed to ensure the enhancement of the town's existing character. The proposed development includes a number of new/ancillary facilities/services (i.e. café, bar, restaurants, function facility) that members of the community will be able to use. The proposed development is expected to add to the identity of the area and to create an additional/different experience for visitors and/or residents to enjoy. The proposed development is also expected to generate employment opportunities and positively contribute to the existing economy.
Strategic Outcome 5 Progress will be made working with Aboriginal and Torres Strait Islander peoples and the wider community, to help remove social and economic disadvantage.	As explained above, proposed development is expected to generate employment opportunities and contribute to the existing economy. These potential job opportunities will be extended to Aboriginal and Torres Strait Islander peoples and the wider community.
Strategic Outcome 6 Equitable access is provided to a complete range of services, open space, active and passive recreation, employment, accommodation and entertainment.	The proposed development will support equitable access to a range of services, passive and active recreation, employment, accommodation and entertainment. Residents will be provided with opportunities to access and use the ancillary features, forming part of the proposed Fairmont Resort. This will include access to the resort's pools, restaurants, bars and function facilities. Accommodation provided on site will be available to residents and tourists alike. The proposed development is also expected to generate employment opportunities and positively contribute to the existing economy.

Strategic Outcome 7 The conservation and adaptive re-use of places of heritage significance is facilitated.	The subject site has no heritage significance.
Strategic Outcome 8 Residents have access to appropriate social infrastructure, community services and adequate housing, including a greater variety of housing choice	The proposal seeks approval to construct a resort complex on privately owned, freehold land that is included within the tourist accommodation zone. The proposed development is not anticipated to impact on the availability of land that may provide for alternative social infrastructure, community services and adequate housing, including a greater variety of housing choice.

3.7.2 Element – Social Planning and Infrastructure	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 There is a need to identify and provide for a range of social, cultural, educational, leisure and health services to meet the needs of the community, including youth, families and the elderly. Adequate provision of social infrastructure assists in sustaining the Shire's population and attracting new residents to the Shire. The correlation between the well-being of the community and the economic prosperity of the Shire is recognised	The proposal seeks approval to construct a resort complex on privately owned, freehold land that is included within the tourist accommodation zone. The proposed development is not anticipated to impact on the availability of land that may provide for a range of new/alternative social infrastructure.

3.7.2.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 The unique characteristics of individual communities are recognised and respected.	The unique character of Port Douglas, being a tropical tourist destination, has been recognised and respected. The proposal seeks approval to construct a resort complex on land included within the tourist accommodation zone. The proposed development is expected to contribute to the unique character of Port

	Douglas by offering new/alternative resort style accommodation and ancillary uses within the area.
Specific Outcome 2 Future planning of the Douglas Shire takes into account changing community characteristics, issues and needs.	The proposal is not expected to impact on future planning processes and is considered to be reflective of the current community characteristics and aspirations.
Specific Outcome 3 Access is available to a range of community services for all residents and is distributed on an equitable basis.	Community services are not proposed.
Specific Outcome 4 Social infrastructure is clustered into a series of co-located Community hubs and with Activity centres as shown on the Strategic framework maps contained in Schedule 2 for ease of accessibility	Social infrastructure is not proposed.

3.7.3 Element – Active Communities	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Planning in the Shire will play an increasingly important role in maintaining healthy communities. Given an increasing awareness of the need to encourage healthy active lifestyles, walkable and cycle-friendly neighbourhoods will be promoted. The Shire is developing a network of inter-connected and accessible open space and green corridors, and this network will be progressively expanded to	The subject site is located within an existing urban area. The site will have direct access to existing public transport networks, including public footpaths and cycle paths, to promote healthy active lifestyles.

encourage opportunities for recreation, while recognising an increasing trend toward self-directed, non-organised sporting activity.	
Strategic Outcome 2 Better use of the Shire's outdoor spaces requires careful urban design to ensure that there is a high perception of personal safety. Highly visible and well-shaded parks and town spaces will be established to encourage dynamic well-used public spaces.	The proposed development will reflect qualities of good urban design and will enhance the casual surveillance of adjoining public areas.

3.7.3.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 The Shire is an active community, providing opportunities for residents to make healthier life choices. Planning and development will make provision for open space, recreation areas and safe walking and cycling paths.	The subject site is located within an existing urban area. The site will have direct access to existing public transport networks, including public footpaths and cycle paths, to promote healthy active lifestyles. The subject site is surrounded by land included within the recreation and open space zone, which incorporates the Mirage Port Douglas Golf Course. Proximity to existing recreational activities is also considered to contribute to the Shire's desire create/promote an active community.
Specific Outcome 2 Sufficient land is available and distributed across the Shire's communities to cater for the development of parks and sport and recreation facilities. Open space areas and sporting and recreational facilities are provided to: <ul style="list-style-type: none"> (a) ensure a diverse range of recreational opportunities. (b) encourage multi-purpose spaces and hubs for recreation. (c) facilitate a recreational trail network, particularly along rivers and creeks. 	The proposal seeks approval to construct a resort complex on privately owned, freehold land that is included within the tourist accommodation zone. The proposed development is not anticipated to impact on the availability of land that may be used to establish new/additional parks and sport and recreation facilities.

<p>(d) form networks of interlinked connections.</p> <p>(e) promote and contribute to the green space character of the Shire.</p> <p>(f) promote a degree of flexibility in the use of open space for other community purposes such as community gardens.</p>	
<p>Specific Outcome 3</p> <p>New publically accessible and usable open space is provided in new development where appropriate. Open space will be the centre piece of new communities contributing to the Shire's overall network of parkland, sporting and recreational facilities and open space corridors.</p>	<p>The proposal seeks approval to construct a resort complex on privately owned, freehold land that is included within the tourist accommodation zone. The proposed development is not anticipated to impact on the availability of land that may be used to establish new/additional parks and sport and recreation facilities. Notwithstanding this, the proposed development will include some publicly accessible and useable open space/recreational facilities (public swimming pools).</p>
<p>Specific Outcome 4</p> <p>Neighbourhoods are to be accessible and safe, providing opportunities for social interaction, participation and improved well-being. In particular, open space is highly visible, being located along road frontages and not placed to the rear of new allotments.</p>	<p>The proposed development is to be constructed in a developed area with existing open space and opportunities for social interaction.</p>
<p>Specific Outcome 5</p> <p>The number of tropical shade producing canopy trees of appropriate species is increased.</p>	<p>The proposed development will include landscaping, which is expected to increase the number of tropical shade producing canopy trees.</p>

3.7.4 Element – Sense of Place, Community and Identity	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Sense of place is the dual characteristic that derives from certain distinctive features of a place and the feelings and perceptions that people hold with respect to that place. Fostering a good sense of place promotes community attachment. New development in the Shire should avoid the delivery of homogeneous development consisting of identical communities. In growth areas, gated enclave communities will not be encouraged. The larger new developments will be supplied with a range of facilities to support new residents and to ensure that these new developments are a good place to live.</p>	<p>The proposed development is infill development. The subject site is located in an existing urban/developed area, with an existing sense of place. The proposed development has been appropriately sited and designed so as to complement this existing sense of place.</p>
<p>Strategic Outcome 2</p> <p>Many areas in the Shire have high quality character that derives from their buildings, pattern of subdivision and street layout. The Shire's older and traditional character is derived from the wide street, grid layout, tin and timber housing and vegetation. Intact commercial precincts consisting of shops, churches, halls, hotels, post offices, banks, hospitals and the like are recognised and will be retained.</p>	<p>The proposal seeks to construct a resort complex on vacant land, included within the tourist accommodation zone. The proposed development is expected to contribute to the unique character of Port Douglas by offering new/alternative resort style accommodation and ancillary uses within the area.</p>
<p>Strategic Outcome 3</p> <p>New development will not be characterised by off-the-shelf corporate designs that have little regard for the Shire's unique communities and sense of place, in order to ensure the experience of living and visiting the Shire is not diminished.</p>	<p>The unique character of Port Douglas, being a tropical tourist destination, has been recognised and respected. The proposal seeks approval to construct a resort complex on privately owned, freehold land that is included within the tourist accommodation zone. The proposed development is expected to contribute to the unique character of Port Douglas by offering new/alternative resort style accommodation and ancillary uses within the area.</p>

3.7.4.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 The distinctive character and unique sense of place of towns and villages and other communities, including Aboriginal communities, throughout the Shire is maintained, promoting community pride and well-being and community safety and prosperity.	The unique character of Port Douglas, being a tropical tourist destination has been recognised and respected. The proposal seeks approval to construct a resort complex on land included within the tourist accommodation zone. The proposed development is expected to contribute to the unique character of Port Douglas by offering new/alternative resort style accommodation and ancillary uses within the area.
Specific Outcome 2 New communities in the Shire are established with a recognisable and distinctive sense of place.	Development of a new community is not proposed.
Specific Outcome 3 Through site specific tropical design, franchise corporate designs are adapted to integrate with established urban qualities that make the Shire's local communities unique and distinct from other places.	The proposal does not incorporate franchise corporate designs.
Specific Outcome 4 Gated communities that offer little or no integration with surrounding and adjacent communities are not the preferred pattern of development in the Shire.	No gated community is proposed.
Specific Outcome 5	Construction of important buildings does not form part of this proposal.

Important buildings in the community are designed to be readily recognisable for their purpose and use.	
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3.7.5 Element – Housing Choice and Affordability	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Douglas Shire contains a range of housing types that contribute to the lifestyle opportunities and attractiveness of the Shire as a place to work, live and visit. However, for the long-term sustainability of the Shire, it is important that adequate opportunities are provided to accommodate the changing lifestyles and housing needs of the existing and future population. In particular, there is a need to cater for an ageing population, while at the same time providing affordable housing opportunities for the Shire's workforce.	Residential development is not proposed
Strategic Outcome 2 Currently, outside Port Douglas, there is a lack of innovative and creative medium density housing development. Opportunities exist to encourage this type of development in appropriate locations in Mossman, without compromising existing local character.	The subject site is located within Port Douglas.

3.7.5.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1	Residential development is not proposed

The community has access to a greater variety of housing choice throughout the urban areas of the Shire, facilitating a variety of lot sizes, house sizes and designs and land tenures.	
Specific Outcome 2 An increase in housing diversity is encouraged in appropriate locations while maintaining the existing character and identity of individual communities.	Residential development is not proposed
Specific Outcome 3 Broad-acre land developments are established with mixed lot sizes and densities of development.	Residential development is not proposed
Specific Outcome 4 The development of appropriate Indigenous Housing or Bayan is recognised as an affordable housing choice and alternative living option on Aboriginal Freehold Land.	Residential development is not proposed

3.7.6 Element – Arts and Culture	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Douglas Shire has an abundance of community-based festivals and events held throughout the year. The Shire attracts a broad cross-section of creative and spirited people that place art and culture at the centre stage. Port Douglas will continue to be a focus for creative activity, adding to the town's liveliness and its	The proposal is expected to provide new/alternative accommodation options to visitors attending and participating in various community-based festivals and events that occur in Port Douglas.

economic buzz. Port Douglas will also be supported by Shire- wide, smaller-scale dispersed cultural activities in local towns, villages and communities.	
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3.7.6.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 Tourist and entertainment areas are planned and developed in appropriate parts of the Shire with a particular focus in Port Douglas. It is recognised that in these areas lively activity contributes significantly to its vibrant tourist-town amenity.	The proposal seeks approval to construct a resort complex on land included within the tourist accommodation zone, within Port Douglas.
Specific Outcome 2 Where appropriate, the public realm is promoted as a place for public art, cultural activities, events, markets and festivals.	The subject site involves freehold land and is not within the public realm.
Specific Outcome 3 Appropriately scaled art and cultural infrastructure is provided in new development.	No art or cultural infrastructure is proposed or required as a result of this development. Notwithstanding this, further detailed design may incorporate appropriately scaled art and cultural infrastructure.

3.7.7 Element – Cultural and Landscape Heritage	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Douglas Shire has many distinctive and important places that give strength to local character. Cultural heritage significance for different cultures and the	Development has been sited and proposed so as to avoid adverse impacts on natural and culturally significant areas.

importance of preserving Indigenous, non-Indigenous, natural and cultural heritage is recognised.	
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3.7.7.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Places and landscapes of cultural and heritage significance are protected and retained for their significance and importance to the history and identity of the Shire.</p>	<p>Development has been sited and proposed so as to avoid adverse impacts on natural and culturally significant areas. The subject site has not been identified as having cultural and/or heritage significance.</p>
<p>Specific Outcome 2</p> <p>Development takes into account:</p> <ul style="list-style-type: none"> (a) the location and cultural heritage significance of world heritage properties, national heritage places and the requirements of the Environment Protection and Biodiversity Conservation Act 1999; (b) the location and cultural heritage significance of Queensland heritage places (c) identification of local heritage places and areas. 	<p>The subject site is not considered to have world, national and/or local heritage significance. The subject site is not included on Queensland's heritage register.</p>
<p>Specific Outcome 3</p> <p>Development in or on a local heritage place or area avoids or otherwise minimise adverse impacts on the heritage significance of the place or area and does not compromise the cultural heritage significance of the place or area.</p>	<p>The subject site is not identified as being a local heritage place or area, pursuant to the planning scheme overlay mapping.</p>

<p>Specific Outcome 4</p> <p>Adaptive reuse of places of cultural heritage significance occurs where the use does not harm the qualities and significance of the place or area.</p>	<p>The subject site includes vacant land and is undeveloped. It is understood that the site has no cultural heritage significance.</p>
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3.7.7.2 Land Use Strategies	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>A Douglas Heritage Study was prepared by Cairns Regional Council in 2011. The purpose of this study was to create a local heritage register to meet the requirements of Part 11 of the Queensland Heritage Act 1992. The associated consultant's report, nominated 47 places for inclusion in the local heritage register (excluding 8 places of the State heritage register), an additional 16 as potential places for inclusion in the local heritage register and rejected a further 41 places as not meeting the threshold for inclusion in the local heritage register.</p>	<p>The subject site is not included on the local heritage register, created as part of the Douglas Shire Heritage Study, prepared in 2011.</p>
<p>Strategic Outcome 2</p> <p>At this point in time, there has been no further verification of any of the nominated places listed in the register following initial public notification, as to the list's accuracy and there is a degree of uncertainty with respect to some places, as outlined in the consultant's report. Prior to including Places of Significance list as planning scheme policy in the planning scheme, further verification is required with respect to the list. This will require an independent review. In addition to being a listing of nominated sites such review should also examine positive incentives for listing in the planning scheme</p>	<p>The subject site is not included on the local heritage register, created as part of the Douglas Shire Heritage Study, prepared in 2011.</p>

<p>Strategic Outcome 3</p> <p>Jabalbina and Douglas Shire Council will work collaboratively to develop a management plan aimed at identifying, protecting and retaining matters of Aboriginal cultural heritage significance.</p>	<p>It is understood that the site has no Aboriginal cultural heritage significance.</p>
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3.7.8 Element – Strengthening Indigenous Communities	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>The Shire has the significant, discrete Indigenous community of Wujal Wujal on its northern boundary, adjacent to the Bloomfield River. The Wujal Wujal Aboriginal Shire Council has developed its own planning scheme for its community. However, this community and its people will also continue to have influence outside their own local government boundary, through the implementation of the Eastern Kuku Yalanji people's Indigenous Land Use Agreement (the 'ILUA') and aspirations to 'return to country' in the northern part of Douglas Shire. Also of particular note is the relatively small Mossman Gorge Community, to the west of Mossman, and the nearby Indigenous eco-tourism development – the Mossman Gorge Centre.</p>	<p>The subject site is not affected by the Eastern Kuku Yalanji people's Indigenous Land Use Agreement and is not located within the Return to Country Local Plan Area.</p>
<p>Strategic Outcome 2</p> <p>The on-going development of the Return to Country Local Plan seeks to address social and economic disadvantage, environmental management of lands and waters, cultural heritage protection and management, the relationship between Jabalbina and Douglas Shire Council and economic development opportunities for the eastern Kuku Yalanji people.</p>	<p>The subject site is not located within the Return to Country Local Plan Area</p>

3.7.8.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Traditional cultural landscapes and significant places are protected in partnership with traditional owners of those landscapes and places.</p>	<p>Development has been designed and sited so as to avoid adverse impacts on natural and culturally significant areas. The subject site has not been identified as having cultural and/or heritage significance.</p>
<p>Specific Outcome 2</p> <p>Traditional knowledge, skills and cultural practices are respected and, where appropriate, reflected in land management decision making, location and in the form of infrastructure and design of community spaces.</p>	<p>Traditional knowledge, skills and cultural practices are respected, where appropriate.</p>
<p>Specific Outcome 3</p> <p>Sites of cultural significance, including sites with cultural significance for clan groups, and sites with broader significance for the community are protected.</p>	<p>The subject site has not been identified as having cultural and/or heritage significance.</p>
<p>Specific Outcome 4</p> <p>Council will continue to support and work with Traditional custodian / Owner groups to progress their ability to care for, work on and live on Country, and create economic development opportunities, particularly within the urban footprint of Mossman Gorge and the good bush-living precincts in certain areas north of the Daintree River.</p>	<p>The subject site is located within a developed area and has not been identified as having cultural and/or heritage significance.</p>

3.8 Theme 5 – Economy

3.8.1 Strategic Outcomes	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>A prosperous community with a strong rural sector, a dynamic tourism industry and commercial and industrial activities offering a diverse range of employment opportunities is supported by the sustainable use and management of the Shire's natural resources.</p>	<p>The proposal seeks approval to construct a resort complex on private freehold land that is included within the tourist accommodation zone, within the township of Port Douglas. The proposed development is expected to contribute to the unique character of Port Douglas, by offering new/alternative resort style accommodation and ancillary uses within the area – enhancing the existing dynamic tourism industry. The proposed development is expected to generate a number of employment opportunities as well as positively contribute to the local economy.</p>
<p>Strategic Outcome 2</p> <p>The Shire's economic base will be broadened to improve employment opportunities and to provide resilience to any future adverse economic, social and environmental conditions.</p>	<p>The proposal involves a six-star luxury resort complex. It is noted that there are currently no six star rated tourist resorts existing within Port Douglas. In considering this, the proposal is expected to contribute to broadening the Shires economic base – specifically within the tourism sector.</p>
<p>Strategic Outcome 3</p> <p>Economic benefits are maximised through the promotion of appropriate land uses, minimisation of land use conflicts and the protection of strategic economic infrastructure.</p>	<p>The proposal seeks approval to construct a resort complex on private freehold land that is included within the tourist accommodation zone, within Port Douglas. The proposed resort complex will not give rise to conflict with surrounding land uses, which comprise of other tourist related development.</p>
<p>Strategic Outcome 4</p> <p>Clustering and co-location of land uses achieve synergies that utilise existing and planned infrastructure and provide opportunities for growth.</p>	<p>The proposal involves infill development, within a significantly developed area, currently utilised for tourist related activities and accommodation options.</p>
<p>Strategic Outcome 5</p>	<p>Development has been designed and sited so as to avoid adverse impacts on natural and culturally significant areas.</p>

The natural values and key lifestyle attributes of the Douglas Shire are protected to maintain its regional competitive advantage.	
Strategic Outcome 6 Higher education facilities are encouraged in the Shire to broaden the economic base, increase employment, inject research and innovation into rural/environment/marine industries.	Higher education facilities are not proposed.
Strategic Outcome 7 Indigenous economic and social disadvantage is reduced over time through carefully planned development of traditional lands.	The subject site has not been recognised as traditional lands.

3.8.2 Element – Economic Growth and Diversification	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 The Mossman Mill, supporting the sugar industry, has been in operation for over 120 years and is the primary economic driver in the Shire. Elsewhere, Douglas Shire is not characterised by a significant industrial base to support its primary economic drivers of tourism and primary production. However, value adding manufacturing and processing, research and education can play an important role in the future of economic growth opportunities in the Shire. The lower operational costs for businesses and the unique lifestyle that the Shire offers should be promoted to attract new employment opportunities and economic growth.	Diversified industrial uses are not proposed.

3.8.2.1 Specific Outcomes

Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Economic growth that supports clean, green businesses and resilient communities is encouraged throughout the Shire. In particular a range of economic initiatives is facilitated in appropriate locations, including:</p> <ul style="list-style-type: none">(a) the growth of new and traditional industries;(b) further development of Port Douglas as a premium tourist destination;(c) establishing Douglas Shire as a player in the global tropical tourist market;(d) targeting infrastructure that strengthens Douglas Shire as a tourist destination and a gateway to the region;(e) promoting the Shire's marine industries;(f) providing for higher value jobs, particularly for young people;(g) focussing on education and knowledge-based industries;(h) taking advantage of the digital age and associated digital economy;(i) protecting the assets on which existing and future business relies, such as agricultural land resources and the beautiful natural environment;(j) enhanced economic opportunities through appropriate development of Aboriginal Freehold Land.	<p>The proposal seeks approval to construct a resort complex on land private freehold that is included within the tourist accommodation zone, within Port Douglas. As such, it is submitted that the proposed development will likely strengthen and enhance the Douglas Shire as a tourist destination.</p>
<p>Specific Outcome 2</p> <p>Business development and innovation are supported in appropriate settings, particularly within the Shire's activity centres and employment clusters.</p>	<p>The subject site is not located within the Shire's activity centres and/or employment clusters.</p>

3.8.3 Element - Tourism	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Tourism is a significant employer and generator of economic activity within the Shire. The Shire boasts two of the State's major icons, the World Heritage Wet Tropics and the Great Barrier Reef. The quality of the Shire's natural and rural landscapes, spectacular mountains, dramatic bluffs, outstanding scenic coastal areas and environmental features, and numerous historic and culturally significant sites are a major tourism drawcard.</p>	<p>The proposal involves tourist related development, which will add to/enhance the existing tourism industry.</p>
<p>Strategic Outcome 2</p> <p>The extent, range and configuration of tourist accommodation, facilities and services is based on the intrinsic and distinctive qualities of the Shire which promotes an efficient and sustainable tourism industry, and the well-being of the resident population.</p>	<p>The proposed development has been appropriately designed and sited to ensure that it is in keeping with the existing character and landscape of Port Douglas. Through the inclusion of suitable landscaping, as well as a reduced built form bulk and scale, the proposed development is unlikely to have an adverse impact on the well-being of the resident population.</p>

3.8.3.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Development that contributes to the tourism industry and complements and promotes the Shire's natural assets is facilitated. In particular, tourism development is consistent with, and sensitive to, the nature of tourism appropriate in different parts of the Shire.</p>	<p>The proposed development will support this outcome, as it is located within an established settlement (Port Douglas), as well as being within an area that has been appropriately zoned and earmarked for future tourist accommodation development. The proposed development aims to not only maintain, but also enhance the natural and scenic qualities of the existing landscape and character, which comprises of lush landscaping that complements the tropical resort town atmosphere.</p>

<p>Specific Outcome 2</p> <p>A distinctive and attractive destination image for the Shire is fostered.</p>	<p>Where possible, existing green vistas observed from neighbouring properties will be maintained throughout the site, via the imposition of appropriate landscaping. Suitable landscaping will be implemented to complement the existing character and landscape and reduce negative visual impacts. It is also noted that the proposed development has been redesigned and the bulk/scale has been significantly reduced to complement the existing character currently observed throughout Port Douglas, contributing to a distinctive and attractive destination image.</p>
<p>Specific Outcome 3</p> <p>An efficient and coordinated pattern of tourism development is established which ensures tourist convenience and accessibility and focusses on the Tourism nodes as shown on the Strategic framework maps contained in Schedule 2.</p>	<p>The proposal involves infill development, on vacant land located within an area that has already been significantly developed for tourism purposes. Land surrounding the subject site comprises of existing tourist related development. In considering this, it is submitted that the proposed development is believed to be appropriate in within this area.</p>
<p>Specific Outcome 4</p> <p>Major tourist accommodation and entertainment areas will continue to focus in the Port Douglas Town Centre.</p>	<p>Although not located within the defined town centre of Port Douglas, the site is within close proximity (approx. 2.4km) to land mapped as being within the Centre Zone and other town centre activities.</p>
<p>Specific Outcome 5</p> <p>Tourist development in the Daintree Village, Cape Tribulation and other parts of the Shire:</p> <ul style="list-style-type: none"> (a) is of a scale appropriate to its location; (b) respects the environmental quality of its surrounds; (c) is limited in size and extent; (d) consists of low-intensity activities, generally based on the appreciation of the natural environment. 	<p>The subject site is not located in the Daintree Village or Cape Tribulation.</p>
<p>Specific Outcome 6</p>	<p>The subject site is not located within a rural area.</p>

Low intensity tourist activities may be acceptable in the rural areas, provided there are no adverse impacts on the natural environment, good quality agricultural land, farming practices and bona-fide extractive resource activities.	
Specific Outcome 7 A limited number of low-intensity tourist accommodation facilities may be acceptable in Conservation areas where it is designed to integrate with its setting (e.g. similar to the Silky Oaks Resort in Mossman)	Low impact tourism accommodation development is not proposed. The subject site is not located within the conservation zone.
Specific Outcome 8 The development of bed and breakfast, cabin, homestead / farm-stay accommodation and other low impact tourism development is encouraged in appropriate locations.	Low impact tourism development is not proposed.

3.8.4 Element – Primary Production	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Primary production, in particular the dominant sugar industry, and associated manufacturing has always been a significant contributor to Douglas Shire's economy. Considerable opportunities exist to add value to primary produce and to diversify the base income of the rural sector and improve employment opportunities. The Shire's competitive strengths of rich agricultural soils, tropical climate with high rainfall, and proximity to Cairns should be promoted to attract new industries complementary to the Shire's lifestyle.	Primary production is not proposed.

3.8.4.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>Opportunities to enhance agricultural industry particularly in the Shire's rural towns through the expansion of existing activities, development of value-adding processes and the introduction of new crops will be supported. However, sugar production will continue to be the most dominant cropping activity in the Shire.</p>	<p>Agricultural industry is not proposed.</p>
<p>Specific Outcome 2</p> <p>Grazing activities are supported where they do not intrude into vegetated and / or steep land.</p>	<p>Grazing activities are not proposed.</p>
<p>Specific Outcome 3</p> <p>The availability and viability of rural land for on-going agricultural uses is not compromised by inappropriate or incompatible development.</p>	<p>The subject site is not located within the rural zone. Proposed development is unlikely to impact on rural zoned land.</p>
<p>Specific Outcome 4</p> <p>Fish habitat areas continue to be protected and managed to ensure that the Shire continues to produce a diverse range of sustainable marine/seafood industry products important to both domestic and international markets.</p>	<p>The subject site is not located within a fish habitat area. Proposed development is unlikely to impact on fish habitats.</p>
<p>Specific Outcome 5</p> <p>Carbon sequestration activities are encouraged on steep, denuded hillslope land and broken corridors and away from areas of quality agricultural land.</p>	<p>Carbon sequestration activities are not proposed.</p>

3.8.5 Element – Innovation and Technology	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Technology is the driver of change. Land use policies for the Shire will as far as practical, anticipate and capitalise on innovation and technology. A focus on innovation and technology will deliver a more robust economy in which opportunities for enterprise are plentiful and innovation is rewarded.</p>	<p>Innovation and technology may be incorporated into the operation of the resort complex once constructed.</p>
<p>Strategic Outcome 2</p> <p>The outstanding natural beauty of the Shire and its proximity to Cairns City and its international airport makes the Shire an attractive location for those who can telecommute or have transportable businesses in the information technology, consulting and business services sectors. The promotion of the Shire to these groups and to those seeking an improved lifestyle will lead to growth and a more sustainable economy for the Shire.</p>	<p>While the proposed development does not provide opportunities for those who can telecommute or have transportable businesses to reside permanently within Port Douglas, the proposed development will incorporate new/state of the art function facilities that can cater up to 250 guests. Function facilities forming part of the proposed development will provide alternative options for companies and the like to host corporate events.</p>

3.8.5.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>The development of specialised economic innovation precincts and service industries occurs in Mossman, Craiglie and other appropriate locations in Port Douglas, in order to provide the Shire with an economic edge and to support the local population and provide employment opportunities.</p>	<p>Specialised economic innovation precincts and service industries are not proposed.</p>

3.9 Theme 6 – Infrastructure and Transport

3.9.1 Strategic Outcomes	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Development is facilitated through the provision of physical infrastructure which complements the economy of the Shire in an efficient, equitable and environmentally safe manner, as well as circulation networks which provide for the efficient movement of people and goods without compromising the Captain Cook Highway as a scenic corridor in the Shire.</p>	<p>Development will incorporate and is within proximity to suitable infrastructure provisions. The site is located within an existing urban area with direct access to existing high quality and available urban infrastructure.</p> <p>Any required upgrades to infrastructure and services will be provided in an efficient and timely manner. The proposed development will be connected to infrastructure for water, stormwater, telecommunications, electricity, and sewerage, which will be upgraded on an as needed basis to current Standards.</p> <p>The development can incorporate suitable transport networks, connecting with existing, well established transport networks.</p>
<p>Strategic Outcome 2</p> <p>Infrastructure and services are provided to Douglas Shire's communities in a planned, timely, economical and efficient manner in order to support community needs.</p>	<p>Infrastructure services are existing.</p>
<p>Strategic Outcome 3</p> <p>Development is appropriately coordinated between all of the government tiers and agencies, and sequenced, to ensure that activities are appropriately serviced by infrastructure.</p>	<p>Infrastructure services are existing.</p>
<p>Strategic Outcome 4</p>	<p>The development can incorporate suitable transport networks, connecting with existing, well established transport networks.</p>

An integrated transport system is developed that promotes increased walking, cycling and the use of public transport.	
Strategic Outcome 5 Sustainable on-site wastewater disposal and electricity supply occurs in parts of the Shire that cannot be connected to reticulated networks.	The subject site has access to existing and available infrastructure services/networks.
Strategic Outcome 6 Key sites and corridors for existing or planned infrastructure are identified and protected from incompatible development.	There are no future infrastructure upgrades planned within the vicinity of the subject site and therefore, the proposed development is unlikely to impact on future planned infrastructure projects.
Strategic Outcome 7 New technologies are utilised to achieve sustainable infrastructure outcomes.	Infrastructure services are existing.
Strategic Outcome 8 Infrastructure is located and designed and sited to respect the visual amenity of the shire.	Infrastructure services are existing.

3.9.2 Element - Energy	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Reticulated energy supplies are provided to all urban communities within the Shire.	A reticulated energy supply is existing and available.
Strategic Outcome 2	A reticulated energy supply is existing and available.

Elsewhere, alternative energy as a source of electricity for dwellings can present significant environmental benefits. The use of alternative, renewable energy sources such as solar and wind power is a small, yet significant method by which the community can address the global issue of climate change through local actions. However, a balance between the potential benefits and negative impacts of using alternative energy technologies is needed.	
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3.9.2.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 The energy needs of the Shire are met in a manner that minimises impacts on the health of the surrounding communities, natural environments and scenic amenity, and wherever possible, supports low emission and/or renewable energy sources.	A reticulated energy supply is existing and available.
Specific Outcome 2 Renewable energy facilities, such as small-scale wind turbine generators and solar panels can connect to an existing, nearby high voltage electricity network (with adequate capacity) without significant environment, social or amenity impact..	Final design is likely to incorporate some renewable energy facilities, such as roof-top solar.
Specific Outcome 3	A reticulated energy supply is existing and available.

Energy infrastructure in urban development areas is located, designed and constructed to ensure continuing functioning after severe natural hazard events.	
Specific Outcome 4 The paradox of polluting fuel-based power generation and the pristine World Heritage setting of the Daintree rainforests, north of the Daintree River is acknowledged. Proposals to extend electricity supply to properties north of the Daintree River take into account the sensitive environmental characteristics of the area, the fact that it is an area that is largely of world heritage significance and the climatic risks impacting the area such as cyclones and other weather events, all of which will influence decisions about the nature and scale of the electricity supply infrastructure which may be provided.	The subject site is not located within a World Heritage Area or north of the Daintree River.

3.9.3 Element – Water and Waste Management	
Strategic Outcomes	Applicant Response
Strategic Outcome 1 Wastewater management is a major issue for the unsewered small towns and communities of the Shire. Reticulated water and sewerage services and stormwater drainage improvements are required to minimise impacts on the environment and accommodate projected population growth.	Reticulated water and sewer infrastructure is existing and available.

3.9.3.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1	

Water supply resources are protected and used efficiently and sustainably. Waste is minimised.	Reticulated water infrastructure is existing and available. Measures will be implemented to ensure that water waste is reduced, where appropriate.
Specific Outcome 2 Water and sewerage infrastructure is provided in locations that maintain the health and well-being of the community and the environment.	Reticulated water and sewer infrastructure is existing and available.
Specific Outcome 3 Development in urban areas is connected to the reticulated water supply and sewerage infrastructure according to demand and service standards.	Reticulated water and sewer infrastructure is existing and available.
Specific Outcome 4 Development in rural and rural residential areas is provided with a reliable, potable water supply and on-site effluent treatment that will ensure no detrimental impacts on amenity, health or well-being of communities or the natural environment.	The subject site is not located within a rural or rural residential area.
Specific Outcome 5 Waste management facilities and recycling infrastructure does not result in impacts on the environment or on scenic amenity. Waste management and recycling facilities are spatially separated from sensitive land uses and receiving waters, and these facilities are equally protected from the intrusion of new sensitive land uses.	The development does not involve construction of waste management facilities and recycling infrastructure.
Specific Outcome 6 Improved technologies and capture/treatment methods, such as water sensitive urban design and recycling, are used to carefully manage stormwater discharge	Stormwater management can and will be incorporated to ensure that increases in impact up or downstream and/or on neighbouring properties do not occur,

to reduce excessive flows, improve water quality discharge and protect the natural environment from adverse impacts.	and that important water quality objectives of aquatic ecosystems can be achieved.
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3.9.4 Element Transport	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>It is important to maintain a safe and efficient transport network across the Shire. Many of the roads within the Shire are multi-purpose in function and face increased usage by large transport vehicles associated with agriculture, as well as domestic and tourism traffic volumes. The Captain Cook Highway connects Douglas Shire to Cairns and is a winding two lane carriageway that caters for the majority of the Shire's freight needs. The Mossman Mount Molloy Road is also an important link connecting the Shire to the Tablelands.</p>	<p>The subject site has frontage and access to Port Douglas Road, which is a State Controlled Road. Port Douglas Road is the main arterial road / access route into Port Douglas and is accessed from the Captain Cook Highway. Port Douglas Road accommodates many roads users, and forms part of the FNQ principal cycle network. The development will connect into existing, well established transport networks.</p>
<p>Strategic Outcome 2</p> <p>The Daintree River will continue to operate as a Ferry Service and the road north of Cape Tribulation to Wujal Wujal and the CREB track will remain as 4 wheel-drive only.</p>	<p>The proposed development is not reliant on the operation of the Daintree River Ferry Service.</p>
<p>Strategic Outcome 3</p> <p>The current lack of public transport facilities within the Shire restricts the movement of residents and impedes visitors to the Shire who do not have access to a vehicle. Additional public transport facilities, such as buses (or mini-</p>	<p>No public transport options are proposed/form part of this proposal. Proposed development will connect into existing pedestrian and cycle pathways.</p>

buses), may be required to improve accessibility for both the resident population and the wider community	
Strategic Outcome 4 There are no commercially operating airfields within Douglas Shire. However, there are a number of smaller scale airstrips. Careful planning is needed to ensure that the distribution of such air strips does not compromise the safety of their function.	The proposal does not involve construction of an airstrip.
Strategic Outcome 5 The cane rail network is an important infrastructure network for the efficient operation of the sugar industry and the mill.	The subject site is not impacted by the existing cane rail network.

3.9.4.1 Specific Outcomes	
Specific Outcomes	Applicant Response
Specific Outcome 1 The current and future function of higher order roads and transport networks (including buffer areas), is taken into account in land use planning, and protected from incompatible development.	The road network and hierarchy is existing and considered to be sufficient to support proposed development/accommodate additional traffic likely to result from proposed development. The subject site has frontage and access to Port Douglas Road, which is a State Controlled Road and the main access into Port Douglas.
Specific Outcome 2 Local and State road networks are integrated with existing communities and new developments.	The proposal does not involve construction of a new road.

<p>Specific Outcome 3</p> <p>Dickson's Inlet close to the Port Douglas Town Centre continues to function as the main passenger terminal for the reef.</p>	<p>Proposed development will not impact on the continued operations of Dickson Inlet as the main passenger terminal for the reef.</p>
<p>Specific Outcome 4</p> <p>Connectivity between neighbourhoods and communities is established in existing and new development areas ensuring road, cycle and pedestrian connections.</p>	<p>The development will connect into existing, well established transport networks.</p>
<p>Specific Outcome 5</p> <p>Development is designed, located, maintained, and where necessary improved, to facilitate pedestrian, cycle and transport functions.</p>	<p>The development will connect into existing, well established transport networks, including existing pedestrian and cycle pathways. The proposed development has been appropriately designed and sited so as to enhance passive surveillance of public areas existing along the frontage of the site.</p>
<p>Specific Outcome 6</p> <p>Development does not compromise the efficient use of the cane-rail network.</p>	<p>The subject site is not impacted by the existing cane rail network.</p>
<p>Specific Outcome 7</p> <p>The Daintree Ferry continues to be the method of transport across the Daintree River.</p>	<p>The proposed development is not reliant on the operation of the Daintree River Ferry Service.</p>
<p>Specific Outcome 8</p> <p>The cane railway network is retained and maintained as a significant infrastructure network for the sugar industry in the Shire.</p>	<p>The subject site is not impacted by the existing cane rail network.</p>

3.9.5 Element – Information Technology	
Strategic Outcomes	Applicant Response
<p>Strategic Outcome 1</p> <p>Information technology advancement and delivery is important to the economic success, education, health and well-being of the Douglas Shire.</p>	<p>The proposed development is unlikely to incorporate any significant information technology advancements.</p>

3.9.5.1 Specific Outcomes	
Specific Outcomes	Applicant Response
<p>Specific Outcome 1</p> <p>The community is not exposed to any harmful infrastructure elements associated with information technology provision. Wherever possible, infrastructure is co-located with existing information technology infrastructure, or is otherwise, visually unobtrusive to the greatest extent possible.</p>	<p>The proposed development is unlikely to incorporate any significant information technology advancements.</p>
<p>Specific Outcome 2</p> <p>Development in new urban areas installs information technology, or makes provision for such information technology, for ease of installation, when networks become available.</p>	<p>The subject site is located within an existing urban area.</p>

Attachment 7

Code Compliance Responses

6.2.14 TOURIST ACCOMMODATION ZONE CODE

Table 6.2.14.3.a – Tourist accommodation zone code – assessable development

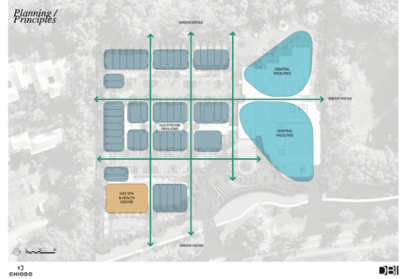
PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>PO1</p> <p>The height of all buildings and structures must be in keeping with the residential character of the area.</p>	<p>AO1</p> <p>Buildings and structures are not more than 13.5 metres and 3 storeys in height.</p>	<p>Complies with PO1.</p> <p>The proposed resort complex has an overall building height of approximately 13.5m (mostly 3 storey's and up to 4 storeys in part). The overall height of the proposed development has been significantly reduced from those under previous proposals, following ongoing discussions with Council's planning officers, whose recent advice confirmed that four (4) storeys would be in keeping with the existing character of the surrounding area.</p> <p>It is submitted that the proposed development has been appropriately designed and sited on site to ensure building height and mass is situated towards the rear of the site, in order to reduce visual impacts when observed from the public realm.</p> <p>Given that the subject site sits at a lower topographical level than Port Douglas Road, and slopes away from the road – it is anticipated that the proposed development height and mass will</p>

		<p>be further reduced when observed from the road/by road users.</p> <p>There is sufficient separation between existing/neighbouring tourist accommodation buildings and the proposed development, which is considered to reduce any visual and/or character impacts resulting from a slight increase in building heights, having regard to what is currently existing and observed throughout the immediate surrounding area.</p> <p>Additionally, it is noted that appropriate landscaping will be implemented along property boundaries which will provide further screening of the proposed development.</p>
Setbacks (Other than for a dwelling house)		
PO1 Buildings are setback to: (a) maintain the character and amenity of the area; (b) achieve separation from neighbouring buildings and from road frontages.	AO1 Buildings are setback: (a) a minimum of 6 metres from the main street frontage; (b) a minimum of 4 metres from any secondary street frontage; (c) 4.5 metres from a rear boundary; (d) 2 metres from a side or an average of half of the height of the building at the side setback, whichever is the greater.	Complies with AO1. The proposed development includes the following building setbacks: <ul style="list-style-type: none"> • Front (eastern boundary) – 6m • Rear (western boundary) – 4.5m • Side (northern boundary) – 3m • Side (southern boundary) – 4.5m
Site Coverage (other than for a dwelling house)		

<p>PO2</p> <p>The site coverage of all buildings does not result in a built form that is bulky or visually obtrusive.</p>	<p>AO2</p> <p>The site coverage of any building is limited to 50%</p>	<p>Complies with PO2.</p> <p>The proposed site coverage is anticipated to be approximately 54%. Although the site coverage exceeds the maximum requirement of 50%, it is submitted that the proposed development does not result in a built form that is bulky or visually intrusive. More specifically, it is noted that:</p> <ul style="list-style-type: none"> • development has been appropriately designed and sited on site to ensure building height and mass is situated towards the rear of the site in order to reduce visual impacts, when observed from the public realm. • Built form has been split up across the site. Development will be accommodated within four (4) separate buildings. • Suitable separation distances between each building located on site will be ensured to further reduce a bulky built form. • Suitable landscaping will be implemented throughout the site and in between the built form to soften the appearance as well as ensure existing green vistas currently observed from adjoining properties are maintained. • Appropriate setbacks from the frontage have been implemented (a minimum of
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		<p>6m). Front boundary setbacks will be landscaped.</p> <ul style="list-style-type: none"> The Porte Cochre proposed within the north east corner of the subject site will further reduce the bulk and scale of the appearance of Building 4, as detailed on the proposal plans, which incorporates a variety of ancillary uses.
Building proportions and scale (other than for a dwelling house)		
<p>PO3</p> <p>The proportions and scale of any development are in character with the area and local streetscape.</p>	<p>AO3.1</p> <p>The overall length of a building does not exceed 30 metres and the overall length of any continuous wall does not exceed 15 metres.</p> <p>AO3.2</p> <p>Balconies, patios and similar spaces are not enclosed or capable of being enclosed and used as a habitable room.</p> <p>AO3.3</p> <p>Balconies, patios and similar spaces are designed to be open and light weight in appearance with a maximum of 20% of the facade being fully enclosed.</p> <p>AO3.4</p> <p>Roof forms, materials and colours of buildings enhance the amenity of the street and locality, including:</p> <p>(a) the roofs of buildings are light coloured and non-reflecting; (b) white and shining metallic finishes are avoided on external</p>	<p>Complies with PO3.</p> <p>The design principles applied to the proposed development ensure that existing green vistas currently observed across/through the site are retained, where possible (refer to Figure 1 below). By retaining green vistas across the site, built form is further broken up and is likely to result in an appearance of several smaller isolated buildings across the site, rather than a few larger buildings with continuous walls in excess of 15m. Breezeways form an integral element of the design, so as to allow the development to be climate responsive and to offer additional opportunities for landscaping within the site. Extensive landscaping will be implemented along the frontage to ensure adequate screening of the proposed development.</p>

	surfaces in prominent view.	<p>In considering this, the scale and proportion of the proposed development is considered to be in character with the area and the local streetscape which can be described as follows:</p> <ul style="list-style-type: none"> • An urban context comprising of resort style tourist accommodation and recreation land uses. • Development is dominated by landscaping where built form is only partially visible. <p>Additionally, it is noted that existing surrounding tourist development adopts the following characteristics:</p> <ul style="list-style-type: none"> • Several separate buildings, up to three storeys in height. • Spaces between buildings are typically interspersed with landscaping. • Breezeways are used to link buildings/areas. • Dense landscaping along the frontage to restrict/conceal views of existing developments. • Generally comprise architecture representative of the tropical Queensland vernacular/responsive to the tropical climate.
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		 <p>Figure 1: proposed vistas</p>
Landscaping (other than for a dwelling House)		
<p>PO4</p> <p>Landscape planting is provided for the recreational amenity of residents/guests and incorporates dominant tropical vegetation which enhances the streetscape and the amenity of the area.</p>	<p>AO4.1</p> <p>A minimum of 35% of the site is provided as open space and recreation area with a minimum of 30% of this total; area provided for landscape planting.</p> <p>AO4.2</p> <p>Within the frontage setback area, a minimum width of 2 metres of landscape area includes a minimum 75% dense planting.</p> <p>AO4.3</p> <p>Within the side and rear setback areas, a minimum width of 1.5 metres of landscape area includes 75% dense planting.</p>	<p>Complies with PO4.</p> <p>Landscaping will be implemented throughout the site, so as to ensure the proposed development integrates seamlessly with its environment and location within the Port Douglas townscape.</p> <p>Specifically, it is submitted that landscaping will be implemented in accordance with the requirements of this assessment benchmark, via the imposition of dense landscaping within the property boundary setback areas. It is also noted that the proposed design incorporates sufficient open space and recreational areas, of which will be suitably landscaped.</p> <p>A preliminary landscape plan has been prepared and is included within the suite of proposal plans provided in Attachment 4.</p>

		<p>It is anticipated that landscaping will be implemented across 60%+ (inclusive of roof top plantings) of the site. It is noted that the proposed development incorporates vertical landscaping, with approximately 1,500m² of roof area to be landscaped.</p> <p>Demonstrated compliance with the requirements of PO3 will be provided in detailed landscape plans, prepared and likely to form part of any future information request response submitted to Council.</p>
<p>PO5</p> <p>The establishment of uses is consistent with the outcomes sought for the Tourist accommodation zone and protects the zone from the intrusion of inconsistent uses.</p>	<p>AO5</p> <p>Inconsistent uses as identified in Table 6.2.14.3.b are not established in the Tourist accommodation zone.</p>	<p>Complies with AO5.</p> <p>The proposal seeks approval to construct a resort complex within the tourist accommodation zone, which is fully consistent with outcomes sought within the Tourist Accommodation zone. No uses identified as inconsistent uses, pursuant to Table 6.2.14.3b, are proposed.</p>
<p>PO6</p> <p>Development is located, designed, operated and managed to respond to the characteristics, features and constraints of the site and surrounds.</p>	<p>AO6</p> <p>No acceptable outcomes are prescribed.</p>	<p>Complies with PO6.</p> <p>The proposed development has been appropriately designed and sited so as to respond to known characteristics, features and constraints of the site and its surrounds.</p> <p>The site remains relatively unconstrained, as it is comprised of vacant, relatively flat land. The majority of the site is clear of vegetation, with some vegetation existing along the perimeters of</p>

		<p>the site. Notwithstanding this, the site is not mapped as containing significant vegetation and/or matters of environmental significance.</p> <p>A portion of the site has previously been partially excavated, in accordance with previous approvals obtained/relevant to the site. The proposed development has been designed to utilise and expand previously excavated areas to create basements to accommodate on site car parking.</p> <p>Additional technical engineering assessment and design, including stormwater management, will be used to further demonstrate compliance with the requirements of PO6. Further technical engineering assessment and design, including stormwater management will be prepared and is likely to form part of any future information request response submitted to Council.</p>
<p>PO7</p> <p>Development does not adversely affect the tropical, tourist and residential character and amenity of the area in terms of traffic, noise, dust, odour, lighting or other physical or environmental impacts.</p>	<p>A07</p> <p>No acceptable outcomes are prescribed.</p>	<p>Complies with PO7.</p> <p>The proposed development involves resort style tourist accommodation, which is consistent with the existing character of the local area. Incorporating natural materials such as timber and stone, and featuring large roof overhangs to provide deep shade, essential for the tropical climate, ensures the development complements the existing townscape.</p>

		<p>The design will maintain appropriate height and scale, using open and articulated forms that harmonize with the modest scale of the surrounding township fabric.</p> <p>Development as proposed is not expected to result in any adverse amenity and/or environmental impacts.</p>
<p>PO8</p> <p>Any loading/unloading areas, servicing areas and outdoor storage areas are screened from public view or adjacent sensitive uses.</p>	<p>AO8</p> <p>Outdoor loading/unloading, servicing and storage areas are sited or screened so they are:</p> <p>(a) not visible from any off-site public place;</p> <p>(b) not located adjacent to premises used for sensitive uses.</p>	<p>Complies with AO8.</p> <p>A separate access for service vehicles will be provided for on site, within the north-east corner, adjacent to the northern property boundary. Service vehicles will be able to utilise this separate driveway to access an outdoor loading/unloading area. This service vehicle access driveway will be setback approximately 3m for the northern property boundary. It is noted that there is a putting green (forming part of the Mirage Golf Course), surrounded by extensive vegetation, directly adjoining the subject site's northern boundary. Land located north of the subject site is included within the Tourist Accommodation zone.</p> <p>Notwithstanding this, it is noted that the Mirage Port Douglas development partially surrounds the subject site and is located within the Integrated Resort Precinct. Land use and planning provisions for the Mirage Port Douglas are managed through the <i>Integrated Resort and</i></p>

		<p><i>Development Act 1987</i> (IRDA) and any subsequent scheme approved in accordance with the relevant legislation. The Mirage Port Douglas Integrated Resort Development Scheme identifies some of the land surrounding the subject site (more specifically, land directly adjoining the subject site's northern boundary) as being suitable for potential future residential land uses. The potential for future residential development surrounding the subject site was taken into consideration when siting and designing the proposed development. Appropriate measures have been implemented to ensure the proposed development is unlikely to negatively impact on any future residential development constructed on adjoining land parcel/s. It is submitted that a 3m landscaped setback will be sufficient in ensuring that the proposed service vehicle access/loading and unloading areas do not impact on any future sensitive land use.</p>
<p>PO9</p> <p>Tourist developments include recreational and ancillary services and facilities for the enjoyment of guests.</p>	<p>AO9.1</p> <p>Development which includes accommodation for tourists incorporates a mix of the following recreational and ancillary services and facilities:</p> <ul style="list-style-type: none"> (a) swimming pools; (b) tennis courts (c) barbecue areas; (d) outdoor lounging / recreation areas; 	<p>Complies with PO9.</p> <p>The proposed resort complex incorporates the following ancillary services/facilities:</p> <ul style="list-style-type: none"> • Day spa and health centre; • Specialty restaurants; • Bars; • Café/coffee shop; • Resort pools; and

	<p>(e) restaurants / bars;</p> <p>(f) tourist-focussed shopping;</p> <p>(g) tour booking office;</p> <p>(h) spa / health clubs.</p>	<ul style="list-style-type: none"> Function/conference facilities. <p>Some of the proposed ancillary uses (i.e. bars, restaurants, function facilities) may be defined as commercial services.</p> <p>Notwithstanding this, commercial services/facilities forming part of the proposed development are intended to complement and support the Fairmont Resort development and are not expected to detract from the existing town centre.</p> <p>Ancillary uses are focussed more on adding to the whole experience and provide convenience for guests staying at the resort who wish to dine and recreate on site.</p>
	<p>AO9.2</p> <p>Any commercial services or facilities incorporated into a tourist development are small scale and predominantly service in-house guests only.</p>	
	<p>AO9.3</p> <p>Where a commercial service or facility offers services to persons over and above in-house guests, the commercial component provides on-site car parking for 50% of the floor area available for use in accordance with the relevant requirements of the Parking and access code.</p>	<p>It is intended that ancillary uses will be made available for use by the wider community/general public.</p> <p>Additional on site car parking will be provided to support the use of commercial services/facilities by the wider community.</p> <p>It is noted that proposed on site car parking provisions are considered to be sufficient in accommodating the commercial components forming part of the proposal, likely to be utilised by the public. Specifically, it is noted that car parking provisions for these commercial components were based off approx. 45% of the total Gross Floor Area for commercial services. Further details on vehicle parking provisions can be found in Attachment 5 – Traffic Engineering Technical Note.</p>

PO10 New lots contain a minimum area of 1000m.	AO10 No acceptable outcomes are prescribed.	Not applicable. The proposed development does not involve the creation of a new lot.
PO11 New lots have a minimum road frontage of 20 metres.	AO11 No acceptable outcomes are prescribed.	Not applicable. The proposed development does not involve the creation of a new lot.
PO12 New lots contain a 25-metre x 20 metre rectangle.	AO12 No acceptable outcomes are prescribed.	Not applicable. The proposed development does not involve the creation of a new lot.

Table 6.2.14.3.b — Inconsistent uses within the Tourist accommodation zone

INCONSISTENT USES		
<ul style="list-style-type: none"> • Adult store • Agricultural supplies store • Air services • Animal husbandry • Animal keeping • Aquaculture • Brothel • Bulk landscape supplies • Cropping • Detention facility • Extractive industry 	<ul style="list-style-type: none"> • Intensive horticulture • Landing • Low impact industry • Major electricity infrastructure • Major sport, recreation and entertainment facility • Marine industry • Market • Medium impact industry • Motor sport facility • Nightclub entertainment facility • Office 	<ul style="list-style-type: none"> • Permanent plantation • Port services • Renewable energy facility • Roadside stall • Rural industry • Rural workers accommodation • Service station • Shopping centre • Showroom • Special industry • Substation

<ul style="list-style-type: none">• Funeral parlour• Garden centre• Hardware and trade supplies• Health care services• High impact industry• Indoor sport and recreation• Intensive animal industry	<ul style="list-style-type: none">• Outdoor sales• Outdoor sport and recreation• Outstation• Park• Parking station	<ul style="list-style-type: none">• Theatre• Transport depot• Veterinary services• Warehouse• Wholesale nursery• Winery
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7.2.4 PORT DOUGLAS/CRAIGLIE LOCAL PLAN CODE

Table 7.2.4.4.a –Port Douglas / Craiglie local plan – assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>PO1</p> <p>Pedestrians, cyclists, motorists and public transport users can easily move into and through the precinct along planned connectivity routes, identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.</p>	<p>AO1</p> <p>A pedestrian and cycle movement network are integrated and delivered through development.</p>	<p>Complies with AO1.</p> <p>The development will connect into existing, well established transport networks, including existing pedestrian and cycle pathways. Notwithstanding this, the proposed development has been appropriately designed to integrate internal pedestrian movement networks that connect development and provide for ease of movement throughout the site.</p>
<p>PO2</p> <p>Development retains and enhances key landscape elements including character trees and areas of significant vegetation contributing to the character and quality of the local plan area and significant views and vistas and other landmarks important to the context of Port Douglas / Craiglie (as identified on the Port Douglas/ Craiglie Townscape Plan map contained in Schedule 2).</p>	<p>AO2.1</p> <p>Development provides for the retention and enhancement of existing mature trees and character vegetation that contribute to the lush tropical character of the town, including:</p> <ul style="list-style-type: none"> (a) the tree covered backdrop of Flagstaff Hill; (b) natural vegetation along watercourses, in particular the Mowbray River, Beor Creek and Dickson Inlet; (c) the tidal vegetation along the foreshore; (d) beachfront vegetation along Four Mile Beach, including the fringe of Coconut Palms; (e) the oil palm avenues along the major roads; (f) the lush landscaping within major roundabouts at key nodes; 	<p>Complies with PO2.</p> <p>The subject site comprises vacant land, predominantly clear of vegetation. Some vegetation exists around the perimeter of the site and where possible, existing vegetation will be retained.</p> <p>Demonstrated compliance with the requirements of PO2 will be provided in detailed landscape plans, prepared and likely to form part of any future information request response submitted to Council.</p>

	(g) Macrossan Street and Warner Street; (h) Port Douglas waterfront.	
	A02.2 Development protects and does not intrude into important views and vistas as identified on the Port Douglas Townscape Plan map contained in Schedule 2, in particular: (a) Flagstaff Hill; (b) Four Mile Beach; (c) Across to the ranges over Dickson Inlet; (d) Mowbray Valley.	
	A02.3 Important landmarks, memorials and monuments are retained.	
PO3 Development contributes to the protection, reinforcement and where necessary enhancement of gateways and key intersections identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.	A03 Development adjacent to the gateways and nodes as identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 incorporates architectural features and landscaping treatments and design elements that enhance the sense of arrival and way finding within the town.	Not applicable. The subject site is not located within proximity to the gateways/nodes identified on the Port Douglas/Craiglie local plan maps.
PO4	A04 Landscaping incorporates the requirements of Planning scheme policy SC6.7 – Landscaping, in particular	Complies with PO4. Landscaping will include the imposition of dense landscaping within the property boundary setback

Landscaping of development sites complements the existing tropical character of Port Douglas and Craiglie.	landscaping should be capable of achieving a 60% screening of development within 5 years and predominantly consists of endemic vegetation.	<p>areas. It is also noted that the proposed design incorporates sufficient open space and recreational areas, of which will be suitably landscaped.</p> <p>A preliminary landscape plan has been prepared and is included within the suite of proposal plans provided in Attachment 4.</p> <p>It is anticipated that landscaping will be implemented across 60%+ (inclusive of roof top plantings) of the site. It is noted that the proposed development incorporates vertical landscaping, with approximately 1,500m2 of roof area to be landscaped.</p> <p>Demonstrated compliance with the requirements of PO4 will be provided in detailed landscape plans, prepared and likely to form part of any future information request response submitted to Council, where deemed necessary.</p>
<p>PO5</p> <p>Development does not compromise the safety and efficiency of the State-controlled road network.</p>	<p>AO5</p> <p>Direct access is not provided to a State-controlled road where legal and practical access from another road is available.</p>	<p>Not applicable.</p> <p>Access to the subject site is gained via Port Douglas Road, which is a State-controlled road. There is no other legal/practical access available to the subject site.</p>
<p>For assessable development</p> <p>Additional requirements in Precinct 1 – Port Douglas precinct</p>		

<p>PO6</p> <p>The views and vistas identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 are maintained.</p>	<p>AO6.1</p> <p>Development does not impede continued views to scenic vistas and key streetscapes within the local plan area.</p> <p>AO6.2</p> <p>Unless otherwise specified within this Local Plan, buildings are set back not less than 6 metres from the primary street frontage.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO7</p> <p>Vehicle access, parking and service areas:</p> <ul style="list-style-type: none"> (a) do not undermine the relationship between buildings and street or dominate the streetscape; (b) are designed to minimise pedestrian vehicle conflict; (c) are clearly identified and maintain ease of access at all times. 	<p>AO7.1</p> <p>For all buildings, parking is:</p> <ul style="list-style-type: none"> (a) to the side of buildings and recessed behind the main building line; or (b) behind buildings; or (c) wrapped by the building facade, and not visible from the street. <p>AO7.2</p> <p>Ground level parking incorporates clearly defined pedestrian routes.</p> <p>AO7.3</p> <p>Any porte-cocheres, disabled and pedestrian accesses are accommodated within the boundary of new or refurbished development.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

	<p>A07.4</p> <p>Where the development is an integrated mixed- use development incorporating short term accommodation or multiple dwellings and either food and drink outlet or hotel or shop or shopping centre or office, on-site parking spaces are provided as per the number prescribed in the Parking and access code with a relaxation of 30% of spaces required for the non-residential uses.</p>	
	<p>A07.5</p> <p>On-site car parking available for public use is clearly signed at the site frontage.</p>	
	<p>A07.6</p> <p>Boom gates pay machines or other regulatory devices to control access to a publicly available car parking area are not constructed or installed.</p>	
<p>PO8</p> <p>Precinct 1 – Port Douglas precinct is not characterised by a proliferation of advertising signs.</p>	<p>A08</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

Additional requirements for Sub-precinct 1a – Town Centre sub-precinct		
<p>PO9</p> <p>Building heights:</p> <ul style="list-style-type: none"> (a) do not overwhelm or dominate the town centre; (b) respect the desired streetscape; (c) ensure a high-quality appearance when viewed from both within the town centre sub- precinct and external to the town centre sub- precinct; (d) remain subservient to the natural environment and the backdrop of Flagstaff Hill. (e) do not exceed 3 storeys. 	<p>AO9</p> <p>Buildings and structures are not more than 3 storeys and 13.5 metres in height, with a roof height of not less than 3 metres.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO10</p> <p>Building design, the streetscape, pedestrian paths and street front spaces promote integration with the surrounding area and the rest of Precinct 1 – Port Douglas Precinct.</p>	<p>AO10</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO11</p> <p>Buildings:</p> <ul style="list-style-type: none"> (a) address street frontages; (b) ensure main entrances front the street or 	<p>AO11</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

public spaces; (c) do not focus principally on internal spaces or parking areas.		
PO12 Setbacks at ground level provide for: (a) connection between pedestrian paths and public places; (b) areas for convenient movement of pedestrians; (c) changes in gradient of the street.	AO12 Setbacks at ground level: (a) are clear of columns and other obstructions; (b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
AO13 Buildings do not result in a reduction of views and vistas from public places to: (a) Flagstaff Hill; (b) Dickson Inlet; (c) public open space; (d) places of significance.	AO13 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO14 Development enhances the distinctive tropical resort town and identity of Port Douglas and	AO14 Development is built up to the street frontage/s at the street level and incorporates a light frame awning, a	Not applicable.

encourages pedestrian activity at street level including shade protection across the footpath for the length of the building.	minimum of 3 metres in width for the length of the street frontage/s; or If a development includes an outdoor dining area at ground/footpath level, the dining area has a maximum setback of 3 metres and the required awning is still maintained along the length of the street frontage/s.	The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO15 Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street-level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.	AO15.1 Centre activities establish: at street level on active street frontages; a maximum of one level above street level.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
	AO15.2 Any residential development activities or short-term accommodation is located above street level of the active frontage, but not on or up to the street frontage in any development, including mixed use development.	

<p>PO16</p> <p>Detailed building design:</p> <ul style="list-style-type: none"> (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements; (c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the lush, vegetated character of the Town Centre sub-precinct is maintained. 	<p>AO16</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO17</p> <p>Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through:</p> <ul style="list-style-type: none"> (a) surface decoration; wall recesses and projections; (b) a variation in wall finishes; windows, balconies, awnings and other visible structural elements. (c) differentiating between the lower, middle and upper 	<p>AO17</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

(d) parts of the building by varying the facade and/or the shape of the built form, where comprised of more than two storeys.		
<p>P018</p> <p>Roofs are not characterised by a cluttered display of plant and equipment, in particular:</p> <ul style="list-style-type: none"> (a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Town Centre sub-precinct; (b) service structures lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view; rooftops are not used for advertising. 	<p>AO18</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>P019</p> <p>Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to:</p> <ul style="list-style-type: none"> (a) shade windows; (b) reduce glare; 	<p>AO19</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

(c) assist in maintaining comfortable indoor temperatures; (d) minimising heat loads; (e) enrich the North Queensland tropical character of the Town Centre sub-precinct; (f) provide architectural interest to building facades.		
PO20 Buildings are finished with high quality materials, selected for: (a) their ability to contribute the character of Town Centre sub-precinct; (b) easy maintenance, durability and an ability not to readily stain, discolour or deteriorate.	AO20 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO21 Buildings do not incorporate any type of glass or other materials that are likely to reflect the sun's rays in a manner that may create a nuisance, discomfort or a hazard.	AO21 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO22 Facades and elevations do not include large blank walls. Openings and setbacks are used to articulate vertical building surfaces.	AO22.1 Development has a maximum length of unbroken building facade of 20 metres and a maximum extent of overall development in the same style/design along the street frontage/s of 40 metres.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.

	<p>AO22.2</p> <p>Any break in the building facade varies the alignment by a 1 metre minimum deviation.</p> <p>AO22.3</p> <p>A minimum of three of the following building design features and architectural elements detailed below are incorporated to break the extended facade of a development:</p> <ul style="list-style-type: none"> (a) a change in roof profile; (b) a change in parapet coping; (c) a change in awning design; (d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development. 	
<p>PO23</p> <p>Building facades that face public spaces at ground level:</p> <ul style="list-style-type: none"> (a) complement the appearance of the development and surrounding streetscape; (b) enhance the visual amenity of the public place; (c) include a variety of human scale architectural elements and details; 	<p>AO23</p> <p>Building facades at the ground floor of development that face public space are designed to ensure:</p> <ul style="list-style-type: none"> (a) a minimum of 70% of the facade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development; (b) a visually prominent main entrance that faces the principal public place; 	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

(d) provide an opportunity for the casual and convenient surveillance of public space from within the development.	(c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the facade.	
PO24 Awnings for pedestrian shelter are consistent with the character setting of the Town Centre sub- precinct and: <ul style="list-style-type: none"> (a) extend and cover the footpath to provide protection from the sun and rain; (b) include lighting under the awning; (c) are continuous across the frontage of the site; (d) align to provide continuity with existing or future awnings on adjoining sites; (e) are a minimum of 3.0 metres in width and generally not more than 3.5 metres above pavement height; (f) do not extend past a vertical plane, 1.2 metres inside the kerb-line to enable street trees to be planted and grow; (g) are cantilevered from the main building with any posts within the footpath being non-load- bearing. 	AO24 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO25	AO25	Not applicable.

Development integrates with the streetscape and landscaping improvements for Port Douglas.	Development fronting Davidson Street, Macrossan Street, Wharf Street, Mowbray Street and Warner Street is designed to integrate with the on-street landscaping and design improvements as outlined within the Port Douglas landscape master plan contained within Planning scheme policy SC6.7 – Landscaping.	The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
Additional requirements for Sub-precinct 1b – Waterfront North sub-precinct		
PO26 The establishment of uses is consistent with the outcomes sought for sub-precinct 1b – Waterfront North.	AO26 Uses identified as inconsistent uses in Table 7.2.4.4.b — Inconsistent uses in sub-precinct 1b - Waterfront North sub-precinct are not established in sub-precinct 1b - Waterfront North.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO27 The bulk and scale of buildings is consistent with surrounding development and steps down to complement the open space areas in the adjoining limited development sub-precinct.	AO27 Buildings and structures are not more than: (a) 3 storeys and 13.5 metres in height, with a roof height of not less than 3 metres, in those parts of the precinct south of Inlet Street; (b) 2 storeys and 8.5 metres in height, with a roof height of not less than 3 metres, in those parts of the precinct north of Inlet Street.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO28 Building design, streetscape, pedestrian paths and street front spaces promote integration with the surrounding area and the rest of Precinct 1 – Port Douglas Precinct.	AO28 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.

<p>PO29</p> <p>Public pedestrian access along the water's edge is maximised.</p>	<p>AO29.1</p> <p>Public pedestrian access is provided along the frontage of the water's edge consisting of a boardwalk of a minimum width of 4 metres that is available of 24-hour use.</p> <p>AO29.2</p> <p>A public plaza is incorporated into the design generally reflecting the requirements of the Port Douglas Waterfront Master Plan, focussing in the vicinity of the 'Duck Pond'.</p> <p>AO29.3</p> <p>Built envelopes are setback a minimum of 3.0 metres from the board walk, with a shelter/shade zone between the building envelopes and the boardwalk consisting of shade structure, canopies, verandas and the like.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO30</p> <p>Buildings:</p> <p>(a) address street frontages;</p> <p>(b) ensure main entrances front the street or public spaces.</p>	<p>AO30</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO31</p> <p>Setbacks at ground level provide for:</p> <p>(a) connection between pedestrian paths and public places;</p>	<p>AO31</p> <p>Setbacks at ground level:</p> <p>(a) are clear of columns and other obstructions;</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

(b) areas for convenient movement of pedestrians; (c) changes in gradient.	(b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths.	
PO32 Buildings do not result in a reduction of views and vistas from public places to: (a) Dickson Inlet; (b) public open space; (c) places of significance.	AO32 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO33 Development enhances the distinctive tropical resort town and identity of Port Douglas and encourages pedestrian activity at ground level including shade protection across the footpath and open space areas.	AO33 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO34 Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street-level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.	<div> AO34.1 Centre activities establish: (a) at street level on active street frontages; (b) a maximum of one level above street level. </div> <div> AO34.2 Residential development activities or short-term accommodation is located above street /ground floor </div>	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.

	level of the active frontage, but not on or up to the street / public frontage in any development, including mixed use development.	
PO35 Detailed building design: <ul style="list-style-type: none"> (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements; (c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the lush, vegetated character of the Waterfront North sub-precinct is maintained. 	AO35 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO36 Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through: <ul style="list-style-type: none"> (a) surface decoration; (b) wall recesses and projections; 	AO36 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.

<p>(c) a variation in wall finishes; windows, balconies, awnings and other visible structural elements.</p> <p>(d) differentiating between the lower, middle and upper parts of the building by varying the facade and/or the shape of the built form, where comprised of more than two storeys.</p>		
<p>PO37</p> <p>Roofs are not characterised by a cluttered display of plant and equipment, in particular:</p> <p>(a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Waterfront North sub-precinct;</p> <p>(b) service structures lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view;</p> <p>(c) roof tops are not used for advertising.</p>	<p>AO37</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO38</p>	<p>AO38</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

<p>Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to:</p> <ul style="list-style-type: none"> (a) shade windows; (b) reduce glare; (c) assist in maintaining comfortable indoor temperatures; (d) minimising heat loads; (e) enriching the North Queensland tropical character of the Waterfront North sub-precinct; (f) architectural interest to building facades. 		
<p>PO39</p> <p>Buildings are finished with high quality materials, selected for:</p> <ul style="list-style-type: none"> (a) their ability to contribute the character of Waterfront North sub-precinct; (b) easy maintenance, durability and an ability not to readily stain, discolour or deteriorate. 	<p>AO39</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO40</p> <p>Buildings do not incorporate any type of glass or other materials that are likely to reflect the sun's rays in a manner that may create a nuisance, discomfort or a hazard.</p>	<p>AO40</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

<p>PO41</p> <p>Facades and elevations do not include large blank walls and openings and setbacks are used to articulate vertical building surfaces.</p>	<p>AO41.1</p> <p>Development has a maximum length of unbroken building facade of 20 metres and a maximum extent of overall development in the same style/design along the street frontage/s of 40 metres.</p> <p>AO41.2</p> <p>Any break in the building facade varies the alignment by a 1 metre minimum deviation.</p> <p>AO41.3</p> <p>A minimum of three of the following building design features and architectural elements detailed below are incorporated to break the extended facade of a development:</p> <ul style="list-style-type: none"> (a) a change in roof profile; (b) a change in parapet coping; (c) a change in awning design; (d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development. 	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO42</p> <p>Building facades that face public spaces at ground level:</p>	<p>AO42</p> <p>Building facades at the ground floor of development that face public space are designed to ensure:</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

<ul style="list-style-type: none"> (a) complement the appearance of the development and surrounding streetscape; (b) enhance the visual amenity of the public place; (c) include a variety of human scale architectural elements and details; (d) provide an opportunity for the casual and convenient surveillance of public space from within the development. 	<ul style="list-style-type: none"> (a) a minimum of 70% of the facade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development; (b) a visually prominent main entrance that faces the principal public place; (c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the facade. 	
<p>PO43</p> <p>Awnings for pedestrian shelter are consistent with the character setting of the Waterfront North sub- precinct and:</p> <ul style="list-style-type: none"> (a) extend and cover the footpath to provide protection from the sun and rain; (b) include lighting under the awning; (c) are continuous across pedestrian circulation areas; (d) align to provide continuity with existing or future awnings on adjoining sites; (e) are a minimum of 3 metres in width and generally not more than 3.5 metres above pavement height; (f) do not extend past a vertical plane, 1.2 metres inside the street kerb-line to 	<p>AO43</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

<p>enable street trees to be planted and grow;</p> <p>(g) are cantilevered from the main building with any posts within the footpath being non-load-bearing.</p>		
<p>PO44</p> <p>The Balley Hooley rail line and turn-table is retained and incorporated into development and maintains its functionality.</p>	<p>AO44.1</p> <p>Bally Hooley rail line and turn-table is retained and incorporated into development to maintain its functionality.</p> <p>AO44.2</p> <p>Where development provides floor area for the Bally Hooley rail station, the gross floor area of the rail line and station does not generate a requirement for additional vehicle parking.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO45</p> <p>Development recognises the importance of and relationship between the marina, commercial and residential development in the Waterfront North sub-precinct, and includes measures to mitigate the impact of:</p> <p>(a) noise;</p> <p>(b) odour;</p> <p>(c) hazardous materials; waste and recyclable material storage.</p>	<p>AO45</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

<p>PO46</p> <p>Formalised public spaces and pedestrian paths/areas on freehold land are made accessible to the public.</p>	<p>AO46</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO47</p> <p>Buildings, civic spaces, roads and pedestrian links are enhanced by:</p> <ul style="list-style-type: none"> (a) appropriate landscape design and planting; (b) themed planting that defines entry points, and creates strong 'entry corridors' into the waterfront; (c) lighting and well-considered discrete signage that complements building and landscape design; (d) public artwork and other similar features that reflect the heritage and character of the Port Douglas Waterfront. 	<p>AO47</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO48</p> <p>Buildings are designed and sited to provide vistas along shared pedestrian/open space and movement areas in suitable locations.</p>	<p>AO48</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO49</p> <p>Development does not diminish the viability of marine-based industrial uses that directly serve</p>	<p>AO49</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p>

the Port Douglas tourist and fishing operators and private boat owners, particularly with respect to the slipway operation.		The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO50 Marine infrastructure to service the tourism, fishing and private boating community is provided.	AO50 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO51 Changes to the Port Douglas Waterfront quay-line do not cause adverse impacts to the environmentally sensitive Dickson Inlet.	AO51 Development that results in changes to the Port Douglas Waterfront quay-line is only established where an Ecological assessment report provides support to the changes.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO52 The establishment of uses is consistent with the outcomes sought for Precinct 1c – Waterfront South.	AO52 Uses identified as inconsistent uses Table 7.2.4.4.c are not established in Precinct 1c – Waterfront South.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO53 Development does not adversely impact on the natural environment, natural vegetation or watercourses.	AO53.1 An Ecological assessment report is prepared identifying the environmental qualities of the surrounding natural and built features which are to be managed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
	AO53.2 An Environmental Management Plan is prepared to manage potential impacts of the operation of the development on surrounding natural areas.	
PO54	AO54	Not applicable.

Development of land at the end of Port Street adjacent to Dickson Inlet incorporates a slipway, or an alternative functioning facility, with capacity to service the Port Douglas marine and tourism industry.	A master plan for the development is provided and implemented to demonstrate the integration of the slipway, or an alternative functioning facility, with other supporting service industry activities that service the marine and tourism industry of Port Douglas.	The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO55 Buildings and structures are of a height and are set back from side boundaries and other sensitive areas to ensure the scenic amenity and environmental qualities of the adjacent area are not adversely affected.	AO55.1 Development has a height of not more than 10 metres.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
	AO55.2 Development is setback from all property boundaries not less than 3 metres.	
PO56 The site coverage of all buildings and structures ensures development: (a) is sited in an existing cleared area or in an area approved for clearing; (b) has sufficient area for the provision of services; (c) development does not have an adverse effect on the environmental, habitat, conservation or landscape values of the on-site and surrounding sensitive areas.	AO56 No acceptable outcomes are prescribed.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO57	AO57.1 Sufficient manoeuvring area is provided on-site to allow a Medium Rigid Vehicle to enter and leave the site in a forward gear.	Not applicable. The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.

<p>Premises include adequate provision for service vehicles, to cater for generated demand. Loading areas for service vehicles are designed to:</p> <ul style="list-style-type: none"> (a) be accommodated on-site; (b) maximise safety and efficiency of loading; (c) protect the visual and acoustic amenity of sensitive land use activities; (c) minimise adverse impacts on natural characteristics of adjacent areas. 	<p>AO57.2</p> <p>Development is designed to ensure all service vehicles are contained within the site when being loaded/unloaded.</p> <p>AO57.3</p> <p>Driveways, parking and manoeuvring areas are constructed and maintained to:</p> <ul style="list-style-type: none"> (a) minimise erosion from storm water runoff; (b) retain all existing vegetation. 	
<p>PO58</p> <p>Development ensures adverse impacts from service vehicles on the road network, external to the site, are minimised.</p>	<p>AO58</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO59</p> <p>Entry to the site is landscaped to enhance the amenity of the area and provide a pleasant working environment.</p>	<p>AO59</p> <p>Areas used for loading and unloading, storage, utilities and car parking are screened from public view:</p> <ul style="list-style-type: none"> (a) by a combination of landscaping and screen fencing; (b) dense planting along any road frontage is a minimum width of 3 metres. 	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO60</p> <p>Landscaping is informal in character and complementary to the existing natural environment, provides screening and enhances the visual appearance of the development.</p>	<p>AO60</p> <p>For any development landscaping is in accordance with the Plant species schedule in Planning scheme policy SC6.7– Landscaping.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>Additional requirements for Sub-precinct 1d – Limited Development sub-precinct</p>		

PO61	AO61	Not applicable.
The height of buildings and structures contributes to the desired form and outcomes for the sub-precinct and are limited to a single storey.	Buildings and structures are not more than one storey and 4 metres in height.	The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
Additional requirements for Sub-precinct 1e – Community and recreation sub-precinct		
PO62	AO62	Not applicable.
The precinct is developed for organised sporting activities and other community uses.	No acceptable outcomes are prescribed.	The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
Additional requirements for Sub-precinct 1f – Flagstaff Hill sub-precinct		
PO63	AO63	Not applicable.
Flagstaff Hill is protected from inappropriate development to protect the hill as an important natural landmark feature of Port Douglas and as a vegetated backdrop to the Town centre.	No acceptable outcomes are prescribed.	The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.
PO64	AO64	Not applicable.
<p>All development on Flagstaff Hill is designed to minimise the visibility of the development and to ensure development is subservient to the natural landscape and topography of the site, including through:</p> <ul style="list-style-type: none"> (a) building design which minimises excavation and filling; (b) buildings being designed to step down the site and incorporate foundations and footings on piers or poles; 	No acceptable outcomes are prescribed.	The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.

<p>(c) buildings being visually unobtrusive and incorporating exterior finishes and muted colours which are non-reflective and complement the colours of the surrounding vegetation and view-shed;</p> <p>(d) protection of the views from public viewing points in the Port Douglas precinct.</p>		
Additional requirements for Precinct 3 – Craiglie Commercial and Light Industry precinct		
<p>PO65</p> <p>Development supports the tourism and marine industries in Port Douglas, along with the small-scale commercial and light industry land uses that support the local economy that would otherwise be better suited to a location outside the Port Douglas Town Centre Precinct.</p>	<p>AO65</p> <p>Development consists of service and light industries and associated small scale commercial activities.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO66</p> <p>Development on lots adjacent to the Captain Cook Highway is sited, designed and landscaped to provide an attractive visual approach to Port Douglas with all buildings, structures and car parking areas setback a sufficient distance from</p>	<p>AO66.1</p> <p>Buildings and structures are setback 8 metres from the Captain Cook Highway frontage, or no closer to the Captain Cook Highway frontage than buildings and structures on adjoining sites (averaged), whichever is the greater.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

the frontage to enable landscaping to soften or screen the appearance of the development.	<p>AO66.2</p> <p>The setback area to the Captain Cook Highway frontage is landscaped with advanced dense planting including tree species (100 litre bag stock), which will, at maturity, exceed the height of the building(s) on the site.</p> <p>AO66.3</p> <p>Advertising signs are discreet in appearance with no large advertising signs, including tenancy signs, located on or near the Captain Cook Highway frontage, or within any landscaped setback area.</p> <p>AO66.4</p> <p>Car parking areas, loading and other service areas are designed to be screened from the Captain Cook Highway and are located so as to not be visually prominent from the Captain Cook Highway.</p>	
Additional requirements for Precinct 6 – Very Low Residential Density / Low Scale Recreation / Low Scale Educational / Low Scale Entertainment Uses precinct		
<p>PO67</p> <p>No additional lots are created within the precinct.</p>	<p>AO67</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>
<p>PO68</p> <p>Reconfigured lots have a minimum lot size of 2 hectares, unless the lot reconfiguration transfers lots to the higher parts of the land, to avoid the</p>	<p>AO68</p> <p>No acceptable outcomes are prescribed.</p>	<p>Not applicable.</p> <p>The subject site is not included within a Precinct and/or Sub-Precinct as identified in the Port Douglas / Craiglie Local Plan.</p>

need to fill existing lots to accommodate dwelling houses.		
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Table 7.2.4.4.b — Inconsistent uses in sub-precinct 1b - Waterfront North sub-precinct

INCONSISTENT USES		
<ul style="list-style-type: none"> • Agricultural supplies store • Air services • Animal husbandry • Animal keeping • Aquaculture • Brothel • Bulk landscape supplies • Car wash • Cemetery • Crematorium • Cropping • Detention facility • Dual occupancy • Dwelling house 	<ul style="list-style-type: none"> • Extractive industry • Funeral parlour • High impact industry • Intensive animal industry • Intensive horticulture • Major electricity infrastructure • Major sport, recreation and entertainment facility • Medium impact industry • Motor sport facility, • Outstation • Permanent plantation 	<ul style="list-style-type: none"> • Relocatable home park • Roadside stall • Rural industry • Rural workers accommodation • Service station • Showroom • Special industry • Tourist park • Transport depot • Veterinary services • Warehouse • Wholesale nursery • Winery

Table 7.2.4.4.c — Inconsistent uses in sub-precinct 1c - Waterfront South sub-precinct

INCONSISTENT USES		
<ul style="list-style-type: none"> • Adult store • Agricultural supplies store • Air services • Animal husbandry • Animal keeping • Brothel • Bulk landscape supplies • Car wash • Cemetery • Child care centre • Community care centre • Community residence • Community use • Crematorium • Cropping • Detention facility • Dual occupancy • Dwelling house • Dwelling unit • Extractive industry • Function facility • Funeral parlour • Garden centre 	<ul style="list-style-type: none"> • Hardware and trade supplies • Health care services • Home based business • Hospital • Hotel • Indoor sport and recreation • Intensive animal industry • Intensive horticulture • Major electricity infrastructure • Major sport, recreation and entertainment facility • Market • Motor sport facility • Multiple dwelling • Nature-based tourism • Nightclub entertainment facility • Outdoor sales • Outdoor sport and recreation • Outstation 	<ul style="list-style-type: none"> • Permanent plantation • Place of worship • Relocatable home park • Residential care facility • Resort complex • Retirement facility • Roadside stall • Rooming accommodation • Rural industry • Rural workers accommodation • Sales office • Shopping centre • Short-term accommodation • Showroom • Special industry • Theatre • Tourist attraction • Tourist park • Transport depot • Veterinary services • Warehouse • Wholesale nursery • Winery

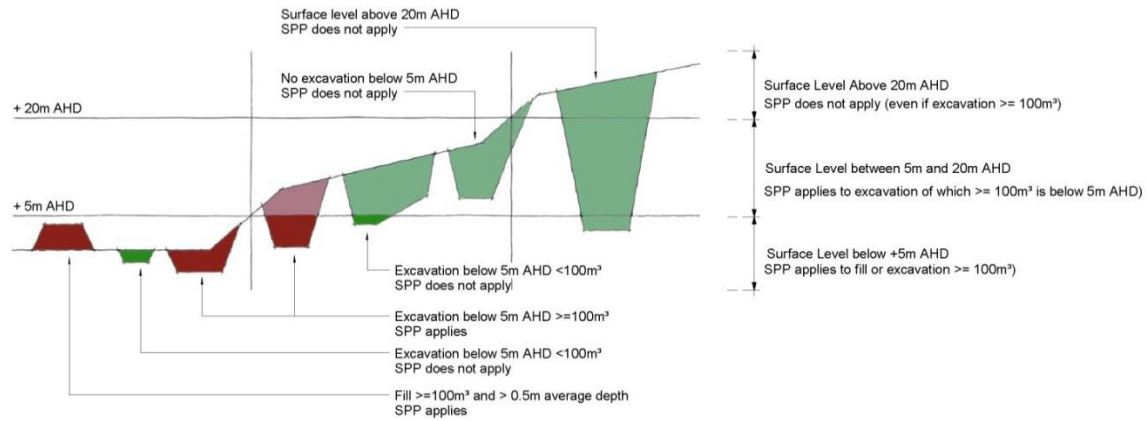
8.2.1 ACID SULPHATE SOILS OVERLAY CODE

8.2.1.3Table 8.2.1.3.a – Acid sulphate soils overlay code – assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
PO1 The extent and location of potential or actual acid sulphate soils is accurately identified.	AO1.1 No excavation or filling occurs on the site.	Complies with PO1. Demonstrated compliance with the requirements of PO1 will be provided in detailed geotechnical reporting, to be prepared and likely to form part of any future information request response submitted to Council.
	AO1.2 An acid sulphate soils investigation is undertaken.	
PO2 Development avoids disturbing potential acid sulphate soils or actual acid sulphate soils or is managed to avoid or minimise the release of acid and metal contaminants.	AO2.1 The disturbance of potential acid sulphate soils or actual acid sulphate soils is avoided by: <ul style="list-style-type: none"> (a) not excavating, or otherwise removing, soil or sediment identified as containing potential or actual acid sulphate soils; (b) not permanently or temporarily extracting groundwater that results in the aeration of previously saturated acid sulphate soils; (c) not undertaking filling that results in: (d) actual acid sulphate soils being moved below the water table; (e) previously saturated acid sulphate soils being aerated. 	Complies with PO2. Demonstrated compliance with the requirements of PO2 will be provided in detailed geotechnical reporting, to be prepared and likely to form part of any future information request response submitted to Council. Additionally, it is noted that the appropriate management of acid sulfate soils and compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of an appropriate Condition(s) upon the Development Permit.
	AO2.2	

	<p>The disturbance of potential acid sulphate soils or actual acid sulphate soils is undertaken in accordance with an acid sulphate soils management plan and avoids the release of metal contaminants by:</p> <ul style="list-style-type: none"> (a) neutralising existing acidity and preventing the generation of acid and metal contaminants; (b) preventing the release of surface or groundwater flows containing acid and metal contaminants into the environment; (c) preventing the in-sit oxidisation of potential acid sulphate soils and actual acid sulphate soils through ground water level management; (d) appropriately treating acid sulphate soils before disposal occurs on or off site; (e) documenting strategies and reporting requirements in an acid sulphate soils environmental management plan. 	
<p>PO3</p> <p>No environmental harm is caused as a result of exposure to potential acid sulphate soils or actual acid sulphate soils.</p>	<p>AO3</p> <p>No acceptable outcomes are prescribed.</p>	<p>Complies with PO3.</p> <p>Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of an appropriate Condition(s) upon the Development Permit.</p>

Figure 8.2.1.3.a – Acid sulphate soils (SPP triggers)



8.2.2 BUSHFIRE HAZARD OVERLAY CODE

Table 8.2.2.3.a – Bushfire hazard overlay code –assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Compatible development		
PO1 A vulnerable use is not established or materially intensified within a bushfire hazard area (bushfire prone area) unless there is an overriding need or other exceptional circumstances.	AO1 Vulnerable uses are not established or expanded.	Not applicable. The proposal seeks approval for a Material Change of Use for a resort complex. Pursuant to the Planning Scheme Bushfire Hazard Overlay Code, a resort complex is not considered to be a vulnerable use.
PO2 Emergency services and uses providing community support services are able to function effectively during and immediately after a bushfire hazard event.	AO2 Emergency Services and uses providing community support services are not located in a bushfire hazard sub-category and have direct access to low hazard evacuation routes.	Not applicable. The proposed development does not involve emergency services and/or uses providing community support services.
PO3 Development involving hazardous materials manufactured or stored in bulk is not located in bushfire hazard sub-category.	AO3 The manufacture or storage of hazardous material in bulk does not occur within bushfire hazard sub-category.	Not applicable. The storage and/or manufacturing of hazardous materials is not proposed.
Development design and separation from bushfire hazard – reconfiguration of lots		
PO4.1 Where reconfiguration is undertaken in an urban area or is for urban purposes or smaller scale rural	AO4.1 No new lots are created within a bushfire hazard sub-category.	Not applicable. Reconfiguration of a lot is not proposed. The subject site is an existing allotment.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>residential purposes, a separation distance from hazardous vegetation is provided to achieve a radiant heat flux level of 29kW/m at the edge of the proposed lot(s)</p> <p>PO4.2</p> <p>Where reconfiguration is undertaken for other purposes, a building envelope of reasonable dimensions is provided on each lot which achieves radiant heat flux level of 29kW/m at any point.</p>	<p>AO4.2</p> <p>Lots are separated from hazardous vegetation by a distance that:</p> <ul style="list-style-type: none"> (a) achieves radiant heat flux level of 29kW/m² at all boundaries; and (b) is contained wholly within the development site. 	<p>Not applicable.</p> <p>Reconfiguration of a lot is not proposed. The subject site is an existing allotment.</p>
<p>PO5</p> <p>Where reconfiguration is undertaken in an urban area or is for urban purposes, a constructed perimeter road with reticulated water supply is established between the lots and the hazardous vegetation and is readily accessible at all times for urban fire fighting vehicles. The access is available for both firefighting and maintenance/defensive works.</p>	<p>AO5.1</p> <p>Lot boundaries are separated from hazardous vegetation by a public road which:</p> <ul style="list-style-type: none"> (a) has a two-lane sealed carriageway; (b) contains a reticulated water supply; (c) is connected to other public roads at both ends and at intervals of no more than 500m; (d) accommodates geometry and turning radii in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; (e) has a minimum of 4.8m vertical clearance above the road; (f) is designed to ensure hydrants and water access points are not located within parking bay allocations; and incorporates roll-over kerbing. 	<p>Not applicable.</p> <p>Reconfiguration of a lot is not proposed. The subject site is an existing allotment.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<p>AO5.2</p> <p>Fire hydrants are designed and installed in accordance with AS2419.1 2005, unless otherwise specified by the relevant water entity.</p>	<p>Not applicable.</p> <p>Reconfiguration of a lot is not proposed. The subject site is an existing allotment.</p>
<p>PO6</p> <p>Where reconfiguration is undertaken for smaller scale rural residential purposes, either a constructed perimeter road or a formed, all weather fire trail is established between the lots and the hazardous vegetation and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>The access is available for both firefighting and maintenance/hazard reduction works.</p>	<p>AO6</p> <p>Lot boundaries are separated from hazardous vegetation by a public road or fire trail which has:</p> <ul style="list-style-type: none"> (a) a reserve or easement width of at least 20m; (b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15-tonne vehicle and which is at least 6m clear of vegetation; (c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path; (d) a minimum of 4.8m vertical clearance; (e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; (f) a maximum gradient of 12.5%; (g) across fall of no greater than 10 degrees; (h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy; (i) vehicular access at each end which is connected to the public road network at intervals of no more than 500m; 	<p>Not applicable.</p> <p>Reconfiguration of a lot is not proposed. The subject site is an existing allotment.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<ul style="list-style-type: none"> (j) designated fire trail signage; (k) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and (l) if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services. 	
<p>PO7</p> <p>Where reconfiguration is undertaken for other purposes, a formed, all weather fire trail is provided between the hazardous vegetation and either the lot boundary or building envelope and is readily accessible at all times for the type of fire fighting vehicles servicing the area.</p> <p>However, a fire trail will not be required where it would not serve a practical fire management purpose.</p>	<p>AO7</p> <p>Lot boundaries are separated from hazardous vegetation by a public road or fire trail which has:</p> <ul style="list-style-type: none"> (a) a reserve or easement width of at least 20m; (b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15-tonne vehicle and which is at least 6m clear of vegetation; no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path; (c) a minimum of 4.8m vertical clearance; (d) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; (e) a maximum gradient of 12.5%; across fall of no greater than 10 degrees; (f) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy; 	<p>Not applicable.</p> <p>Reconfiguration of a lot is not proposed. The subject site is an existing allotment.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<ul style="list-style-type: none"> (g) vehicular access at each end which is connected to the public road network; (h) designated fire trail signage; (i) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and if a fire trail, has an access easement that is granted in favour of Council and Queensland Fire and Emergency Services. 	
<p>PO8</p> <p>The development design responds to the potential threat of bushfire and establishes clear evacuation routes which demonstrate an acceptable or tolerable risk to people.</p>	<p>AO8</p> <p>The lot layout:</p> <ul style="list-style-type: none"> (a) minimises the length of the development perimeter exposed to, or adjoining hazardous vegetation; (b) avoids the creation of potential bottle-neck points in the movement network; (c) establishes direct access to a safe assembly /evacuation area in the event of an approaching bushfire; and ensures roads likely to be used in the event of a fire are designed to minimise traffic congestion. 	<p>Not applicable.</p> <p>Reconfiguration of a lot is not proposed. The subject site is an existing allotment.</p>
<p>PO9</p> <p>Critical infrastructure does not increase the potential bushfire hazard.</p>	<p>AO9</p> <p>Critical or potentially hazardous infrastructure such as water supply, electricity, gas and telecommunications are placed underground.</p>	<p>Not applicable.</p> <p>Reconfiguration of a lot is not proposed. The subject site is an existing allotment.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Development design and separation from bushfire hazard – material change of use		
<p>PO10</p> <p>Development is located and designed to ensure proposed buildings or building envelopes achieve a radiant heat flux level at any point on the building or envelope respectively, of:</p> <ul style="list-style-type: none"> (a) 10kW/m where involving a vulnerable use; or (b) 29kW/m otherwise. <p>The radiant heat flux level is achieved by separation unless this is not practically achievable.</p>	<p>AO10</p> <p>Buildings or building envelopes are separated from hazardous vegetation by a distance that:</p> <ul style="list-style-type: none"> (a) achieves a radiant heat flux level of at any point on the building or envelope respectively, of 10kW/m for a vulnerable use or 29kW/m otherwise; and (b) is contained wholly within the development site. 	<p>Complies with AO10.</p> <p>The subject site is an existing allotment which is predominantly clear of vegetation. The site has been cleared, in accordance with previous approvals issued/relevant to the subject land.</p> <p>The proposed development will be contained wholly within the development site.</p> <p>It is submitted that vegetation surrounding the subject site is minimal and mostly comprised of landscaping associated with surrounding land uses. Small patches of vegetation that exist throughout the Port Douglas Mirage Golf Course (which borders the western and northern property boundaries), is limited and mostly bound by well-maintained/grassed golf fairways and greens.</p> <p>In considering this, it is submitted that any vegetation present/directly surrounding the subject site is not considered to be hazardous in nature.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>PO11</p> <p>A formed, all weather fire trail is provided between the hazardous vegetation and the site boundary or building envelope and is readily accessible at all times for the type of fire fighting vehicles servicing the area. However, a fire trail will not be required where it would not serve a practical fire management purpose.</p>	<p>AO11</p> <p>Development sites are separated from hazardous vegetation by a public road or fire trail which has:</p> <ul style="list-style-type: none"> (a) a reserve or easement width of at least 20m; (b) a minimum trafficable (cleared and formed) width of 4m capable of accommodating a 15-tonne vehicle and which is at least 6m clear of vegetation; (c) no cut or fill embankments or retaining walls adjacent to the 4m wide trafficable path; (d) a minimum of 4.8m vertical clearance; (e) turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; (f) a maximum gradient of 12.5%; (g) across fall of no greater than 10 degrees; (h) drainage and erosion control devices in accordance with the standards prescribed in a planning scheme policy; (i) vehicular access at each end which is connected to the public road network which is connected to the public road network at intervals of no more than 500m; (j) designated fire trail signage; (k) if used, has gates locked with a system authorised by Queensland Fire and Emergency Services; and if a fire trail, has an 	<p>Complies with PO11.</p> <p>The subject site is an existing allotment which is predominantly clear of vegetation. The site has been cleared, in accordance with previous approvals issued/relevant to the subject land.</p> <p>It is submitted that vegetation surrounding the subject site is minimal and mostly comprised of landscaping associated with the surrounding land uses. Small patches of vegetation that exist throughout the Port Douglas Mirage Golf Course (which borders the western and northern property boundaries), is limited and mostly bound by well-maintained/grassed golf fairways and greens. It is noted that the Port Douglas Mirage Golf Course comprises relatively flat land and provides an effective buffer (approx. 150m) between the subject site and the closest hazardous vegetation.</p> <p>Given the developed nature of land surrounding the subject site, it is submitted that a fire trail will not be required, as it is unlikely to serve a practical fire management purpose. Notwithstanding this, it is submitted that the land used for recreational purposes (Port Douglas Mirage Golf Course) to the west and north may be used as a fire trail in the event of a bushfire.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	access easement that is granted in favour of Council and Queensland Fire and Emergency Services.	In considering the abovementioned, it is submitted that the risk of bushfires if significantly reduced.
PO12 All premises are provided with vehicular access that enables safe evacuation for occupants and easy access by firefighting appliances.	AO12 Private driveways: <ul style="list-style-type: none"> (a) do not exceed a length of 60m from the street to the building; (b) do not exceed a gradient of 12.5%; (c) have a minimum width of 3.5m; (d) have a minimum of 4.8m vertical clearance; 	Not applicable. The proposal does not include a private driveway.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	(e) accommodate turning areas for fire-fighting appliances in accordance with Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; and serve no more than 3 dwellings or buildings.	
PO13 Development outside reticulated water supply areas includes a dedicated static supply that is available solely for firefighting purposes and can be accessed by firefighting appliances.	AO13 A water tank is provided within 10m of each building (other than a class 10 building) which: <ul style="list-style-type: none"> (a) is either below ground level or of non-flammable construction; has a take-off connection at a level that allows the following dedicated, static water supply to be left available for access by fire fighters: <ul style="list-style-type: none"> (i) 10,000l for residential buildings (ii) 45,000l for industrial buildings; and (iii) 20,000l for other buildings; (a) includes shielding of tanks and pumps in accordance with the relevant standards; (b) includes a hardstand area allowing medium rigid vehicle (15 tonne fire appliance) access within 6m of the tank; (c) is provided with fire brigade tank fittings – 50mm ball valve and male camlock coupling and, if underground, an access hole of 200mm (minimum) to accommodate suction lines; and 	Not applicable. The subject site is located within an existing urban environment, where connections to Councils reticulated water supply are existing/available.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	is clearly identified by directional signage provided at the street frontage.	
PO14 Landscaping does not increase the potential bushfire risk.	AO14 Landscaping uses species that are less likely to exacerbate a bushfire event and does not increase fuel loads within separation areas.	Complies with AO14. A list of preferred species that will be incorporated into the proposed development has been provided in Attachment 4 – Proposal Plans. It is submitted that the species identified in the concept landscape plan are not likely to exacerbate a bushfire event, nor increase fuel loads within separation areas.
PO15 The risk of bushfire and the need to mitigate that risk is balanced against other factors (such as but not limited to, biodiversity or scenic amenity).	AO15 Bushfire risk mitigation treatments do not have a significant impact on the natural environment or landscape character of the locality where this has value.	Complies with AO15. The subject site is an existing allotment which is predominantly clear of vegetation. The site has been cleared, in accordance with previous approvals issued/relevant to the subject land. The proposed development will be located entirely within the subject site and incorporates firefighting equipment/infrastructure that are not expected to impact on the natural environment or landscape character. A fire hydrant booster is proposed within the north-east corner of the subject site, adjacent to the service vehicle access. Fire fighting infrastructure will be suitably landscaped and screened from view.

8.2.3 COASTAL ENVIRONMENT OVERLAY CODE

Table 8.2.3.3.a – Coastal environment overlay code – self-assessable and assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
PO1 No works other than coastal protection works extend seaward of the coastal building line.	AO1.1 Development (including all buildings and other permanent structures such as swimming pools and retaining walls) does not extend seaward of a coastal building line.	Not applicable. There is no coastal building line applicable to the subject site.
	AO1.2 Coastal protection works are only undertaken as a last resort where coastal erosion presents an immediate threat to public safety or existing buildings or structures and the property cannot be relocated or abandoned.	Not applicable. Coastal protection works are not proposed.
	AO1.3 Coastal protection works are as far landward as practicable on the lot containing the property to the maximum extent reasonable.	Not applicable. Coastal protection works are not proposed.
	AO1.4 Coastal protection work mitigates any increase in the coastal hazard.	Not applicable. Coastal protection works are not proposed.
PO2 Where a coastal building line does not exist on a lot fronting the coast or a reserve adjoining the coast, development is setback to maintain the amenity and use of the coastal resource.	AO2 Where a coastal building line does not exist on a lot fronting the coast or a reserve adjoining the coast, development (including all buildings and structures such as swimming pools) and retaining walls are set back not less than 6 metres from the seaward boundary of the lot.	Not applicable. The subject site does not adjoin the coast. It is noted however that the land adjoining the subject site to the north and west is included within the coastal management district. Notwithstanding this, it is

		submitted the adjoining land is used for recreational purposes (golf course) and is not a coastal resource.
For assessable development		
Erosion prone areas		
PO3 Development identifies erosion prone areas (coastal hazards).	AO3 No acceptable outcomes are prescribed.	Complies with PO3. Erosion prone areas have been identified within the site and development has been appropriately designed/sited so as to ensure erosion prone areas are avoided. A small portion of the site's western boundary is impacted by the Planning Scheme's Coastal Processes Overlay mapping (Erosion Prone Area). Development will be setback approximately 4.5m from the rear (western) property boundary and will therefore avoid mapped erosion prone areas.
PO4 Erosion prone areas are free from development to allow for natural coastal processes.	AO4.1 Development is not located within the Erosion prone area, unless it can be demonstrated that the development is for: (a) community infrastructure where no suitable alternative location or site exists for this infrastructure; (b) development that reflects the preferred development outcomes in accordance with the zoning of the site (i.e. in the Low-density residential zone, a dwelling house is a preferred development outcome in accordance with the zoning of the site)	Complies with AO4.1. Erosion prone areas have been identified within the site and development has been appropriately designed/sited so as to ensure impacts upon erosion prone areas are avoided. A small portion of the site's western boundary is impacted by the Planning Scheme's Coastal Processes Overlay mapping (Erosion Prone Area). Development will be setback approximately 4.5m from the rear (western) property boundary and will therefore avoid mapped erosion prone areas.

	<p>AO4.2</p> <p>Development involving existing permanent buildings and structures within an erosion prone area does not increase in intensity of its use by:</p> <ul style="list-style-type: none"> (a) adding additional buildings or structures; or (b) incorporating a land use that will result in an increase in the number of people or employees occupying the site. 	<p>Not applicable.</p> <p>The subject site comprises vacant land, with no existing permanent buildings and/or structures.</p>
Coastal management districts		
<p>PO5</p> <p>Natural processes and protective functions of landforms and vegetation are maintained.</p>	<p>PO5.1</p> <p>Development within the coastal management district:</p> <ul style="list-style-type: none"> (a) maintains vegetation on coastal land forms where its removal or damage may: <ul style="list-style-type: none"> (i) destabilise the area and increase the potential for coastal erosion, or (ii) interrupt the natural sediment trapping processes or dune or land building processes; (a) maintains sediment volumes of dunes and near-shore coastal landforms, or where a reduction in sediment volumes cannot be avoided, increased risks to development from coastal erosion are mitigated by location, design and construction and operating standards; 	<p>Not applicable.</p> <p>The subject site is not mapped as being impacted by the Coastal Management District.</p>

	<ul style="list-style-type: none"> (b) minimises the need for erosion control structures or riverine hardening through location, design and construction standards; (c) maintains physical coastal processes outside the development footprint for the development, including longshore transport of sediment along the coast; (d) reduces the risk of shoreline erosion for areas adjacent to the development footprint to the maximum extent feasible in the case of erosion control structures. 	
	<p>PO5.2</p> <p>Where development proposes the construction of an erosion control structure:</p> <ul style="list-style-type: none"> (a) it is demonstrated that it is the only feasible option for protecting permanent structures from coastal erosion; and (b) those permanent structures cannot be abandoned or relocated in the event of coastal erosion occurring. 	<p>Not applicable.</p> <p>The subject site is not mapped as being impacted by the Coastal Management District.</p>

	<p>PO5.3</p> <p>Development involving reclamation:</p> <p>(a) does not alter, or otherwise minimises impacts on, the physical characteristics of a waterway or the seabed near the reclamation, including flow regimes, hydrodynamic forces, tidal water and riverbank stability;</p> <p>(b) is located outside active sediment transport area, or otherwise maintains sediment transport processes as close as possible to their natural state;</p> <p>(c) ensures activities associated with the operation of the development maintain the structure and condition of vegetation communities and avoid wind and water run-off erosion.</p>	<p>Not applicable.</p> <p>The subject site is not mapped as being impacted by the Coastal Management District.</p>
<p>PO6</p> <p>Development avoids or minimises adverse impacts on coastal resources and their values to the maximum extent reasonable.</p>	<p>AO6.1</p> <p>Coastal protection work that is in the form of beach nourishment uses methods of placement suitable for the location that do not interfere with the long-term use of the locality, or natural values within or neighbouring the proposed placement site.</p>	<p>Not applicable.</p> <p>The subject site is not mapped as being impacted by the Coastal Management District.</p>
	<p>AO6.2</p> <p>Marine development is located and designed to expand on or redevelop existing marine infrastructure unless it is demonstrated that it is not practicable to co-locate the development with existing marine infrastructure;</p>	<p>Not applicable.</p> <p>The subject site is not mapped as being impacted by the Coastal Management District.</p>

	AO6.3 Measures are incorporated as part of siting and design of the development to maintain or enhance water quality to achieve the environmental values and water quality objectives outlined in the Environmental Protection (Water) Policy 2009.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
	AO6.4 Development avoids the disturbance of acid sulphate soils, or where it is demonstrated that this is not possible, the disturbance of acid sulphate soils is carefully managed to minimise and mitigate the adverse effects of disturbance on coastal resources.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
	AO6.5 Design and siting of development protects and retains identified ecological values and underlying ecosystem processes within the development site to the greatest extent practicable.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
PO7 Development is to maintain access to and along the foreshore for general public access.	AO7.1 Development provides for regular access points for pedestrians including approved walking tracks, boardwalks and viewing platforms.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
	AO7.2 Development provides for regular access points for vehicles including approved roads and tracks.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
	AO7.3	Not applicable.

	Development demonstrates an alternative solution to achieve an equivalent standard of performance.	The subject site is not mapped as being impacted by the Coastal Management District.
PO8 Public access to the coast is appropriately located, designed and operated.	AO8.1 Development maintains or enhances public access to the coast.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
	AO8.2 Development is located adjacent to state coastal land or tidal water and minimises and offsets any loss of access to and along the foreshore within 500 metres.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
	AO8.3 Development adjacent to state coastal land or tidal water demonstrates an alternative solution to achieve an equivalent standard and quality of access.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
PO9 Development adjacent to state coastal land or tidal water is located, designed and operated to: <ul style="list-style-type: none"> (a) maintain existing access to and along the foreshore; (b) minimise any loss of access to and along the foreshore, or (c) offset any loss of access to and along the foreshore by providing for enhanced alternative access in the general location. 	AO9.1 Development adjacent to state coastal land or tidal water: <ul style="list-style-type: none"> (a) demonstrates that restrictions to public access are necessary for: <ul style="list-style-type: none"> (i) the safe and secure operation of development; (ii) the maintenance of coastal all and forms and coastal habitat; (a) maintains public access (including public access infrastructure that has been approved by the local government or relevant authority) through the site to the foreshore for: 	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.

	<ul style="list-style-type: none"> (i) pedestrians via access points including approved walking tracks, boardwalks and viewing platforms; (ii) vehicles via access points including approved roads or tracks. 	
	<p>AO9.2</p> <p>Development adjacent to state coastal land or tidal water: (a) is located and designed to:</p> <ul style="list-style-type: none"> (i) allow safe unimpeded access to, over, under or around built infrastructure located on, over or along the foreshore, for example through the provision of esplanades or easement corridors to preserve future access; ensure emergency vehicles can access the area near the development. <p>(a) minimises and offsets any loss of access to and along the foreshore within 500m of existing access points and development is located and designed to:</p> <ul style="list-style-type: none"> (i) allow safe unimpeded access to, over, under or around built infrastructure located on, over or along the foreshore, and ensure emergency vehicles can access the area near the development. 	<p>Not applicable.</p> <p>The subject site is not mapped as being impacted by the Coastal Management District.</p>
AO10	AO10.1	Not applicable.

Development that involves reconfiguring a lot for urban purposes adjacent to the coast is designed to ensure public access to the coast in consideration of public access demand from a whole-of-community basis and the maintenance of coastal landforms and coastal habitat.	Development complies if consideration of public access demand from a whole-of-community basis and the maintenance of coastal landforms and coastal habitat is undertaken.	The subject site is not mapped as being impacted by the Coastal Management District.
	AO10.2 Development demonstrates an alternative solution to achieve an equivalent standard and quality of access.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
PO11 Development maintains public access to State coastal land by avoiding private marine development attaching to, or extending across, non-tidal State coastal land.	AO11 Private marine access structures and other structures such as decks or boardwalks for private use do not attach to or extend across State coastal land that is situated above high water mark	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
PO12 Development in connection with an artificial waterway enhances public access to coastal waters.	AO12 The artificial waterway avoids intersecting with or connection to inundated land or leased land where the passage, use or movement of vessels in water on the land could be restricted or prohibited by the registered proprietor of the inundated land or leased land.	Not applicable. The subject site is not mapped as being impacted by the Coastal Management District.
Coastal landscapes, views and vistas		
PO13 Development maintains and / or enhances natural coastal landscapes, views and vistas.	AO13 No acceptable outcomes are prescribed.	Complies with PO13. No major impacts to existing views and vistas of the natural coastal landscape are anticipated to occur as a result of this development. The proposed development seeks approval to construct a resort complex (up to 13.5m in height) within the subject site. It may be submitted that the proposed development is likely to enhance views/vistas across

		the natural coastal landscape (Dickson Inlet to the west), through the provision of better vantage points across the site.
PO14 Coastal settlements are consolidated through the concentration of development within the existing urban areas through infill and conserving the natural state of the coastal area outside existing urban areas.	AO14 No acceptable outcomes are prescribed.	Complies with PO14. The proposal involves consolidating, infill development, on land currently situated within an existing urban environment. The subject site comprises of vacant land and is surrounded by existing tourist related development.
Private marine development		
PO15 Private marine development is to avoid attaching to, or extending across, non-tidal State coastal land.	AO15 Private marine development and other structures such as decks or boardwalks for private use do not attach to, or extend across, State coastal land that is situated above high water mark.	Not applicable. The proposal does not involve private marine development.
PO16 The location and design of private marine development does not adversely affect the safety of members of the public access to the foreshore.	AO16 Private marine development does not involve the erection or placement of any physical barrier preventing existing access, along a public access way to the foreshores.	Not applicable. The proposal does not involve private marine development.

<p>PO17</p> <p>Private marine development is of a height and scale and size compatible with the character and amenity of the location.</p>	<p>AO17</p> <p>Private marine development has regard to:</p> <ul style="list-style-type: none"> (a) the height, scale and size of the natural features of the immediate surroundings and locality; (b) the height, scale and size of existing buildings or other structures in the immediate surroundings and the locality; (c) if the relevant planning scheme states that desired height, scale or size of buildings or other structures in the immediate surroundings or locality – the stated desired height, scale or size. 	<p>Not applicable.</p> <p>The proposal does not involve private marine development.</p>
<p>PO18</p> <p>Private marine development avoids adverse impacts on coastal landforms and coastal processes.</p>	<p>AO18</p> <p>Private marine development does not require the construction of coastal protection works, shoreline or riverbank hardening or dredging for marine access.</p>	<p>Not applicable.</p> <p>The proposal does not involve private marine development.</p>
<p>For dry land marinas and artificial waterways</p>		
<p>PO19</p> <p>Dry land marinas and artificial waterways:</p> <ul style="list-style-type: none"> (a) avoid impacts on coastal resources; (b) do not contribute to the degradation of water quality; (c) do not increase the risk of flooding; (d) do not result in the degradation or loss of MSES; 	<p>AO19</p> <p>No acceptable solutions are prescribed.</p>	<p>Not applicable.</p> <p>The proposal does not involve dry land marinas and/or artificial waterways.</p>

<p>(e) do not result in an adverse change to the tidal prism of the natural waterway to which development is connected.</p> <p>(f) does not involve reclamation of tidal land other than for the purpose of:</p> <p>(i) coastal dependent development, public marine development; or</p> <p>(ii) community infra structure, where there is no feasible alternative; or</p> <p>(iii) strategic ports, boat harbours or strategic airports and aviation facilities in accordance with a statutory land use plan; or</p> <p>(iv) coastal protection works or works necessary to protect coastal resources and processes.</p>		
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8.2.4 FLOOD AND STORM TIDE HAZARD OVERLAY CODE

Table 8.2.4.3.a – Flood and storm tide hazards overlay code –assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>PO1</p> <p>Development is located and designed to: ensure the safety of all persons;</p> <p>(a) minimise damage to the development and contents of buildings;</p> <p>(b) provide suitable amenity; minimise disruption to residents, recovery time, and rebuilding or restoration costs after inundation events.</p>	<p>AO1.1</p> <p>Development is sited on parts of the land that is not within the Flood and Storm Tide Hazards overlay maps contained in Schedule 2;</p> <p>or</p> <p>For dwelling houses,</p>	<p>Complies with AO1.1 and AO1.3.</p> <p>It is submitted that the Flood and Storm Tide Hazard Overlay mapping relevant to this site is incorrect, given the partial excavation works that have previously occurred on site. Areas that have been previously excavated, in accordance with old approvals issued/relevant to the site, are mapped as being impacted by the Medium Storm Tide Hazard and 100 Yr ARI – Flood mapping. It is submitted that development is proposed to occur across the site, including within areas that have been previously excavated.</p> <p>Notwithstanding this, it is also noted that a small portion of the site's western boundary is impacted by the Planning Scheme's Flood and Storm Tide Hazard Overlay mapping. Development will be setback approximately 4.5m from the rear (western) property boundary, thus avoiding areas mapped as being impacted by storm tide hazards and/or flooding.</p>
	<p>AO1.2</p> <p>Development within the Flood and Storm Tide hazards overlay maps (excluding the Flood plain assessment sub-category) is designed to provide immunity to the Defined Inundation Event as outlined within Table 8.2.4.3.b plus a freeboard of 300mm.</p>	
	<p>AO1.3</p> <p>New buildings are:</p> <p>(a) not located within the overlay area;</p> <p>(b) located on the highest part of the site to minimise entrance of flood waters; provided with clear and direct pedestrian and vehicle evacuation routes off the site.</p>	
	<p>AO1.4</p>	<p>Not applicable.</p>

	In non-urban areas, buildings and infrastructure are set back 50 metres from natural riparian corridors to maintain their natural function of reducing velocity of floodwaters.	The subject site is located within an existing urban area.
For assessable development		
PO2 The development is compatible with the level of risk associated with the natural hazard.	AO2 The following uses are not located in land inundated by the Defined Flood Event (DFE) / Storm tide: (a) Retirement facility; (b) Community care facility; or Child care centre.	Not applicable. The proposal seeks approval to construct a resort complex and does not propose any “restricted” land uses.
PO3 Development siting and layout responds to flooding potential and maintains personal safety	For Material change of use AO3.1 New buildings are: (a) not located within the overlay area; (b) located on the highest part of the site to minimise entrance of flood waters; (c) provided with clear and direct pedestrian and vehicle evacuation routes off the site.	Complies with PO3. Demonstrated compliance with the requirements of PO3 will be provided as part of a detailed stormwater and flood assessment, to be prepared and likely to form part of any future information request response submitted to Council.
	AO3.2 The development incorporates an area on site that is at least 300mm above the highest known flood inundation level with sufficient space to accommodate the likely population of the development safely for a relatively short	

	time until flash flooding subsides, or people can be evacuated.	
	<p>AO3.3</p> <p>Where involving an extension to an existing dwelling house that is situated below DFE /Storm tide, the maximum size of the extension does not exceed 70m gross floor area.</p> <p>For Reconfiguring a lot</p>	
	<p>AO3.4</p> <p>Additional lots:</p> <p>(a) are not located in the hazard overlay area;</p> <p>(b) are demonstrated to be above the flood level identified for the site.</p>	
	<p>AO3.5</p> <p>Road and/or pathway layout ensures residents are not physically isolated from adjacent flood free urban areas and provides a safe and clear evacuation route path:</p> <p>(a) by locating entry points into the reconfiguration above the flood level and avoiding culs-de-sac or other non-permeable layouts; and</p> <p>(b) by direct and simple routes to main carriageways.</p>	

	<p>AO3.6</p> <p>Signage is provided on site (regardless of whether the land is in public or private ownership) indicating the position and path of all safe evacuation routes off the site and if the site contains, or is within 100m of a floodable waterway, hazard warning signage and depth indicators are also provided at key hazard points, such as at floodway crossings or entrances to low-lying reserves.</p>	
	<p>AO3.7</p> <p>There is no intensification of residential uses within the flood affected areas on land situated below the DFE/Storm tide.</p>	
	<p>For Material change of use (Residential uses)</p> <p>AO3.1</p> <p>The design and layout of buildings used for residential purposes minimise risk from flooding by providing: (a) parking and other low intensive, non-habitable uses at ground level;</p>	
<p>PO4</p> <p>Development is resilient to flood events by ensuring design and built form account for the potential risks of flooding.</p>	<p>For Material change of use (Non-residential uses)</p> <p>AO4.2</p> <p>Non-residential buildings and structures allow for the flow through of flood waters on the ground floor.</p>	<p>Complies with PO4.</p> <p>Demonstrated compliance with the requirements of PO4 will be provided as part of a detailed stormwater and flood assessment, to be prepared and likely to</p>

	<p>AO4.3</p> <p>Materials are stored on-site:</p> <p>(a) are those that are readily able to be moved in a flood event;</p> <p>(b) where capable of creating a safety hazard by being shifted by flood waters, are contained in order to minimise movement in times of flood.</p>	form part of any future information request response submitted to Council.
<p>PO5</p> <p>Development directly, indirectly and cumulatively avoids any increase in water flow velocity or flood level and does not increase the potential flood damage either on site or on other properties.</p>	<p>For Operational works</p> <p>AO5.1</p> <p>Works in urban areas associated with the proposed development do not involve:</p> <p>(a) any physical alteration to a watercourse or floodway including vegetation clearing; or</p> <p>(b) a net increase in filling (including berms and mounds).</p>	<p>Not applicable.</p> <p>The application seeks approval for a Material Change of Use for a Resort Complex.</p>
	<p>AO5.2</p> <p>(a) Works (including buildings and earthworks) in non-urban areas either:</p> <p>(a) do not involve a net increase in filling greater than 50m³; or</p> <p>(b) do not result in any reductions of on-site flood storage capacity and contain within the subject site any changes to depth/duration/velocity of flood waters;</p>	<p>Not applicable.</p> <p>The subject site is located within an urban area.</p>

	<p>(b) do not change flood characteristics outside the subject site in ways that result in:</p> <ul style="list-style-type: none"> (i) loss of flood storage; (ii) loss of/changes to flow paths; (iii) acceleration or retardation of flows or any reduction in flood warning times elsewhere on the flood plain. 	
	<p>For Material change of use</p> <p>AO5.3</p> <p>Where development is located in an area affected by DFE/Storm tide, a hydraulic and hydrology report, prepared by a suitably qualified professional, demonstrates that the development maintains the flood storage capacity on the subject site; and</p> <ul style="list-style-type: none"> (a) does not increase the volume, velocity, concentration of flow path alignment of stormwater flow across sites upstream, downstream or in the general vicinity of the subject site; and (b) does not increase ponding on sites upstream, downstream or in the general vicinity of the subject site. 	<p>Can comply with AO5.3.</p> <p>Demonstrated compliance with the requirements of AO5.3 will be provided as part of a detailed stormwater and flood assessment, to be prepared and likely to form part of any future information request response submitted to Council.</p> <p>Or alternatively, where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of an appropriate Condition(s) under the Development Permit.</p>
	<p>AO5.4</p>	<p>Not applicable.</p> <p>The subject site is located within an urban area.</p>

	In non-urban areas, buildings and infrastructure are set back 50 metres from natural riparian corridors to maintain their natural function of reducing velocity of floodwaters.	
PO6 Development avoids the release of hazardous materials into floodwaters.	For Material change of use AO6.1 Materials manufactured or stored on site are not hazardous or noxious, or comprise materials that may cause a detrimental effect on the environment if discharged in a flood event;	Not applicable. Hazardous materials are not likely to be stored on site.
	AO6.2 If a DFE level is adopted, structures used for the manufacture or storage of hazardous materials are: (a) located above the DFE level; (b) designed to prevent the intrusion of floodwaters.	Not applicable. Hazardous materials are not likely to be stored on site.
	AO6.3 Infrastructure is designed and constructed to resist hydrostatic and hydrodynamic forces as a result of inundation by the DFE.	Not applicable. Hazardous materials are not likely to be stored on site.
	AO6.4 If a flood level is not adopted, hazardous materials and their manufacturing equipment are located on the highest	Not applicable. Hazardous materials are not likely to be stored on site.

	part of the site to enhance flood immunity and designed to prevent the intrusion of floodwaters.	
PO7 The development supports, and does not unduly burden, disaster management response or recovery capacity and capabilities.	A07 Development does not: <ul style="list-style-type: none"> (a) increase the number of people calculated to be at risk of flooding; (b) increase the number of people likely to need evacuation; (c) shorten flood warning times; and (d) impact on the ability of traffic to use evacuation routes, or unreasonably increase traffic volumes on evacuation routes. 	Complies with PO7. Demonstrated compliance with the requirements of PO7 will be provided as part of a detailed stormwater and flood assessment, to be prepared and likely to form part of any future information request response submitted to Council.
PO8 Development involving community infrastructure: <ul style="list-style-type: none"> (a) remains functional to serve community need during and immediately after a flood event; (b) is designed, sited and operated to avoid adverse impacts on the community or environment due to impacts of flooding on infrastructure, facilities or access and egress routes; 	AO8.1 The following uses are not located on land inundated during a DFE/Storm tide: <ul style="list-style-type: none"> (a) community residence; and (b) emergency services; and (c) residential care facility; and (d) utility installations involving water and sewerage treatment plants; and (e) storage of valuable records or items of historic or cultural significance (e.g. archives, museums, galleries, libraries). 	Not applicable. The proposal seeks approval for a Material Change of Use for a Resort Complex. The proposal does not involve community infrastructure.

<p>(c) retains essential site access during a flood event; is able to remain functional even when other infrastructure or services may be compromised in a flood event.</p>	<p>AO8.2</p> <p>The following uses are not located on land inundated during a 1% AEP flood event:</p> <ul style="list-style-type: none"> (a) Community and cultural facilities, including facilities where an education and care service under the Education and care Services National law (Queensland) is operated or child care service under the <i>Child Care Act 2002</i> is conducted, (b) community centres; (c) meeting halls; (d) galleries; (e) libraries. <p>The following uses are not located on land inundated during a 0.5% AEP flood event.</p> <ul style="list-style-type: none"> (a) emergency shelters; (b) police facilities; (c) sub stations; (d) water treatment plant <p>The following uses are not located on land inundated during a 0.2% AEP flood event:</p> <ul style="list-style-type: none"> (a) correctional facilities; (b) emergency services; (c) power stations; (d) major switch yards. 	<p>Not applicable.</p> <p>The proposal seeks approval for a Material Change of Use for a Resort Complex. The proposal does not involve community infrastructure.</p>
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	<p>AO8.3</p> <p>The following uses have direct access to low hazard evacuation routes as defined in Table 8.2.4.3.c:</p> <ul style="list-style-type: none"> (a) community residence; and (b) emergency services; and (c) hospitals; and (d) residential care facility; and (e) sub stations; and (f) utility installations involving water and sewerage treatment plants. 	<p>Not applicable.</p> <p>The proposal seeks approval for a Material Change of Use for a Resort Complex. The proposal does not involve community infrastructure.</p>
	<p>AO8.4</p> <p>Any components of infrastructure that are likely to fail to function or may result in contamination when inundated by flood, such as electrical switch gear and motors, telecommunications connections, or water supply pipeline air valves are:</p> <ul style="list-style-type: none"> (a) located above DFE/Storm tide or the highest known flood level for the site; (b) designed and constructed to exclude floodwater intrusion / infiltration. 	<p>Not applicable.</p> <p>The proposal seeks approval for a Material Change of Use for a Resort Complex. The proposal does not involve community infrastructure.</p>
	<p>AO8.5</p> <p>Infrastructure is designed and constructed to resist hydrostatic and hydrodynamic forces as a result of inundation by a flood.</p>	<p>Not applicable.</p> <p>The proposal seeks approval for a Material Change of Use for a Resort Complex. The proposal does not involve community infrastructure.</p>

Table 8.2.4.3.b - Minimum immunity (floor levels) for development

MINIMUM IMMUNITY TO BE ACHIEVED (FLOOR LEVELS)	USES AND ELEMENTS OF ACTIVITIES ACCEPTABLE IN THE EVENT
20% AEP level	• Parks and open space.
5% AEP level	• Car parking facilities (including car parking associated with use of land).
1% AEP level	• All development (where not otherwise requiring an alternative level of minimum immunity).
0.5% AEP level	<ul style="list-style-type: none"> • Emergency services (if for a police station); • Industry activities (if including components which store, treat or use hazardous materials); • Substation; • Utility installation.
0.2% AEP level	<ul style="list-style-type: none"> • Emergency services; • Hospital; • Major electricity infrastructure; • Special industry.

Table 8.2.4.3.c - Degree of flood

CRITERIA	LOW	MEDIUM	HIGH	EXTREME
Wading ability	If necessary children and the elderly could wade. (Generally, safe wading velocity depth product is less than 0.25)	Fit adults can wade. (Generally, safe wading velocity depth product is less than 0.4)	Fit adults would have difficulty wading. (Generally, safe wading velocity depth product is less than 0.6)	Wading is not an option.
Evacuation distances	< 200 metres	200-400 metres	400-600 metres	600 metres
Maximum flood depths	< 0.3 metre	< 0.6 metre	< 1.2 metres	1.2 metres
Maximum flood velocity	< 0.4 metres per second	< 0.8 metres per second	< 1.5 metres per second	1.5 metres per second
Typical means of egress	Sedan	Sedan early, but 4WD or trucks later	4WD or trucks only in early stages, boats or helicopters	Large trucks, boats or helicopters

Timing Note: This category cannot be implemented until evacuation times have been established in the Counter Disaster Plan (Flooding)	Ample flood forecasting. Warning and evacuation routes remain passable for twice as long as evacuation time.	Evacuation routes remain trafficable for 1.5 times as long as the evacuation.	Evacuation routes remain trafficable for only up to minimum evacuation time.	There is insufficient evacuation time.
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8.2.6 LANDSCAPE VALUES OVERLAY CODE

Table 8.2.6.3.a – Landscape values overlay code – assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Development in a High landscape value area		
PO1 Development within High landscape value areas identified on the Landscape values overlay maps contained in Schedule 2: (a) avoids detrimental impacts on the landscape values of forested skylines, visible hillslopes, ridgelines, the coastal foreshore or the shoreline of other water bodies through the loss of vegetation; (b) is effectively screened from view from a road, lookout or other public place by an existing natural landform or native vegetation, or will be effectively screened by native vegetation within 3 years of construction; (c) retains existing vegetation and incorporates new landscaping to enhance existing vegetation and visually soften built form elements;	AO1.1 Buildings and structures are not more than 8.5 metres and two storeys in height.	Not applicable. The subject site is not mapped as having high landscape values, pursuant to the Landscape Values Code Overlay Mapping.
	AO1.2 Buildings and structures are setback not less than 50 metres from ridgelines or peaks.	Not applicable. The subject site is not mapped as having high landscape values, pursuant to the Landscape Values Code Overlay Mapping.
	AO1.3 Development is screened from view from roads or other public places by an existing natural landform or an existing native vegetation buffer.	Not applicable. The subject site is not mapped as having high landscape values, pursuant to the Landscape Values Code Overlay Mapping.
	AO1.4 Where development on land steeper than 1 in 6 (16.6%) cannot be avoided: (a) development follows the natural; contours of the site;	Not applicable. The subject site is not mapped as having high landscape values, pursuant to the Landscape Values Code Overlay Mapping.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>(d) incorporates development of a scale, design, height, position on site, construction materials and external finishes that are compatible with the landscape values of the locality;</p> <p>(e) avoids detrimental impacts on landscape values and excessive changes to the natural landform as a result of the location, position on site, scale, design, extent and alignment of earthworks, roads, driveways, retaining walls and other on-ground or in-ground infrastructure;</p> <p>(f) avoids detrimental impacts on landscape values and views as a result of the location, position on site, scale, design and alignment of telecommunications facilities, electricity towers, poles and lines and other tall infrastructure;</p> <p>(g) extractive industry operations are avoided.</p>	<p>(b) buildings are split level or suspended floor construction, or a combination of the two; lightweight materials are used to areas with suspended floors.</p>	
	<p>AO1.5</p> <p>The external features, walls and roofs of buildings and structures have a subdued and non-reflective palette.</p>	<p>Not applicable.</p> <p>The subject site is not mapped as having high landscape values, pursuant to the Landscape Values Code Overlay Mapping.</p>
	<p>AO1.6</p> <p>No clearing of native vegetation occurs on land with a slope greater than 1 in 6 (16.5%).</p>	<p>Not applicable.</p> <p>The subject site is not mapped as having high landscape values, pursuant to the Landscape Values Code Overlay Mapping.</p>
	<p>AO1.7</p> <p>Where for accommodation activities or reconfiguration of a lot in a High landscape value area, development demonstrates that the height, design, scale, positioning on-site, proposed construction materials and external finishes are compatible with the landscape values.</p>	<p>Not applicable.</p> <p>The subject site is not mapped as having high landscape values, pursuant to the Landscape Values Code Overlay Mapping.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	AO1.8 Advertising devices do not occur.	
Development within the Medium landscape value area		
PO2 Development within Medium landscape value areas identified on the Landscape values overlay maps contained in Schedule 2: (a) avoids detrimental impacts on the landscape values of forested skylines, visible hillslopes, ridgelines, the coastal foreshore or the shoreline of other water bodies through the loss of vegetation; (b) is effectively screened from view from a road, lookout or other public place by an existing natural landform or native vegetation, or will be effectively screened by native vegetation within 5 years of construction; (c) retains existing vegetation and incorporates new landscaping to enhance existing vegetation and visually soften built form elements; (d) incorporates development of a scale, design, height, position on site, construction materials and external	AO2.1 Buildings and structures are not more than 8.5 metres and two storeys in height.	Not applicable. The subject site is not mapped as having medium landscape values, pursuant to the Landscape Values Code Overlay Mapping.
	AO2.2 Development is screened from view from roads or other public places by an existing natural landform or an existing native vegetation buffer.	Not applicable. The subject site is not mapped as having medium landscape values, pursuant to the Landscape Values Code Overlay Mapping.
	AO2.3 Where development on land steeper than 1 in 6 (16.6%) cannot be avoided: (a) development follows the natural; contours of the site; (b) buildings are split level or suspended floor construction, or a combination of the two; (c) lightweight materials are used to areas with suspended floors.	Not applicable. The subject site is not mapped as having medium landscape values, pursuant to the Landscape Values Code Overlay Mapping.
	AO2.4 The external features, walls and roofs of buildings and structures have a subdued and non-reflective palette.	Not applicable.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>finishes that are compatible with the landscape values of the locality;</p> <p>(e) avoids detrimental impacts on landscape values and excessive changes to the natural landform as a result of the location, position on site, scale, design and alignment of earthworks, roads, driveways, retaining walls and other on-ground or in-ground infrastructure;</p> <p>(f) avoids detrimental impacts on landscape values and views as a result of the location, position on site, scale, design and alignment of telecommunications facilities, electricity towers, poles and lines and other tall infrastructure;</p> <p>(g) extractive industry operations are avoided, or where they cannot be avoided, are screened from view.</p>		The subject site is not mapped as having medium landscape values, pursuant to the Landscape Values Code Overlay Mapping.
	<p>AO2.5</p> <p>No clearing of native vegetation occurs on land with a slope greater than 1 in 6 (16.6%).</p>	<p>Not applicable.</p> <p>The subject site is not mapped as having medium landscape values, pursuant to the Landscape Values Code Overlay Mapping.</p>
	<p>AO2.6</p> <p>Advertising devices do not occur.</p>	<p>Not applicable.</p> <p>The subject site is not mapped as having medium landscape values, pursuant to the Landscape Values Code Overlay Mapping.</p>
Development within a Scenic route buffer / view corridor area		
<p>PO3</p> <p>Development within a Scenic route buffer / view corridor area as identified on the</p>	<p>AO3.1</p> <p>Where within a Scenic route buffer / view corridor area, the height of buildings and structures is not more than identified within the acceptable outcomes of the applicable zone code.</p>	<p>Complies with PO3.</p> <p>The eastern portion of the subject site is mapped as being within a scenic route buffer area.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>Landscape values overlay maps contained in Schedule 2:</p> <ul style="list-style-type: none"> (a) retains visual access to views of the surrounding landscape, the sea and other water bodies (b) retains existing vegetation and incorporates landscaping to visually screen and soften built form elements whilst not impeding distant views or view corridors; (c) incorporates building materials and external finishes that are compatible with the visual amenity and the landscape character; (d) minimises visual impacts on the setting and views in terms of: <ul style="list-style-type: none"> (i) the scale, height and setback of buildings; (ii) the extent of earthworks and impacts on the landform including the location and configuration of access roads and driveways; (iii) the scale, extent and visual prominence of advertising devices. 	<p>AO3.2</p> <p>No clearing of native vegetation is undertaken within a Scenic route buffer area.</p> <p>AO3.3</p> <p>Where within a Scenic route buffer / view corridor area development is set back and screened from view from a scenic route by existing native vegetation with a width of at least 10 metres and landscaped in accordance with the requirements of the landscaping code.</p> <p>AO3.4</p> <p>Development does not result in the replacement of, or creation of new, additional, or enlarged advertising devices.</p>	<ul style="list-style-type: none"> (a) It is understood that views of the surrounding landscape, from the subject site, are somewhat limited, given the vegetation that exists along the perimeter of the site as well as within the boundary setbacks of adjoining properties. Where possible, green vistas will be retained through the site (north/south and east/west), via the implementation of suitable landscaping. The proposed development has been appropriately sited and designed so as to ensure that the building's bulk and scale does not adversely impact upon existing green vistas observed across/through the site. (b) Landscaping will be applied to all property boundaries and throughout the site, to soften the built form. A preliminary landscape plan has been prepared and is included within the suite of proposal plans provided in Attachment 4. It is anticipated that landscaping will be implemented across 60%+ (inclusive of roof top plantings) of the site. It is noted that the proposed development incorporates vertical landscaping, with approximately 1,500m² of roof area to be landscaped. (c) Appropriate building materials and external finishes will be incorporated into the design. A list of proposed building materials and

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		<p>finishes has been included within the suite of proposal plans provided in Attachment 4. Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of appropriate Condition(s) upon the Development Permit.</p> <p>(d) Additionally, it is noted that visual impacts will be reduced, as a result of the following adopted design principals:</p> <ul style="list-style-type: none"> • development has been appropriately designed and sited on site to ensure building height and mass is situated towards the rear of the site, thus reducing visual impacts when observed from the public realm. • Built form has been split up across the site. Development will be accommodated within four (4) separate buildings. • Suitable separation distances between each building located on site will be ensured to further reduce the bulk of the built form. • Suitable landscaping will be implemented throughout the site and in between the built form, so as to soften the appearance as well as ensure existing green vistas currently

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		<p>observed from adjoining properties are maintained.</p> <ul style="list-style-type: none"> • Appropriate setbacks from the frontage have been implemented (a minimum of 6m). All boundary setbacks will be landscaped. • The Porte Cochre proposed within the north east corner of the subject site will further reduce the bulk and scale of the appearance of Building 4, as detailed on the proposal plans, which incorporates a variety of ancillary uses. <p>Excavation will be required to facilitate construction of the proposed development. Notwithstanding this, it is noted that a portion of the site has already been excavated (in part), in accordance with previously approvals issued/relevant to the subject site. In considering this, excavation works are not anticipated to be extensive. Excavation works will form part of future downstream operational works approvals relevant to the site. Additionally, it is noted that two (2) access driveways are proposed. One will be used by guests and the like, with the other to provide a separate service vehicle access. The site has approximately 168m of frontage to Port Douglas Road and therefore two separate vehicle accesses is</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		not considered to result in significant impacts to the existing landform.
Development within the Coastal scenery area		
PO4 The landscape values of the Coastal scenery zone as identified on the Landscape values overlay maps contained in Schedule 2 are managed to be integrated and limit the visual impact of development.	AO4.1 The dominance of the natural character of the coast is maintained or enhanced when viewed from the foreshore.	Not applicable. The subject site is not mapped as being within a coastal scenery area, pursuant to the Landscape Values Code Overlay Mapping.
	AO4.2 Where located adjacent to the foreshore buildings and structures are setback: <ul style="list-style-type: none"> (a) Where no adjoining development, a minimum of 50 metres from the coastal high-water mark and the setback area is landscaped with a native vegetation buffer that has a minimum width of 25 metres; (b) Where there is adjoining development, setbacks will be consistent with that of adjoining buildings and structures, but not less than 10 metres from the coastal high-water mark. The 	

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<p>setback area is landscaped in accordance with the requirements of the Landscaping code.</p> <p>AO4.3</p> <p>Where separated from the foreshore by land contained within public ownership (e.g. unallocated State land, esplanade or other public open space), buildings and structures area setback:</p> <p>(a) where no adjoining development, a minimum of 6 metres from the coastward property boundary. The setback area is landscaped in accordance with the requirements of the Landscaping code; or</p> <p>(b) where there is adjoining development, setbacks will be consistent with that of adjoining buildings and structures. The setback area is landscaped in accordance with the requirements of the Landscaping code.</p>	
<p>PO5</p> <p>Development is to maximise opportunities to maintain and/or enhance natural landscape values through the maintenance and restoration of vegetated buffers between development and coastal waters, where practical.</p>	<p>AO5</p> <p>No clearing of native vegetation is undertaken within a Coastal scenery area zone, except for exempt vegetation damage undertaken in accordance with the Vegetation management code</p>	<p>Not applicable.</p> <p>The subject site is not mapped as being within a coastal scenery area, pursuant to the Landscape Values Code Overlay Mapping.</p>

8.2.10 TRANSPORT NETWORK OVERLAY CODE

Table 8.2.10.3.a – Transport network overlay code – assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
PO1 Development supports the road hierarchy for the region.	AO1.1 Development is compatible with the intended role and function of the transport network as identified on the Transport network overlay maps contained in Schedule 2.	Complies with PO1. The subject site has frontage to Port Douglas Road, being a State-controlled Road. Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO1. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by Council. Notwithstanding this, a technical note has been prepared by PTT Traffic and Transport Engineers and is provided in Attachment 5. Upon initial (high-level) review/assessment of the proposed concept plan/design, it is submitted that the available sight distance at the proposed site access appears to be adequate for the local speed limit (60km/hr). Suggested upgrades to the road environment have been included in the technical note and will likely contribute to ensuring the proposed development does not compromise the safety and efficiency of the transport network.
	AO1.2 Development does not compromise the safety and efficiency of the transport network.	
	AO1.3 Development is designed to provide access via the lowest order road, where legal and practicable access can be provided to that road.	
PO2	AO2	Not applicable.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Transport infrastructure is provided in an integrated and timely manner.	Development provides infrastructure (including improvements to existing infrastructure) in accordance with: (a) the Transport network overlay maps contained in Schedule 2; any relevant Local Plan.	The proposed development does not involve the establishment/construction of transport infrastructure.
PO3 Development involving sensitive land uses within a major transport corridor buffer area is located, designed and maintained to avoid or mitigate adverse impacts on amenity for the sensitive land use.	AO3 No acceptable outcomes are prescribed.	Complies with PO3. The site is mapped as being impacted by a major transport corridor buffer area. The proposed development has been appropriately designed and sited to ensure amenity impacts are reduced/mitigated where possible. Appropriate boundary setbacks and dense landscaping along the site's primary road frontage are proposed to further reduce amenity impacts.
PO4 Development does not compromise the intended role and function or safety and efficiency of major transport corridors.	AO4.1 Development is compatible with the role and function (including the future role and function) of major transport corridors. AO4.2 Direct access is not provided to a major transport corridor where legal and practical access from another road is available.	Complies with PO4. The subject site has frontage to Port Douglas Road, being a State-controlled Road. Two access points are proposed to support the development. The main access will be used by guests and the like, and will be a shared access, allowing for vehicles to both enter and exit the site. The second vehicle access, proposed to be located adjacent to the northern property boundary, will be restricted to service vehicles only. The service

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	AO4.3 Intersection and access points associated with major transport corridors are located in accordance with: (a) the Transport network overlay maps contained in Schedule 2; and (b) any relevant Local Plan.	<p>vehicle access is located approximately 100m from the nearest roundabout (Lakeland Avenue/Avenue of Palms roundabout), which is located north of the site, on Port Douglas Road. Additionally, it is noted that there is an adequate separation distance between the two proposed accesses (approximately 70m) sufficient to mitigate potential traffic conflicts.</p> <p>Port Douglas Road is a low speed environment (60km/hour). Additionally, it is noted that the site has no legal/practical access to any other road.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO4. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by Council.</p>
	AO4.4 The layout of development and the design of the associated access is compatible with existing and future boundaries of the major transport corridor or major transport facility.	
PO5 Development retains and enhances existing vegetation between a development and a major transport corridor, so as to provide screening to potential noise, dust, odour and visual impacts emanating from the corridor.	AO5 No acceptable outcomes are prescribed.	Complies with PO5. Landscaping to the frontage of the subject site is minimal at present. Notwithstanding this, where possible, existing vegetation will be retained along the frontage. Extensive landscaping within the front boundary setback is proposed and forms part of the application. Demonstrated compliance with the requirements of PO5 will be provided in detailed landscape plans, to be

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		prepared and likely to form part of any future information request response submitted to Council.
Pedestrian and cycle network		
PO6 Lot reconfiguration assists in the implementation of the pedestrian and cycle movement network to achieve safe, attractive and efficient pedestrian and cycle networks.	AO6.1 Where a lot is subject to, or adjacent to an element of the pedestrian and cycle Movement network (identified on the Transport network overlay maps contained in Schedule 2) the specific location of this element of the pedestrian and cycle network is incorporated in the design of the lot layout.	Not applicable. Reconfiguration of a lot is not proposed.
	AO6.2 The element of the pedestrian and cycle network is constructed in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme policy SC6.5 – FNQROC Regional Development Manual.	Not applicable. Reconfiguration of a lot is not proposed.

9.4.1 ACCESS, PARKING AND SERVICING CODE

Table 9.4.1.3.a – Access, parking and servicing code – assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>PO1</p> <p>Sufficient on-site car parking is provided to cater for the amount and type of vehicle traffic expected to be generated by the use or uses of the site, having particular regard to:</p> <ul style="list-style-type: none"> (a) the desired character of the area; (b) the nature of the particular use and its specific characteristics and scale; (c) the number of employees and the likely number of visitors to the site; (d) the level of local accessibility; (e) the nature and frequency of any public transport serving the area; (f) whether or not the use involves the retention of an existing building and the previous requirements for car parking for the building (g) whether or not the use involves a heritage building or place of local significance; 	<p>AO1.1</p> <p>The minimum number of on-site vehicle parking spaces is not less than the number prescribed in Table 9.4.1.3.b for that particular use or uses.</p>	<p>Complies with PO1.</p> <p>Further technical/engineering assessment and detailed design is required to be undertaken to confirm compliance with the requirements of PO1. It is noted that further engineering and detailed design is required to fully comprehend on-site car parking opportunities. Traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by Council. Notwithstanding this, it is noted that a technical note has been prepared by PTT Traffic and Transport Engineers and is provided in Attachment 5. Upon initial (high-level) review/assessment of the proposed concept plan/design, it is submitted that the proposed car parking provisions are considered to be sufficient to support the proposed development.</p> <p>The proposal seeks approval for a Material Change of Use for a Resort Complex. Pursuant to Table 9.4.1.3.b of the Douglas Shire Planning Scheme 2018 V 1.0, the following car parking provisions are required:</p> <ul style="list-style-type: none"> • Short-term accommodation - For over 10 units: 0.75 car spaces per dwelling unit, plus 3 spaces for visitors and 2 service/staff parking for the first 10 units and 0.5 additional service/staff space per 10 units there-above. In all cases, 60% of the car parking area is to be covered.
	<p>AO1.2</p> <p>Car parking spaces are freely available for the parking of vehicles at all times and are not used for external storage purposes, the display of products or rented/sub-leased.</p>	
	<p>AO1.3</p> <p>Parking for motorcycles is substituted for ordinary vehicle parking to a maximum level of 2% of total ordinary vehicle parking.</p>	
	<p>AO1.4</p> <p>For parking areas exceeding 50 spaces parking, is provided for recreational vehicles as a substitute for ordinary vehicle parking to</p>	

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE																																			
(h) whether or not the proposed use involves the retention of significant vegetation	a maximum of 5% of total ordinary vehicle parking rate.	<ul style="list-style-type: none">Food and Drink Outlet - 1 space per 25m2 of GFA and outdoor dining area.Function Facility - 1 space per 15m2 GFA, plus 1 space per 100m2 of GFA.Indoor Sport and Recreation (Gymnasium) - 1 space per 15m2 of GFA, plus 1 space per 4 employees. <p>Preliminary concept plans provide indicative car parking provisions within the proposed development – these have been summarised below and are subject to change following the completion of further detailed design/engineering assessments.</p> <table><tr><th>Proposed Use</th><th>Planning Scheme Requirements</th><th>Total GFA</th><th>No. of Car parks required</th><th>Assumptions</th></tr><tr><td>202 suites</td><td>0.75 spaces/unit</td><td>-</td><td>151.5</td><td>-</td></tr><tr><td>Visitors</td><td>3 spaces</td><td>-</td><td>3</td><td>-</td></tr><tr><td>Service/Staff</td><td>2 spaces for the first 10 Units</td><td>-</td><td>2</td><td>-</td></tr><tr><td>Service/Staff</td><td>0.50 spaces/per 10 units above the first 10 units</td><td>-</td><td>9.6</td><td>-</td></tr><tr><td>Function Rooms</td><td>1 space/15sqm</td><td>1150sqm</td><td>34.5</td><td>Assumed most users are hotel guests. 55% deduction rate applied – refer to traffic report – Attachment 5.</td></tr><tr><td>Prefunction</td><td>1 space/15sqm</td><td>500sqm</td><td>0</td><td>Assumed that the prefunction facilities and function rooms will not be used at the same time. 100% deduction rate applied – refer to</td></tr></table>	Proposed Use	Planning Scheme Requirements	Total GFA	No. of Car parks required	Assumptions	202 suites	0.75 spaces/unit	-	151.5	-	Visitors	3 spaces	-	3	-	Service/Staff	2 spaces for the first 10 Units	-	2	-	Service/Staff	0.50 spaces/per 10 units above the first 10 units	-	9.6	-	Function Rooms	1 space/15sqm	1150sqm	34.5	Assumed most users are hotel guests. 55% deduction rate applied – refer to traffic report – Attachment 5.	Prefunction	1 space/15sqm	500sqm	0	Assumed that the prefunction facilities and function rooms will not be used at the same time. 100% deduction rate applied – refer to
Proposed Use	Planning Scheme Requirements	Total GFA	No. of Car parks required	Assumptions																																	
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Prefunction	1 space/15sqm	500sqm	0	Assumed that the prefunction facilities and function rooms will not be used at the same time. 100% deduction rate applied – refer to																																	

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE				
						traffic report – Attachment 5.
		Food and Drink	1 space/25sqm	3900sqm	70.2	Assumed that most users are hotel guests. 55% deduction rate applied – refer to traffic report – Attachment 5.
		Gym	1 space/15sqm	86sqm	0	Assumed that the gym is reserved for hotel guests only. 100% deduction rate applied – refer to traffic report – Attachment 5.
		Spa	1 space/90sqm	450sqm	11.3	Assumed most users are hotel guests. 50% deduction rate applied – refer to traffic report – Attachment 5.
		Total No. of car parks required.			283	Based on a performance based approach – refer to Traffic Report – Attachment 5.
		Total No. of car parking proposed.			301	
		<p>Car parking provisions and assumptions have been summarised in Attachment 4, Proposal Plans and assessed against the planning scheme requirements in Attachment 5 – Traffic Engineering Technical Note.</p> <p>Assumptions made in relation to car parking provision are likely to result in a surplus of car parks – an additional 18 car parks. On site car parking will be provided for on the Basement Level. Further negotiation with</p>				

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		Council is required to determine whether assumptions made in relation to car parking provision are appropriate.
PO2 Vehicle parking areas are designed and constructed in accordance with relevant standards.	AO2 Vehicle parking areas are designed and constructed in accordance with Australian Standard: (a) AS2890.1; (b) AS2890.3; (c) AS2890.6.	Complies with AO2. Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of appropriate Condition(s) upon the Development Permit.
PO3 Access points are designed and constructed: (a) to operate safely and efficiently; (b) to accommodate the anticipated type and volume of vehicles	AO3.1 Access is limited to one access cross over per site and is an access point located, designed and constructed in accordance with: (a) Australian Standard AS2890.1; (b) Planning scheme policy SC6.5 – FNQROC Regional Development Manual - access crossovers.	Complies with PO3. The subject site has frontage to Port Douglas Road, being a State-controlled Road. Two access points are proposed to support the development. The main access will be used by guests and the like, and will be a shared access, allowing for vehicles to both enter and exit the site. The second vehicle access, proposed to be located adjacent to the northern property boundary, will be restricted to service vehicles only. The service vehicle access is located approximately 100m from the nearest roundabout (Lakeland Avenue/Avenue of Palms roundabout),

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>(c) to provide for shared vehicle (including cyclists) and pedestrian use, where appropriate;</p> <p>(d) so that they do not impede traffic or pedestrian movement on the adjacent road area;</p> <p>(e) so that they do not adversely impact upon existing intersections or future road or intersection improvements;</p> <p>(f) so that they do not adversely impact current and future on-street parking arrangements;</p> <p>(g) so that they do not adversely impact on existing services within the road reserve adjacent to the site;</p> <p>(h) so that they do not involve ramping, cutting of the adjoining road reserve or any built structures (other than what may be necessary to cross over a stormwater channel).</p>	<p>AO3.2</p> <p>Access, including driveways or access crossovers:</p> <p>-are not placed over an existing:</p> <ul style="list-style-type: none"> (i) telecommunications pit; (ii) storm water kerb inlet; (iii) sewer utility hole; (iv) water valve or hydrant. <p>-are designed to accommodate any adjacent footpath;</p> <p>-adhere to minimum sight distance requirements in accordance with AS2980.1.</p> <p>AO3.3</p> <p>Driveways are:</p> <ul style="list-style-type: none"> (a) designed to follow as closely as possible to the existing contours but are no steeper than the gradients outlined in Planning scheme policy SC6.5 – FNQROC Regional Development Manual; (b) constructed such that where there is a grade shift to 1 in 4 (25%), there is 	<p>which is located north of the site, on Port Douglas Road. Additionally, it is noted that there is adequate separation distance between the two proposed access points (approximately 70m) to mitigate any potential traffic conflicts. Proposed access points are not likely to impact on existing services infrastructure.</p> <p>Port Douglas Road is a low speed environment (60km/hour). Additionally, it is noted that the site has no legal/practical access to any other road.</p> <p>It is noted that there is a shared pedestrian/cycle pathway existing along the site's frontage. Proposed development and/or access driveways have been appropriately sited and designed so as to ensure that no impacts to the existing pedestrian network are likely to result.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO3. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by Council.</p> <p>Notwithstanding this, a technical note has been prepared by PTT Traffic and Transport Engineers and is provided in Attachment 5. Upon initial (high-level) review/assessment of the proposed concept plan/design, it is submitted that the available sight distance at the proposed site access appears to be adequate for the local speed limit (60km/hr). Suggested upgrades to the road environment have been included in the technical</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<p>an area with a grade of no more than 1 in in 6 (16.6%) prior to this area, for a distance of at least 5 metres;</p> <p>(c) on gradients greater than 1 in 6 (16.6%) driveways are constructed to ensure the cross-fall of the driveway is one way and directed into the hill, for vehicle safety and drainage purposes;</p> <p>(d) constructed such that the transitional change in grade from the road to the lot is fully contained within the lot and not within the road reserve;</p> <p>(e) designed to include all necessary associated drainage that intercepts and directs storm water runoff to the storm water drainage system.</p>	note and will likely contribute to ensuring the proposed development does not compromise the safety and efficiency of the transport network.
	<p>AO3.4</p> <p>Surface construction materials are consistent with the current or intended future streetscape or character of the area and contrast with the surface construction materials of any adjacent footpath.</p>	
PO4	AO4	Complies with PO4.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Sufficient on-site wheel chair accessible car parking spaces are provided and are identified and reserved for such purposes.	The number of on-site wheel chair accessible car parking spaces complies with the rates specified in AS2890 Parking Facilities.	<p>Further detailed design and technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO4. It is noted that further detailed design and assessment will be completed and are likely to form part of any future response to a request for further information issued by Council.</p> <p>Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of appropriate Condition(s) upon the Development Permit.</p>
PO5 Access for people with disabilities is provided to the building from the parking area and from the street.	AO5 Access for people with disabilities is provided in accordance with the relevant Australian Standard.	<p>Complies with AO5.</p> <p>The proposal has been appropriately designed so as to ensure that people with disabilities will have access to all facilities provided on site. Specifically, it is noted that lifts are proposed throughout the development, to provide for ease of access to all upper levels.</p> <p>Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of appropriate Condition(s) upon the Development Permit.</p>
PO6 Sufficient on-site bicycle parking is provided to cater for the anticipated demand generated by the development.	AO6 The number of on-site bicycle parking spaces complies with the rates specified in Table 9.4.1.3.b.	<p>Complies with PO6.</p> <p>Further technical/engineering assessment and detailed design is required to be undertaken to confirm compliance with the requirements of PO6. It is noted that further engineering and detailed design is required to fully comprehend on-site bicycle parking opportunities. Traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by Council.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		<p>Notwithstanding this, it is noted that the proposal seeks approval for a Material Change of Use for a Resort Complex. Pursuant to Table 9.4.1.3.b of the Douglas Shire Planning Scheme 2018 V 1.0, the following bicycle parking provisions are required:</p> <ul style="list-style-type: none"> • Short-term accommodation – 1 space per 10 rooms. • Food and Drink Outlet - 1 space per 100m2 GFA and outdoor dining area. • Function Facility - 1 space per 100m2 of GFA. • Prefunction - 1 space per 100m2 of GFA. • Gym - 1 space per 4 employees. • Spa – 1 space per 100m2 of GFA. <p>As previously outlined in the code compliance response to PO1 (above), a significant proportion of patrons of the non-residential uses are likely to be guests of the resort. This means that the net bicycle parking demand is likely to be significantly less than the strict application of the above rates would suggest. Adopting the same deductions detailed in the code compliance response to PO1 (above), it is estimated the peak bicycle parking demand generated by the proposed development to be 46 spaces. This calculation is detailed within Attachment 5.</p> <p>Further negotiation with Council is required to determine whether assumptions made in relation to bicycle parking provision are appropriate. Additionally, it is noted that further detailed design will confirm whether the areas nominated for bicycle storage, on the proposal plans, are sufficient to accommodate the required number of bicycle parks.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
PO7 Development provides secure and convenient bicycle parking which: <ul style="list-style-type: none"> (a) for visitors is obvious and located close to the building's main entrance; (b) for employees is conveniently located to provide secure and convenient access between the bicycle storage area, end-of-trip facilities and the main area of the building; (c) is easily and safely accessible from outside the site. 	AO7.1 Development provides bicycle parking spaces for employees which are co-located with end-of-trip facilities (shower cubicles and lockers);	Complies with AO7.1. The proposal includes bicycle parking/storage areas on the Basement Level. Additionally, it is noted that some of the identified bicycle parking/storage is closely located to staff amenities, also located on the Basement Level, close the northern boundary.
	AO7.2 Development ensures that the location of visitor bicycle parking is discernible either by direct view or using signs from the street.	Complies with AO7.2. Bicycle parking areas will be clearly sign posted. Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of appropriate Condition(s) upon the Development Permit.
	AO7.3 Development provides visitor bicycle parking which does not impede pedestrian movement.	Complies with AO7.3. The proposal includes bicycle parking/storage areas on the Basement Level and therefore, is considered unlikely to impede upon proposed/existing pedestrian networks located on the upper levels.
PO8 Development provides walking and cycle routes through the site which: <ul style="list-style-type: none"> (a) link to the external network and pedestrian and cyclist destinations such as schools, shopping centres, open space, public transport stations, shops and local activity centres along the 	AO8 Development provides walking and cycle routes which are constructed on the carriageway or through the site to: <ul style="list-style-type: none"> (a) create a walking or cycle route along the full frontage of the site; 	Complies with AO8. Public pedestrian and/or cycle pathways existing along the site's frontage connect to the broader public transport network. No new cycle/walking paths are proposed to be constructed within the carriageway. Notwithstanding this, it is noted that the proposed development has been designed to ensure internal pedestrian pathways have direct links to the existing public networks.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>safest, most direct and convenient routes;</p> <p>(b) encourage walking and cycling;</p> <p>(c) ensure pedestrian and cyclist safety.</p>	<p>(b) connect to public transport and existing cycle and walking routes at the frontage or boundary of the site.</p>	
<p>PO9</p> <p>Access, internal circulation and on-site parking for service vehicles are designed and constructed:</p> <p>(a) in accordance with relevant standards;</p> <p>(b) so that they do not interfere with the amenity of the surrounding area;</p> <p>(c) so that they allow for the safe and convenient movement of pedestrians, cyclists and other vehicles.</p>	<p>AO9.1</p> <p>Access driveways, vehicle manoeuvring and on-site parking for service vehicles are designed and constructed in accordance with AS2890.1 and AS2890.2.</p>	<p>Complies with AO9.1.</p> <p>Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of appropriate Condition(s) upon the Development Permit.</p>
	<p>AO9.2</p> <p>Service and loading areas are contained fully within the site.</p>	<p>Complies with AO9.2.</p> <p>A separate/dedicated area for service vehicles is to be located adjacent to the northern property boundary. This area is to be accessed via a separate service vehicle access from Port Douglas Road. Proposed service and loading areas are to be contained fully within the subject site and will be setback a minimum of 3m from the northern property boundary. Landscaping will be provided in between proposed service/loading areas and the northern boundary to ensure appropriate buffering.</p>
	<p>AO9.3</p> <p>The movement of service vehicles and service operations are designed so they:</p> <p>(a) do not impede access to parking spaces;</p>	<p>Complies with AO9.3.</p> <p>A separate/dedicated area for service vehicles is to be located adjacent to the northern property boundary. This area is to be accessed via a separate service vehicle access from Port Douglas Road.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	(b) do not impede vehicle or pedestrian traffic movement.	The proposed separate service vehicle access ensures service vehicles/operations do not impede on access to on-site car parking, vehicle and/or pedestrian traffic movement.
<p>PO10</p> <p>Sufficient queuing and set down areas are provided to accommodate the demand generated by the development.</p>	<p>AO10.1</p> <p>Development provides adequate area on-site for vehicle queuing to accommodate the demand generated by the development where drive through facilities or drop-off/pick-up services are proposed as part of the use, including, but not limited to, the following land uses:</p> <ul style="list-style-type: none"> (a) car wash; (b) child care centre; (c) educational establishment where for a school; (d) food and drink outlet, where including a drive- through facility; (e) hardware and trade supplies, where including a drive-through facility; (f) hotel, where including a drive-through facility; (g) service station. 	<p>Complies with AO10.1.</p> <p>A Porte Cochre, accessible from Port Douglas Road, has been included in the proposed design to enable the movement of vehicles through the primary entrance of the resort complex. The proposed Porte Cochre provides adequate space for vehicles to queue on site, when arriving and/or dropping off/picking up guests and the like. Part of this facility will be undercover, to ensure guests are protected from the weather upon arrival/departure.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<p>AO10.2</p> <p>Queuing and set-down areas are designed and constructed in accordance with AS2890.1.</p>	<p>Complies with AO10.2.</p> <p>Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of appropriate Condition(s) upon the Development Permit.</p>

Table 9.4.1.3.b – Access, parking and servicing requirements

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
Agricultural supplies store	1 space per 50m ² of GFA and outdoor display area.	1 space per 200m ² of GFA.	n/a	LRV
Air services	1 car space per 20m ² of covered reception area, plus 1 car space per 2 staff, plus a covered bus set down area adjacent to the entry of the reception area and 2 bus parking spaces.	n/a	n/a	LRV
Bulk landscape supplies	1 space per 50m ² GFA and outdoor display area.	1 space per 200m ² of GFA.	n/a	MRV
Caretaker's accommodation	A minimum of 1 space	n/a	n/a	n/a
Child care centre	1 space per 10 children to be used for setting down and picking up of children, with a	n/a	n/a	VAN

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
	<p>minimum of 3 car spaces to be provided for set down and collection; plus 1 space per employee.</p> <p>Any drive-through facility can provide tandem short term parking for 3 car spaces for setting down/picking up of children, on the basis that a passing lane is provided and line- marked to be kept clear of standing vehicles at all times.</p>			
Club	<p>Unlicensed clubrooms: 1 space per 45m² of GFA.</p> <p>Licensed clubrooms: 1 space per 15m² of GFA.</p>	1 space per 4 employees.	n/a	<p>Licensed and equal or greater₂ than 1500m : RCV</p> <p>Other: VAN</p>
Community care centre	1 space per 20m ² of GFA.	A minimum of 1 space.	n/a	RCV
Community residence	A minimum of 2 spaces.	A minimum of 1 space.	n/a	VAN
Community use	1 space per 15m ² GFA.	1 space per 100m ² of GFA.	n/a	RCV
Dual occupancy	A minimum of 2 spaces per dwelling unit which may be in tandem with a minimum of 1 covered space per dwelling unit.	n/a	n/a	n/a
Dwelling house	A minimum of 2 spaces which may be in tandem plus 1 space for a secondary dwelling	n/a	n/a	n/a

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
Dwelling unit	1.5 spaces per one or two-bedroom unit; or 2 spaces per three-bedroom unit.	n/a	n/a	n/a
Educational establishment	Primary school or secondary schools: 1 car space per 2 staff members, plus provision of space to be used for setting down and picking up of students. Tertiary and further education: 1 car space per 2 staff members, plus 1 car space per 10 students, plus provision of space to be used for setting down and picking up of students.	Primary school or secondary schools: 1 space per 5 students over year 4. Tertiary and further education: 2 spaces per 50 full time students.	Required for all educational establishments with a GFA greater ₂ than 2000m.	RCV
Food and drink outlet	1 space per 25m ² GFA and outdoor dining area. or If within Precinct 1 : Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre	1 space per 100m ² of GFA, and outdoor dining area.	n/a	See Table 9.4.1.3.d

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
	precinct in the Mossman local plan: 1 space per 50m ² of GFA, and outdoor dining area.			
Function facility	1 space per 15m ² GFA.	1 space per 100m ² of GFA.	n/a	RCV
Funeral parlour	1 space per 15m ² GFA.	n/a	n/a	RCV
Garden centre	1 space per 50m ² GFA and outdoor display area	1 space per 200m ² of GFA.	n/a	AV
Hardware and trade supplies	1 space per 50m ² GFA and outdoor display area	1 space per 200m ² of GFA.	n/a	AV
Health care services	1 space per 20m ² of GFA.	1 space per 100m ² of GFA.	Required for all health care services with a GFA greater than 2000m ² .	VAN
High impact industry	1 space per 90m ² of GFA.	n/a	n/a	AV
Home based business	The parking required for the dwelling house, plus 1 space per bedroom where the Home-based business involves the provision of accommodation; or 2 1spaceper25m GFA for any other Home-Based Business.	n/a	n/a	n/a

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
Hospital	The greater of 1 space per 2 bedrooms or 1 space per 4 beds; plus 1 car space for ambulance parking, designated accordingly.	1 space per 100m ² of GFA.	Required for all hospitals with a GFA greater ₂ than 2000m.	RCV
Hotel	1 space per 10m ² GFA and licensed outdoor area; plus For 1 space per 50m ² GFA of floor area of liquor barn or bulk liquor sales area; plus, if a drive-in bottle shop is provided, queuing lane/s on site for 12 vehicles.	1 space per 100m ² of GFA.	n/a	LRV
Indoor sport and recreation	Squash court or another court game: 4 spaces per court. Basketball, netball, soccer, cricket: 25 spaces per court / pitch. Ten pin bowling: 3 spaces per bowling lane. Gymnasium: ₂ 1spaceper15m of GFA.	1 space per 4 employees.	n/a	RCV
Low impact industry	1 space per 90m ² of GFA.	n/a	n/a	AV

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
Marine industry	1 space per 90m ² of GFA.	n/a	n/a	AV
Medium impact industry	1 space per 90m ² of GFA.	n/a	n/a	AV
Multiple dwelling	<p>If within Precinct 1: Port Douglas precinct in the Port Douglas / Craiglie Local plan: 1 car space per dwelling unit.</p> <p>If outside Precinct 1: Port Douglas precinct in the Port Douglas / Craiglie Local plan: 1.5 car spaces per dwelling unit in all cases 60% of the car parking area is to be covered.</p>	1 bicycle space per 3 units and 1 visitor bicycle space per 12 units.	n/a	RCV (over 10 units)
Office	<p>1 space per 25m² of GFA or</p> <p>If within Precinct 1: Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan; 1 space per 50m² of GFA</p>	1 space per 200m ² GFA	Required for all office development with a GFA greater ₂ than 2000m.	See Table 9.4.1.3.e
Outdoor sales	1 space per 50m ² GFA and outdoor display area	1 space per 200m ² of GFA.	n/a	AV
Outdoor sport and recreation	<p>Coursing, horse racing, pacing, trotting:</p> <p>1 space per 5 seated</p>	<p>Football:</p> <p>5 space per field.</p>	n/a	RCV

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
	<p>spectators, plus 1 space per 5m² of other spectator areas.</p> <p>Football: 50 spaces per field.</p> <p>Lawn bowls: 30 spaces per green.</p> <p>Swimming pool: 15 spaces; plus 2 1 space per 100m of useable site area.</p> <p>Tennis court or other court game: 4 spaces per court.</p> <p>Golf course: 4 spaces per tee on the course</p>	<p>Lawn bowls: 5 spaces per green.</p> <p>Swimming pool: 1 space per swimming lane.</p> <p>Tennis court or other court game: 4 space per court.</p> <p>Golf course: 1 space per 15m² of GFA for clubhouse component.</p>		
Place of worship	1 space per 15m ² of GFA.	1 space per 100m ² of GFA.	n/a	LRV
Relocatable home park	1 space per relocatable home site; plus 0.1 space per relocatable home site for visitor parking; plus 1 space for an on-site manager	n/a	n/a	LRV

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
Research and technology industry	1 space per 90m ² of GFA.	n/a	n/a	MRV
Residential care facility	1 visitor car space per 5-bedroom units; plus 1 car space per 2 staff members	n/a	n/a	LRV
Resort complex	Use standard for relevant standard for each component. For example: Use Short Term Accommodation standard for accommodation component and Food and Drink Outlet for restaurant component.	Use standard for relevant standard for each component. For example: Use Short Term Accommodation standard for accommodation component and Food and Drink Outlet for restaurant component.	n/a	RCV
Retirement facility	1 space per dwelling unit; plus 1 visitor space per 5 dwelling units; plus 1 visitor car space per 10 hostel units, nursing home or similar beds, plus 1 car space per 2 staff members; plus 1 car parking space for ambulance parking.	n/a	n/a	LRV
Sales office	A minimum of 1 space.	n/a	n/a	n/a
Service industry	1 space per 90m ² of GFA.	n/a	n/a	SRV

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
Service station	1 space per 25m ² of GFA	n/a	n/a	AV
Shop	1 space per 25m ² of GFA. or If within Precinct 1: Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m ² of GFA.	1 space per 100m ² of GFA.	Required for all shops with a GFA greater than 2000m ² .	See Table 9.4.1.3.d
Shopping centre	1 space per 25m ² of GFA. or If within Precinct 1: Port Douglas precinct in the Port Douglas / Craiglie local plan or if with Precinct 5: Town centre precinct in the Mossman local plan: 1 space per 50m ² of GFA.	1 space per 200m ² GFA.	Required for all shopping centres with a GFA greater than 2000m ² .	See Table 9.4.1.3.d
Short term accommodation	If within Precinct 1: Port Douglas precinct in the Port Douglas / Craiglie local plan:	1 space per 10 rooms	n/a	SRV

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
	<p>0.5 car spaces per dwelling unit.</p> <p>If outside Precinct 1: Port Douglas precinct in the Port Douglas / Craiglie local plan: For up to 5 units: 1 car space per dwelling unit, plus 1 space for visitors and 1 service/staff spaces.</p> <p>For 5 – 10 units: 1 car space per dwelling unit, plus 2 spaces for visitors and 1 service/staff spaces.</p> <p>For over 10 units: 0.75 car spaces per dwelling unit, plus 3 spaces for visitors and 2 service/staff parking for the first 10 units and 0.5 additional service/staff space per 10 units, there-above.</p> <p>In all cases 60% of the car parking area is to be covered.</p>			
Showroom	1 space per 50m ² GFA.	1 space per 200m ² GFA.	n/a	AV

LAND USE	MINIMUM NUMBER OF ORDINARY VEHICLE SPACES	MINIMUM NUMBER OF BICYCLE SPACES	END OF TRIP FACILITIES	MINIMUM STANDARD DESIGN SERVICE VEHICLE
Special industry	1 space per 90m ² of GFA.	n/a	n/a	AV
Tourist park	1 car space per caravan site, tent site or cabin; plus 1 visitor car space per 10 caravan sites, tent sites or cabins; plus 1 car space for an on-site manager.	n/a	n/a	LRV
Theatre	Indoor: $\frac{1}{2}$ space per 15m of GFA. Outdoor cinema: 1 space per 5m ² of designated viewing area, plus 1 car space per 2 employees.	1 space per 200m ² GFA.	n/a	VAN
Veterinary services	1 space per 50m ² of GFA.	n/a	n/a	VAN
Warehouse	1 space per 90m ² of GFA.	n/a	n/a	Where self- storage: RCV Other: AV
Any use not otherwise specified in this table.	Sufficient spaces to accommodate number of vehicles likely to be parked at any one time.	Sufficient spaces to accommodate number of vehicles likely to be parked at any one time.		To be determined

Table 9.4.1.3.c – Design vehicles

VAN	A 99.8th percentile vehicle equivalent to a large car.
SRV	Small rigid vehicle as in AS2890.2-2002 parking facilities – Off-street commercial vehicle facilities but incorporating a body width of 2.33m
MRV	Medium rigid vehicle equivalent to an 8-tonne truck.
LRV	Large rigid vehicle described by AS2890.2-2002 parking facilities – Off-street commercial vehicle facilities as heavy rigid vehicle.
RCV	Industrial refuse collection vehicle
AV	19 metre articulated vehicle from AUSTROADS

Table 9.4.1.3.d – Standard number of service bays required for Food and drink outlet, Shop or Shopping centre

GROSS FLOOR AREA (M ²)	SERVICE BAYS REQUIRED			
	VAN	SRV	MRV	LRV
0-199	-	1	-	-
200 – 599	1	-	1	-
600 – 999	1	1	1	-
1000 – 1499	2	1	1	-
1500 – 1999	2	2	1	-
2000 – 2799	2	2	2	-
2800 – 3599	2	2	2	1
3600 and over	To be determined via a parking study.			

GROSS FLOOR AREA(M ²)	SERVICE BAYS REQUIRED			
	VAN	SRV	MRV	LRV
0-999	-	1	-	-
1000 – 2499	1	-	1	-
2500 – 3999	2	1	1	-
4000 – 5999	3	1	1	-
6000 – 7999	4	1	1	-

8000 – 9999	4	2	1	-
10000 and over	To be determined via a parking study.			

Table 9.4.1.3.e – Standard number of service bays required for Office

9.4.3 ENVIRONMENTAL PERFORMANCE CODE

Table 9.4.3.3.a – Environmental performance code – assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
PO1 Lighting incorporated within development does not cause an adverse impact on the amenity of adjacent uses and nearby sensitive land uses.	AO1.1 Technical parameters, design, installation, operation and maintenance of outdoor lighting comply with the requirements of Australian standard AS4282-1997 Control of the obtrusive effects of outdoor lighting.	Complies with AO1.1, AO1.2 and AO1.3. The proposed Resort Complex will include lighting for safety and security. All lighting will be provided in accordance with relevant Standards and be set-up to avoid impacts on adjoining Lots. Compliance with these requirements can be ensured through the imposition of appropriate Condition(s) upon the Development Permit.
	AO1.2 Development that involves flood lighting is restricted to a type that gives no upward component of light where mounted horizontally.	
	AO1.3 Access, car parking and manoeuvring areas are designed to shield nearby residential premises from impacts of vehicle headlights.	
Noise		

<p>PO2</p> <p>Potential noise generated from the development is avoided through design, location and operation of the activity.</p>	<p>AO2.1</p> <p>Development does not involve activities that would cause noise related environmental harm or nuisance; or</p> <p>AO2.2</p> <p>Development ensures noise does not emanate from the site through the use of materials, structures and architectural features to not cause an adverse noise impact on adjacent uses.</p> <p>AO2.3</p> <p>The design and layout of development ensures car parking areas avoid noise impacting directly on adjacent sensitive land uses through one or more of the following:</p> <ul style="list-style-type: none"> (a) car parking is located away from adjacent sensitive land uses; (b) car parking is enclosed within a building; (c) a noise ameliorating fence or structure is established adjacent to car parking areas where the fence or structure will not have a visual amenity impact on the adjoining premises; (d) buffered with dense landscaping. 	<p>Complies with AO2.1</p> <p>The proposed development does not include activities that will create noise emissions classed as environmental nuisance.</p>
<p>Airborne particles and other emissions</p>		

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
PO3 Potential airborne particles and emissions generated from the development are avoided through design, location and operation of the activity.	AO3.1 Development does not involve activities that will result in airborne particles or emissions being generated;	Complies with AO3.1. The proposed development does not involve activities that will release airborne particles or emissions, classed as an environmental nuisance.
	or AO3.2 The design, layout and operation of the development activity ensures that no airborne particles or emissions cause environmental harm or nuisance.	
Odours		
PO4 Potential odour causing activities associated with the development are avoided through design, location and operation of the activity.	AO4.1 The development does not involve activities that create odorous emissions;	Complies with AO4.1. The proposed development does not include activities that will create odorous emissions.
	or AO4.2 The use does not result in odour that causes environmental harm or nuisance with respect to surrounding land uses.	
Waste and recyclable material storage		
PO5 Potential odour causing activities associated with the development are avoided through design, location and operation of the activity.	AO5.1 The use ensures that all putrescent waste is stored in a manner that prevents odour nuisance and is disposed of at regular intervals.	Complies with AO5.1 and AO5.2. All waste streams will be segregated and stored in suitable, lidded waste containers awaiting collection. It is noted that waste storage will be contained within an

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Waste and recyclable material storage facilities are located and maintained to not cause adverse impacts on adjacent uses.	<p>AO5.2</p> <p>Waste and recyclable material storage facilities are located, designed and maintained to not cause an adverse impact on users of the premises and adjacent uses through consideration of:</p> <ul style="list-style-type: none"> (a) the location of the waste and recyclable material storage areas in relation to the noise and odour generated; (b) the number of receptacles provided in relation to the collection, maintenance and use of the receptacles; (c) the durability of the receptacles, sheltering and potential impacts of local climatic conditions; (d) the ability to mitigate spillage, seepage or leakage from receptacles into adjacent areas and sensitive receiving waters and environments. 	<p>enclosed room, located on the basement level, to prevent odour and/or visual nuisance. Prior to collection, waste will be transferred to the lower ground level, to a dedicated bin holding area, to be located directly adjacent to the proposed refuse pick up area (adjacent to the northern property boundary).</p> <p>All waste will be collected and removed from the site by appropriately licenced third-party contractors</p>
Sensitive land use activities		
<p>PO6</p> <p>Sensitive land use activities are not established in areas which will receive potentially incompatible impacts on amenity from</p>	<p>AO6.1</p> <p>Sensitive land use activities are not established in areas that will be adversely impacted upon by existing land uses, activities and potential development possible in an area;</p>	<p>Complies with AO6.1.</p> <p>The proposed resort complex is located within an urban area. The proposed development will be compatible with existing surrounding land uses, which include</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
surrounding, existing development activities and land uses.	or	similar style tourist accommodation and/or recreational land uses.
	AO6.2 Sensitive land activities are located in areas where potential adverse amenity impacts mitigate all potential impacts through layout, design, operation and maintenance.	Future development of land located north of the subject site has been identified as being suitable for potential residential development. Future residential land uses are not considered likely to impact upon, or be impacted by, the proposed development.
Stormwater quality		
PO7 The quality of stormwater flowing over, through or being discharged from development activities into watercourses and drainage lines is of adequate quality for downstream environments, with respect to: <ul style="list-style-type: none"> (a) the amount and type of pollutants borne from the activity; (b) maintaining natural stream flows; (c) the amount and type of site disturbance; (d) site management and control measures. 	AO7.1 Development activities are designed to ensure stormwater over roofed and hard stand areas is directed to a lawful point of discharge.	Complies with AO7.1. All collected stormwater will be conveyed into a network of underground pipes which will convey stormwater to a suitable lawful point or points. Detail regarding stormwater management will be provided at the Operational Works stage, with conditions anticipated to confirm compliance. Notwithstanding this, it is proposed that stormwater flows will 'generally' be managed via: <ul style="list-style-type: none"> • Underground Stormwater Tanks - underground stormwater tanks will be installed to capture and store rainwater during periods of heavy rainfall. These tanks are designed to handle the anticipated volume of stormwater, with a calculated capacity to be based upon

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		<p>historical rainfall data and site-specific conditions. The tanks will be made of high-density polyethylene (HDPE) to ensure durability and longevity.</p> <ul style="list-style-type: none"> • Controlled Release System - stored stormwater will be gradually released from the tanks through a controlled release system. This system will include flow control devices such as orifice plates and weirs to control discharge rates. Water will be released to both the front and rear of the site, through properly sized pipes and channels, ensuring that outflow rates are limited to a rate of discharge that does not exceed pre-development flow rates, nor the capacity of existing drainage infrastructure. Detailed hydraulic modelling will be undertaken so as to confirm that proposed release rates will not contribute to downstream flooding. • No Impact on Neighbouring Properties - a detailed stormwater management plan will be prepared and submitted to Council – likely to form part of any future response to a request for further information, issued by Council. Careful consideration will be given to local topography and drainage patterns, as well as the benefits of capturing and controlling stormwater flows on-site, so as to ultimately prevent any increase in runoff to neighbouring properties.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	AO7.2 Development ensures movement of stormwater over the site is not impeded or directed through potentially polluting activities.	Complies with AO7.2. Stormwater will not be directed through any source of contaminants prior to being discharged from the subject site to a lawful point of discharge.
	AO7.3 Soil and water control measures are incorporated into the activity's design and operation to control sediment and erosion potentially entering watercourses, drainage lines and downstream receiving waters.	Complies with AO7.3. A detailed stormwater management plan will be prepared and submitted to Council – likely to form part of any future response to a request for further information, issued by Council. Control measures will be outlined in any stormwater management plan to be prepared and submitted to council. Compliance with this requirement can be ensured through the imposition of appropriate Condition(s) upon the Development Permit.
Pest plants (for material change of use on vacant land over 1,000m²)		
PO8 Development activities and sites provide for the removal of all pest plants and implement ongoing measures to ensure that pest plants do not reinfest the site or nearby sites.	AO8.1 The land is free of declared pest plants before development establishes new buildings, structures and practices; or	Complies with AO8.1. The subject site is not known to contain any pest plants, given the site is predominantly clear of vegetation.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<p>AO8.2</p> <p>Pest plants detected on a development site are removed in accordance with a management plan prepared by an appropriately qualified person prior to construction of buildings and structures or earthworks.</p>	

9.4.5 INFRASTRUCTURE WORKS CODE

Table 9.4.5.3.a – Infrastructure works code –assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Works on a local government road		
PO1 Works on a local government road do not adversely impact on footpaths or existing infrastructure within the road verge and maintain the flow, safety and efficiency of pedestrians, cyclists and vehicles.	AO1.1 Footpaths/pathways are located in the road verge and are provided for the hierarchy of the road and located and designed and constructed in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual.	Complies with AO1.1. Compliance may be confirmed if necessary by imposition of appropriate condition(s) upon the development permit. Notwithstanding this, it is noted that there is a shared pedestrian/cycle path that exists along the frontage of the site. It is understood that this path is well maintained and fit for purpose. No new pedestrian/cycle path is proposed within the road verge.
	AO1.2 Kerb ramp crossovers are constructed in accordance with Planning scheme policy SC 5 – FNQROC Regional Development Manual.	Complies with AO1.2. Compliance may be confirmed if necessary by imposition of appropriate condition(s) upon the Development Permit.
	AO1.3 New pipes, cables, conduits or other similar infrastructure required to cross existing footpaths: (a) are installed via trenchless methods; or (b) where footpath infrastructure is removed to install infrastructure, the new section of footpath is installed to the standard detailed in the Planning scheme policy SC5 –	Complies with AO1.3. Any new pipes, cables, conduits or other similar infrastructure installed as part of the proposed development and that cross existing footpaths will be constructed in accordance with the requirements of AO1.3.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	FNQROC Regional Development Manual and is not less than a 1.2 metre section.	Compliance with this requirement can be achieved through the imposition of appropriate condition(s) upon the Development Permit.
	AO1.4 Where existing footpaths are damaged as a result of development, footpaths are reinstated ensuring: <ul style="list-style-type: none"> (a) similar surface finishes are used; (b) there is no change in level at joins of new and existing sections; (c) new sections are matched to existing in terms of dimension and reinforcement. 	Complies with AO1.4. In the event that a footpath is damaged as a result of the proposed development, works will be carried out to rectify the damage in accordance with the relevant standards. Compliance with this requirement can be achieved through the imposition of appropriate condition(s) upon the Development Permit.
	AO1.5 Decks, verandas, stairs, posts and other structures located in the road reserve do not restrict or impede pedestrian movement on footpaths or change the level of the road verges.	Not applicable. No structures are proposed within the road reserve.
Accessibility structures		
PO2 Development is designed to ensure it is accessible for people of all abilities and	AO2.1 Accessibility structures are not located within the road reserve.	Complies with AO2.1. Accessibility structures will be located within the site boundary. It is proposed that most

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
accessibility features do not impact on the efficient and safe use of footpaths.		buildings/structures will be appropriately setback (approx. 6m) from the road reserve.
	AO2.2 Accessibility structures are designed in accordance with AS1428.3.	Complies with AO2.2. Accessibility structures will be constructed in accordance with the requirements of AO2.2. Compliance with this requirement can be achieved through the imposition of appropriate condition(s) upon the Development Permit.
	AO2.3 When retrofitting accessibility features in existing buildings, all structures and changes in grade are contained within the boundaries of the lot and not within the road reserve.	Not applicable. The subject site comprises vacant land, with no existing buildings and/or structures (not including the temporary construction site office). Therefore, the proposed development does not involve the retrofitting of an existing building.
Water supply		
PO3 An adequate, safe and reliable supply of potable, firefighting and general use water is provided.	AO3.1 The premises is connected to Council's reticulated water supply system in accordance with the Design Guidelines set out in Section D6 of the Planning scheme policy SC5 – FNQROC Regional Development Manual; or	Complies with AO3.1. The proposed development will be connected to Council's reticulated water supply. It is noted that recent searches indicate there is an existing water main that runs parallel to the sites front (eastern) property boundary.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<p>AO3.2</p> <p>Where a reticulated water supply system is not available to the premises, on site water storage tank/s with a minimum capacity of 10,000 litres of stored water, with a minimum 7,500 litre tank, with the balance from other sources (e.g. accessible swimming pool, dam etc.) and access to the tank/s for fire trucks is provided for each new house or other development. Tank/s are to be fitted with a 50mm ball valve with a camlock fitting and installed and connected prior to occupation of the house and sited to be visually unobtrusive.</p>	<p>Compliance with reasonable and relevant connections requirements can be confirmed by imposition of appropriate condition(s) upon the development permit, if necessary.</p>
Treatment and disposal of effluent		
<p>PO4</p> <p>Provision is made for the treatment and disposal of effluent to ensure that there are no adverse impacts on water quality and no adverse ecological impacts as a result of the system or as a result of increasing the cumulative effect of systems in the locality.</p>	<p>AO4.1</p> <p>The site is connected to Council's sewerage system and the extension of or connection to the sewerage system is designed and constructed in accordance with the Design Guidelines set out in Section D7 of the Planning scheme policy SC5 – FNQROC Regional Development Manual;</p> <p>or</p> <p>AO4.2</p> <p>Where not in a sewerage scheme area, the proposed disposal system meets the requirements of Section 33 of the <i>Environmental Protection Policy (Water) 1997</i> and the proposed on-site effluent disposal system is designed in accordance with the <i>Plumbing and Drainage Act (2002)</i>.</p>	<p>Complies with AO4.1.</p> <p>The proposed development will be connected to Council's sewerage system.</p> <p>It is noted that the site is within the Port Douglas Sewer Catchment area and is also located within the Priority Infrastructure Area.</p> <p>Compliance with reasonable and relevant connections requirements can be confirmed by imposition of appropriate condition(s) upon the Development Permit, if necessary.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Stormwater quality		
PO5 Development is planned, designed, constructed and operated to avoid or minimise adverse impacts on stormwater quality in natural and developed catchments by: <ul style="list-style-type: none"> (a) achieving stormwater quality objectives; (b) protecting water environmental values; (c) maintaining waterway hydrology. 	AO5.1 A connection is provided from the premises to Council's drainage system;	Complies with PO5. All collected stormwater will be conveyed into a network of underground pipes which will convey stormwater to a suitable lawful point or points. Detail regarding stormwater management will be provided at the Operational Works stage, with conditions anticipated to confirm compliance. Notwithstanding this, it is proposed that stormwater flows will 'generally' be managed via: <ul style="list-style-type: none"> • Underground Stormwater Tanks - underground stormwater tanks will be installed to capture and store rainwater during periods of heavy rainfall. These tanks will be designed to handle the anticipated volume of stormwater, with a calculated capacity to be based on historical rainfall data and site-specific conditions. These tanks will be made of high-density polyethylene (HDPE) to ensure durability and longevity. • Controlled Release System - stored stormwater will be gradually released from the tanks through a controlled release system. This
	AO5.2 An underground drainage system is constructed to convey stormwater from the premises to Council's drainage system in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.	
	AO5.3 A stormwater quality management plan is prepared, and provides for achievable stormwater quality treatment measures meeting design objectives listed in Table 9.4.5.3.b and Table 9.4.5.3.c, reflecting land use constraints, such as: <ul style="list-style-type: none"> (a) erosive, dispersive and/or saline soil types; (b) landscape features (including landform); (c) acid sulphate soil and management of nutrients of concern; (d) rainfall erosivity. 	
	AO5.4	

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<p>Erosion and sediment control practices are designed, installed, constructed, monitored, maintained, and carried out in accordance with an erosion and sediment control plan.</p> <p>AO5.5</p> <p>Development incorporates stormwater flow control measures to achieve the design objectives set out in Table 9.4.5.3.b and Table 9.4.5.3.c, including management of frequent flows, peak flows, and construction phase hydrological impacts.</p>	<p>system will include flow control devices such as orifice plates and weirs to manage the discharge rate. Water will be released to both the front and rear of the site through appropriately-sized pipes and channels, so as to ensure that outflow rates do not exceed pre-development flow rates and/or the capacity of existing drainage infrastructure. Detailed hydraulic modelling will be undertaken so as to confirm that proposed release rates will not give rise to any increase in downstream flooding.</p> <ul style="list-style-type: none"> • No Impact on Neighbouring Properties - a detailed stormwater management plan will be prepared and submitted to Council – likely to form part of any future response to a request for further information, issued by Council. Care consideration will be given to local topography and drainage patterns, as well as the benefits of capturing and controlling the stormwater on-site, so as to ultimately prevent any increase in runoff to neighbouring properties.
Non-tidal artificial waterways		

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
PO6 Development involving non-tidal artificial waterways is planned, designed, constructed and operated to: <ul style="list-style-type: none"> (a) protect water environmental values; (b) be compatible with the land use constraints for the site for protecting water environmental values; (c) be compatible with existing tidal and non-tidal waterways; (d) perform a function in addition to stormwater management; (e) achieve water quality objectives. 	AO6.1 Development involving non-tidal artificial waterways ensures: <ul style="list-style-type: none"> (a) environmental values in downstream waterways are protected; (b) any ground water recharge areas are not affected; (c) the location of the waterway incorporates slow lying areas of the catchment connected to an existing waterway; (d) existing areas of ponded water are included. 	Not applicable. The proposed development does not involve a non-tidal artificial waterway.
	AO6.2 Non-tidal artificial waterways are located: <ul style="list-style-type: none"> (a) outside natural wetlands and any associated buffer areas; (b) to minimise disturbing soils or sediments; to avoid altering the natural hydro logic regime in acid sulphate soil and nutrient hazardous areas. 	Not applicable. The proposed development does not involve a non-tidal artificial waterway.
	AO6.3	Not applicable. The proposed development does not involve a non-tidal artificial waterway.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<p>Non-tidal artificial waterways located adjacent to, or connected to a tidal waterway by means of a weir, lock, pumping system or similar ensures:</p> <p>(a) there is sufficient flushing or a tidal range of >0.3 m; or any tidal flow alteration does not adversely impact on the tidal waterway; or there is no introduction of saltwater into freshwater environments.</p>	
	<p>AO6.4</p> <p>Non-tidal artificial waterways are designed and managed for any of the following end-use purposes:</p> <p>(a) amenity (including aesthetics), landscaping or recreation; or</p> <p>(b) flood management, in accordance with a drainage catchment management plan; or</p> <p>(c) stormwater harvesting plan as part of an integrated water cycle management plan; or</p> <p>(d) aquatic habitat.</p>	<p>Not applicable.</p> <p>The proposed development does not involve a non-tidal artificial waterway.</p>
	<p>AO6.5</p> <p>The end-use purpose of the non-tidal artificial waterway is designed and operated in a way that protects water environmental values.</p>	<p>Not applicable.</p> <p>The proposed development does not involve a non-tidal artificial waterway.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	AO6.6 Monitoring and maintenance programs adaptively manage water quality to achieve relevant water quality objectives downstream of the waterway.	Not applicable. The proposed development does not involve a non-tidal artificial waterway.
	AO6.7 Aquatic weeds are managed to achieve a low percentage of coverage of the water surface area, and pests and vectors are managed through design and maintenance.	Not applicable. The proposed development does not involve a non-tidal artificial waterway.
Wastewater discharge		
P07 Discharge of wastewater to waterways, or off site: (a) meets best practice environmental management; is treated to: <ul style="list-style-type: none"> i. meet water quality objectives for its receiving waters; ii. avoid adverse impact on ecosystem health or waterway health; iii. maintain ecological processes, riparian vegetation and waterway integrity; 	AO7.1 A wastewater management plan is prepared and addresses: <ul style="list-style-type: none"> (a) wastewater type; (b) climatic conditions; water quality objectives; best practice environmental management. 	Complies with P07. Where applicable, wastewater discharge will be designed to meet best practice environmental standards. Detail regarding wastewater discharge will be provided at the Operational Works stage, with conditions anticipated to confirm compliance.
	AO7.2 The waste water management plan is managed in accordance with a waste management hierarchy that: <ul style="list-style-type: none"> (a) avoids wastewater discharge to waterways; (b) if wastewater discharge cannot practicably be avoided, minimises wastewater discharge to waterways by re-use, recycling, recovery and treatment for disposal to sewer, surface water and ground water. 	

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
iv. offset impacts on high ecological value waters.	<p>AO7.3</p> <p>Wastewater discharge is managed to avoid or minimise the release of nutrients of concern so as to minimise the occurrence, frequency and intensity of algal blooms.</p> <p>AO7.4</p> <p>Development in coastal catchments avoids or minimises and appropriately manages soil disturbance or altering natural hydrology and:</p> <p>(a) avoids lowering ground water levels where potential or actual acid sulphate soils are present;</p> <p>(b) manages wastewater so that:</p> <ul style="list-style-type: none"> (i) the pH of any wastewater discharges is maintained between 6.5 and 8.5 to avoid mobilisation of acid, iron, aluminium and other metals; (ii) holding times of neutralised waste water ensures the flocculation and removal of any dissolved iron prior to release; visible iron floc is not present in any (iii) precipitated iron floc is contained and disposed of; (iv) wastewater and precipitates that cannot 	

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	(v) be contained and treated for discharge on site are removed and disposed of through trade waste or another lawful method.	
Electricity supply		
PO8 Development is provided with a source of power that will meet its energy needs.	AO8.1 A connection is provided from the premises to the electricity distribution network; or AO8.2 The premises are connected to the electricity distribution network in accordance with the Design Guidelines set out in Section D8 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.	Complies with AO8.1. It is expected that the proposed development will utilise existing connections to the electricity distribution network. Upgrades in the form of a new pad mount substation may be required to support the proposed development. In the event that new connections are required, they will be constructed in accordance with appropriate requirements.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
PO9 Development incorporating pad-mount electricity infrastructure does not cause an adverse impact on amenity.	AO9.1 Pad-mount electricity infrastructure is: (a) not located in land for open space or sport and recreation purposes; (b) screened from view by landscaping or fencing; (c) accessible for maintenance.	Complies with AO9.1. Where required/necessary to support proposed development, pad mount electricity infrastructure will be appropriately located on site, away from areas used for open space and/or recreational activities. A preliminary location for potential pad mount infrastructure has been identified within the north-east corner of the subject site, as detailed on the Proposal Plans provided in Attachment 4.
	AO9.2 Pad-mount electricity infrastructure within a building, in a Town Centre is designed and located to enable an active street frontage.	Not applicable. The subject site is not located within the Town Centre.
Telecommunications		
PO10 Development is connected to a telecommunications service approved by the relevant telecommunication regulatory authority.	AO10 The development is connected to telecommunications infrastructure in accordance with the standards of the relevant regulatory authority.	Complies with AO10. Compliance with reasonable and relevant connection requirements can be confirmed by imposition of appropriate condition(s) upon the Development permit, if necessary.
PO11 Provision is made for future telecommunications services (e.g. fibre optic cable).	AO11 Conduits are provided in accordance with Planning scheme policy SC5 – FNQROC Regional Development Manual.	Complies with AO11. Conduits will be provided as per the requirements of the relevant planning scheme policy. Compliance with reasonable and relevant connections requirements can be confirmed by

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		imposition of appropriate condition(s) upon the Development Permit, if necessary.
Road construction		
PO12 The road to the frontage of the premises is constructed to provide for the safe and efficient movement of: (a) pedestrians and cyclists to and from the site; (b) pedestrians and cyclists adjacent to the site; (c) vehicles on the road adjacent to the site; (d) vehicles to and from the site; (e) emergency vehicles.	AO12.1 The road to the frontage of the site is constructed in accordance with the Design Guidelines set out in Sections D1 and D3 of the Planning scheme policy SC5 – FNQROC Regional Development Manual, for the particular class of road, as identified in the road hierarchy.	Not applicable. The proposed development has frontage to Pot Douglas Road, which is existing and a State Controlled Road. No new roads are proposed.
	AO12.2 There is existing road, kerb and channel for the full road frontage of the site.	Not applicable. The proposed development has frontage to Pot Douglas Road, which is existing and a State Controlled Road. No new roads are proposed. It is noted that there is no kerb and channelling afforded to that stretch of Port Douglas Road in proximity to the subject site. Therefore, the requirement for kerb and channelling in this instance is not considered to be relevant.
	AO12.3 Road access minimum clearances of 3.5 metres wide and 4.8 metres high are provided for the safe passage of emergency vehicles.	Complies with AO12.3. Road access to the proposed development will be constructed in accordance with the relevant requirements. Compliance with reasonable and relevant connections requirements can be confirmed by

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		imposition of appropriate condition(s) upon the Development Permit, if necessary.
Alterations and repairs to public utility services		
PO13 Infrastructure is integrated with, and efficiently extends, existing networks.	AO13 Development is designed to allow for efficient connection to existing infrastructure networks.	Complies with AO13. Development has been appropriately designed and sited so as to ensure that connections to existing infrastructure networks can be achieved efficiently. Compliance with reasonable and relevant connections requirements can be confirmed by imposition of appropriate condition(s) upon the Development permit, if necessary.
PO14 Development and works do not affect the efficient functioning of public utility mains, services or installations.	AO14.1 Public utility mains, services and installations are not required to be altered or repaired as a result of the development; or	Complies with AO14.2. Where deemed necessary, alterations and/or repairs to public utility mains, services and installations, required as a result of proposed development, will be carried out in accordance with the relevant standards.
	AO14.2 Public utility mains, services and installations are altered or repaired in association with the works so that they continue to function and satisfy the relevant Design Guidelines set out in Section D8 of the Planning scheme policy SC5 – FNQROC Regional Development Manual.	Compliance with reasonable and relevant connections requirements can be confirmed by imposition of appropriate condition(s) upon the Development permit, if necessary.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Construction management		
PO15 Work is undertaken in a manner which minimises adverse impacts on vegetation that is to be retained.	AO15 Works include, at a minimum: <ul style="list-style-type: none"> (a) installation of protective fencing around retained vegetation during construction; (b) erection of advisory signage; (c) no disturbance, due to earthworks or storage of plant, materials and equipment, of ground level and soils below the canopy of any retained vegetation; (d) removal from the site of all declared noxious weeds. 	Not applicable. The subject site comprises of vacant land, that is predominantly clear of vegetation. It is noted that there is some (minimal) existing vegetation around the perimeter of the site. It is anticipated that all remaining vegetation will be removed to facilitate construction of the proposed development. Notwithstanding this, it is submitted that the site will be appropriately landscaped.
PO16 Existing infrastructure is not damaged by construction activities.	AO16 Construction, alterations and any repairs to infrastructure is undertaken in accordance with the Planning scheme policy SC5 – FNQROC Regional Development Manual.	Complies with AO16. Construction, alterations and any repairs to infrastructure will be carried out in accordance with the relevant standards. Compliance with reasonable and relevant connections requirements can be confirmed by imposition of appropriate condition(s) upon the Development Permit, if necessary.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
High speed telecommunication infrastructure		
PO17 Development provides infrastructure to facilitate the roll out of high speed telecommunications infrastructure.	AO17 No acceptable outcomes are prescribed.	Complies with PO17. Compliance with this requirement can be achieved by imposition of appropriate condition(s) upon the Development Permit.
Trade waste		
PO18 Where relevant, the development is capable of providing for the storage, collection treatment and disposal of trade waste such that: <ul style="list-style-type: none"> (a) off-site releases of contaminants do not occur; (b) the health and safety of people and the environment is protected; (c) the performance of the wastewater system is not put at risk. 	AO18 No acceptable outcomes are prescribed.	Complies with PO18. Compliance with this requirement can be achieved by imposition of appropriate condition(s) upon the Development Permit.
Fire services in developments accessed by common private title		
PO19 Hydrants are located in positions that will enable fire services to access water safely, effectively and efficiently.	AO19.1 Residential streets and common access ways within a common private title places hydrant at intervals of no more than 120 metres and at each intersection. Hydrants may	Complies with AO19.1 and AO19.2. Where deemed necessary, fire hydrants can be installed in accordance with the relevant standards.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	<p>have a single outlet and be situated above or below ground.</p> <p>AO19.2</p> <p>Commercial and industrial streets and access ways within a common private title serving commercial properties such as factories and warehouses and offices are provided with above or below ground fire hydrants located at not more than 90 metre intervals and at each intersection. Above ground fire hydrants have dual-valved outlets.</p>	<p>Notwithstanding this, it is submitted that a fire hydrant booster is proposed within the north-east corner of the site, adjacent to the service vehicle access driveway, to enable safe and easy access. Compliance with this requirement can be achieved by imposition of appropriate condition(s) upon the Development Permit.</p>
<p>PO20</p> <p>Hydrants are suitable identified so that fire services can locate them at all hours.</p>	<p>AO20</p> <p>No acceptable outcomes are prescribed.</p>	<p>Complies with PO20.</p> <p>A fire hydrant booster is proposed within the north-east corner of the site, adjacent to the service vehicle access driveway, to enable safe and easy access. Compliance with this requirement can be achieved by imposition of a condition.</p>

Table 9.4.5.3.b – Stormwater management design objectives (Construction phase).

ISSUES	DESIGN OBJECTIVES
<p>Drainage control</p> <p>(Temporary drainage works)</p>	<p>Design life and design storm for temporary drainage works:</p> <ul style="list-style-type: none"> (a) Disturbed open area for <12 months – 1 in 2-year ARI event; (b) Disturbed open area for 12-24 months – 1 in 5 year ARI event;

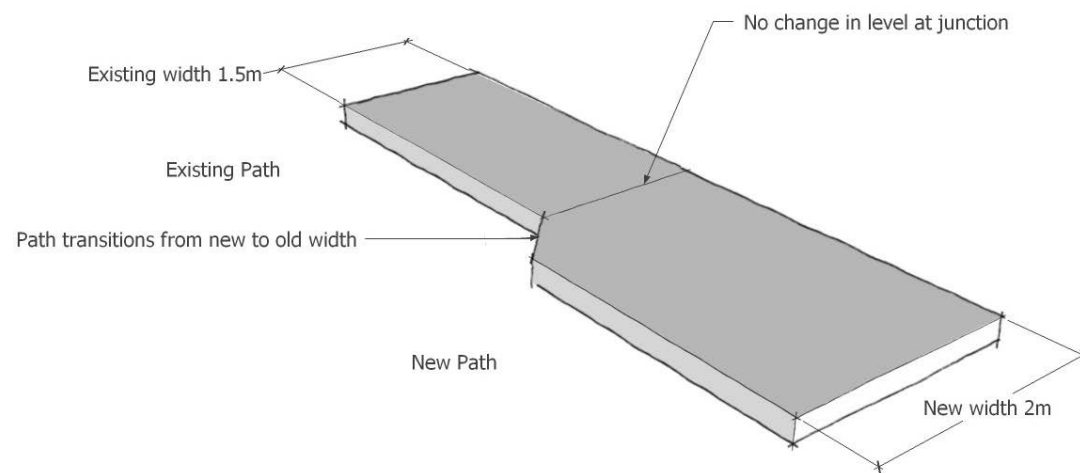
	<ul style="list-style-type: none"> (c) Disturbed open area for >24 months – 1 in 10-year ARI event. (d) Design capacity excludes minimum 150mm freeboard. (e) Temporary culvert crossing – minimum of 1 in 1-year ARI hydraulic capacity.
Erosion control (Erosion control measures)	<ul style="list-style-type: none"> (a) Minimise exposure of disturbed soils at any time. (b) Divert water run-off from undisturbed areas around disturbed areas. (c) Determine erosion risk rating using local rainfall erosivity, rainfall depth, soil loss rate or other acceptable methods. (d) Implement erosion control methods corresponding to identified erosion risk rating.
Sediment control measures (sediment control measures, design storm for sediment control basins, Sediment basin dewatering)	<ul style="list-style-type: none"> (a) Determine appropriate sediment control measures using: <ul style="list-style-type: none"> (i) potential soil loss rate; or (ii) monthly erosivity; or (iii) average monthly rainfall. (b) Collect and drain stormwater from disturbed soils to sediment basin for design storm event: <ul style="list-style-type: none"> (i) design storm for sediment basin sizing is 80th% five-day event or similar. (c) Site discharge during sediment basin dewatering: <ul style="list-style-type: none"> (i) TSS < 50mg/L TSS; (ii) Turbidity not>10%receivingwater'sturbidity; (iii) pH 6.5-8.5.
Water quality (Litter and other waste, hydrocarbons and other contaminants)	<ul style="list-style-type: none"> (a) Avoid wind-blown litter; remove grass pollutants. (b) Ensure there is no visible oil or grease sheen on released waters. (c) Dispose of waste containing contaminants at authorised facilities.

Waterway stability and flood flow management (Changes to the natural hydraulics and hydrology)	(a) For peak flow for the 100% AEP event and 1% AEP event, use constructed sediment basins to attenuate the discharge rate of stormwater from the site.
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Table 9.4.5.3.c – Stormwater management design objectives (post-construction phase)

DESIGN OBJECTIVES				APPLICATION
MINIMUM REDUCTIONS IN MEAN ANNUAL LOAD FROM UNMITIGATED DEVELOPMENT (%)				
Total suspended solids (TSS)	Total phosphorus (TP)	Total nitrogen (TN)	Gross pollutants >5mm	
80	60	40	90	<p>Development for urban purposes</p> <p>Excludes development that is less than 25% pervious. In lieu of modelling, the default bio-retention treatment area to comply with load reduction targets of 1.5% of contributing catchment area.</p>
<p>Water stability management</p> <p>(a) Limit peak 100% AEP event discharge within the receiving waterway to the pre-development peak 100% AEP event discharge.</p>				<p>Catchments contributing to un-lined receiving waterway. Degraded waterways may seek alternative discharge management objectives to achieve waterway stability.</p> <p>For peak flow for the 100% AEP event, use co-located storages to attenuate site discharge rate of stormwater.</p>

Figure 9.4.5.3.a – New footpath sections



9.4.6 LANDSCAPING CODE

Table 9.4.6.3.a – Landscaping code –assessable development

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Landscape design		
<p>PO1</p> <p>Development provides landscaping that contributes to and creates a high-quality landscape character for the site, street and local areas of the Shire by:</p> <ul style="list-style-type: none"> (a) promoting the Shire's character as a tropical environment; (b) softening the built form of development; (c) enhancing the appearance of the development from within and outside the development and makes a positive contribution to the streetscape; (d) screening the view of buildings, structures, open storage areas, service equipment, machinery plant and the like from public places, residences and other sensitive development; (e) where necessary, ensuring the privacy of habitable rooms and private outdoor recreation areas; (f) contributing to a comfortable living environment and improved energy 	<p>AO1</p> <p>Development provides landscaping:</p> <ul style="list-style-type: none"> (a) in accordance with the minimum area, dimensions and other requirements of applicable development codes; (b) that is designed and planned in a way that meets the guidelines for landscaping outlined in Planning Scheme Policy SC6.7 – Landscaping; (c) that is carried out and maintained in accordance with a landscaping plan that meets the guidelines for landscaping outlined in Planning Scheme Policy SC6.7 – Landscaping. 	<p>Complies with PO1.</p> <p>Landscaping will be implemented throughout the site, so as to ensure the proposed development integrates seamlessly with its environment and location within the Port Douglas townscape.</p> <p>Specifically, it is submitted that landscaping will be implemented in accordance with the requirements specified within the Tourist Accommodation Zone Code, via the imposition of dense landscaping within the property boundary setback areas.</p> <p>A preliminary landscape plan has been prepared and is included within the suite of proposal plans provided in Attachment 4.</p> <p>It is anticipated that landscaping will be implemented across 60%+ of the site. Additionally, it is noted that the proposed development incorporates vertical landscaping, with approximately 1,500m² of roof area to be landscaped.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
<p>efficiency, by providing shade to reduce glare and heat absorption and re-radiation from buildings, parking areas and other hard surfaces;</p> <p>(g) ensuring private outdoor recreation space is useable;</p> <p>(h) providing long term soil erosion protection;</p> <p>(i) providing a safe environment;</p> <p>(j) integrating existing vegetation and other natural features of the premises into the development;</p> <p>(k) not adversely affecting vehicular and pedestrian sightlines and road safety.</p>		<p>Landscaping is not expected to impact upon infrastructure provisions and/or internal vehicle movements.</p>
For assessable development		
<p>PO2</p> <p>Landscaping contributes to a sense of place, is functional to the surroundings and enhances the streetscape and visual appearance of the development.</p>	<p>AO2.1</p> <p>No acceptable outcomes are specified.</p>	<p>Complies with PO2.</p> <p>The proposed development incorporates landscaping that is expected to contribute to the existing character and landscape - creating a sense of place. It is submitted that the existing character has been defined within Section 3.3 of the Planning Report and comprises similar tourist accommodation style developments that are suitably screened from view, as a result of established/dense landscaping along road frontages. It is submitted that landscaping is</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		proposed along the site's frontage. It is intended that similar species congruent to the existing tropical environment will be used. A list of preferred species that will be incorporated into the proposed development has been provided in Attachment 4 – Proposal Plans. Landscaping will be implemented throughout the site, as well as vertically up buildings, to further enhance the visual appearance.
	AO2.2 Tropical urbanism is incorporated into building design.	Complies with AO2.2. The proposed development has been appropriately designed and incorporates tropical urbanism features/elements. Vertical landscaping is proposed to reduce the appearance of bulk and scale, as well as enhance the cooling of buildings externally. The building bulk has been separated to facilitate the incorporation of breezeways throughout the site to improve and enhance ventilation and circulation throughout the site.
PO3 Development provides landscaping that is, as far as practical, consistent with the existing desirable landscape character of the area and protects trees, vegetation and other features of ecological, recreational, aesthetic and cultural value.	AO3.1 Existing vegetation on site is retained and incorporated into the site design, wherever possible, utilising the methodologies and principles outline in AS4970-2009 Protection of Trees on Development Sites.	Complies with PO3. Landscaping will be implemented throughout the site to ensure the proposed development integrates seamlessly with its environment and location within the Port Douglas townscape.
	AO3.2	

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
	Mature vegetation on the site that is removed or damaged during development is replaced with advanced species.	<p>The proposed development incorporates landscaping that is expected to contribute to the existing character and landscape of the locality - creating a sense of place. It is submitted that the existing character has been defined within Section 3.3 of the Planning Report and comprises similar tourist accommodation style developments that are suitably screened from view, as a result of established/dense landscaping along road frontages.</p> <p>It is submitted that landscaping is proposed along the site's frontage. It is intended that similar species congruent to the existing tropical environment will be used. A list of preferred species that will be incorporated into the proposed development has been provided in Attachment 4 – Proposal Plans.</p> <p>It is anticipated that landscaping will be implemented across 60%+ of the site. Additionally, it is noted that the proposed development incorporates vertical landscaping, with approximately 1,500m² of roof areas being landscaped.</p>
	AO3.3	
	Where there is an existing landscape character in a street or locality which results from existing vegetation, similar species are incorporated into new development.	
	AO3.4	
	Street trees are species which enhance the landscape character of the streetscape, with species chosen from the Planning scheme policy SC6.7 – Landscaping.	
PO4	AO4	Complies with AO4.
Plant species are selected with consideration to the scale and form of development, screening, buffering, streetscape, shading and the locality of the area.	Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.	The species selected, as detailed within Attachment 4 – Proposal Plans, are considered to be consistent with

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
		<p>those species identified within the relevant Planning Scheme Policy.</p> <p>Compliance with the requirements of AO4 can be ensured through the imposition of appropriate condition(s) upon the Development Permit.</p>
<p>PO5</p> <p>Shade planting is provided in car parking areas where uncovered or open, and adjacent to driveways and internal roadways.</p>	<p>AO5</p> <p>Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.</p>	<p>Complies with AO5.</p> <p>The species selected, as detailed within Attachment 4 – Proposal Plans, are considered to be consistent with those species identified within the relevant Planning Scheme Policy.</p> <p>Compliance with the requirements of AO5 can be ensured through the imposition of appropriate condition(s) upon the Development Permit.</p>
<p>PO6</p> <p>Landscaped areas are designed in order to allow for efficient maintenance.</p>	<p>AO6.1</p> <p>A maintenance program is undertaken in accordance with Planning scheme policy SC6.7 – Landscaping.</p>	<p>Complies with AO6.1 and AO6.2.</p> <p>A maintenance program which is consistent with the Planning Scheme Policy for Landscaping will be developed and implemented.</p> <p>Compliance with the specified requirements can be ensured through the imposition of appropriate condition(s) upon the Development Permit.</p>
	<p>AO6.2</p> <p>Tree maintenance is to have regard to the 'Safe Useful Life Expectancy of Trees (SULE).</p>	
<p>PO7</p>	<p>AO7.1</p>	<p>Complies with AO7.1 and AO7.2.</p>

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
Podium planting is provided with appropriate species for long term survival and ease of maintenance, with beds capable of proper drainage.	Podium planting beds are provided with irrigation and are connected to stormwater infrastructure to permit flush out.	Podium planting will be appropriately irrigated.
	AO7.2 Species of plants are selected for long term performance designed to suit the degree of access to podiums and roof tops for maintenance.	Compliance with reasonable and relevant connections requirements can be confirmed by imposition of appropriate condition(s) upon the Development Permit, if necessary. Additionally, it is noted that the species list provided in Attachment 4 – Proposal Plans, incorporates a number of species suited to podium planting.
PO8 Development provides for the removal of all weed and invasive species and implement on-going measures to ensure that weeds and invasive species do not reinfest the site and nearby premises.	AO8 Weed and invasive species detected on a development site are removed in accordance with a management plan prepared by an appropriately qualified person.	Complies with AO8. The subject site is not known to contain any weed or invasive species. The site will be extensively landscaped in accordance with the Planning Scheme Policy for Landscaping and be subjected to an ongoing maintenance program.
PO9 The landscape design enhances personal safety and reduces the potential for crime and vandalism.	AO9 No acceptable outcomes are specified.	Complies with AO9. The proposed development includes landscaping which is strategically designed to maintain personal safety through species selection and placement and prevents crime / vandalism by not creating secluded areas.
PO10	AO10 Species are selected in accordance with Planning scheme policy SC6.7 – Landscaping.	Complies with AO10.

PERFORMANCE OUTCOMES	ACCEPTABLE OUTCOMES	APPLICANT RESPONSE
The location and type of plant species does not adversely affect the function and accessibility of services and facilities and service areas.		<p>All landscaping will be consistent with the Planning Scheme Policy for Landscaping.</p> <p>Compliance with the requirements of AO10 can be ensured through the imposition of appropriate condition(s) upon the Development Permit.</p>

Attachment 8

SDAP Code Compliance Responses

State code 1: Development in a state-controlled road environment

State Development Assessment Provisions guideline - State Code 1: Development in a state-controlled road environment. This guideline provides direction on how to address State Code 1.

Table 1.1 Development in general

Performance outcomes	Acceptable outcomes	Response
Buildings, structures, infrastructure, services and utilities		
PO1 The location of the development does not create a safety hazard for users of the state-controlled road .	AO1.1 Development is not located in a state-controlled road . AND AO1.2 Development can be maintained without requiring access to a state-controlled road .	Complies with PO1 The Subject site is located adjacent to Port Douglas Road, being a State-controlled road. Notwithstanding this, it is submitted that the proposed development will be contained entirely within the development site. Development will not compromise the safety of users of the state-controlled road network. Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO1. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by SARA.
PO2 The design and construction of the development does not adversely impact the structural integrity or physical condition of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies with PO2 Development will not compromise the safety of users of the state-controlled road network. Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO1. It is noted that technical traffic and engineering assessments will be completed and are likely to form part

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Performance outcomes	Acceptable outcomes	Response
		of any future response to a request for further information issued by SARA.
PO3 The location of the development does not obstruct road transport infrastructure or adversely impact the operating performance of the state-controlled road .	No acceptable outcome is prescribed.	<p>Complies with PO3</p> <p>Development will not compromise the safety of users of the state-controlled road network.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO1. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by SARA.</p>
PO4 The location, placement, design and operation of advertising devices, visible from the state-controlled road , do not create a safety hazard for users of the state-controlled road .	No acceptable outcome is prescribed.	<p>Complies with PO4.</p> <p>Where required, advertising devices will be appropriately located, designed so as to ensure they do not create a safety hazard for users of the state-controlled road.</p> <p>Compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of a Condition within the Development Permit.</p>
PO5 The design and construction of buildings and structures does not create a safety hazard by distracting users of the state-controlled road .	<p>AO5.1 Facades of buildings and structures fronting the state-controlled road are made of non-reflective materials.</p> <p>AND</p> <p>AO5.2 Facades of buildings and structures do not direct or reflect point light sources into the face of oncoming traffic on the state-controlled road.</p> <p>AND</p>	<p>Complies with AO5.1, AO5.2, AO5.3, AO5.4.</p> <p>Suitable building colours and finishes will be used. A schedule of materials and finishes is provided in Attachment 4 – Proposal Plans and involve a number of earthy, natural/raw tones. Proposed materials and finishes are unlikely to be reflective.</p> <p>Outdoor lighting will be installed in accordance with the relevant standards, and is unlikely to directly impact on road users. It is submitted that landscaping will be established along the site's frontage and will further reduce visual impacts.</p>

Performance outcomes	Acceptable outcomes	Response
	<p>AO5.3 External lighting of buildings and structures is not directed into the face of oncoming traffic on the state-controlled road.</p> <p>AND</p> <p>AO5.4 External lighting of buildings and structures does not involve flashing or laser lights.</p>	Compliance with the requirements of these Assessment Benchmarks can be ensured through the imposition of a Condition/s within the Development Permit.
PO6 Road, pedestrian and bikeway bridges over a state-controlled road are designed and constructed to prevent projectiles from being thrown onto the state-controlled road .	AO6.1 Road, pedestrian and bikeway bridges over the state-controlled road include throw protection screens in accordance with section 4.11 of the Design Criteria for Bridges and Other Structures Manual, Department of Transport and Main Roads, 2020.	<p>Not applicable .</p> <p>The proposed development does not involve a road, pedestrian and bikeway bridge.</p>
Landscaping		
PO7 The location of landscaping does not create a safety hazard for users of the state-controlled road .	<p>AO7.1 Landscaping is not located in a state-controlled road.</p> <p>AND</p> <p>AO7.2 Landscaping can be maintained without requiring access to a state-controlled road.</p> <p>AND</p> <p>AO7.3 Landscaping does not block or obscure the sight lines for vehicular access to a state-controlled road.</p>	<p>Complies with AO7.1, AO7.2 and AO7.3.</p> <p>Landscaping will be contained wholly within the subject site. Maintenance of landscaping throughout the site can be achieved internally.</p> <p>Additionally, it is noted that the site boundary, including any landscaping established within the site boundary, is suitably setback (approx. 39m) from the road pavement and therefore, landscaping is not considered to impact on/obscure the sight lines for vehicles accessing the state-controlled road.</p>
Stormwater and overland flow		
PO8 Stormwater run-off or overland flow from the development site does not create or exacerbate a safety hazard for users of the state-controlled road .	No acceptable outcome is prescribed.	<p>Complies with PO8</p> <p>Development will not result in stormwater flows/run-off likely to compromise the safety of users of the state-controlled road network.</p> <p>Stormwater flows will be appropriately managed via the installation of underground holding tanks and a controlled release system. Stormwater will be slowly released and</p>

Performance outcomes	Acceptable outcomes	Response
		<p>discharged offsite, via a lawful point of discharge) to the rear of the site (which comprises of the Port Douglas Mirage Gold Course) as well as to the drainage swales existing within the road reserve, along the site's frontage.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO8. It is noted that further technical assessments will be undertaken to inform a detailed stormwater management plan, likely to form part of any future response to a request for further information issued by SARA.</p>
<p>PO9 Stormwater run-off or overland flow from the development site does not result in a material worsening of the operating performance of the state-controlled road or road transport infrastructure.</p>	No acceptable outcome is prescribed.	<p>Complies with PO9</p> <p>Development will not result in stormwater flows/run-off likely to result in a material worsening of operating performance of a state transport corridor or state transport infrastructure.</p> <p>Stormwater flows will be appropriately managed via the installation of underground holding tanks and a controlled release system. Stormwater will be slowly released and discharged offsite, via a lawful point of discharge) to the rear of the site (which comprises of the Port Douglas Mirage Gold Course) as well as to the drainage swales existing within the road reserve, along the site's frontage.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO9. It is noted that further technical assessments will be undertaken to inform a detailed stormwater management plan, likely to form part of any future response to a request for further information issued by SARA.</p>
<p>PO10 Stormwater run-off or overland flow from the development site does not adversely impact the structural integrity or physical condition of the state-controlled road or road transport infrastructure.</p>	No acceptable outcome is prescribed.	<p>Complies with PO10</p> <p>Development will not result in stormwater flows/run-off likely to interfere with the structural integrity or physical</p>

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Performance outcomes	Acceptable outcomes	Response
		<p>condition of a state transport corridor or state transport infrastructure.</p> <p>Stormwater flows will be appropriately managed via the installation of underground holding tanks and a controlled release system. Stormwater will be slowly released and discharged offsite, via a lawful point of discharge) to the rear of the site (which comprises of the Port Douglas Mirage Gold Course) as well as to the drainage swales existing within the road reserve, along the site's frontage.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO10. It is noted that further technical assessments will be undertaken to inform a detailed stormwater management plan, likely to form part of any future response to a request for further information issued by SARA.</p>
PO11 Development ensures that stormwater is lawfully discharged.	<p>AO11.1 Development does not create any new points of discharge to a state-controlled road.</p> <p>AND</p> <p>AO11.2 Development does not concentrate flows to a state-controlled road.</p> <p>AND</p> <p>AO11.3 Stormwater run-off is discharged to a lawful point of discharge.</p> <p>AND</p> <p>AO11.4 Development does not worsen the condition of an existing lawful point of discharge to the state-controlled road.</p>	<p>Complies with PO11</p> <p>Development will not result in stormwater flows/run-off likely to result in a worsening condition of an existing lawful point of discharge to a state transport corridor or state transport infrastructure.</p> <p>Stormwater flows will be appropriately managed via the installation of underground holding tanks and a controlled release system. Stormwater will be slowly released and discharged offsite, via a lawful point of discharge) to the rear of the site (which comprises of the Port Douglas Mirage Gold Course) as well as to the drainage swales existing within the road reserve, along the site's frontage. In considering this, it is submitted that not all stormwater flows will be concentrated to the state transport corridor.</p> <p>Additionally, it is noted that two new lawful points of discharge are proposed along the frontage of the site, to facilitate stormwater discharge into the existing drainage swales, which are located within the state-controlled corridor.</p>

Performance outcomes	Acceptable outcomes	Response
		Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO11. It is noted that further technical assessments will be undertaken to inform a detailed stormwater management plan, likely to form part of any future response to a request for further information issued by SARA.
Flooding		
PO12 Development does not result in a material worsening of flooding impacts within a state-controlled road .	<p>AO12.1 For all flood events up to 1% annual exceedance probability, development results in negligible impacts (within +/- 10mm) to existing flood levels within a state-controlled road.</p> <p>AND</p> <p>AO12.2 For all flood events up to 1% annual exceedance probability, development results in negligible impacts (up to a 10% increase) to existing peak velocities within a state-controlled road.</p> <p>AND</p> <p>AO12.3 For all flood events up to 1% annual exceedance probability, development results in negligible impacts (up to a 10% increase) to existing time of submergence of a state-controlled road.</p>	<p>Complies with PO12</p> <p>Development will not result in a material worsening of flooding impacts within a state transport corridor or state transport infrastructure.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO12. It is noted that a detailed flood assessment will be undertaken and is likely to form part of any future response to a request for further information issued by SARA.</p>
Drainage Infrastructure		
PO13 Drainage infrastructure does not create a safety hazard for users in the state-controlled road .	<p>AO13.1 Drainage infrastructure is wholly contained within the development site, except at the lawful point of discharge.</p> <p>AND</p>	<p>Complies with AO13.1 and AO13.2</p> <p>All proposed drainage infrastructure will be located wholly within the development site. Access to drainage infrastructure can be gained from within the site.</p>

Performance outcomes	Acceptable outcomes	Response
	AO13.2 Drainage infrastructure can be maintained without requiring access to a state-controlled road .	
PO14 Drainage infrastructure associated with, or within, a state-controlled road is constructed, and designed to ensure the structural integrity and physical condition of existing drainage infrastructure and the surrounding drainage network.	No acceptable outcome is prescribed.	<p>Complies with PO14.</p> <p>Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of a Condition within the Development Permit.</p>

Table 1.2 Vehicular access, road layout and local roads

Performance outcomes	Acceptable outcomes	Response
Vehicular access to a state-controlled road or within 100 metres of a state-controlled road intersection		
PO15 The location, design and operation of a new or changed access to a state-controlled road does not compromise the safety of users of the state-controlled road .	No acceptable outcome is prescribed.	<p>Complies with PO15</p> <p>The subject site has frontage to Port Douglas Road, being a State-controlled Road. Two access points are proposed to support the development. The main access will be used by guests and the like, and will be a shared access, allowing for vehicles to both enter and exit the site. The second vehicle access, is proposed to be located adjacent to the northern property boundary and will be restricted to service vehicles only. The service vehicle access is located approximately 100m from the nearest roundabout, which is located north of the site, on Port Douglas Road. Additionally, it is noted that there is a suitable amount of distance between the two proposed access points (approximately 70m).</p> <p>Port Douglas Road is a low speed environment (60km/hour). Additionally, it is noted that the site has</p>

Performance outcomes	Acceptable outcomes	Response
		<p>no legal/practical access to an alternative/lower order road.</p> <p>In considering this, it is submitted that the proposed location and design of new access points will not compromise the safety of users of the state-controlled road network.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO15. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by SARA.</p>
<p>PO16 The location, design and operation of a new or changed access does not adversely impact the functional requirements of the state-controlled road.</p>	<p>No acceptable outcome is prescribed.</p>	<p>Complies with PO16</p> <p>The subject site has frontage to Port Douglas Road, being a State-controlled Road. Two access points are proposed to support the development. The main access will be used by guests and the like, and will be a shared access, allowing for vehicles to both enter and exit the site. The second vehicle access, is proposed to be located adjacent to the northern property boundary and will be restricted to service vehicles only. The service vehicle access is located approximately 100m from the nearest roundabout, which is located north of the site, on Port Douglas Road. Additionally, it is noted that there is a suitable amount of distance between the two proposed access points (approximately 70m).</p>

Performance outcomes	Acceptable outcomes	Response
		<p>Port Douglas Road is a low speed environment (60km/hour). Additionally, it is noted that the site has no legal/practical access to an alternative/lower order road.</p> <p>In considering this, it is submitted that the proposed location and design of new access points will not compromise the safety of users of the state-controlled road network.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO16. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by SARA.</p>
PO17 The location, design and operation of a new or changed access is consistent with the future intent of the state-controlled road .	No acceptable outcome is prescribed.	<p>Complies with PO17.</p> <p>The proposed new access points are not likely to impact on the future intent of the state-controlled road. It is understood that there are no future upgrades planned for Port Douglas Road.</p>
<p>PO18 New or changed access is consistent with the access for the relevant limited access road policy:</p> <ol style="list-style-type: none"> LAR 1 where direct access is prohibited; or LAR 2 where access may be permitted, subject to assessment. 	No acceptable outcome is prescribed.	<p>Not applicable.</p> <p>Port Douglas Road is not classified as LAR1 or LAR 2.</p>
PO19 New or changed access to a local road within 100 metres of an intersection with a state-controlled road does not compromise the safety of users of the state-controlled road .	No acceptable outcome is prescribed.	<p>Not applicable.</p> <p>The subject site/proposed development is not located within 100m of an intersection with a state-controlled road.</p>
PO20 New or changed access to a local road within 100 metres of an intersection with a state-	No acceptable outcome is prescribed.	Not applicable.

Performance outcomes	Acceptable outcomes	Response
controlled road does not adversely impact on the operating performance of the intersection.		The subject site/proposed development is not located within 100m of an intersection with a state-controlled road.
Public passenger transport and active transport		
PO21 Development does not compromise the safety of users of public passenger transport infrastructure, public passenger services and active transport infrastructure .	No acceptable outcome is prescribed.	Complies with PO21 Development will not compromise the safety of public transport infrastructure, active transport networks or public passenger services.
PO22 Development maintains the ability for people to access public passenger transport infrastructure, public passenger services and active transport infrastructure .	No acceptable outcome is prescribed.	Complies with PO22 Development will not impact on peoples ability to safely access public transport infrastructure, active transport networks or public passenger services.
PO23 Development does not adversely impact the operating performance of public passenger transport infrastructure, public passenger services and active transport infrastructure .	No acceptable outcome is prescribed.	Complies with PO23 Development will not adversely impact the operational performance of public transport infrastructure, active transport networks or public passenger services.
PO24 Development does not adversely impact the structural integrity or physical condition of public passenger transport infrastructure and active transport infrastructure .	No acceptable outcome is prescribed.	Complies with PO24 Development will not adversely impact the structural integrity or physical condition of public transport infrastructure, active transport networks or public passenger services.

Table 1.3 Network impacts

Performance outcomes	Acceptable outcomes	Response
PO25 Development does not compromise the safety of users of the state-controlled road network.	No acceptable outcome is prescribed.	Complies with PO25 Development will not compromise the safety of users of the state-controlled road network. Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO25. It is noted that technical traffic

Performance outcomes	Acceptable outcomes	Response
		and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by SARA.
PO26 Development ensures no net worsening of the operating performance of the state-controlled road network.	No acceptable outcome is prescribed.	<p>Complies with PO26</p> <p>The subject site has frontage to Port Douglas Road, being a State-controlled Road. Two access points are proposed to support the development. The main access will be used by guests and the like, and will be a shared access, allowing for vehicles to both enter and exit the site. The second vehicle access, is proposed to be located adjacent to the northern property boundary and will be restricted to service vehicles only. The service vehicle access is located approximately 100m from the nearest roundabout, which is located north of the site, on Port Douglas Road. Additionally, it is noted that there is a suitable amount of distance between the two proposed access points (approximately 70m).</p> <p>Port Douglas Road is a low speed environment (60km/hour). Additionally, it is noted that the site has no legal/practical access to an alternative/lower order road.</p> <p>In considering this, it is submitted that the proposed development will not result in a net worsening of operating performance of the state-controlled road network.</p>

Performance outcomes	Acceptable outcomes	Response
		Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO26. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by SARA.
PO27 Traffic movements are not directed onto a state-controlled road where they can be accommodated on the local road network.	No acceptable outcome is prescribed.	Not applicable. It is noted that the site has no legal/practical access to an alternative/lower order road.
PO28 Development involving haulage exceeding 10,000 tonnes per year does not adversely impact the pavement of a state-controlled road .	No acceptable outcome is prescribed.	Not applicable. The development is not proposed to include haulage exceeding 10,000 tonnes per year.
PO29 Development does not impede delivery of planned upgrades of state-controlled roads .	No acceptable outcome is prescribed.	Complies with PO29. Proposed development is not likely to impact on the future intent of the state-controlled road. It is understood that there are no future upgrades planned for Port Douglas Road.
PO30 Development does not impede delivery of corridor improvements located entirely within the state-controlled road corridor .	No acceptable outcome is prescribed.	Complies with PO30. Proposed development is not likely to impede delivery of corridor improvements located entirely within the state-controlled road corridor. It is understood that there are no future upgrades planned for Port Douglas Road. Notwithstanding this, proposed development will be contained entirely within the bounds of the subject site – no part of the development will be located within the road reserve.

Table 1.4 Filling, excavation, building foundations and retaining structures

Performance outcomes	Acceptable outcomes	Response
PO31 Development does not create a safety hazard for users of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Not applicable. The proposed development will be contained wholly within the subject site. No part of the proposed development will be constructed within the road corridor.
PO32 Development does not adversely impact the operating performance of the state-controlled road .	No acceptable outcome is prescribed.	Not applicable. The proposal seeks approval for a Material Change of Use – Resort Complex. Filling and excavation required to facilitate construction of the proposed development will form part of subsequent/downstream operational works approvals. The proposed development will be contained wholly within the subject site. No part of the proposed development will be constructed within the road corridor.
PO33 Development does not undermine, damage or cause subsidence of a state-controlled road .	No acceptable outcome is prescribed.	Not applicable. The proposal seeks approval for a Material Change of Use – Resort Complex. Filling and excavation required to facilitate construction of the proposed development will form part of subsequent/downstream operational works approvals. The proposed development will be contained wholly within the subject site. No part of the proposed development will be constructed within the road corridor.
PO34 Development does not cause ground water disturbance in a state-controlled road .	No acceptable outcome is prescribed.	Not applicable. The proposal seeks approval for a Material Change of Use – Resort Complex. Filling and excavation required to facilitate construction of the proposed development will form part of

Performance outcomes	Acceptable outcomes	Response
		<p>subsequent/downstream operational works approvals.</p> <p>The proposed development will be contained wholly within the subject site. No part of the proposed development will be constructed within the road corridor.</p>
PO35 Excavation, boring, piling, blasting and fill compaction do not adversely impact the physical condition or structural integrity of a state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	<p>Not applicable.</p> <p>The proposal seeks approval for a Material Change of Use – Resort Complex. Filling and excavation required to facilitate construction of the proposed development will form part of subsequent/downstream operational works approvals.</p> <p>The proposed development will be contained wholly within the subject site. No part of the proposed development will be constructed within the road corridor.</p>
PO36 Filling and excavation associated with the construction of new or changed access do not compromise the operation or capacity of existing drainage infrastructure for a state-controlled road .	No acceptable outcome is prescribed.	<p>Not applicable.</p> <p>The proposal seeks approval for a Material Change of Use – Resort Complex. Filling and excavation required to facilitate construction of the proposed development will form part of subsequent/downstream operational works approvals.</p>

Table 1.5 Environmental emissions

Statutory note: Where a **state-controlled road** is co-located in the same transport corridor as a railway, the development should instead comply with Environmental emissions in State code 2: Development in a railway environment.

Performance outcomes	Acceptable outcomes	Response
Reconfiguring a lot		
Involving the creation of 5 or fewer new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor		

Performance outcomes	Acceptable outcomes	Response
PO37 Development minimises free field noise intrusion from a state-controlled road .	<p>AO37.1 Development provides a noise barrier or earth mound which is designed, sited and constructed:</p> <ol style="list-style-type: none"> to achieve the maximum free field acoustic levels in reference table 2 (item 2.1); in accordance with: <ol style="list-style-type: none"> Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. <p>OR</p> <p>AO37.2 Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.</p> <p>OR</p> <p>AO37.3 Development provides a solid gap-free fence or other solid gap-free structure along the full extent of the boundary closest to the state-controlled road.</p>	<p>Complies with PO37.</p> <p>The proposed development has been appropriately design and sited to ensure free field noise intrusions from the state-controlled road is minimised.</p> <p>Notwithstanding this, further technical assessment may be required to demonstrate compliance with the requirements of PO37.</p>
Involving the creation of 6 or more new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor		
PO38 Reconfiguring a lot minimises free field noise intrusion from a state-controlled road .	<p>AO38.1 Development provides noise barrier or earth mound which is designed, sited and constructed:</p> <ol style="list-style-type: none"> to achieve the maximum free field acoustic levels in reference table 2 (item 2.1); in accordance with: <ol style="list-style-type: none"> Chapter 7 integrated noise barrier design of the Transport Noise Management Code of 	<p>Not applicable.</p> <p>The proposal seeks approval for a Material Change of Use – Resort Complex.</p>

Performance outcomes	Acceptable outcomes	Response
	<p>Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</p> <p>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</p> <p>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.</p> <p>OR</p> <p>AO38.2 Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.</p>	
Material change of use (accommodation activity)		
Ground floor level requirements adjacent to a state-controlled road or type 1 multi-modal corridor		
PO39 Development minimises noise intrusion from a state-controlled road in private open space .	<p>AO39.1 Development provides a noise barrier or earth mound which is designed, sited and constructed:</p> <ol style="list-style-type: none"> 1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.2) for private open space at the ground floor level; 2. in accordance with: <ol style="list-style-type: none"> a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. <p>OR</p> <p>AO39.2 Development achieves the maximum free field acoustic level in reference table 2 (item 2.2) for private open space by alternative noise</p>	<p>Complies with PO39.</p> <p>The proposed development has been appropriately designed and sited to ensure free field noise intrusion from the state-controlled road is minimised.</p> <p>Notwithstanding this, further technical assessment may be required to demonstrate compliance with the requirements of PO39.</p>

Performance outcomes	Acceptable outcomes	Response
	attenuation measures where it is not practical to provide a noise barrier or earth mound.	
PO40 Development (excluding a relevant residential building or relocated building) minimises noise intrusion from a state-controlled road in habitable rooms at the facade.	<p>AO40.1 Development (excluding a relevant residential building or relocated building) provides a noise barrier or earth mound which is designed, sited and constructed:</p> <ol style="list-style-type: none"> 1. to achieve the maximum building façade acoustic level in reference table 1 (item 1.1) for habitable rooms; 2. in accordance with: <ol style="list-style-type: none"> a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. <p>OR</p> <p>AO40.2 Development (excluding a relevant residential building or relocated building) achieves the maximum building façade acoustic level in reference table 1 (item 1.1) for habitable rooms by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.</p>	<p>Complies with PO40.</p> <p>The proposed development has been appropriately designed and sited to ensure noise intrusion from the state-controlled road is minimised.</p> <p>The number of habitable rooms located adjacent to the state controlled road is minimal, with most habitable rooms being located towards the rear of the site.</p> <p>Notwithstanding this, further technical assessment may be required to demonstrate compliance with the requirements of PO40.</p>
PO41 Habitable rooms (excluding a relevant residential building or relocated building) are designed and constructed using materials to achieve the maximum internal acoustic level in reference table 3 (item 3.1).	No acceptable outcome is provided.	<p>Complies with PO41.</p> <p>The proposed development has been appropriately designed and sited to ensure noise intrusion from the state-controlled road is minimised.</p> <p>The number of habitable rooms located adjacent to the state controlled road is minimal, with most habitable rooms being located towards the rear of the site.</p>

Performance outcomes	Acceptable outcomes	Response
		Compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of a Condition within the Development Permit.
Above ground floor level requirements (accommodation activity) adjacent to a state-controlled road or type 1 multi-modal corridor		
PO42 Balconies, podiums, and roof decks include: <ol style="list-style-type: none"> 1. a continuous solid gap-free structure or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia); 2. highly acoustically absorbent material treatment for the total area of the soffit above balconies, podiums, and roof decks. 	No acceptable outcome is provided.	Complies with PO42. The proposed development has been appropriately designed and sited to ensure noise intrusion from the state-controlled road is minimised. The number of habitable rooms located adjacent to the state controlled road is minimal, with most habitable rooms being located towards the rear of the site. Compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of a Condition within the Development Permit.
PO43 Habitable rooms (excluding a relevant residential building or relocated building) are designed and constructed using materials to achieve the maximum internal acoustic level in reference table 3 (item 3.1).	No acceptable outcome is provided.	Complies with PO43. The proposed development has been appropriately designed and sited to ensure noise intrusion from the state-controlled road is minimised. The number of habitable rooms located adjacent to the state controlled road is minimal, with most habitable rooms being located towards the rear of the site. Compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of a Condition within the Development Permit.
Material change of use (other uses)		
Ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor		
PO44 Development:	No acceptable outcome is provided.	Not applicable.

Performance outcomes	Acceptable outcomes	Response
<ol style="list-style-type: none"> 1. provides a noise barrier or earth mound that is designed, sited and constructed: <ol style="list-style-type: none"> a. to achieve the maximum free field acoustic level in reference table 2 (item 2.3) for all outdoor education areas and outdoor play areas; b. in accordance with: <ol style="list-style-type: none"> i. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; ii. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; iii. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or 2. achieves the maximum free field acoustic level in reference table 2 (item 2.3) for all outdoor education areas and outdoor play areas by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound. 		<p>The proposal seeks approval for a Material Change of Use – Resort Complex.</p>
<p>PO45 Development involving a childcare centre or educational establishment:</p> <ol style="list-style-type: none"> 1. provides a noise barrier or earth mound that is designed, sited and constructed: 2. to achieve the maximum building facade acoustic level in reference table 1 (item 1.2); 3. in accordance with: <ol style="list-style-type: none"> a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; 	No acceptable outcome is provided.	<p>Not applicable.</p> <p>The proposal seeks approval for a Material Change of Use – Resort Complex.</p>

Performance outcomes	Acceptable outcomes	Response
c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or 4. achieves the maximum building facade acoustic level in reference table 1 (item 1.2) by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.		
PO46 Development involving: 1. indoor education areas and indoor play areas ; or 2. sleeping rooms in a childcare centre ; or 3. patient care areas in a hospital achieves the maximum internal acoustic level in reference table 3 (items 3.2-3.4).	No acceptable outcome is provided.	Not applicable. The proposal seeks approval for a Material Change of Use – Resort Complex.
Above ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor		
PO47 Development involving a childcare centre or educational establishment which have balconies, podiums or elevated outdoor play areas predicted to exceed the maximum free field acoustic level in reference table 2 (item 2.3) due to noise from a state-controlled road are provided with: 1. a continuous solid gap-free structure or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia); 2. highly acoustically absorbent material treatment for the total area of the soffit above balconies or elevated outdoor play areas .	No acceptable outcome is provided.	Not applicable. The proposal seeks approval for a Material Change of Use – Resort Complex.

Performance outcomes	Acceptable outcomes	Response
PO48 Development including: 1. indoor education areas and indoor play areas in a childcare centre or educational establishment ; or 2. sleeping rooms in a childcare centre ; or 3. patient care areas in a hospital located above ground level, is designed and constructed to achieve the maximum internal acoustic level in reference table 3 (items 3.2-3.4).	No acceptable outcome is provided.	Not applicable. The proposal seeks approval for a Material Change of Use – Resort Complex.
Air, light and vibration		
PO49 Private open space, outdoor education areas and outdoor play areas are protected from air quality impacts from a state-controlled road .	AO49.1 Each dwelling or unit has access to a private open space which is shielded from a state-controlled road by a building, solid gap-free fence , or other solid gap-free structure . OR AO49.2 Each outdoor education area and outdoor play area is shielded from a state-controlled road by a building, solid gap-free fence , or other solid gap-free structure .	Not applicable. The proposal seeks approval for a Material Change of Use – Resort Complex.
PO50 Patient care areas within hospitals are protected from vibration impacts from a state-controlled road or type 1 multi-modal corridor .	AO50.1 Hospitals are designed and constructed to ensure vibration in the patient treatment area does not exceed a vibration dose value of $0.1\text{m/s}^{1.75}$. AND AO50.2 Hospitals are designed and constructed to ensure vibration in the ward of a patient care area does not exceed a vibration dose value of $0.4\text{m/s}^{1.75}$.	Not applicable. The proposal seeks approval for a Material Change of Use – Resort Complex.
PO51 Development is designed and sited to ensure light from infrastructure within, and from users of, a	No acceptable outcomes are prescribed.	Not applicable.

Performance outcomes	Acceptable outcomes	Response
state-controlled road or type 1 multi-modal corridor , does not: 1. intrude into buildings during night hours (10pm to 6am); 2. create unreasonable disturbance during evening hours (6pm to 10pm).		The proposal seeks approval for a Material Change of Use – Resort Complex.

Table 1.6: Development in a future state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
PO52 Development does not impede delivery of a future state-controlled road .	AO52.1 Development is not located in a future state-controlled road . OR ALL OF THE FOLLOWING APPLY: AO52.2 Development does not involve filling and excavation of, or material changes to, a future state-controlled road . AND AO52.3 The intensification of lots does not occur within a future state-controlled road . AND AO52.4 Development does not result in the landlocking of parcels once a future state-controlled road is delivered.	Not applicable. The site is not within proximity to a future state-controlled road environment.
PO53 The location and design of new or changed access does not create a safety hazard for users of a future state-controlled road .	AO53.1 Development does not include new or changed access to a future state-controlled road .	Not applicable. The site is not within proximity to a future state-controlled road environment.
PO54 Filling, excavation, building foundations and retaining structures do not undermine, damage	No acceptable outcome is prescribed.	Not applicable.

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Performance outcomes	Acceptable outcomes	Response
or cause subsidence of a future state-controlled road .		The site is not within proximity to a future state-controlled road environment.
PO55 Development does not result in a material worsening of stormwater, flooding, overland flow or drainage impacts in a future state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Not applicable. The site is not within proximity to a future state-controlled road environment.
PO56 Development ensures that stormwater is lawfully discharged.	AO56.1 Development does not create any new points of discharge to a future state-controlled road . AND AO56.2 Development does not concentrate flows to a future state-controlled road . AND AO56.3 Stormwater run-off is discharged to a lawful point of discharge . AND AO56.4 Development does not worsen the condition of an existing lawful point of discharge to the future state-controlled road .	Not applicable. The site is not within proximity to a future state-controlled road environment.

State code 6: Protection of state transport networks

Table 6.2 Development in general

Performance outcomes	Acceptable outcomes	Response
Network impacts		
PO1 Development does not compromise the safety of users of the state-controlled road network.	No acceptable outcome is prescribed.	<p>Complies with PO1</p> <p>Development will not compromise the safety of users of the state-controlled road network.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO1. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by SARA.</p>
PO2 Development does not adversely impact the structural integrity or physical condition of a state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	<p>Complies with PO2</p> <p>Development will not compromise the safety of users of the state-controlled road network.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO1. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by SARA.</p>
PO3 Development ensures no net worsening of the operating performance the state-controlled road network.	No acceptable outcome is prescribed.	<p>Complies with PO3</p> <p>Development will not compromise the safety of users of the state-controlled road network.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO1. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future</p>

Performance outcomes	Acceptable outcomes	Response
		response to a request for further information issued by SARA.
PO4 Traffic movements are not directed onto a state-controlled road where they can be accommodated on the local road network.	No acceptable outcome is prescribed.	<p>Complies with PO4</p> <p>The subject site has frontage to Port Douglas Road, being a State-controlled Road. The site does not have frontage to any other local road networks.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO1. It is noted that technical traffic and engineering assessments will be completed and are likely to form part of any future response to a request for further information issued by SARA.</p>
PO5 Development involving haulage exceeding 10,000 tonnes per year does not damage the pavement of a state-controlled road .	No acceptable outcome is prescribed.	<p>Not applicable.</p> <p>The development is not proposed to include haulage exceeding 10,000 tonnes per year.</p>
PO6 Development does not require a new railway level crossing.	No acceptable outcome is prescribed.	<p>Not applicable.</p> <p>The development does not require a new railway level crossing.</p>
PO7 Development does not adversely impact the operating performance of an existing railway crossing .	No acceptable outcome is prescribed.	<p>Not applicable.</p> <p>The development is not expected to impact on an existing railway crossing. No railway crossings exist within proximity to the subject site.</p>
PO8 Development does not adversely impact on the safety of an existing railway crossing .	No acceptable outcome is prescribed.	<p>Not applicable.</p> <p>The development is not expected to impact on an existing railway crossing. No railway crossings exist within proximity to the subject site.</p>
PO9 Development is designed and constructed to allow for on-site circulation to ensure vehicles do not queue in a railway crossing .	No acceptable outcome is prescribed.	<p>Not applicable.</p> <p>The development is not expected to impact on an existing railway crossing. No railway crossings exist within proximity to the subject site.</p>

Performance outcomes	Acceptable outcomes	Response
PO10 Development does not create a safety hazard within the railway corridor .	No acceptable outcome is prescribed.	Not applicable. The development is not expected to impact on an existing railway corridor. No railway corridors exist within proximity to the subject site.
PO11 Development does not adversely impact the operating performance of the railway corridor .	No acceptable outcome is prescribed.	Not applicable. The development is not expected to impact on an existing railway corridor. No railway corridors exist within proximity to the subject site.
PO12 Development does not interfere with or obstruct the railway transport infrastructure or other rail infrastructure .	No acceptable outcome is prescribed.	Not applicable. The development is not expected to impact on/interfere with railway transport infrastructure or other rail infrastructure. No railway infrastructure and/or corridors exist within proximity to the subject site.
PO13 Development does not adversely impact the structural integrity or physical condition of a railway corridor or rail transport infrastructure .	No acceptable outcome is prescribed.	Not applicable. The development is not expected to impact on/interfere with railway transport infrastructure or other rail infrastructure. No railway infrastructure and/or corridors exist within proximity to the subject site.
Stormwater and overland flow		
PO14 Stormwater run-off or overland flow from the development site does not create or exacerbate a safety hazard for users of a state transport corridor or state transport infrastructure .	No acceptable outcome is prescribed.	Complies with PO14 Development will not result in stormwater flows/run-off likely to compromise the safety of users of the state-controlled road network. Stormwater flows will be appropriately managed via the installation of underground holding tanks and a controlled release system. Stormwater will be slowly released and discharged offsite, via a lawful point of discharge) to the rear of the site (which comprises of the Port Douglas Mirage Gold Course) as well as to the drainage swales existing within the road reserve, along the site's frontage.

Performance outcomes	Acceptable outcomes	Response
		Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO14. It is noted that further technical assessments will be undertaken to inform a detailed stormwater management plan, likely to form part of any future response to a request for further information issued by SARA.
PO15 Stormwater run-off or overland flow from the development site does not result in a material worsening of operating performance of a state transport corridor or state transport infrastructure .	No acceptable outcome is prescribed.	<p>Complies with PO15</p> <p>Development will not result in stormwater flows/run-off likely to result in a material worsening of operating performance of a state transport corridor or state transport infrastructure.</p> <p>Stormwater flows will be appropriately managed via the installation of underground holding tanks and a controlled release system. Stormwater will be slowly released and discharged offsite, via a lawful point of discharge) to the rear of the site (which comprises of the Port Douglas Mirage Gold Course) as well as to the drainage swales existing within the road reserve, along the site's frontage.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO15. It is noted that further technical assessments will be undertaken to inform a detailed stormwater management plan, likely to form part of any future response to a request for further information issued by SARA.</p>
PO16 Stormwater run-off or overland flow from the development site does not interfere with the structural integrity or physical condition of the state transport corridor or state transport infrastructure .	No acceptable outcome is prescribed.	<p>Complies with PO16</p> <p>Development will not result in stormwater flows/run-off likely to interfere with the structural integrity or physical condition of a state transport corridor or state transport infrastructure.</p> <p>Stormwater flows will be appropriately managed via the installation of underground holding tanks and a controlled release system. Stormwater will be slowly</p>

Performance outcomes	Acceptable outcomes	Response
		<p>released and discharged offsite, via a lawful point of discharge) to the rear of the site (which comprises of the Port Douglas Mirage Gold Course) as well as to the drainage swales existing within the road reserve, along the site's frontage.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO16. It is noted that further technical assessments will be undertaken to inform a detailed stormwater management plan, likely to form part of any future response to a request for further information issued by SARA.</p>
<p>PO17 Development associated with a state-controlled road or road transport infrastructure ensures that stormwater is lawfully discharged.</p>	<p>AO17.1 Development does not create any new points of discharge to a state transport corridor or state transport infrastructure.</p> <p>AND</p> <p>AO17.2 Development does not concentrate flows to a state transport corridor.</p> <p>AND</p> <p>AO17.3 Stormwater run-off is discharged to a lawful point of discharge.</p> <p>AND</p> <p>AO17.4 Development does not worsen the condition of an existing lawful point of discharge to a state transport corridor or state transport infrastructure.</p>	<p>Complies with PO17</p> <p>Development will not result in stormwater flows/run-off likely to result in a worsening condition of an existing lawful point of discharge to a state transport corridor or state transport infrastructure.</p> <p>Stormwater flows will be appropriately managed via the installation of underground holding tanks and a controlled release system. Stormwater will be slowly released and discharged offsite, via a lawful point of discharge) to the rear of the site (which comprises of the Port Douglas Mirage Gold Course) as well as to the drainage swales existing within the road reserve, along the site's frontage. In considering this, it is submitted that not all stormwater flows will be concentrated to the state transport corridor.</p> <p>Additionally, it is noted that two new lawful points of discharge are proposed along the frontage of the site, to facilitate stormwater discharge into the existing drainage swales, which are located within the state-controlled corridor.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO17. It is noted that further technical assessments will be undertaken to inform</p>

Performance outcomes	Acceptable outcomes	Response
		a detailed stormwater management plan, likely to form part of any future response to a request for further information issued by SARA.
Flooding		
PO18 Development does not result in a material worsening of flooding impacts within a state transport corridor or state transport infrastructure	<p><i>For a state-controlled road or road transport infrastructure, all of the following apply:</i></p> <p>AO18.1 For all flood events up to 1% annual exceedance probability, development ensures there are negligible impacts (within +/- 10mm) to existing flood levels within a state transport corridor.</p> <p>AND</p> <p>AO18.2 For all flood events up to 1% annual exceedance probability, development ensures there are negligible impacts (up to a 10% increase) to existing peak velocities within a state transport corridor.</p> <p>AND</p> <p>AO18.3 For all flood events up to 1% annual exceedance probability, development ensures there are negligible impacts (up to a 10% increase) to existing time of submergence of a state transport corridor.</p> <p><i>No acceptable outcome is prescribed for a railway corridor or rail transport infrastructure.</i></p>	<p>Complies with PO18</p> <p>Development will not result in a material worsening of flooding impacts within a state transport corridor or state transport infrastructure.</p> <p>Further technical/engineering assessment is required to be undertaken to confirm compliance with the requirements of PO18. It is noted that a detailed flood assessment will be undertaken and is likely to form part of any future response to a request for further information issued by SARA.</p>
Drainage infrastructure		
PO19 Drainage infrastructure does not create a safety hazard in a state transport corridor .	<p><i>For a state-controlled road environment, both of the following apply:</i></p> <p>AO19.1 Drainage infrastructure associated with, or in a state-controlled road is wholly contained</p>	<p>Complies with AO19.1 and AO19.2</p> <p>All proposed drainage infrastructure will be located wholly within the development site. Access to drainage infrastructure can be gained from within the site.</p>

Performance outcomes	Acceptable outcomes	Response
	<p>within the development site, except at the lawful point of discharge.</p> <p>AND</p> <p>AO19.2 Drainage infrastructure can be maintained without requiring access to a state transport corridor.</p> <p><i>For a railway environment both of the following apply:</i></p> <p>AO19.3 Drainage infrastructure associated with a railway corridor or rail transport infrastructure is wholly contained within the development site.</p> <p>AND</p> <p>AO19.4 Drainage infrastructure can be maintained without requiring access to a state transport corridor.</p>	
PO20 Drainage infrastructure associated with, or in a state-controlled road or road transport infrastructure is constructed and designed to ensure the structural integrity and physical condition of existing drainage infrastructure and the surrounding drainage network is maintained.	No acceptable outcome is prescribed.	<p>Complies with PO20.</p> <p>Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of a Condition within the Development Permit.</p>
Planned upgrades		
PO21 Development does not impede delivery of planned upgrades of state transport infrastructure .	No acceptable outcome is prescribed.	<p>Complies with PO21.</p> <p>The site is not mapped as being impacted or affected by a planned upgrade to state transport infrastructure.</p> <p>Where deemed necessary, compliance with the requirements of this Assessment Benchmark can be ensured through the imposition of a Condition within the Development Permit.</p>

Table 6.3 Public passenger transport infrastructure and active transport

Performance outcomes	Acceptable outcomes	Response
PO22 Development does not damage or interfere with public passenger transport infrastructure, active transport infrastructure or public passenger services .	No acceptable outcome is prescribed.	Complies with PO22 Development will not damage or interfere with public transport infrastructure, active transport networks or public passenger services.
PO23 Development does not compromise the safety of public passenger transport infrastructure, public passenger services and active transport infrastructure .	No acceptable outcome is prescribed.	Complies with PO23 Development will not compromise the safety of public transport infrastructure, active transport networks or public passenger services.
PO24 Development does not adversely impact the operating performance of public passenger transport infrastructure, public passenger services and active transport infrastructure .	No acceptable outcome is prescribed.	Complies with PO24 Development will not adversely impact the operating performance of public transport infrastructure, active transport networks or public passenger services.
PO25 Development does not adversely impact the structural integrity or physical condition of public passenger transport infrastructure and active transport infrastructure .	No acceptable outcome is prescribed.	Complies with PO25 Development will not adversely impact the structural integrity or physical condition of public transport infrastructure, active transport networks or public passenger services.
PO26 Upgraded or new public passenger transport infrastructure and active transport infrastructure is provided to accommodate the demand for public passenger transport and active transport generated by the development.	No acceptable outcome is prescribed.	Not applicable Development is not for activities as nominated in Table 6.1.
PO27 Development is designed to ensure the location of public passenger transport infrastructure prioritises and enables efficient public passenger services .	No acceptable outcome is prescribed.	Not applicable Development is not for activities as nominated in Table 6.1.
PO28 Development enables the provision or extension of public passenger services, public passenger transport infrastructure and active transport infrastructure to the development and avoids creating indirect or inefficient routes for public passenger services .	No acceptable outcome is prescribed.	Not applicable Development is not for activities as nominated in Table 6.1.

Performance outcomes	Acceptable outcomes	Response
PO29 New or modified road networks are designed to enable development to be serviced by public passenger services .	<p>AO29.1 Roads catering for buses are arterial or sub-arterial roads, collector or their equivalent.</p> <p>AND</p> <p>AO29.2 Roads intended to accommodate buses are designed and constructed in accordance with:</p> <ol style="list-style-type: none"> 1. Road Planning and Design Manual, 2nd Edition, Volume 3 – Guide to Road Design; Department of Transport and Main Roads; 2. Supplement to Austroads Guide to Road Design (Parts 3, 4-4C and 6), Department of Transport and Main Roads; 3. Austroads Guide to Road Design (Parts 3, 4-4C and 6); 4. Austroads Design Vehicles and Turning Path Templates; 5. Queensland Manual of Uniform Traffic Control Devices, Part 13: Local Area Traffic Management and AS 1742.13-2009 Manual of Uniform Traffic Control Devices – Local Area Traffic Management; <p>AND</p> <p>AO29.3 Traffic calming devices are not installed on roads used for buses in accordance with section 2.3.2 Bus Route Infrastructure, Public Transport Infrastructure Manual, Department of Transport and Main Roads, 2015.</p>	<p>Not applicable</p> <p>Development is not for activities as nominated in Table 6.1.</p>
PO30 Development provides safe, direct and convenient access to existing and future public passenger transport infrastructure and active transport infrastructure .	No acceptable outcome is prescribed.	<p>Not applicable</p> <p>Development is not for activities as nominated in Table 6.1.</p>
PO31 On-site vehicular circulation ensures the safety of both public passenger transport services and pedestrians.	No acceptable outcome is prescribed.	<p>Not applicable</p> <p>Development is not for activities as nominated in Table 6.1.</p>

Performance outcomes	Acceptable outcomes	Response
PO32 Taxi facilities are provided to accommodate the demand generated by the development.	No acceptable outcome is prescribed.	Not applicable Development is not for activities as nominated in Table 6.1.
PO33 Facilities are provided to accommodate the demand generated by the development for community transport services, courtesy transport services, and booked hire services other than taxis.	No acceptable outcome is prescribed.	Not applicable Development is not for activities as nominated in Table 6.1.
PO34 Taxi facilities are located and designed to provide convenient, safe and equitable access for passengers.	<p>AO34.1 A taxi facility is provided parallel to the kerb and adjacent to the main entrance.</p> <p>AND</p> <p>AO34.2 Taxi facilities are designed in accordance with:</p> <ol style="list-style-type: none"> 1. AS2890.5–1993 Parking facilities – on-street parking and AS1428.1–2009 Design for access and mobility – general requirements for access – new building work; 2. AS1742.11–1999 Parking controls – manual of uniform traffic control devices 3. AS/NZS 2890.6–2009 Parking facilities –off street parking for people with disabilities; 4. Disability standards for accessible public 5. transport 2002 made under section 31(1) of the Disability Discrimination Act 1992; 6. AS/NZS 1158.3.1 – Lighting for roads and public spaces, Part 3.1: Pedestrian area (category P) lighting – Performance and design requirements; 7. Chapter 7 Taxi Facilities, Public Transport Infrastructure Manual, Department of Transport and Main Roads, 2015. 	Not applicable Development is not for activities as nominated in Table 6.1.
PO35 Educational establishments are designed to ensure the safe and efficient operation of public passenger services , pedestrian and cyclist access and active transport infrastructure .	AO35.1 Educational establishments are designed in accordance with the provisions of the Planning for Safe Transport Infrastructure at Schools, Department of Transport and Main Roads, 2011.	Not applicable Development is not for activities as nominated in Table 6.1.