

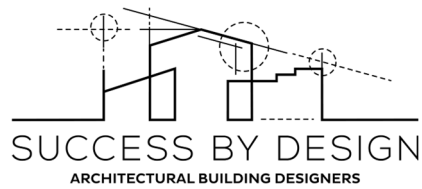


APPLICATION FOR CHANGES TO OPERATIONAL WORKS

3 Sonata Close Port Douglas 4877 QLD

Client: Adrian & Lynn Jones

Ryan Fudala
info@successbydesignaustralia.com



Success By Design
Architectural Building Designers
QBCC: 1114352
ABN: 85 517 619 070
Po Box 58
Port Douglas Qld 4877
0404 637 739

22 Oct 2024

Attn:

Chief Executive Officer

Douglas Shire Council

64-66 Front Street

MOSSMAN QLD 4873

Via email: enquiries@douglas.qld.gov.au

RE: Application for changes to Operational Works- Second cross over access,

On Land described as 3 Sonata Close Port Douglas 4877 Qld Lot 101 RP729080.

Success By Design Architectural Building Designers act on behalf of our Clients Adrian & Lynn Jones

Enclosed with this cover letter is the following,

1. DA Form 1
2. Owners Consent
3. Proposed Plans showing setbacks and FFL's to the AHD.
4. Planning Report to all relevant codes.

The relevant Application Fee as mentioned in Previous correspondence with Council
has been paid.

For Further information or enquiries please contact myself.

Kind regards

Ryan Fudala

Ryan Fudala

Director Success By Design Architectural Building Designers

DA Form 1 – Development application details

Approved form (version 1.4 effective 15 December 2023) made under section 282 of the Planning Act 2016.

This form **must** be used to make a development application **involving code assessment or impact assessment**, except when applying for development involving only building work.

For a development application involving **building work only**, use *DA Form 2 – Building work details*.

For a development application involving **building work associated with any other type of assessable development (i.e. material change of use, operational work or reconfiguring a lot)**, use this form (*DA Form 1*) and parts 4 to 6 of *DA Form 2 – Building work details*.

Unless stated otherwise, all parts of this form **must** be completed in full and all required supporting information **must** accompany the development application.

One or more additional pages may be attached as a schedule to this development application if there is insufficient space on the form to include all the necessary information.

This form and any other form relevant to the development application must be used to make a development application relating to strategic port land and Brisbane core port land under the *Transport Infrastructure Act 1994*, and airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*. For the purpose of assessing a development application relating to strategic port land and Brisbane core port land, any reference to a planning scheme is taken to mean a land use plan for the strategic port land, Brisbane port land use plan for Brisbane core port land, or a land use plan for airport land.

Note: All terms used in this form have the meaning given under the Planning Act 2016, the Planning Regulation 2017, or the Development Assessment Rules (DA Rules).

PART 1 – APPLICANT DETAILS

1) Applicant details	
Applicant name(s) (individual or company full name)	Success By Design Architectural Building Designers
Contact name (only applicable for companies)	Ryan Fudala
Postal address (P.O. Box or street address)	Po Box 58
Suburb	Port Douglas
State	Qld
Postcode	4877
Country	Australia
Contact number	0404 637 739
Email address (non-mandatory)	ryanfudala@hotmail.com
Mobile number (non-mandatory)	
Fax number (non-mandatory)	
Applicant's reference number(s) (if applicable)	

2) Owner's consent	
2.1) Is written consent of the owner required for this development application?	
<input checked="" type="checkbox"/> Yes – the written consent of the owner(s) is attached to this development application	
<input type="checkbox"/> No – proceed to 3)	

PART 2 – LOCATION DETAILS

3) Location of the premises (complete 3.1) or 3.2), and 3.3) as applicable)

Note: Provide details below and attach a site plan for any or all premises part of the development application. For further information, see [DA Forms Guide: Relevant plans](#).

3.1) Street address and lot on plan

- ☒ Street address **AND** lot on plan (all lots must be listed), **or**
☐ Street address **AND** lot on plan for an adjoining or adjacent property of the premises (appropriate for development in water but adjoining or adjacent to land e.g. jetty, pontoon. All lots must be listed).

a)	Unit No.	Street No.	Street Name and Type	Suburb
		3	Sonata Close	Port Douglas
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)
	4877	101	RP729080	Douglas Shire
b)	Unit No.	Street No.	Street Name and Type	Suburb
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)

3.2) Coordinates of premises (appropriate for development in remote areas, over part of a lot or in water not adjoining or adjacent to land e.g. channel dredging in Moreton Bay)

Note: Place each set of coordinates in a separate row.

- ☐ Coordinates of premises by longitude and latitude

Longitude(s)	Latitude(s)	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

- ☐ Coordinates of premises by easting and northing

Easting(s)	Northing(s)	Zone Ref.	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> 54 <input type="checkbox"/> 55 <input type="checkbox"/> 56	<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

3.3) Additional premises

- ☐ Additional premises are relevant to this development application and the details of these premises have been attached in a schedule to this development application
☒ Not required

4) Identify any of the following that apply to the premises and provide any relevant details

- ☐ In or adjacent to a water body or watercourse or in or above an aquifer

Name of water body, watercourse or aquifer:

- ☐ On strategic port land under the *Transport Infrastructure Act 1994*

Lot on plan description of strategic port land:

Name of port authority for the lot:

- ☐ In a tidal area

Name of local government for the tidal area (if applicable):

Name of port authority for tidal area (if applicable):

- ☐ On airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*

Name of airport:

<input type="checkbox"/> Listed on the Environmental Management Register (EMR) under the <i>Environmental Protection Act 1994</i>
EMR site identification: <input type="text"/>
<input type="checkbox"/> Listed on the Contaminated Land Register (CLR) under the <i>Environmental Protection Act 1994</i>
CLR site identification: <input type="text"/>

5) Are there any existing easements over the premises?

Note: Easement uses vary throughout Queensland and are to be identified correctly and accurately. For further information on easements and how they may affect the proposed development, see [DA Forms Guide](#).

- ☐ Yes – All easement locations, types and dimensions are included in plans submitted with this development application
- ☒ No

PART 3 – DEVELOPMENT DETAILS

Section 1 – Aspects of development

6.1) Provide details about the first development aspect

a) What is the type of development? *(tick only one box)*

- ☐ Material change of use ☐ Reconfiguring a lot ☒ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☒ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☒ Code assessment ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

Extra driveway crossover, this would be a total of 2 crossovers for the Property (see plans)

e) Relevant plans

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms guide: Relevant plans](#).

- ☒ Relevant plans of the proposed development are attached to the development application

6.2) Provide details about the second development aspect

a) What is the type of development? *(tick only one box)*

- ☐ Material change of use ☐ Reconfiguring a lot ☐ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☐ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☐ Code assessment ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

e) Relevant plans

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).

- ☒ Relevant plans of the proposed development are attached to the development application

6.3) Additional aspects of development

☐ Additional aspects of development are relevant to this development application and the details for these aspects that would be required under Part 3 Section 1 of this form have been attached to this development application

- ☒ Not required

Section 2 – Further development details

7) Does the proposed development application involve any of the following?	
Material change of use	<input type="checkbox"/> Yes – complete division 1 if assessable against a local planning instrument
Reconfiguring a lot	<input type="checkbox"/> Yes – complete division 2
Operational work	<input checked="" type="checkbox"/> Yes – complete division 3
Building work	<input type="checkbox"/> Yes – complete <i>DA Form 2 – Building work details</i>

Division 1 – Material change of use

Note: This division is only required to be completed if any part of the development application involves a material change of use assessable against a local planning instrument.

8.1) Describe the proposed material change of use			
Provide a general description of the proposed use	Provide the planning scheme definition (include each definition in a new row)	Number of dwelling units (if applicable)	Gross floor area (m ²) (if applicable)
8.2) Does the proposed use involve the use of existing buildings on the premises?			
<input type="checkbox"/> Yes			
<input type="checkbox"/> No			

Division 2 – Reconfiguring a lot

Note: This division is only required to be completed if any part of the development application involves reconfiguring a lot.

9.1) What is the total number of existing lots making up the premises?	
9.2) What is the nature of the lot reconfiguration? (tick all applicable boxes)	
<input type="checkbox"/> Subdivision (complete 10))	<input type="checkbox"/> Dividing land into parts by agreement (complete 11))
<input type="checkbox"/> Boundary realignment (complete 12))	<input type="checkbox"/> Creating or changing an easement giving access to a lot from a constructed road (complete 13))

10) Subdivision				
10.1) For this development, how many lots are being created and what is the intended use of those lots:				
Intended use of lots created	Residential	Commercial	Industrial	Other, please specify:
Number of lots created				
10.2) Will the subdivision be staged?				
<input type="checkbox"/> Yes – provide additional details below				
<input type="checkbox"/> No				
How many stages will the works include?				
What stage(s) will this development application apply to?				

11) Dividing land into parts by agreement – how many parts are being created and what is the intended use of the parts?				
Intended use of parts created	Residential	Commercial	Industrial	Other, please specify:
Number of parts created				

12) Boundary realignment			
12.1) What are the current and proposed areas for each lot comprising the premises?			
Current lot		Proposed lot	
Lot on plan description	Area (m ²)	Lot on plan description	Area (m ²)
12.2) What is the reason for the boundary realignment?			

13) What are the dimensions and nature of any existing easements being changed and/or any proposed easement? (attach schedule if there are more than two easements)				
Existing or proposed?	Width (m)	Length (m)	Purpose of the easement? (e.g. pedestrian access)	Identify the land/lot(s) benefitted by the easement

Division 3 – Operational work

Note: This division is only required to be completed if any part of the development application involves operational work.

14.1) What is the nature of the operational work?	
<input type="checkbox"/> Road work <input checked="" type="checkbox"/> Drainage work <input checked="" type="checkbox"/> Landscaping <input checked="" type="checkbox"/> Other – please specify:	<input checked="" type="checkbox"/> Stormwater <input type="checkbox"/> Earthworks <input type="checkbox"/> Signage <input type="checkbox"/> Water infrastructure <input type="checkbox"/> Sewage infrastructure <input type="checkbox"/> Clearing vegetation
Second Crossover to the road frontage	
14.2) Is the operational work necessary to facilitate the creation of new lots? (e.g. subdivision)	
<input type="checkbox"/> Yes – specify number of new lots:	
<input checked="" type="checkbox"/> No	
14.3) What is the monetary value of the proposed operational work? (include GST, materials and labour)	
\$50000 inc GST	

PART 4 – ASSESSMENT MANAGER DETAILS

15) Identify the assessment manager(s) who will be assessing this development application
Chief Executive Officer Douglas Shire Council
16) Has the local government agreed to apply a superseded planning scheme for this development application?
<input type="checkbox"/> Yes – a copy of the decision notice is attached to this development application <input type="checkbox"/> The local government is taken to have agreed to the superseded planning scheme request – relevant documents attached <input checked="" type="checkbox"/> No

PART 5 – REFERRAL DETAILS

17) Does this development application include any aspects that have any referral requirements?

Note: A development application will require referral if prescribed by the Planning Regulation 2017.

☒ No, there are no referral requirements relevant to any development aspects identified in this development application – proceed to Part 6

Matters requiring referral to the **Chief Executive of the Planning Act 2016:**

- ☐ Clearing native vegetation
- ☐ Contaminated land (*unexploded ordnance*)
- ☐ Environmentally relevant activities (ERA) (*only if the ERA has not been devolved to a local government*)
- ☐ Fisheries – aquaculture
- ☐ Fisheries – declared fish habitat area
- ☐ Fisheries – marine plants
- ☐ Fisheries – waterway barrier works
- ☐ Hazardous chemical facilities
- ☐ Heritage places – Queensland heritage place (*on or near a Queensland heritage place*)
- ☐ Infrastructure-related referrals – designated premises
- ☐ Infrastructure-related referrals – state transport infrastructure
- ☐ Infrastructure-related referrals – State transport corridor and future State transport corridor
- ☐ Infrastructure-related referrals – State-controlled transport tunnels and future state-controlled transport tunnels
- ☐ Infrastructure-related referrals – near a state-controlled road intersection
- ☐ Koala habitat in SEQ region – interfering with koala habitat in koala habitat areas outside koala priority areas
- ☐ Koala habitat in SEQ region – key resource areas
- ☐ Ports – Brisbane core port land – near a State transport corridor or future State transport corridor
- ☐ Ports – Brisbane core port land – environmentally relevant activity (ERA)
- ☐ Ports – Brisbane core port land – tidal works or work in a coastal management district
- ☐ Ports – Brisbane core port land – hazardous chemical facility
- ☐ Ports – Brisbane core port land – taking or interfering with water
- ☐ Ports – Brisbane core port land – referable dams
- ☐ Ports – Brisbane core port land – fisheries
- ☐ Ports – Land within Port of Brisbane's port limits (*below high-water mark*)
- ☐ SEQ development area
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – tourist activity or sport and recreation activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – community activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – indoor recreation
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – urban activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – combined use
- ☐ SEQ northern inter-urban break – tourist activity or sport and recreation activity
- ☐ SEQ northern inter-urban break – community activity
- ☐ SEQ northern inter-urban break – indoor recreation
- ☐ SEQ northern inter-urban break – urban activity
- ☐ SEQ northern inter-urban break – combined use
- ☐ Tidal works or works in a coastal management district
- ☐ Reconfiguring a lot in a coastal management district or for a canal
- ☐ Erosion prone area in a coastal management district
- ☐ Urban design
- ☐ Water-related development – taking or interfering with water
- ☐ Water-related development – removing quarry material (*from a watercourse or lake*)
- ☐ Water-related development – referable dams
- ☐ Water-related development – levees (*category 3 levees only*)
- ☐ Wetland protection area

Matters requiring referral to the **local government**:

☐ Airport land

☐ Environmentally relevant activities (ERA) *(only if the ERA has been devolved to local government)*

☐ Heritage places – Local heritage places

Matters requiring referral to the **Chief Executive of the distribution entity or transmission entity**:

☐ Infrastructure-related referrals – Electricity infrastructure

Matters requiring referral to:

- The **Chief Executive of the holder of the licence**, if not an individual
- The **holder of the licence**, if the holder of the licence is an individual

☐ Infrastructure-related referrals – Oil and gas infrastructure

Matters requiring referral to the **Brisbane City Council**:

☐ Ports – Brisbane core port land

Matters requiring referral to the **Minister responsible for administering the *Transport Infrastructure Act 1994***:

☐ Ports – Brisbane core port land *(where inconsistent with the Brisbane port LUP for transport reasons)*

☐ Ports – Strategic port land

Matters requiring referral to the **relevant port operator**, if applicant is not port operator:

☐ Ports – Land within Port of Brisbane's port limits *(below high-water mark)*

Matters requiring referral to the **Chief Executive of the relevant port authority**:

☐ Ports – Land within limits of another port *(below high-water mark)*

Matters requiring referral to the **Gold Coast Waterways Authority**:

☐ Tidal works or work in a coastal management district *(in Gold Coast waters)*

Matters requiring referral to the **Queensland Fire and Emergency Service**:

☐ Tidal works or work in a coastal management district *(involving a marina (more than six vessel berths))*

18) Has any referral agency provided a referral response for this development application?

☐ Yes – referral response(s) received and listed below are attached to this development application

☒ No

Referral requirement	Referral agency	Date of referral response

Identify and describe any changes made to the proposed development application that was the subject of the referral response and this development application, or include details in a schedule to this development application *(if applicable)*.

PART 6 – INFORMATION REQUEST

19) Information request under Part 3 of the DA Rules

☒ I agree to receive an information request if determined necessary for this development application

☐ I do not agree to accept an information request for this development application

Note: By not agreeing to accept an information request I, the applicant, acknowledge:

- that this development application will be assessed and decided based on the information provided when making this development application and the assessment manager and any referral agencies relevant to the development application are not obligated under the DA Rules to accept any additional information provided by the applicant for the development application unless agreed to by the relevant parties
- Part 3 of the DA Rules will still apply if the application is an application listed under section 11.3 of the DA Rules.

Further advice about information requests is contained in the [DA Forms Guide](#).

PART 7 – FURTHER DETAILS

20) Are there any associated development applications or current approvals? (e.g. a preliminary approval)

- ☐ Yes – provide details below or include details in a schedule to this development application
☒ No

List of approval/development application references	Reference number	Date	Assessment manager
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			
<input type="checkbox"/> Approval <input type="checkbox"/> Development application			

21) Has the portable long service leave levy been paid? (only applicable to development applications involving building work or operational work)

- ☐ Yes – a copy of the receipted QLeave form is attached to this development application
☐ No – I, the applicant will provide evidence that the portable long service leave levy has been paid before the assessment manager decides the development application. I acknowledge that the assessment manager may give a development approval only if I provide evidence that the portable long service leave levy has been paid
☒ Not applicable (e.g. building and construction work is less than \$150,000 excluding GST)

Amount paid	Date paid (dd/mm/yy)	QLeave levy number (A, B or E)
\$		

22) Is this development application in response to a show cause notice or required as a result of an enforcement notice?

- ☐ Yes – show cause or enforcement notice is attached
☒ No

23) Further legislative requirements

Environmentally relevant activities

23.1) Is this development application also taken to be an application for an environmental authority for an **Environmentally Relevant Activity (ERA)** under section 115 of the *Environmental Protection Act 1994*?

- ☐ Yes – the required attachment (form ESR/2015/1791) for an application for an environmental authority accompanies this development application, and details are provided in the table below
☒ No

Note: Application for an environmental authority can be found by searching "ESR/2015/1791" as a search term at www.qld.gov.au. An ERA requires an environmental authority to operate. See www.business.qld.gov.au for further information.

Proposed ERA number:		Proposed ERA threshold:	
Proposed ERA name:			

- ☐ Multiple ERAs are applicable to this development application and the details have been attached in a schedule to this development application.

Hazardous chemical facilities

23.2) Is this development application for a **hazardous chemical facility**?

- ☐ Yes – Form 69: Notification of a facility exceeding 10% of schedule 15 threshold is attached to this development application
☒ No

Note: See www.business.qld.gov.au for further information about hazardous chemical notifications.

Clearing native vegetation

23.3) Does this development application involve **clearing native vegetation** that requires written confirmation that the chief executive of the *Vegetation Management Act 1999* is satisfied the clearing is for a relevant purpose under section 22A of the *Vegetation Management Act 1999*?

☐ Yes – this development application includes written confirmation from the chief executive of the *Vegetation Management Act 1999* (s22A determination)

☒ No

Note: 1. Where a development application for operational work or material change of use requires a s22A determination and this is not included, the development application is prohibited development.
2. See <https://www.qld.gov.au/environment/land/vegetation/applying> for further information on how to obtain a s22A determination.

Environmental offsets

23.4) Is this development application taken to be a prescribed activity that may have a significant residual impact on a **prescribed environmental matter** under the *Environmental Offsets Act 2014*?

☐ Yes – I acknowledge that an environmental offset must be provided for any prescribed activity assessed as having a significant residual impact on a prescribed environmental matter

☒ No

Note: The environmental offset section of the Queensland Government's website can be accessed at www.qld.gov.au for further information on environmental offsets.

Koala habitat in SEQ Region

23.5) Does this development application involve a material change of use, reconfiguring a lot or operational work which is assessable development under Schedule 10, Part 10 of the Planning Regulation 2017?

☐ Yes – the development application involves premises in the koala habitat area in the koala priority area

☐ Yes – the development application involves premises in the koala habitat area outside the koala priority area

☒ No

Note: If a koala habitat area determination has been obtained for this premises and is current over the land, it should be provided as part of this development application. See koala habitat area guidance materials at www.des.qld.gov.au for further information.

Water resources

23.6) Does this development application involve **taking or interfering with underground water through an artesian or subartesian bore, taking or interfering with water in a watercourse, lake or spring, or taking overland flow water under the Water Act 2000**?

☐ Yes – the relevant template is completed and attached to this development application and I acknowledge that a relevant authorisation or licence under the *Water Act 2000* may be required prior to commencing development

☒ No

Note: Contact the Department of Natural Resources, Mines and Energy at www.dnrme.qld.gov.au for further information.

DA templates are available from <https://planning.dsdmp.qld.gov.au/>. If the development application involves:

- Taking or interfering with underground water through an artesian or subartesian bore: complete DA Form 1 Template 1
- Taking or interfering with water in a watercourse, lake or spring: complete DA Form 1 Template 2
- Taking overland flow water: complete DA Form 1 Template 3.

Waterway barrier works

23.7) Does this application involve **waterway barrier works**?

☐ Yes – the relevant template is completed and attached to this development application

☒ No

DA templates are available from <https://planning.dsdmp.qld.gov.au/>. For a development application involving waterway barrier works, complete DA Form 1 Template 4.

Marine activities

23.8) Does this development application involve **aquaculture, works within a declared fish habitat area or removal, disturbance or destruction of marine plants**?

☐ Yes – an associated resource allocation authority is attached to this development application, if required under the *Fisheries Act 1994*

☒ No

Note: See guidance materials at www.daf.qld.gov.au for further information.

Quarry materials from a watercourse or lake

23.9) Does this development application involve the **removal of quarry materials from a watercourse or lake** under the *Water Act 2000*?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
☒ No

Note: Contact the Department of Natural Resources, Mines and Energy at www.dnrme.qld.gov.au and www.business.qld.gov.au for further information.

Quarry materials from land under tidal waters

23.10) Does this development application involve the **removal of quarry materials from land under tidal water** under the *Coastal Protection and Management Act 1995*?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
☒ No

Note: Contact the Department of Environment and Science at www.des.qld.gov.au for further information.

Referable dams

23.11) Does this development application involve a **referable dam** required to be failure impact assessed under section 343 of the *Water Supply (Safety and Reliability) Act 2008* (the Water Supply Act)?

- ☐ Yes – the 'Notice Accepting a Failure Impact Assessment' from the chief executive administering the Water Supply Act is attached to this development application
☒ No

Note: See guidance materials at www.dnrme.qld.gov.au for further information.

Tidal work or development within a coastal management district

23.12) Does this development application involve **tidal work or development in a coastal management district**?

- ☐ Yes – the following is included with this development application:
- ☐ Evidence the proposal meets the code for assessable development that is prescribed tidal work (*only required if application involves prescribed tidal work*)
 - ☐ A certificate of title
- ☒ No

Note: See guidance materials at www.des.qld.gov.au for further information.

Queensland and local heritage places

23.13) Does this development application propose development on or adjoining a place entered in the **Queensland heritage register** or on a place entered in a local government's **Local Heritage Register**?

- ☐ Yes – details of the heritage place are provided in the table below
☒ No

Note: See guidance materials at www.des.qld.gov.au for information requirements regarding development of Queensland heritage places.

Name of the heritage place:		Place ID:	
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Brothels

23.14) Does this development application involve a **material change of use for a brothel**?

- ☐ Yes – this development application demonstrates how the proposal meets the code for a development application for a brothel under Schedule 3 of the *Prostitution Regulation 2014*
☒ No

Decision under section 62 of the Transport Infrastructure Act 1994

23.15) Does this development application involve new or changed access to a state-controlled road?

- ☐ Yes – this application will be taken to be an application for a decision under section 62 of the *Transport Infrastructure Act 1994* (subject to the conditions in section 75 of the *Transport Infrastructure Act 1994* being satisfied)
☒ No

Walkable neighbourhoods assessment benchmarks under Schedule 12A of the Planning Regulation

23.16) Does this development application involve reconfiguring a lot into 2 or more lots in certain residential zones (except rural residential zones), where at least one road is created or extended?

☐ Yes – Schedule 12A is applicable to the development application and the assessment benchmarks contained in schedule 12A have been considered

☒ No

Note: See guidance materials at www.planning.dsdmip.qld.gov.au for further information.

PART 8 – CHECKLIST AND APPLICANT DECLARATION

24) Development application checklist

I have identified the assessment manager in question 15 and all relevant referral requirement(s) in question 17

☒ Yes

Note: See the *Planning Regulation 2017* for referral requirements

If building work is associated with the proposed development, Parts 4 to 6 of [DA Form 2 – Building work details](#) have been completed and attached to this development application

☐ Yes

☒ Not applicable

Supporting information addressing any applicable assessment benchmarks is with the development application

Note: This is a mandatory requirement and includes any relevant templates under question 23, a planning report and any technical reports required by the relevant categorising instruments (e.g. local government planning schemes, State Planning Policy, State Development Assessment Provisions). For further information, see [DA Forms Guide: Planning Report Template](#).

☒ Yes

Relevant plans of the development are attached to this development application

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).

☒ Yes

The portable long service leave levy for QLeave has been paid, or will be paid before a development permit is issued (see 21)

☐ Yes

☒ Not applicable

25) Applicant declaration

☒ By making this development application, I declare that all information in this development application is true and correct

☒ Where an email address is provided in Part 1 of this form, I consent to receive future electronic communications from the assessment manager and any referral agency for the development application where written information is required or permitted pursuant to sections 11 and 12 of the *Electronic Transactions Act 2001*

Note: It is unlawful to intentionally provide false or misleading information.

Privacy – Personal information collected in this form will be used by the assessment manager and/or chosen assessment manager, any relevant referral agency and/or building certifier (including any professional advisers which may be engaged by those entities) while processing, assessing and deciding the development application. All information relating to this development application may be available for inspection and purchase, and/or published on the assessment manager's and/or referral agency's website.

Personal information will not be disclosed for a purpose unrelated to the *Planning Act 2016*, *Planning Regulation 2017* and the DA Rules except where:

- such disclosure is in accordance with the provisions about public access to documents contained in the *Planning Act 2016* and the *Planning Regulation 2017*, and the access rules made under the *Planning Act 2016* and *Planning Regulation 2017*; or
- required by other legislation (including the *Right to Information Act 2009*); or
- otherwise required by law.

This information may be stored in relevant databases. The information collected will be retained as required by the *Public Records Act 2002*.

PART 9 – FOR COMPLETION OF THE ASSESSMENT MANAGER – FOR OFFICE USE ONLY

Date received: Reference number(s):

Notification of engagement of alternative assessment manager	
Prescribed assessment manager	
Name of chosen assessment manager	
Date chosen assessment manager engaged	
Contact number of chosen assessment manager	
Relevant licence number(s) of chosen assessment manager	

QLeave notification and payment			
<i>Note: For completion by assessment manager if applicable</i>			
Description of the work			
QLeave project number			
Amount paid (\$)		Date paid (dd/mm/yy)	
Date receipted form sighted by assessment manager			
Name of officer who sighted the form			

To Douglas Shire Council

This is consent for Ryan Fudala from Success By Design Architectural Building
Designers to submit plans and applications

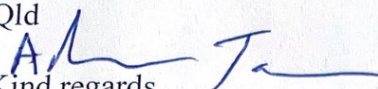
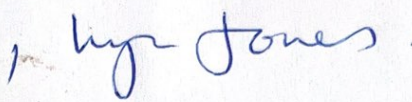
On our behalf in regard to our Property at 3 Sonata Close Port Douglas 4877

Qld

Kind regards

Adrian and Lyn Jones

Phone and other details here

ADRIAN & LYNN JONES

3 SONATA CLOSE, PORT DOUGLAS



GENERAL NOTES -

GENERAL

- ALL WORKS SHALL BE IN STRICT ACCORDANCE WITH THE NATIONAL CONSTRUCTION CODE OF AUSTRALIA 2016 AND AMENDMENTS, AS1984.3-2010 RESIDENTIAL TIMBER FRAMED CONSTRUCTION PART 3 AND AMENDMENTS.
- ALL RELEVANT STANDARDS, LOCAL AUTHORITY BY LAWS AND REGULATIONS AND WORKPLACE HEALTH & SAFETY REGULATIONS, ACCREDITED BUILDING PRODUCTS REGISTER AND MANUFACTURERS CURRENT WRITTEN SPECIFICATIONS AND RECOMMENDATIONS. BUILDER TO VERIFY ALL DIMENSIONS AND LEVELS ON SITE BEFORE COMMENCING ANY SITE WORKS OR WORKSHOP DRAWINGS.
- DO NOT SCALE DRAWINGS - TAKE FIGURED DIMENSIONS.
- SUBSTITUTION OF ANY STRUCTURAL MEMBERS, AND OR ANY WALL VOID VARIATION TO ANY PART OF THE DESIGN & ANY RESPONSIBILITIES OF THE BUILDING DESIGNER FOR THE STRUCTURAL INTEGRITY AND PERFORMANCE OF THE BUILDING.

SITE PREPARATION:

- STRIP BUILDING SITE OF ALL TOPSOIL AND ORGANICS.
- BUILDING PLATFORM AND PAVEMENT SUPPORT AREAS SHOULD BE UNIFORMLY COMPACTED BY HEAVY SURFACE ROLLING TO A MINIMUM DRY DENSITY RATIO OF 95% SRD. SOFT SPOTS ENCOUNTERED DURING COMPACTION TO BE TREATED BY TYING, DRYING AND RE-COMPACTION.
- THE USE OF VIBRATORY ROLLERS FOR EARTHWORKS COMPACTION MAY CAUSE SIGNIFICANT GROUND VIBRATION AND CAREFUL SITE CONTROL OR THE USE HEAVY STATIC COMPACTION PLANT WILL BE REQUIRED TO AVOID DAMAGE TO ADJOINING MASONRY BUILDINGS.
- THE USE OF VIBRATORY ROLLERS FOR EARTHWORKS COMPACTION MAY CAUSE SIGNIFICANT GROUND VIBRATION AND CAREFUL SITE CONTROL OR THE USE HEAVY STATIC COMPACTION PLANT WILL BE REQUIRED TO AVOID DAMAGE TO ADJOINING MASONRY BUILDINGS.
- ALL FILL MATERIAL TO BE OF LOW PLASTICITY (P<15) GRANULAR SELECT FILL, PLACED IN LAYERS NOT MORE THAN 200mm COMPACTED THICKNESS, AND UNIFORMLY COMPACTED TO A MINIMUM DRY DENSITY RATION OF 95% SRD.
- BUILDER TO SURVEY SITE PRIOR TO COMMENCEMENT OF EARTHWORKS AND CONSTRUCT BUILDING PLATFORM TO A LEVEL SUCH THAT ALL SURFACE WATER IS DIRECTED AWAY FROM THE BUILDING TO A SATISFACTORY DRAINAGE OUTLET.
- BUILDER SHALL ENSURE THAT SUITABLE AND APPROPRIATE VEHICULAR ACCESS IS PROVIDED TO THE BUILDING.
- BUILDER SHALL ENSURE THAT SUITABLE SOIL EROSION BARRIERS ARE INSTALLED COMPLYING WITH EPA AND LOCAL AUTHORITY REQUIREMENTS.
- REFER TO SOIL TEST CLASSIFICATION CARRIED OUT BY DIRT PROFESSIONALS- REPORT No. 1672561 DATE: 23/01/2021
- SITE TO BE RE-TESTED AFTER ROLLER COMPACTION FOLLOWED BY ENGINEER CONSULTATION AND POSSIBLE ENGINEERING AMENDMENTS DEPENDANT

SERVICE LOADS:

- THE STRUCTURAL WORK SHOWN ON THESE DRAWINGS HAS BEEN DESIGNED FOR THE FOLLOWING LIVE LOADS UNLESS NOTED OTHERWISE: 0.25 kPa ROOF, 1.50 kPa INTERNAL SUSPENDED FLOORS 3.00 kPa EXTERNAL SUSPENDED FLOORS 1.50 kPa GROUND FLOOR LIVE LOADS TO AS 1170 PART 1 REGION C. DESIGN GUST WIND SPEED 50m/s PERMISSIBLE, 61m ULTIMATE LIMIT STATE.

FOOTINGS & SLABS :

- FOOTINGS HAVE BEEN DESIGNED FOR A MINIMUM ALLOWABLE BEARING PRESSURE OF 100kPa & CLASS 'P' SITE CLASSIFICATION ACCORDING TO A.S. 2870
- BUILDER TO VERIFY SITE CONDITIONS PRIOR TO CONSTRUCTION
- NATURAL FOUNDATIONS TO BE GRUBBED OUT & FREE OF ORGANIC MATTER & DEBRIS & COMPACTED TO A MIN. 85% SRD AT -5% TO -25% OF OPTIMUM MOISTURE CONTENT OR NOT LESS THAN 70% SRD FOR COHESIONLESS SOILS.
- FILL TO SLAB & A FOUNDATIONS SHALL BE APPROVED NON-PLASTIC MATERIAL COMPACTED IN MIN 150mm LAYERS TO 95% SRD AT -5% TO -25% OF THE OPTIMUM MOISTURE CONTENT OR NOT LESS THAN 70% SRD FOR COHESIONLESS SOILS.
- FOOTING TRENCHES SHALL BE CLEAN & DRY AT THE TIME OF CASTING WITH ANY SOFTENED MATERIAL REMOVED BASE OF FOOTING TO BE FOUND ON FIRM NATURAL GROUND WITH MINIMUM SAFE BEARING CAPACITY OF 100kPa. REMOVE GRASS & TOPSOIL, CONTAINING ROOTS FROM SLAB SITE PROVIDE COMPACTED SAND BEDDING UNDER SLAB.
- PROVIDE 0.2mm POLYTHENE MOISTURE BARRIER UNDER SLAB & FOOTINGS
- CONCRETE TO SLAB & FOOTINGS TO BE N20, 80mm SLUMP, 20mm AGGREGATE
- VIBRATE ALL CONCRETE, CURE SLAB 7 DAYS MINIMUM.
- CONCRETE COVER TO BE MAINTAINED BY THE USE OF APPROVED BAR CHAIRS SPACED AT APPROX 750mm CRS.
- CONDUITS & PIPES SHALL NOT BE PLACED WITHIN COVER CONCRETE
- LAP SLAB MESH 2 CROSSWIRES MINIMUM LAPS UNLESS OTHERWISE NOTED:- N12 - 100mm, N16 - 80mm, N20 - 100mm, N24 - 120mm, N28 - 140mm, REINFORCEMENT COVER: FOOTINGS - 50mm BOTTOM, SLABS - 40mm TOP / 50mm BOTTOM
- CAST-IN ITEMS SHALL BE HOT DIPPED GALVANIZED
- FOOTINGS SHALL NOT BE LOCATED CLOSER TO THE NEAREST EDGE OF A STORMWATER/SEWER TRENCH THAN THE DEPTH OF THE TRENCH.
- SITE AREA TO BE GRADED TO READILY REMOVE SURFACE WATER & PREVENT PONDING ADJACENT TO FOUNDATIONS & DRIVEWAY.
- EXECUTION & CONTROL TESTING OF EARTHWORKS & ASSOCIATED SITE PREPARATION WORKS SHALL COMPLY WITH A.S. 3708

CONCRETE STRENGTH:

- ALL CONCRETE WORK SHALL BE IN ACCORDANCE WITH AS3600.
- N20 GRADE CONCRETE TO ALL FOOTINGS.
- REFER SLAB PLANS FOR FLOOR SLAB CONCRETE GRADES

CONCRETE

- CONCRETE GENERALLY IN ACCORDANCE WITH AS 3600
- CONCRETE SPECIFICATION UNLESS NOTED OTHERWISE:
- | ELEMENT | CLASS & GRADE | MAX. AGG. | MAX. SLUMP |
|----------------|---------------|-----------|------------|
| GROUND SLAB | N20 | 20mm | 100mm |
| SUSPENDED SLAB | N40 | 20mm | 100mm |
| ROOF SLAB | N40 | 20mm | 100mm |
| FOOTINGS | N25 | 20mm | 100mm |
| CORE FILL | S20 | 10mm | 230mm |

REINFORCEMENT NOTATION:

- 'N' DENOTES GRADE D500N HOT ROLLED REBAR TO AS4671.
- 'S' DENOTES GRADE D250N HOT ROLLED REBAR TO AS4671.
- 'X' DENOTES GRADE R500L COLD DRAWN ROUND WIRE TO AS4671.
- 'W' DENOTES GRADE R500L COLD DRAWN ROUND WIRE TO AS4671.
- 'W' DENOTES GRADE R500L COLD DRAWN RIBBED WIRE TO AS4671.
- 'RL', 'SL', 'L', 'LM' DENOTES FRAYED D500 DEFORMED WIRE MESHES TO AS4671.
- ADDITIVES SHALL NOT BE USED WITHOUT THE SUPERINTENDENTS APPROVAL.
- MECHANICALLY VIBRATE CONCRETE IN THE FORM TO GIVE MAXIMUM COMPACTION WITHOUT SEGREGATION OF THE CONCRETE.
- CURE CONCRETE AS REQUIRED BY SECTION 19 OF AS3600.
- CONCRETE SIZES SHOWN ARE MINIMUM AND DO NOT INCLUDE APPLIED FINISHES.
- DO NOT REDUCE OR HOLE CONCRETE WITHOUT SUPERINTENDENTS APPROVAL.
- DO NOT PLACE CONDUITS, PIPES AND THE LIKE WITHIN THE COVER CONCRETE.
- FORM WORK SHALL GENERALLY COMPLY WITH AS3610
- STRIPPING OF FORM WORK SHALL COMPLY WITH SECTION 19 OF AS3600.

CONCRETE MASONRY NOTES:

- GENERAL WALLS U.N.O
- 190 SERIES CONC MASONRY BLOCKS IN ACCORDANCE WITH AS 3700 & AS 2733.
- MORTAR 1:1:6 (C:L:S) DOTS DENOTE N12 VERTICAL BARS (U.N.O.) AT ENDS, CORNERS, INTERSECTIONS, EACH SIDE OF ALL OPENINGS AND AT CRS NOTED ON PLANS.
- LAP VERTICAL BARS 600mm WITH N12 STARTER BARS COGGED 200mm INTO FOOTING
- PROVIDE ADDITIONAL N12 VERTICAL BAR EACH SIDE OF OPENINGS >2400 WIDE.
- PROVIDE DOUBLE COURSE (U.N.O.) CONTINUOUS BOND BEAM TO TOP OF 190 SERIES WALLS. REINF. WITH 1-N12 BAR EACH COURSE - LAP 800mm MIN.
- ALL EXTERNAL JOINTS TO BE 'FLUSHED' LEFT READY FOR RENDERING.
- ALL CMB WALLS THAT ARE TO BE CONCRETE FILLED ARE TO BE WATER HOSED DURING THE WALL CONSTRUCTION TO REMOVE MORTAR DAGS IN THE MASONRY CORES.

NOT FOR CONSTRUCTION GUIDE ONLY

WALL CONSTRUCTION - FRAMING:

- EXTERNAL STUDWORK WALLS / LOAD BEARING WALLS :**
- MGP12 (H2)
- 90 x 35 STUDS @ MAX. 450mm CRS.
- 90 x 45 BOTTOM PLATES.
- 2 / 90 x 35 TOP PLATES.
- 90 x 35 NOGS @ 1500 CRS GENERALLY
- N12 W/6 TS DOWN ROD POSITIONS AS SHOWN ON PLAN.
- LINTELS AS PER SCHEDULE.
- 2 STUDS BESIDE OPENINGS UP TO 1800mm
- 3 STUDS BESIDE OPENINGS UP TO 3600mm
- 4 STUDS BESIDE OPENINGS UP TO 5000mm

INTERNAL STUDWORK WALLS LOAD BEARING

90MM THICK WALLS :

- MGP12 (H2)
- 90 x 35 STUDS @ MAX. 450MM CRS.
- 90 x 35 TOP & BOTTOM PLATES - 1 ROW NOGGS.
- 90 x 45 TOP & BOTTOM PLATES TO BRACE WALLS.

BRACING WALLS :

- 4mm STRUCTURAL PLY FIXED WITH 2.8 x 30 GAL FLATHEAD NAILS @ : 50mm CRS. TOP AND BOTTOM PLATE.
- 50mm CRS. TO VERTICAL EDGES.
- 300mm CRS. INTERMEDIATE STUDS.
- ANCHOR ENDS OF WALLS TOP AND BOTTOM.
- USE 6mm VILLABOARD IN LIEU OF PLY TO WET AREAS.
- FIX BRACE WALLS TO ROOF FRAMING WHERE NOT OTHERWISE CONNECTED.

- FIX BOTTOM AND TOP OF WALL TO FLOOR AND ROOF STRUCTURE WITH M-12 BOLTS AT MAX 900 CRS. WHERE WALL IS PARALLEL TO JOISTS OR TRUSSES, PROVIDE 100X50 F14 HWD SOLID NOGGING AT REQUIRED CRS FIXED WITH 2-100mm BATTEN SCREWS EACH END WHERE WALL IS PERPENDICULAR TO TRUSSES FOR WALL WITH 125X75X6 MS ANGLE WITH 1-M12 BOLT THRU TOP PLATES AND 1-M12 THRU TRUSS/PURLIN

- STRAP WALL JOCTIONS WITH 2-30X0 & 30 GAL STRAPS WITH 6-2-90X30 GAL FLAT HEAD NAILS STRAP
- USE 6mm VILLABOARD IN LIEU OF PLY IN WET AREAS
- NON LOAD BEARING 70X35 MGP10

TRUSSES

- PREFABRICATED ROOF TRUSSES DESIGNED BY THE TRUSS MANUFACTURER INCLUDING ALL NECESSARY BRACING AND CONNECTIONS.
- J2 JOINT GROUP FOR HWD TRUSSES.
- J04 JOINT GROUP FOR PINE TRUSSES.

ROOF BRACING

- METAL STRAP BRACING TO TRUSS MANUFACTURERS DESIGN.
- ROOF SHEET & BATTEN FIXINGS
- LYSAGHT SHEETING OVER LYSAGHT BATTENS

- ALL ROOF SHEETING AND BATTEN FIXINGS ARE TO BE IN ACCORDANCE WITH THE MANUFACTURERS SPECIFICATION FOR THE REQUIRED WIND SPEED.
- THE FIXING SYSTEMS FOR THE WHOLE METAL ROOF ASSEMBLY SUPPLIED BY THE MANUFACTURER, ARE TO BE COMPLIANT WITH THE LOW-HIGH-LOW CYCLIC TESTING REQUIREMENTS OF THE BUILDING CODE OF AUSTRALIA (SPECIFICATION B1.2 VOLUME 1 FOR CLASS 2 TO 9 BUILDINGS) OR (SECTION 3.10.1 VOLUME 2 FOR CLASS 1 & 10 BUILDINGS).
- A COMPLIANCE CERTIFICATE SHALL BE REQUESTED FROM THE MANUFACTURER & THE INSTALLER

GENERAL

- LAPS, FLASHINGS AND GENERAL INSTALLATION IN ACCORDANCE WITH THE MANUFACTURERS SPECIFICATION.

WET AREA WALLS:

- ALL WET AREA WALLS AND FLOORS TO BE WATERPROOFED WITH APPROVED MEMBRANES IN ACCORDANCE WITH ASINZS 4688.
- WET AREAS TO BE WATERPROOFED IN ACCORDANCE WITH NCC 2016 PART 3.8.1.2.
- WALL/FLOOR COVERINGS: BUILDER TO CONSULT OWNER FOR FULL EXTENT OF FLOOR COVERING REQUIREMENTS.
- SELECTED WALL TILES TO WET AREAS AND SPLASH-BACKS.
- WET AREAS TO BE WATERPROOFED IN ACCORDANCE WITH NCC 2016 PART 3.8.1.2.
- WALL/FLOOR COVERINGS: BUILDER TO CONSULT OWNER FOR FULL EXTENT OF FLOOR COVERING REQUIREMENTS.
- SELECTED WALL TILES TO WET AREAS AND SPLASH-BACKS.

STEELWORK:

- ALL STEEL WORKS TO BE CARRIED OUT TO AUSTRALIAN STANDARDS & 1100 STEEL STRUCTURES CODE
- STEEL SHALL BE AS 3678 & 3678 GENERALLY GRADE 300 PLUS FOR HOT ROLLED SECTIONS
- AS 1163 GRADE 350 FOR HOLLOW SECTIONS
- BOLTS SHALL BE COMMERCIAL GRADE 4.6 S/NUG TIGHTENED GENERALLY U.N.O.
- BOLTS SHALL BE GALVANIZED OR & OF SUFFICIENT LENGTH TO EXCLUDE THE THREAD FROM THE SHEAR PLANE
- A SUITABLE WASHER SHALL BE USED UNDER ALL NUTS UNLESS OTHERWISE SPECIFIED
- THE FOLLOWING TO APPLY
- BEAM AND BEARER SPLICE TO BE FPBW TO AS 1554.1 CLASS SP
- WELDING 6MM CONTINUOUS FILLET WELD TO FULL PERIMETER AT CONTACT
- CLEATS, BRACKETS, STIFFENERS ETC. TO BE 10mm PLATE U.N.O.
- SPL END PL TO ALL HOLLOW SECTIONS
- BOLT HOLE CLEARANCE TO BE 2mm
- HOLD DOWN BOLT CLEARANCE 2mm
- GROUT OF 2:1 CEMENT/SAND, MORTAR OF DAMP EARTH CONSISTENCY UNDER ALL BASE PLATES
- CHEMSET ANCHORS TO BE RAMSET SPIN CAPSULES OR SERIES 800 OR EQUIVALENT
- ALL STEEL WORK NOT HOT DIPPED GALVANIZED SHALL BE ABRASIVE CLEANED TO CLASS 2 & LEVEL & PAINTED
- PAINTING SHALL CONSIST OF ONE COAT OF APPROVED METAL PRIMER & TWO FINISH COATS
- ALL CAST IN ITEMS TO BE HOT DIPPED GALVANIZED U.N.O

DRAWING TERMS AND NOTATION -

- DRAWING DIMENSIONS ARE TO BE CHECKED AND CONFIRMED ONSITE BY THE BUILDER.
- FURNITURE AND DISPLAY ITEMS WITHIN PLANS AND RENDERS ARE FOR DRAWING PURPOSES AND NOT TO BE INCLUDED IN TENDERING DOCUMENTATION UNLESS NOTED ON PLAN.
- ARTISTIC DRAWING AND RENDERS ARE FOR VISUAL REPRESENTATION ONLY AND MAY NOT BE THE EXACT FINISH, COLOUR, ITEM OR MATERIAL TO BE USED. ALWAYS CONSULT BUILDING SPECIFICATIONS FOR EXACT FINISH, COLOUR, ITEM OR MATERIAL.

LEGEND -

@	ACCORD.	AT
AL	ACCORDING	AL
AS	SELECT ALUCOBOND FINISH	AS
B	AUSTRALIAN STANDARD CODES	B
BFC	BROOM FINISH CONCRETE	BFC
CMB	CONCRETE MASONRY BLOCK	CMB
CNC	CONCRETE	CNC
COS	CONFIRM ON SITE	COS
CPT	SELECT CARPET	CPT
CPD	CURBBOARD	CPD
CS	CENTRES	CS
CS	CAVITY SLIDER	CS
CSK	COUNTERSUNK	CSK
CTK	SELECT CERAMIC TILE	CTK
CPW	COOKTOP	CPW
DIA	CONTINUOUS FILLET WELD	DIA
DIA	DIAMETER	DIA
DKP	DAMP PROOF COURSE	DKP
DECK	DECKING	DECK
DW	DOWN PIPE	DW
D.P	DOWN PIPE	D.P
EA	EQUAL ANGLE	EA
EC	EXPANSION JOINT	EC
FJ	FIRE-CEMENT	FJ
FTL	FINISHED FLOOR LEVEL	FTL
FH	FLAT HEAD NAILS	FH
G	GAUGE (BOLTS, SCREWS)	G
GAL	GALVANIZING	GAL
GB	SELECT GLASS BALUSTRADE	GB
H4	HEAD HEIGHT	H4
HEX	HEXAGONAL HEAD (BOLT)	HEX
HR	SELECT SS HANDRAIL	HR
HT	HEIGHT	HT
HWD	HARDWOOD	HWD
HVS	HOT WATER SYSTEM	HVS
LOSP	LIGHT ORGANIC SOLVENT PRESERVATIVE	LOSP
MM	MANUFACTURER	MM
MANUF.	MANUFACTURER	MANUF.
MAX	MAXIMUM	MAX
MIN	MINIMUM	MIN
MGP	MACHINE GRADED PINE	MGP
MS	MICROWAVE OVEN	MS
NIS	NLD STEEL	NIS
NCH	NICHE	NCH
NCC	NATIONAL CONSTRUCTION CODE	NCC
NSL	NATURAL GROUND LEVEL	NSL
OCF	OFF-FORM CONCRETE FINISH	OCF
OS	OPSCURE GLASS	OS
OHC	OVERHEAD CURBBOARD	OHC
PB	PLASTERBOARD LINING	PB
PC	POLISHED CONCRETE	PC
PF	SELECT 1200H POOL FENCE	PF
PL	PLATE	PL
PVC	POLYVINYL CHLORIDE	PVC
REINF	REINFORCING	REINF
RCH	ROUGHER HEADER H3 TREATED PINE	RCH
RHS	RECTANGULAR HOLLOW SECTION	RHS
SCJ	SAW CUT JOINT	SCJ
SFL	STRUCTURAL FLOOR LEVEL	SFL
SLS	SQUARE HOLLOW SECTION	SLS
SLC	SELECT HMD SHUPLAPPED CLADDING	SLC
SS	STAINLESS STEEL	SS
ST	SELECT TILES	ST
SW	STONE WALL	SW
SPEC	SPECIFICATION	SPEC
SHS	SQUARE HOLLOW SECTION	SHS
TLR	SELECT TIMBER LAMINATE FLOORING	TLR
TC	TOP OF WALL	TC
TOW	TOP OF WALL	TOW
UA	UNEQUAL ANGLE	UA
UNO	UNLESS NOTED OTHERWISE	UNO

[LIT]	LINTEL NUMBER
[D01]	DOOR NUMBER
[W01]	WINDOW NUMBER
[SECTION REFERENCE]	SECTION MARKER
[DETAIL REFERENCE]	DETAIL CALLOUT
[ELEVATION KEY]	ELEVATION KEY
[REDUCED LEVEL]	REDUCED LEVEL
[SLAB SETDOWN]	SLAB SETDOWN
[FALL]	SLAB FALL
[SLAB THICKNESS]	SLAB THICKNESS
[SPOT LEVEL]	SPOT LEVEL

FLOOR AREA	
Area	0 m ²
EXTERNAL COVER AREAS	0 m ²
INTERNAL AREAS	0 m ²

WIND CLASSIFICATION

ULTIMATE & SERVICEABILITY LIMIT STATE DESIGN WIND PRESSURES						
WIND CLASS	DESIGN GUST WIND SPEED (m/s)		DESIGN PRESSURES (kPa)			
	V _{h10}	V _{h5}	GREATER THAN 1.2m FROM CORNERS		UP TO 1.2m FROM CORNERS	
			ULS	SLS	ULS	SLS
C2	61	39	+2.68	+0.88	-4.02	-1.23

PLAN ISSUE:

CONCEPT DESIGN NOT FOR CONSTRUCTION

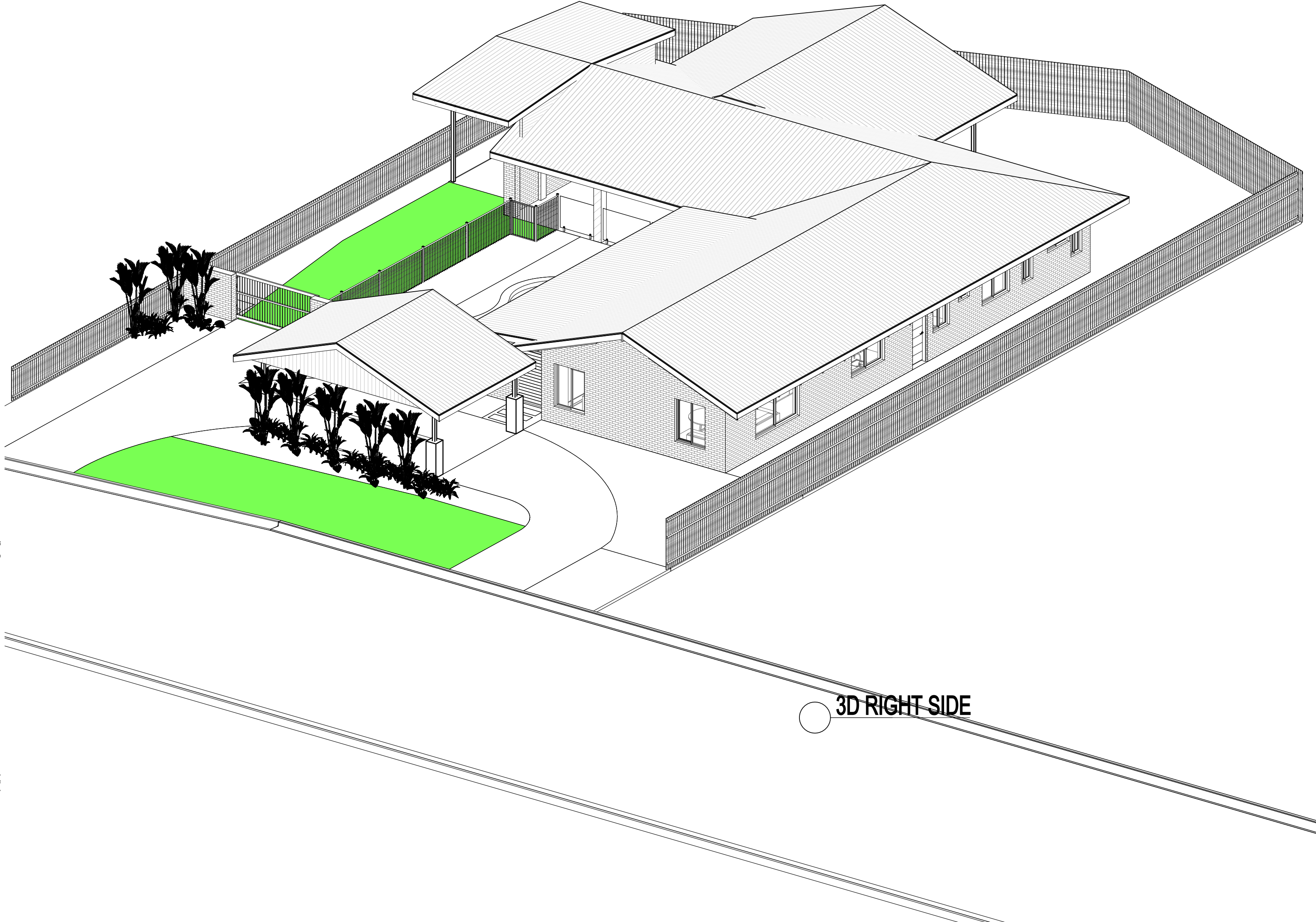
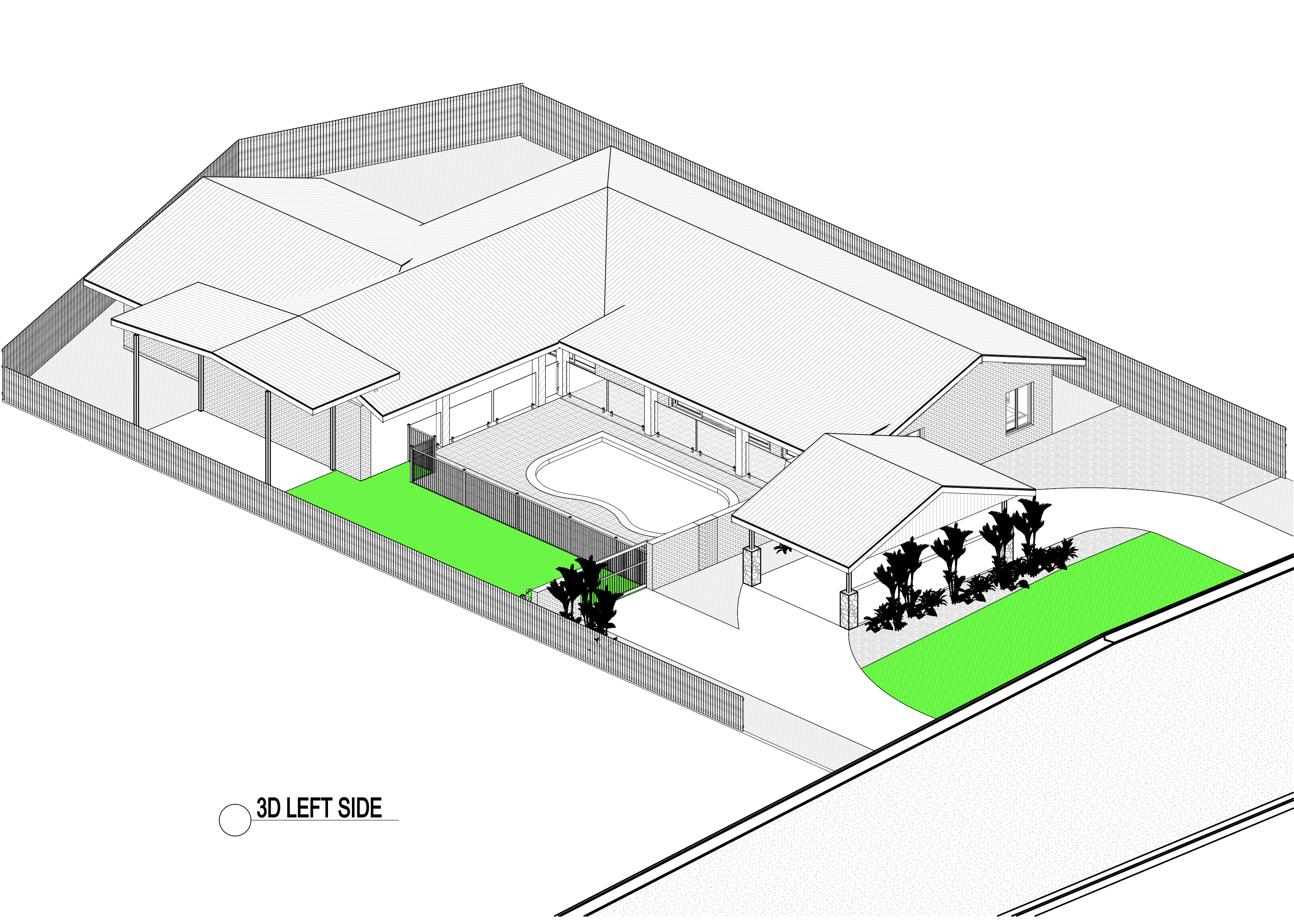
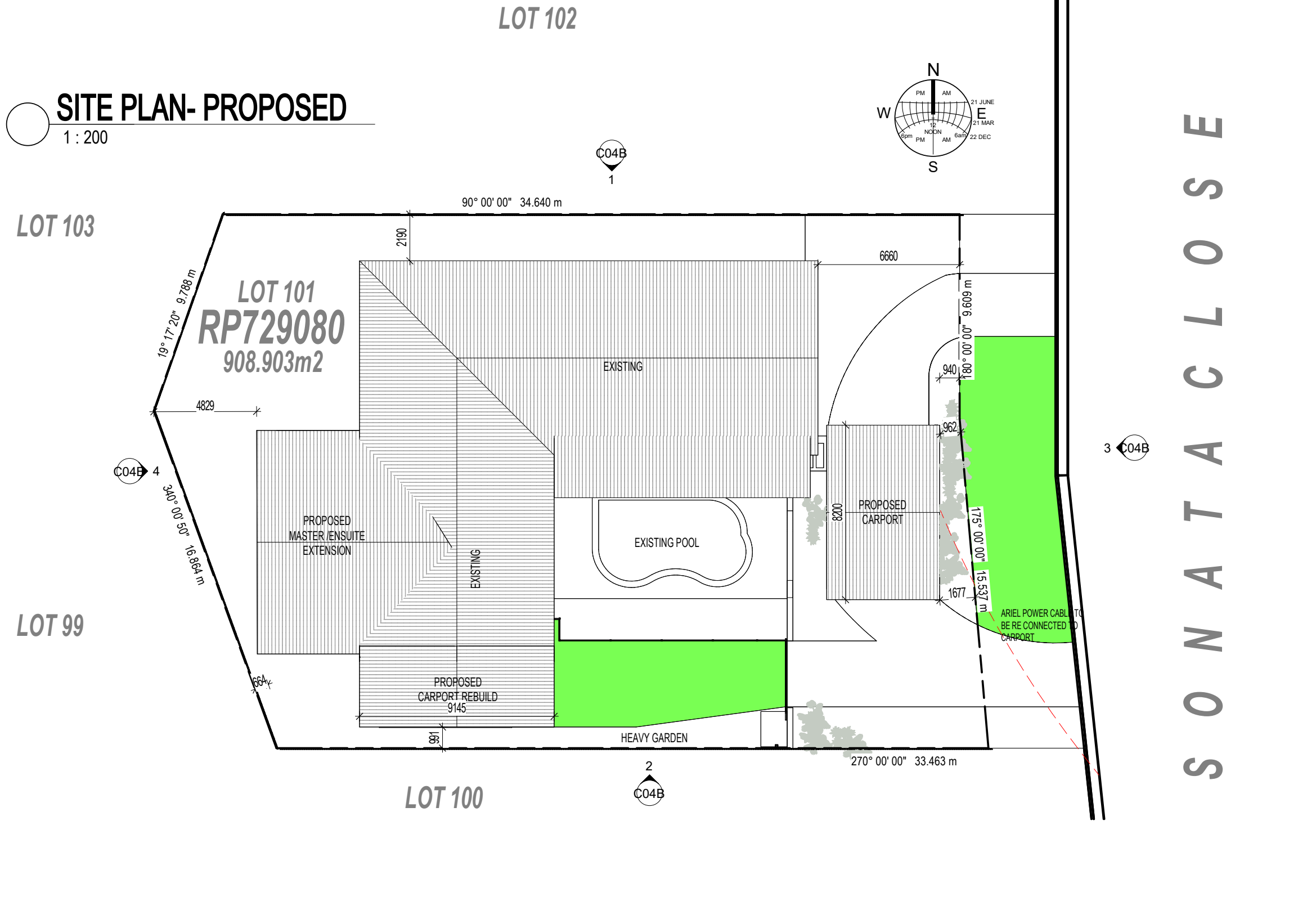
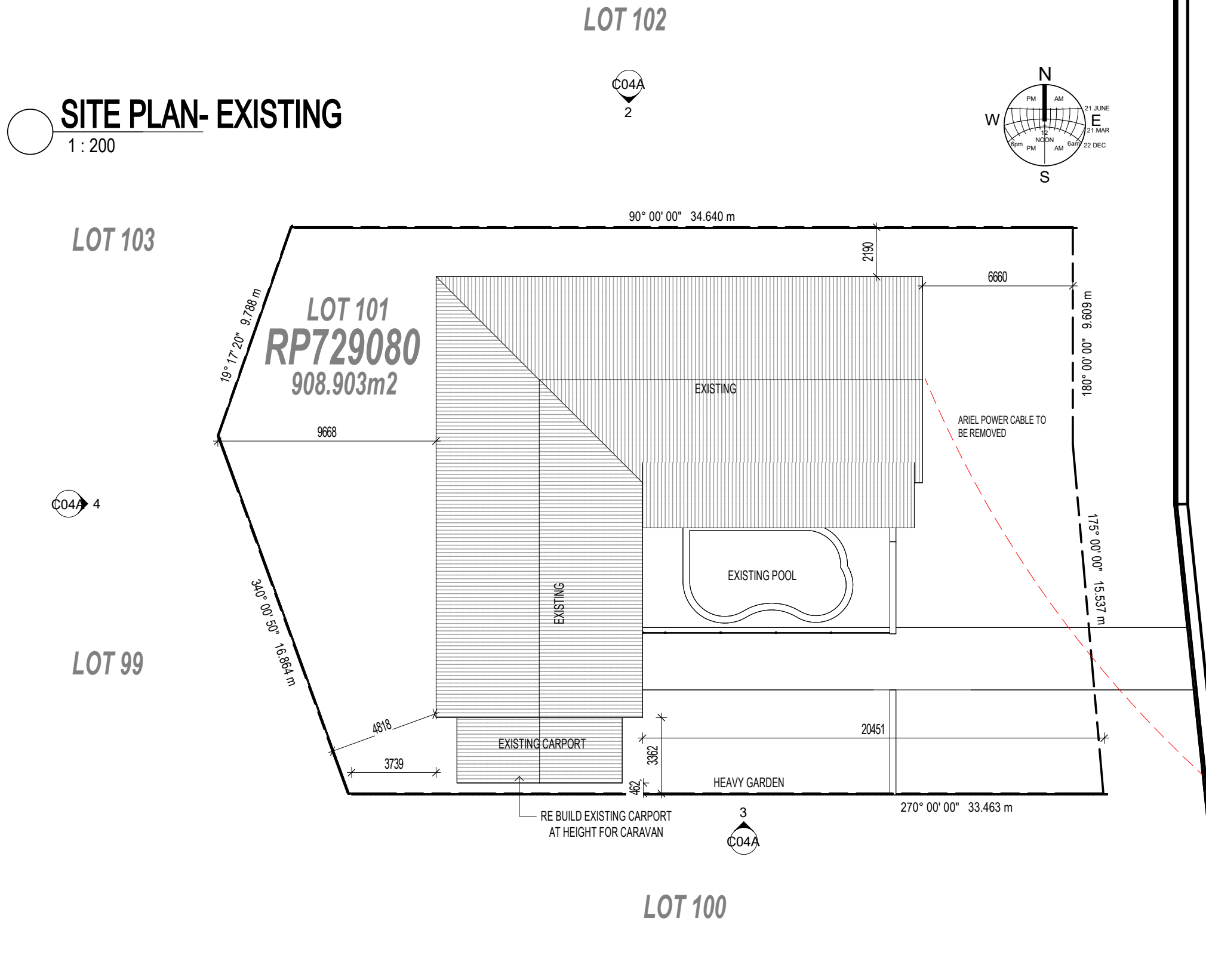
**CERTIFIED AS
STRUCTURALLY ADEQUATE**

KFB Engineers Civil & Structural
138-42 Phase St, Cairns | PO Box 927, Cairns Q 4870
P: 07 40320402 | F: 07 40320992 | E: email@kfbeng.com.au

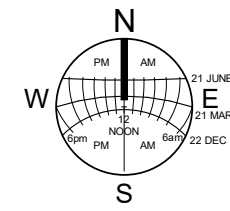
Date: _____ Signed: _____
Job No: _____ RPEQ No: _____

PROJECT: ADRIAN & LYNN JONES
PROJECT ADDRESS:
3 SONATA CLOSE
PORT DOUGLAS
QLD 4877

SHEET	TITLE PAGE, GENERAL NOTES & LEGEND	PROJECT NUMBER	PROJECT CODE
DESIGNER	RYAN FUDALA	DRAWN	RYAN FUDALA
SCALE	AS SHOWN @A1	SHEET	C01
REV			

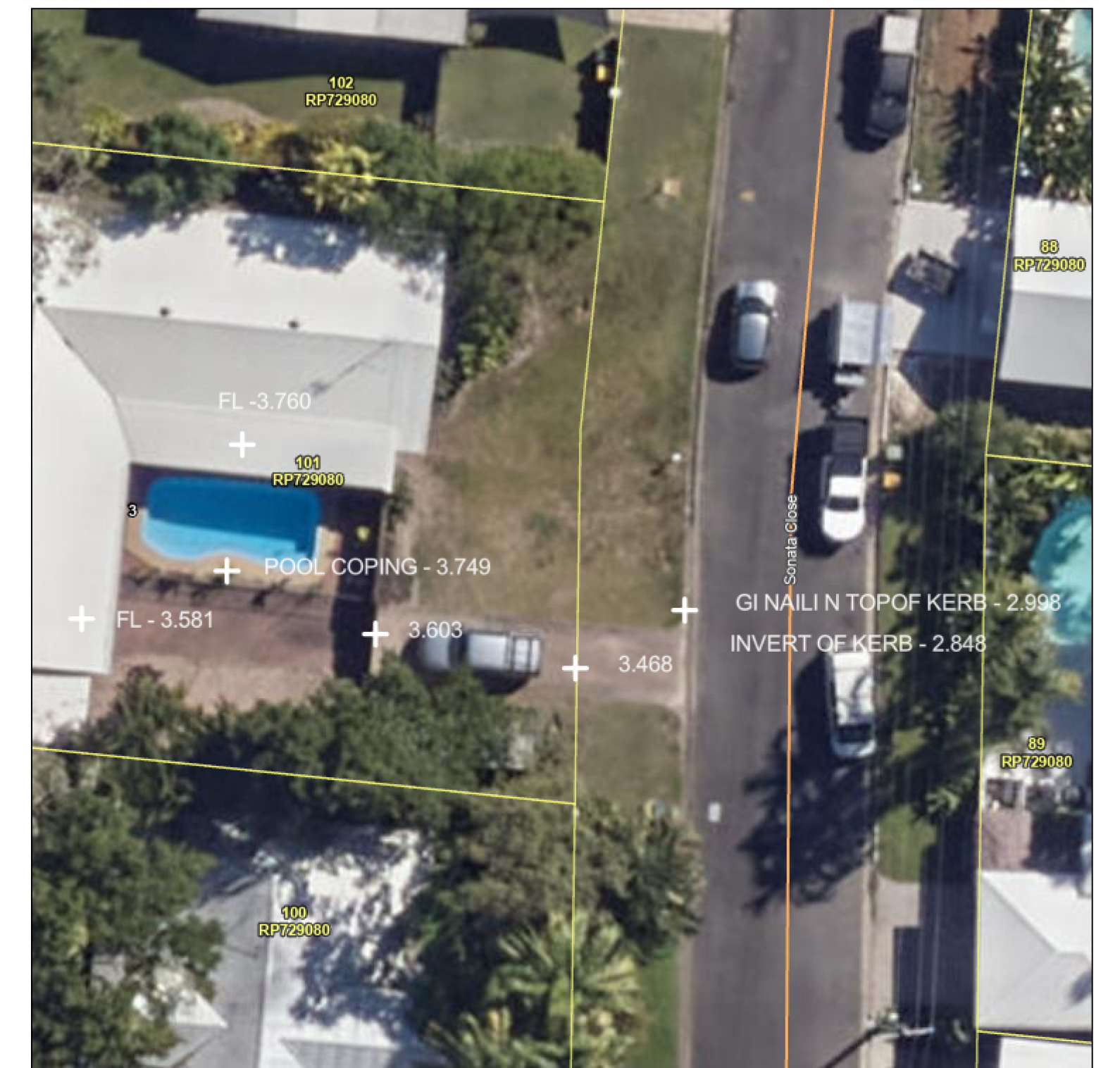


FLOOR PLAN
1 : 100




SONATA CLOSE

16°30'51"S 145°27'57"E



16°30'52"S 145°27'57"E

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 Queensland Globe



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Scale: 1:233

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Date: 4/12/2024

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RYAN FUDALA QBCC LIC NO. 1114352
licensed building designer

CONCEPT DESIGN
NOT FOR CONSTRUCTION

**CERTIFIED AS
STRUCTURALLY ADEQUATE**



Date: _____ Signed: _____
Job No: _____ RPEQ No: _____

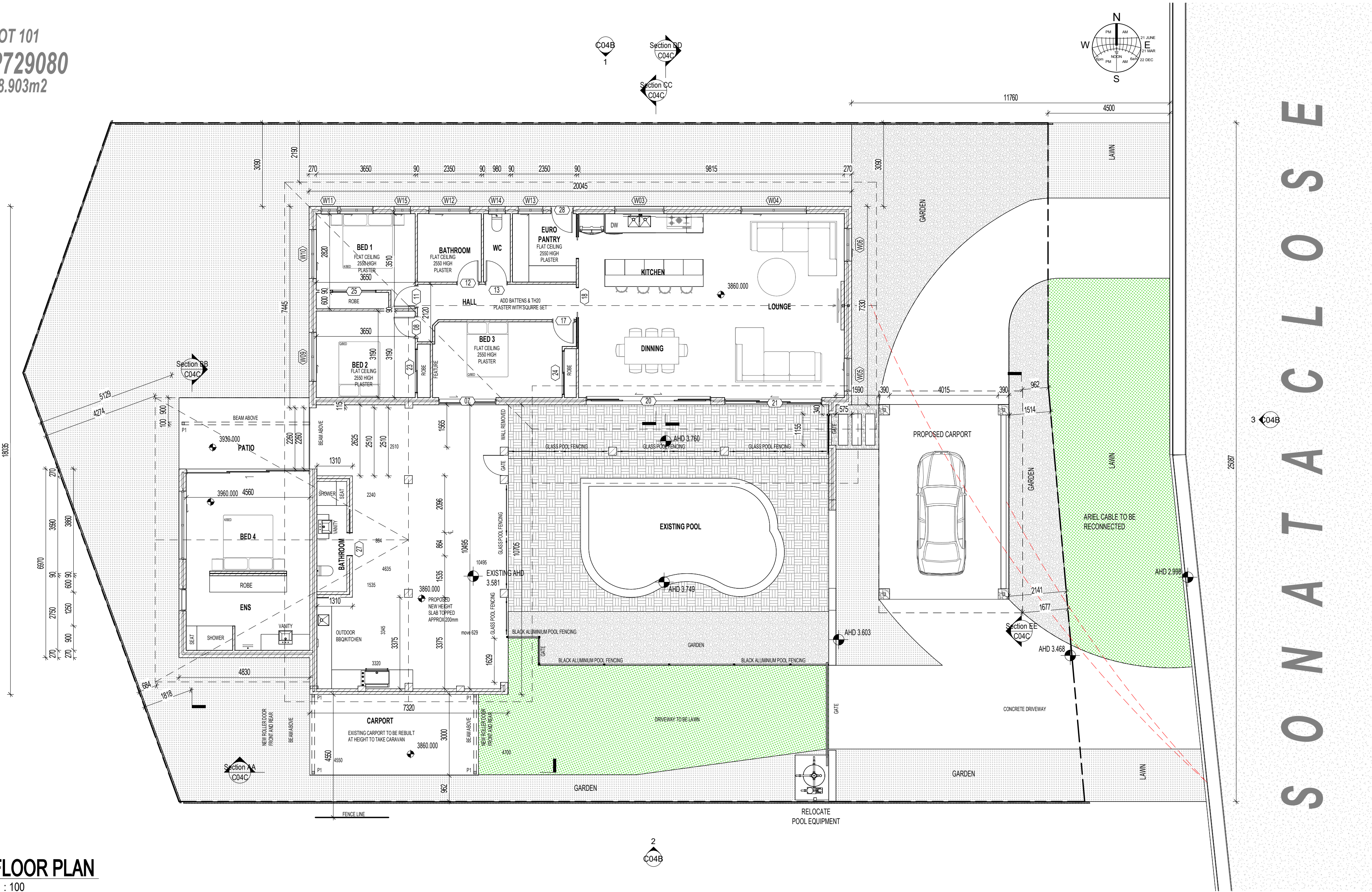
PROJECT ADDRESS:
3 SONATA CLOSE
PORT DOUGLAS
QLD 4877

PROJECT NUMBER

SCALE AS SHOWN @ A1

SHEET C03A REV

LOT 101
RP729080
908.903m2



FLOOR PLAN

1:100

SCHEDULE - WINDOWS

MARK	WINDOW NAME	HEIGHT	WIDTH	FRAME MATERIAL	GLASS TYPE	SCREENS	LOCK	FRAME COLOUR	WINDOW BRAND
03	SLIDING GLASS WINDOW FX	1210	1810	ALUMINUM MATT BLACK					
04	SLIDING GLASS WINDOW FX	1510	2410	ALUMINUM MATT BLACK					
05	SLIDING GLASS WINDOW FX	1510	1210	ALUMINUM MATT BLACK					
06	SLIDING GLASS WINDOW FX	1510	1210	ALUMINUM MATT BLACK					
07	SLIDING GLASS WINDOW FX	1510	1010	ALUMINUM MATT BLACK					
09	SLIDING GLASS WINDOW FX	1510	1810	ALUMINUM MATT BLACK					
10	SLIDING GLASS WINDOW FX	1510	1810	ALUMINUM MATT BLACK					
11	SLIDING GLASS WINDOW FX	1210	610	ALUMINUM MATT BLACK					
12	SLIDING GLASS WINDOW FX	1210	1510	ALUMINUM MATT BLACK					
13	SLIDING GLASS WINDOW FX	1210	910	ALUMINUM MATT BLACK					
14	SLIDING GLASS WINDOW FX	610	610	ALUMINUM MATT BLACK					
15	SLIDING GLASS WINDOW FX	1210	610	ALUMINUM MATT BLACK					
16	SLIDING GLASS WINDOW FX	610	1210	ALUMINUM MATT BLACK					
17	SLIDING GLASS WINDOW FX	1510	2510	ALUMINUM MATT BLACK					
18	SINGLE LOUVER	2110	810	ALUMINUM MATT BLACK					

SCHEDULE - DOORS

MARK	NAME	WIDTH	HEIGHT	HINGES	LOCK	SCREENS	COLOUR	GLAZING TYPE	USE	VILLA NUMBER	BRAND	MODEL
07	SLIDING GLASS DOOR FX ALUMINUM	2110	2095									
08	INTERIOR SINGLE SWING DOOR	820	2040									
10	INTERIOR SINGLE SWING DOOR	820	2040									
11	INTERIOR SINGLE SWING DOOR	820	2040									
12	INTERIOR SINGLE SWING DOOR	820	2040									
13	INTERIOR SINGLE SWING DOOR	820	2040									
14	INTERIOR SINGLE SWING DOOR	820	2040									
15	CAVITY SLIDER	810	2340									
17	INTERIOR SINGLE SWING DOOR	820	2040									
18	CAVITY SLIDER	1120	2040									
20	SLIDING GLASS DOOR FX OF ALUMINUM	3010	2410									
21	SLIDING GLASS DOOR FX ALUMINUM	3684	2095									
22	SLIDING GLASS DOOR FX ALUMINUM	2110	2095									
23	DOUBLE ROBE DOOR	1610	2040									
24	DOUBLE ROBE DOOR	1610	2040									
25	DOUBLE ROBE DOOR	1610	2040									
26	DOUBLE ROBE DOOR	1610	2040									
27	CAVITY SLIDER	854	2340									
28	INTERIOR SINGLE SWING DOOR	820	2040									
29	SLIDING GLASS DOOR FX ALUMINUM	3684	2095									
30	CAVITY SLIDER	810	2340									
31	INTERIOR SINGLE SWING DOOR	820	2040									

STRUCTURAL FRAMING LEGEND:

P1	SHS DURAGAL 100x4mm
P2	SHS 78X78X3.2mm EXISTING POST
B1	RHS DURAGAL 150X100X4mm
B2	PFC 200
B3	UC 300
B4	DURAGAL RHS 200X100X6
B5	DURAGAL RHS 150X100X6
PL1	C PURLIN 10019, NO BRIDGING
PL2	C PURLIN 10012, NO BRIDGING
TR1	TYPICAL TIMBER TRUSS (ENGINEERED BY TRUSS SUPPLIER)
BR	STRAP BRACING 32X1mm GAL

CONCRETE SLAB NOTES -

TYPICAL LOWER FLOOR SLAB :
N32, SL72 MESH 30 TO MIN.
REFER FLOOR PLAN FOR FINISHES

TYPICAL DRIVEWAY SLAB :
N25, SL72 MESH CENTRALLY.
CROSSING AS PER FNQROC GUIDELINES

TYPICAL UNDER SLAB FILL :
SAND OR CRUSHER DUST COMPACTED IN
MAX. 200 LAYERS TO NO APPARENT
MOVEMENT UNO

SCJ :
25MM SAW CUT WITHIN 6 HOURS OF POUR,
BREAK EVERY 2ND TOP MESH BAR UNDER
CUT.

DOVELS: R12 STEEL DOVELS 600 LONG @ 450
CNS CHEM SET 150 INTO EXISTING SLAB

LEGEND - TIMBER FRAMING & LINTELS

WALL CONSTRUCTION - FRAMING:

EXTERNAL STUDWORK WALLS / LOAD BEARING WALLS :

- MGP12 (H2)
- 90 x 35 STUDS @ MAX. 450mm CRS
- 90 x 45 BOTTOM PLATES
- 2 / 90 x 35 TOP PLATES
- 90 x 35 NOGS @ 1300 CRS GENERALLY
- M12 / M16 TIE-DOWN ROD POSITIONS AS SHOWN ON PLAN
- LINTELS AS PER SCHEDULE
- 2 STUDS BESIDE OPENINGS UP TO 1800mm
- 3 STUDS BESIDE OPENINGS UP TO 3600mm
- 4 STUDS BESIDE OPENINGS UP TO 5000mm

INTERNAL STUDWORK WALLS LOAD BEARING

90MM THICK WALLS :

- MGP12 (H2)
- 90 x 35 STUDS @ MAX. 450mm CRS
- 90 x 35 TOP & BOTTOM PLATES - 1 ROW NOGGIN.
- 90 x 45 TOP & BOTTOM PLATES TO BRACE WALLS

BRACING WALLS :

- 4mm STRUCTURAL PLY FIXED WITH 2.8 x 30 GAL FLATHEAD NAILS @ :
50mm CRS. TOP AND BOTTOM PLATE.
150mm CRS. TO VERTICAL EDGES.
300mm CRS. INTERMEDIATE STUDS
- ANCHOR ENDS OF WALLS TOP AND BOTTOM.
- USE 6mm VILLABOARD IN LIEU OF PLY TO WET AREAS.
- FIX BRACE WALLS TO ROOF FRAMING WHERE NOT OTHERWISE CONNECTED.

- FIX BOTTOM AND TOP OF WALL TO FLOOR AND ROOF STRUCTURE
WITH M12 BOLTS AT MAX 800 CNS. WHERE WALL IS PARALLEL
TO JOISTS OR TRUSSES, PROVIDE 100X50 F14 HWD SOLID NOGGING
AT REQUIRED CNS FIXED WITH 2-100mm BATTEN SCREWS EACH
END. WHERE WALL IS PERPENDICULAR TO TRUSSES FIX WALL WITH
125X75X6 MS ANGLE WITH 1-M12 BOLT THRU TOP PLATES AND 1-M12 THRU
TRUSS/PURLIN

- STRAP WALL JOINTS WITH 2-30X18 GAL STRAPS WITH
6-2.8X30 GAL FLAT HEAD NAILS EACH STRAP

- USE 6mm VILLABOARD IN LIEU OF PLY IN WET AREAS
- NON LOAD BEARING 70x35 MGP10

NON-LOADBEARING DOOR / ROBE HEAD

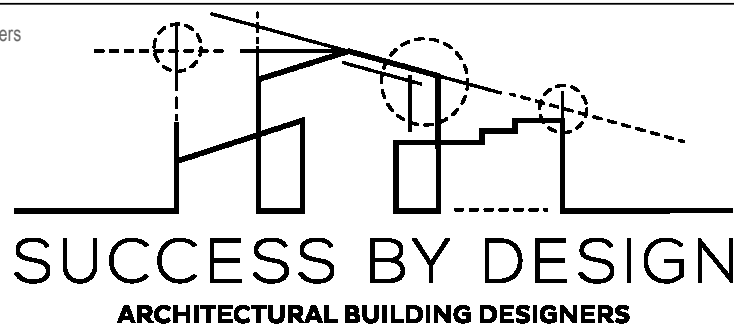
4MM F27 STRUCTURAL PLY BRACING - REFER NOTES

(B1)

DRH

- INTERNAL WALL FRAMES TO BE 70 X 35 MGP10 UNO
- INTERNAL FINISH TO BE 10MM PB GENERALLY (6MM VILLABOARD TO WET AREAS & LAUNDRY) UNO
- ALL EXTERNAL FINISHES INSTALLED AS PER THE MANUFACTURERS DETAILS
- TYP SKIRTING : 68 X 19 SQUARE DRESSED PINE, PAINT FINISH
- TYP INTERNAL ARCHITRAVES : 42 X 19 SQUARE DRESSED PINE, PAINT FINISH
- INTERNAL / EXTERNAL CORNICE : 31 X 11 SQUARE DRESSED PINE, PAINT FINISH (H3 EXTERNAL)
- ALL EXTERNAL WALLS TO BE INSULATED WITH R2.0 CAVITY BATTS UP TO SOFFIT LEVEL

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PLAN ISSUE:

CONCEPT DESIGN
NOT FOR CONSTRUCTION

CERTIFIED AS
STRUCTURALLY ADEQUATE

KFB Engineers Civil & Structural
1/38-42 Phase St, Cairns | PO Box 927, Cairns Q 4870
P: 07 40320402 | F: 07 40320902 | E: email@kfbeng.com.au

Date: _____ Signed: _____
Job No: _____ RPEQ No: _____

13/01/2025 11:10:59

PROJECT: ADRIAN & LYNN JONES

PROJECT ADDRESS:

3 SONATA CLOSE
PORT DOUGLAS
QLD 4877

SHEET FLOOR PLANS PROPOSED

PROJECT NUMBER

PROJECT

CODE

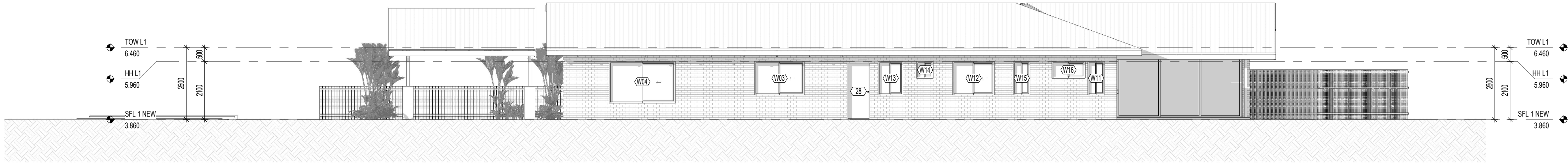
DESIGNER RYAN FUDALA

DRAWN RYAN FUDALA

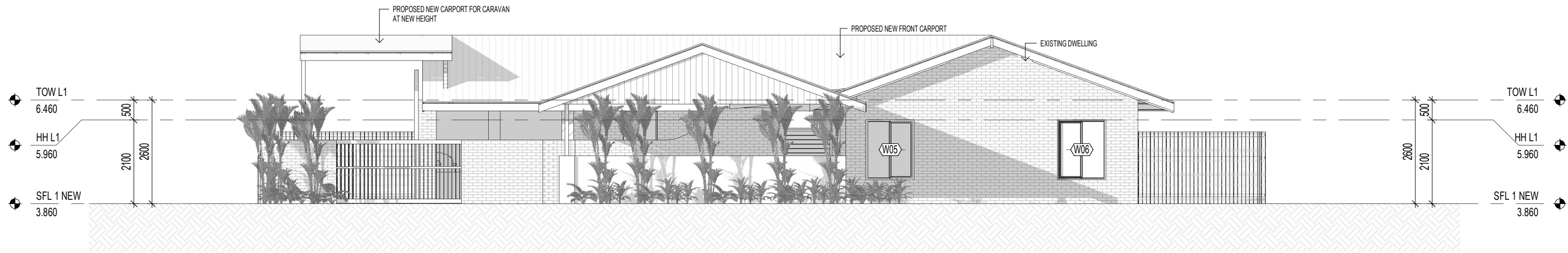
SCALE AS SHOWN @ A1

SHEET C03B REV

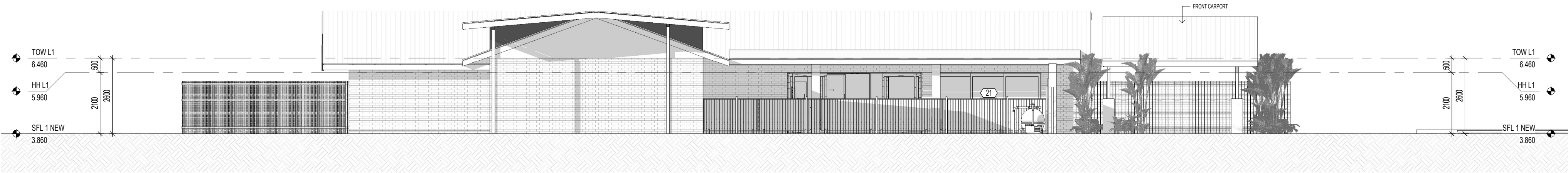




NORTH
1 : 100



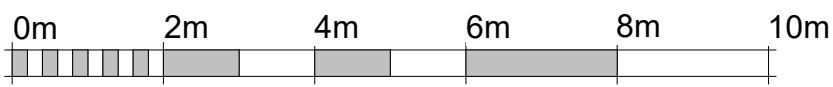
EAST
1 : 100



SOUTH
1 : 100

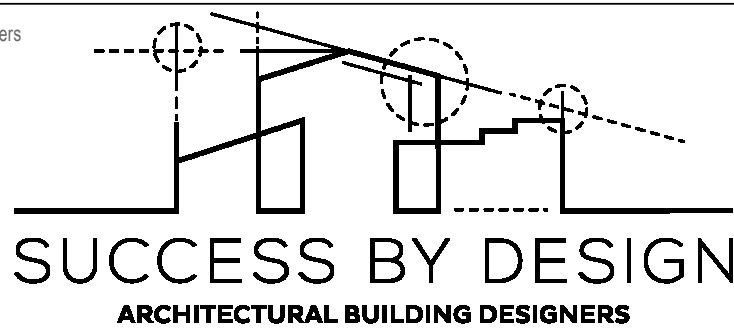


WEST
1 : 100



1:100

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PLAN ISSUE:

**CONCEPT DESIGN
NOT FOR CONSTRUCTION**

**CERTIFIED AS
STRUCTURALLY ADEQUATE**



Date: _____ Signed: _____
Job No: _____ RPEQ No: _____

13/01/2025 11:11:20

PROJECT: ADRIAN & LYNN JONES

PROJECT ADDRESS:
3 SONATA CLOSE
PORT DOUGLAS
QLD 4877

SHEET **ELEVATIONS PROPOSED**

PROJECT NUMBER **PROJECT CODE**

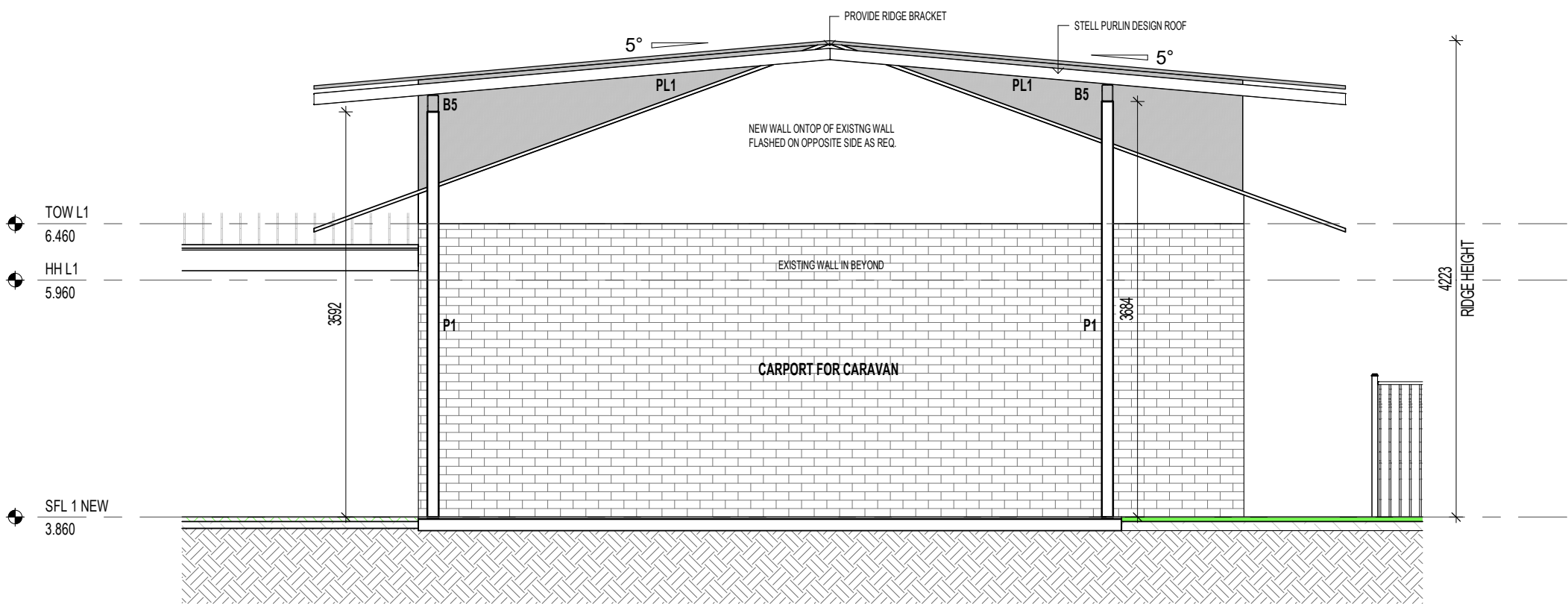
DESIGNER RYAN FUDALA

DRAWN RYAN FUDALA

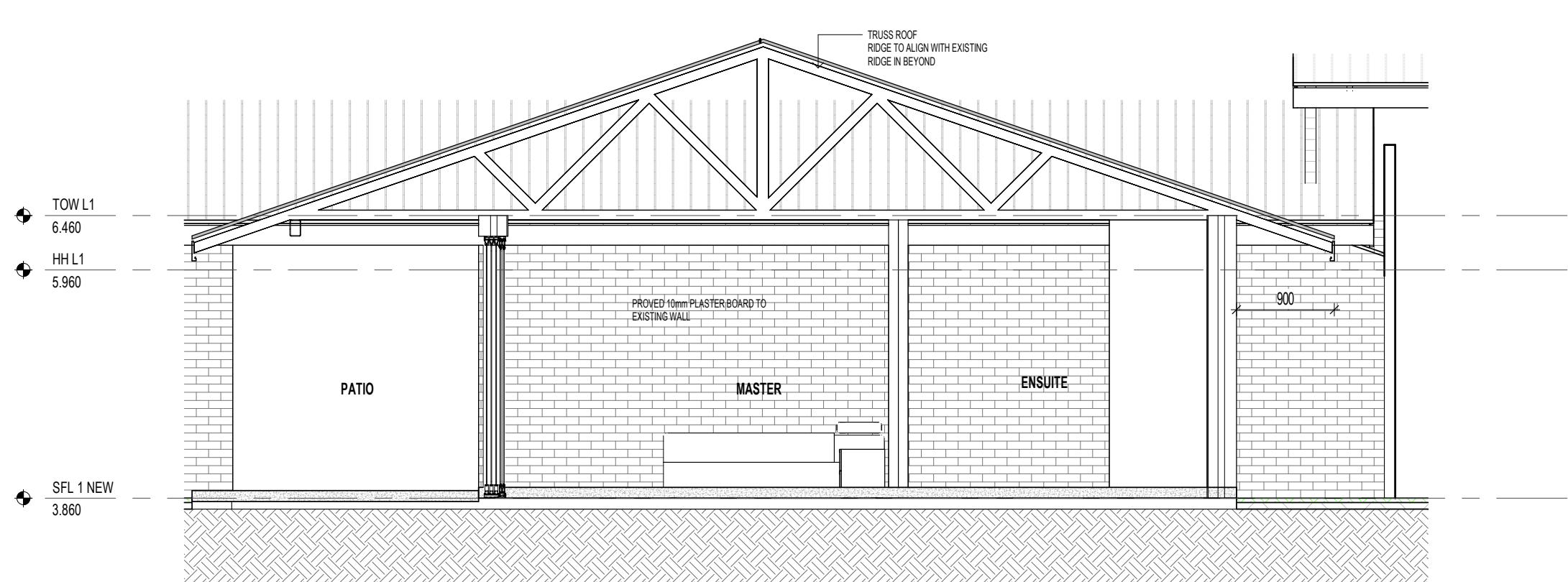
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SHEET **C04B** REV

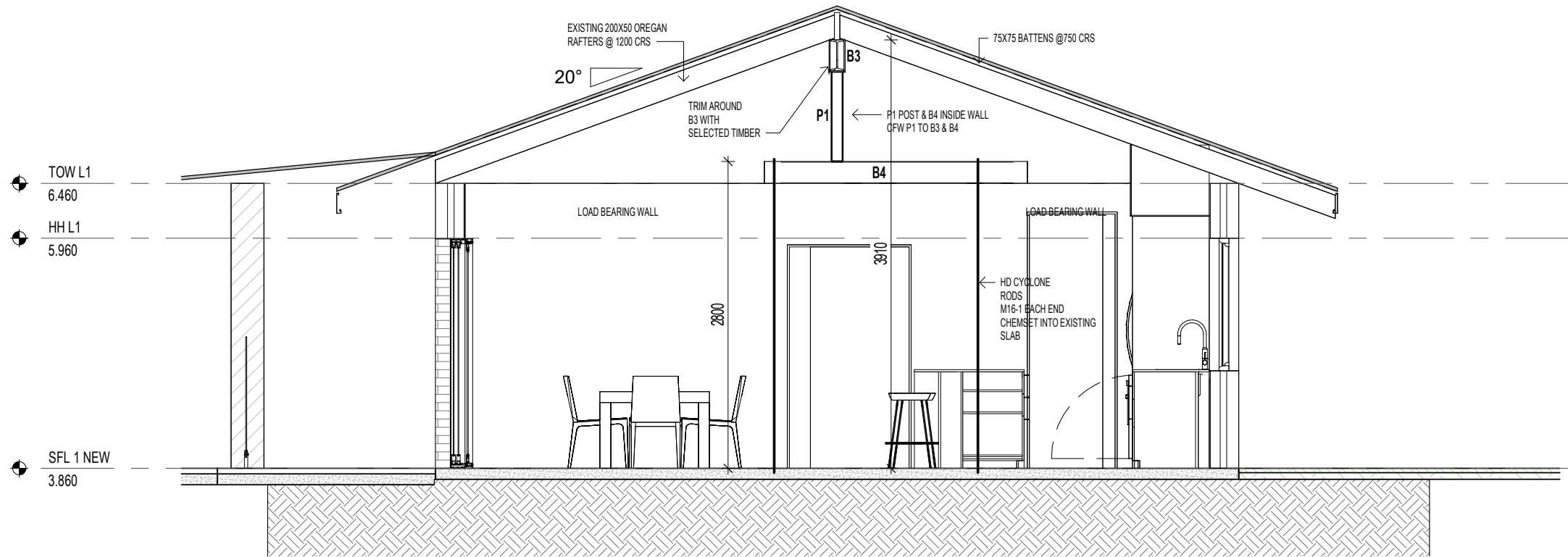
STRUCTURAL FRAMING LEGEND:	
P1	SHS DURAGAL 100X4mm
P2	SHS 75X75X3.2mm EXISTING POST
B1	RHS DURAGAL 150X100X4mm
B2	PFC 200
B3	UC 300
B4	DURAGAL RHS 200X100X6
B5	DURAGAL RHS 150X100X6
PL1	C PURLIN 10019, NO BRIDGING
PL2	C PURLIN 10012, NO BRIDGING
TR1	TYPICAL TIMBER TRUSS (ENGINEERED BY TRUSS SUPPLIER)
BR	STRAP BRACING 32X1mm GAL



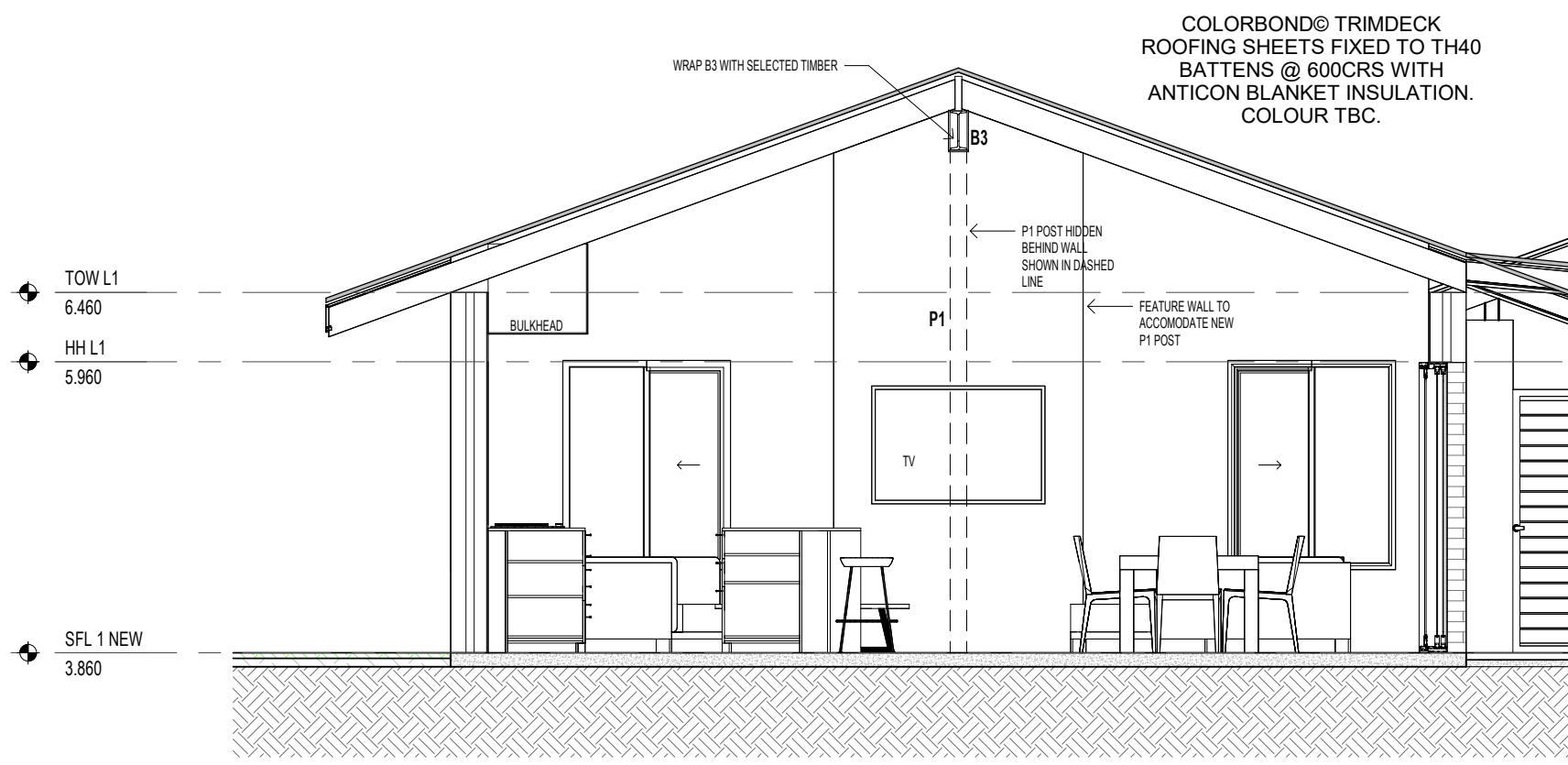
Section AA
1 : 50



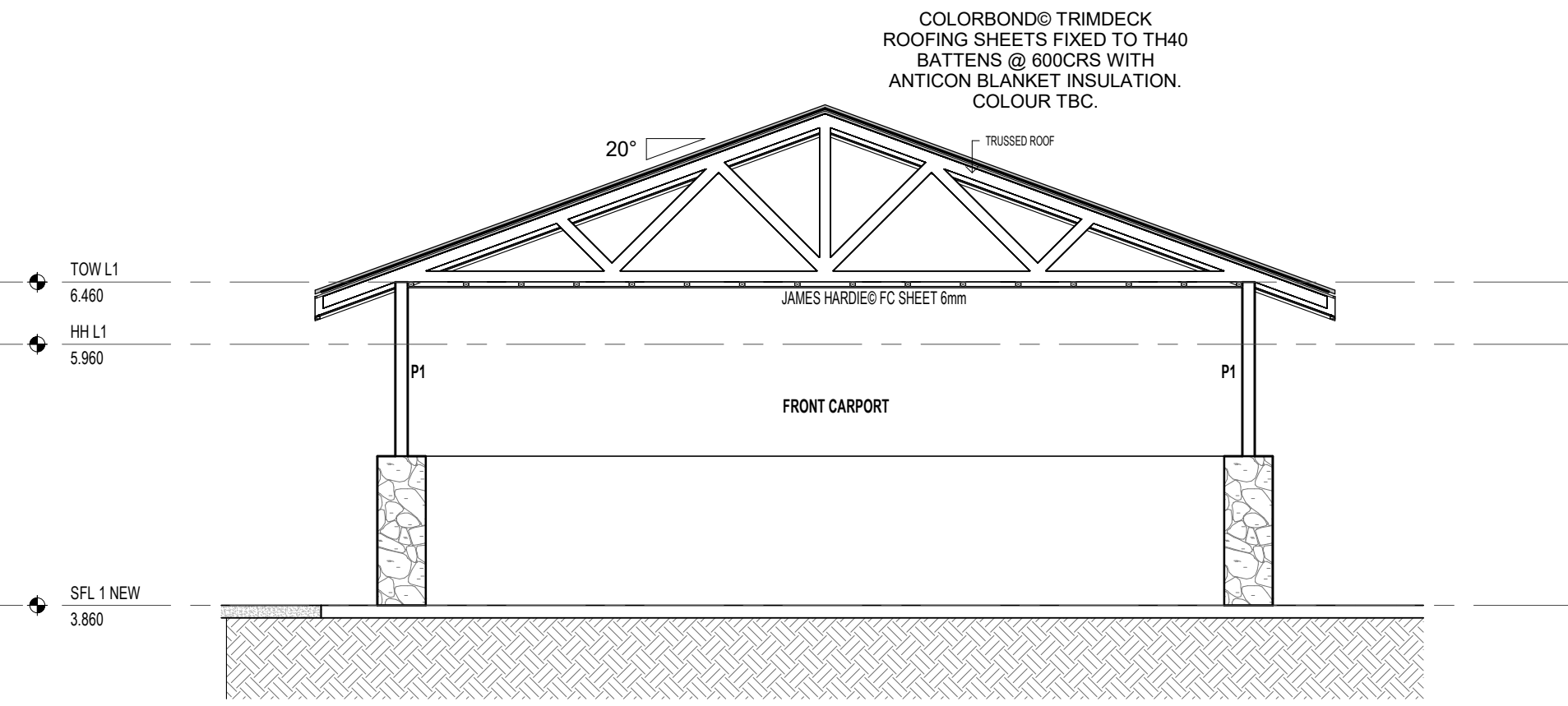
Section BB
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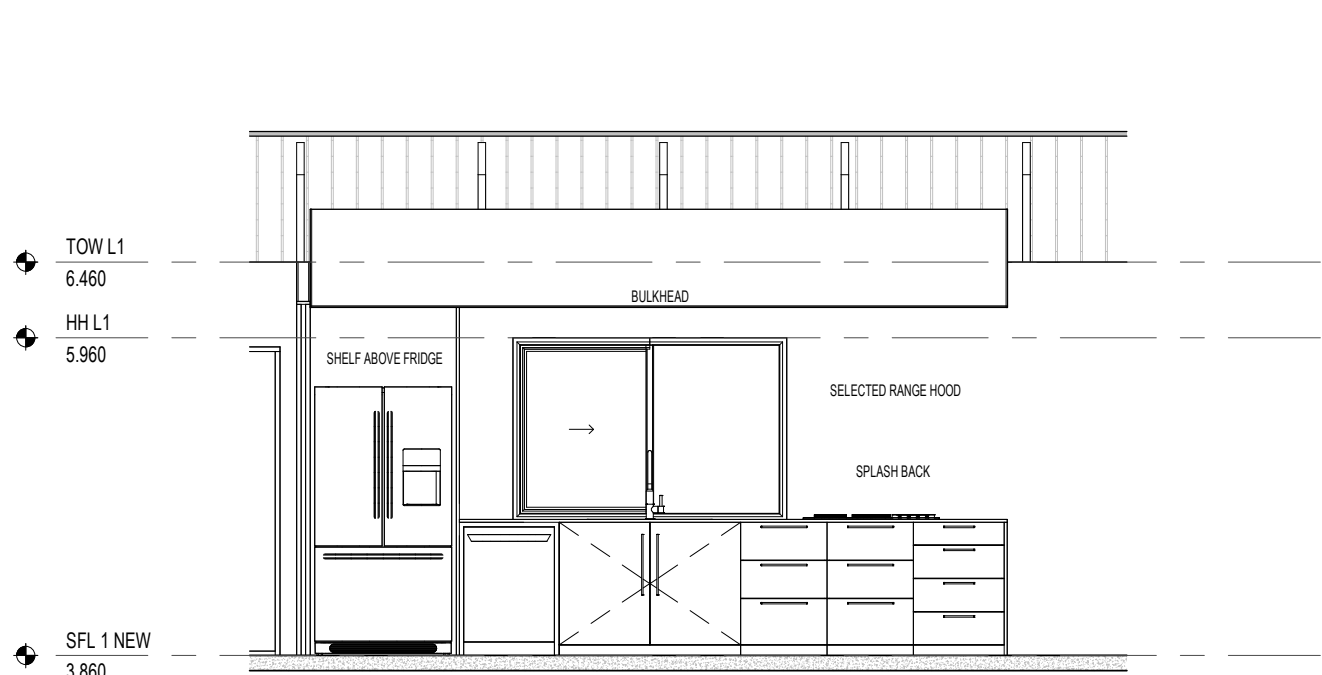
Section CC
1 : 50



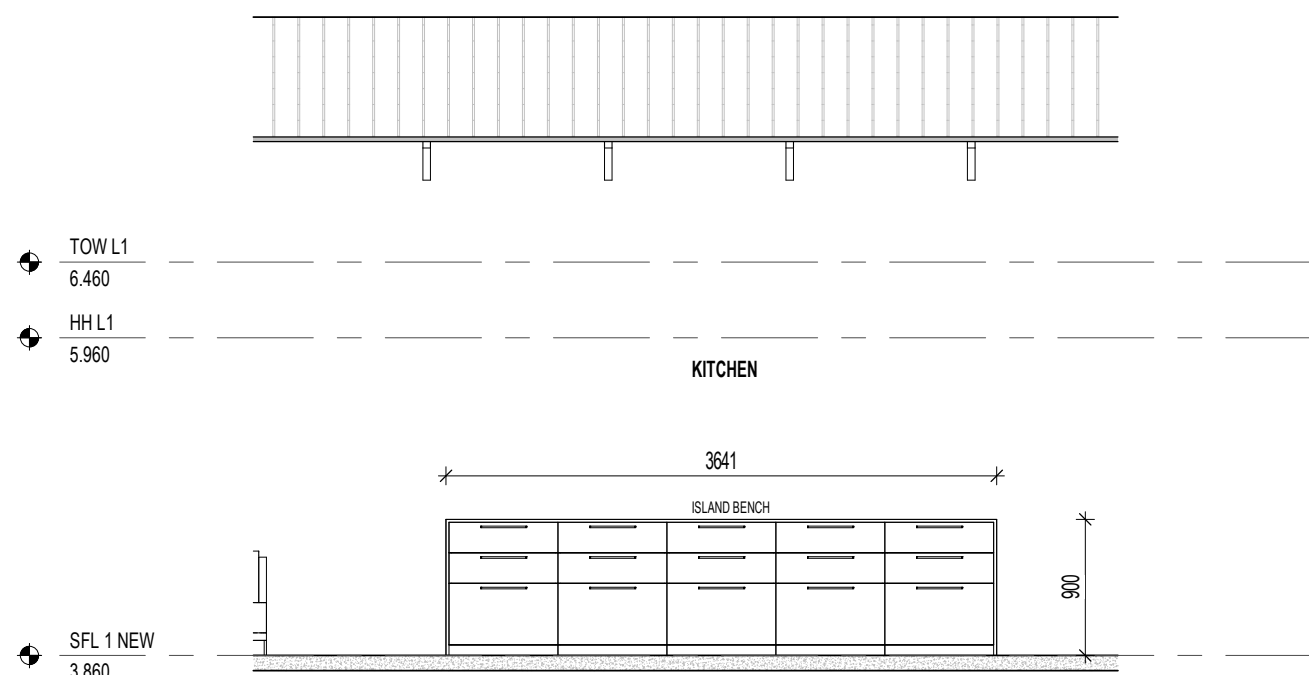
Section DD
1 : 50



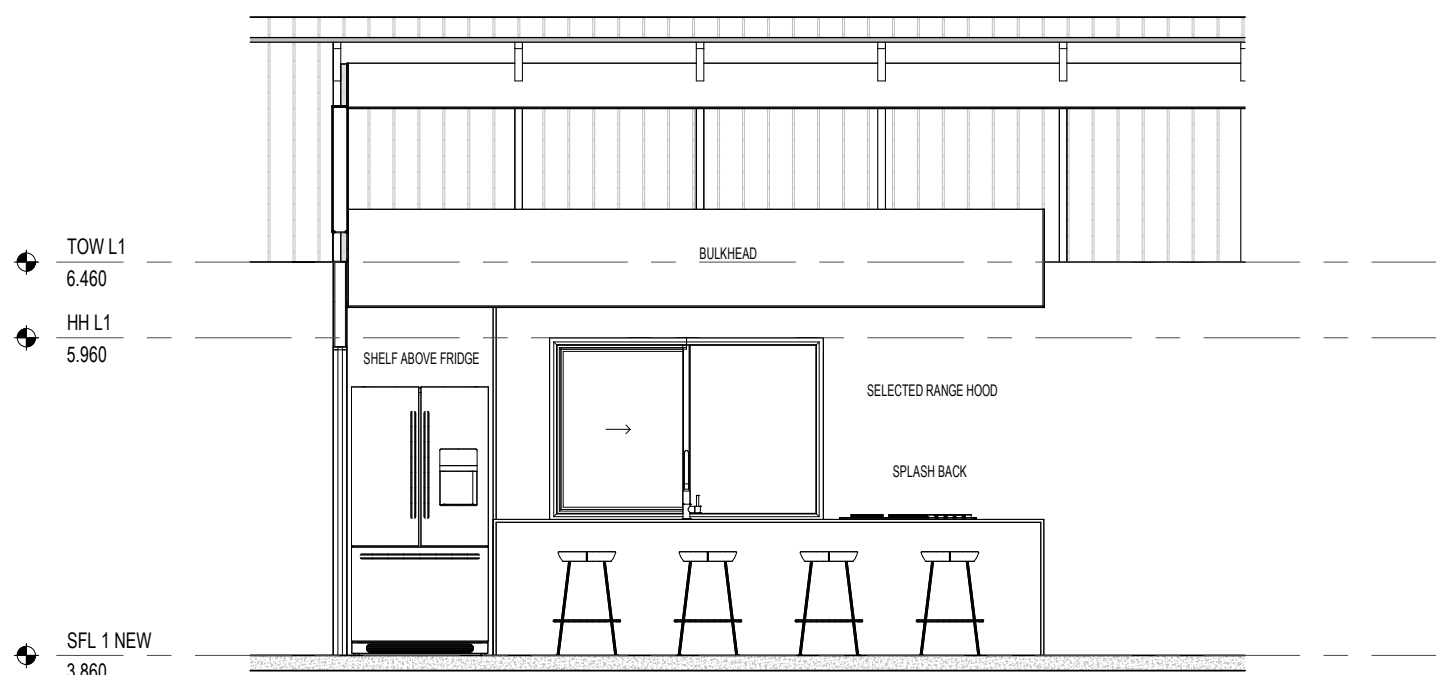
Section EE
1 : 50



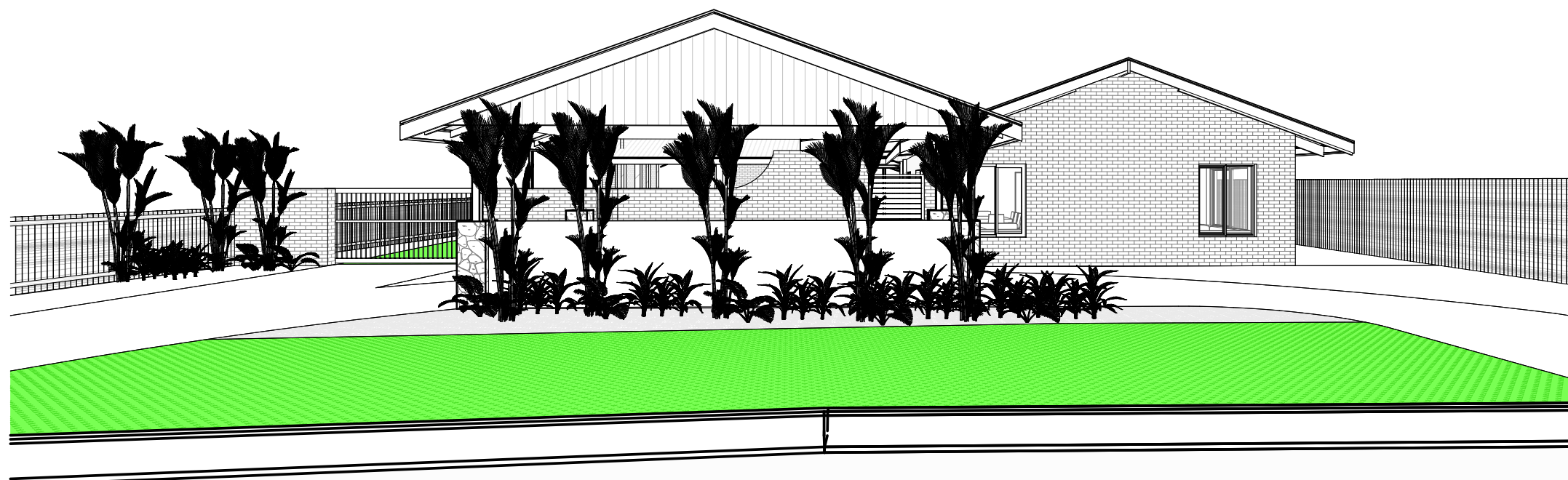
KITCHEN BACK WALL
1 : 50



KITCHEN ISLAND BENCH
1 : 50



KITCHEN OVER VIEW
1 : 50



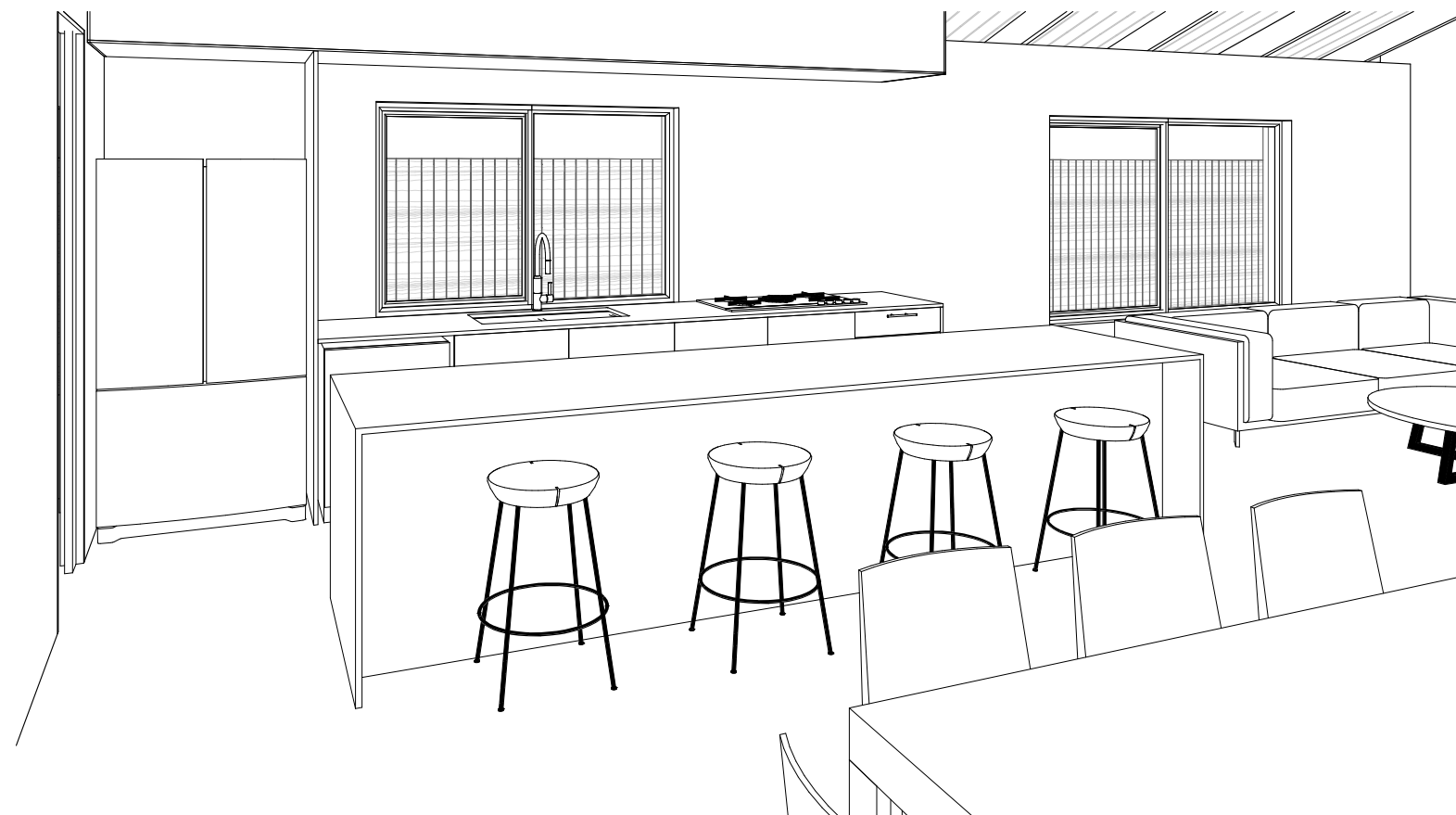
FRONT VIEW



RIGHT FRONT VIEW



LEFT FRONT VIEW



KITCHEN VIEW

8.2.1 Acid sulfate soils overlay code

8.2.1.1 Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Acid sulfate soils overlay, if:
 - (a) self-assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
 - (b) impact assessable development.
- (2) Land in the Acid sulphate soils overlay is identified on the Acid sulfate soils overlay map in Schedule 2 and includes the following sub-categories:
 - (a) Land at or below the 5m AHD sub-category;
 - (b) Land above the 5m AHD and below the 20m AHD sub-category.
- (3) When using this code, reference should be made to Part 5.

8.2.1.2 Purpose

- (1) The purpose of the acid sulfate soils overlay code is to:
 - (a) implement the policy direction in the Strategic Framework, in particular:
 - (i) Theme 2: Environment and landscape values, Element 3.5.4 Coastal zones.
 - (ii) Theme 3: Natural resource management, Element 3.6.2 land and catchment management, Element 3.6.3 Primary production, forestry and fisheries.
- (2) enable an assessment of whether development is suitable on land within the Acid sulfate soils overlay sub-categories.
- (3) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Development ensures that the release of any acid and associated metal contaminant is avoided by not disturbing acid sulfate soils when excavating, removing soil or extracting ground water or filling land;
 - (b) Development ensures that disturbed acid sulfate soils, or drainage waters, are treated and, if required, on-going management practices are adopted that minimise the potential for environmental harm from acid sulfate soil and protect corrodible assets from acid sulfate soil.

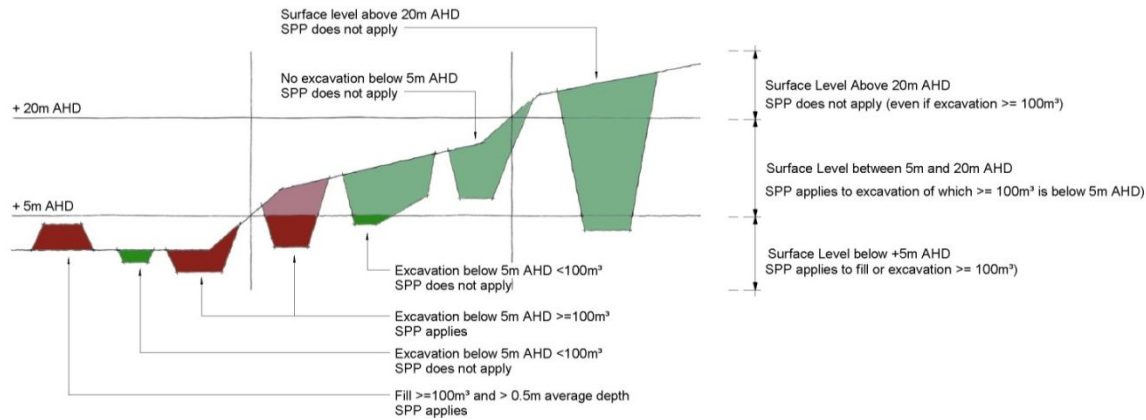
Criteria for assessment**Table 8.2.1.3.a – Acid sulfate soils overlay code – assessable development**

Performance outcomes	Acceptable outcomes	Applicant response
For assessable development		
PO1 The extent and location of potential or actual acid sulfate soils is accurately identified.	AO1.1 No excavation or filling occurs on the site. or AO1.2 An acid sulfate soils investigation is undertaken. Note - Planning scheme policy SC 6.12– Potential and actual acid sulfate soils provides guidance on preparing an acid sulfate soils investigation.	<div>Compliant</div>
PO2 Development avoids disturbing potential acid sulfate soils or actual acid sulfate soils, or is managed to avoid or minimise the release of acid and metal contaminants.	AO2.1 The disturbance of potential acid sulfate soils or actual acid sulfate soils is avoided by: (a) not excavating, or otherwise removing, soil or sediment identified as containing potential or actual acid sulfate soils; (b) not permanently or temporarily extracting groundwater that results in the aeration of previously saturated acid sulfate soils; (c) not undertaking filling that results in: (i) actual acid sulfate soils being moved below the water table; (ii) previously saturated acid sulfate soils being aerated. or	<div>Compliant</div>



Performance outcomes	Acceptable outcomes	Applicant response
	<p>AO2.2</p> <p>The disturbance of potential acid sulfate soils or actual acid sulfate soils is undertaken in accordance with an acid sulfate soils management plan and avoids the release of metal contaminants by:</p> <ul style="list-style-type: none"> (a) neutralising existing acidity and preventing the generation of acid and metal contaminants; (b) preventing the release of surface or groundwater flows containing acid and metal contaminants into the environment; (c) preventing the in situ oxidisation of potential acid sulfate soils and actual acid sulfate soils through ground water level management; (d) appropriately treating acid sulfate soils before disposal occurs on or off site; (e) documenting strategies and reporting requirements in an acid sulfate soils environmental management plan. <p>Note - Planning scheme policy SC 6.12 – Acid sulfate soils provides guidance on preparing an acid sulfate soils management plan.</p>	
<p>PO3</p> <p>No environmental harm is caused as a result of exposure to potential acid sulfate soils or actual acid sulfate soils.</p>	<p>AO3</p> <p>No acceptable outcomes are prescribed.</p>	<p>Compliant</p>

Figure 8.2.1.3.a – Acid sulfate soils (SPP triggers)



8.2.4 Flood and storm tide hazard overlay code

8.2.4.1 Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Flood and storm tide hazard overlay, if:
 - (a) self assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
 - (b) impact assessable development.
- (2) Land in the Flood and storm tide hazard overlay is identified on the Flood and storm tide hazard overlay map in Schedule 2 and includes the:
 - (a) Storm tide – high hazard sub-category;
 - (b) Storm tide – medium hazard sub-category;
 - (c) Flood plain assessment sub-category;
 - (d) 100 ARI Mossman, Port Douglas and Daintree Township Flood Studies sub-category.
- (3) When using this code, reference should be made to Part 5.

Note - The Flood and storm tide hazards overlay maps contained in Schedule 2 identify areas (Flood and storm tide inundation areas) where flood and storm tide inundation modelling has been undertaken by the Council. Other areas not identified by the Flood and inundation hazards overlay maps contained in Schedule 2 may also be subject to the defined flood event or defined storm tide event.

8.2.4.2 Purpose

- (1) The purpose of the Flood and storm tide hazard overlay code is to:
 - (a) implement the policy direction in the Strategic Framework, in particular:
 - (i) Theme 1 Settlement pattern: Element 3.4.7 Mitigation of hazards;
 - (ii) Theme 6 Infrastructure and transport: Element 3.9.2 Energy.
 - (b) enable an assessment of whether development is suitable on land within the Flood and storm tide hazard sub-categories.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) development siting, layout and access responds to the risk of the natural hazard and minimises risk to personal safety;
 - (b) development achieves an acceptable or tolerable risk level, based on a fit for purpose risk assessment;
 - (c) the development is resilient to natural hazard events by ensuring siting and design accounts for the potential risks of natural hazards to property;

- (d) the development supports, and does not unduly burden disaster management response or recovery capacity and capabilities;
- (e) the development directly, indirectly and cumulatively avoids an unacceptable increase in severity of the natural hazards and does not significantly increase the potential for damage on site or to other properties;
- (f) the development avoids the release of hazardous materials as a result of a natural hazard event;
- (g) natural processes and the protective function of landforms and/or vegetation are maintained in natural hazard areas;
- (h) community infrastructure is located and designed to maintain the required level of functionality during and immediately after a hazard event.

Criteria for assessment

Table 8.2.4.3.a – Flood and storm tide hazards overlay code –assessable development

Performance outcomes	Acceptable outcomes	Applicant response
For self-assessable and assessable development		
<p>PO1 Development is located and designed to: ensure the safety of all persons; minimise damage to the development and contents of buildings; provide suitable amenity; minimise disruption to residents, recovery time, and rebuilding or restoration costs after inundation events.</p> <p>Note – For assessable development within the flood plain assessment sub-category, a flood study by a suitably qualified professional is required to identify compliance with the intent of the acceptable outcome.</p>	<p>AO1.1 Development is sited on parts of the land that is not within the Flood and Storm tide hazards overlay maps contained in Schedule 2;</p> <p>or</p> <p>For dwelling houses,</p> <p>AO1.2 Development within the Flood and Storm Tide hazards overlay maps (excluding the Flood plain assessment sub-category) is designed to provide immunity to the Defined Inundation Event as outlined within Table 8.2.4.3.b plus a freeboard of 300mm.</p>	<p>Compliant</p>



Performance outcomes	Acceptable outcomes	Applicant response
	<p>AO1.3 New buildings are:</p> <ul style="list-style-type: none"> (a) not located within the overlay area; (b) located on the highest part of the site to minimise entrance of flood waters; (c) provided with clear and direct pedestrian and vehicle evacuation routes off the site. <p>AO1.4 In non urban areas, buildings and infrastructure are set back 50 metres from natural riparian corridors to maintain their natural function of reducing velocity of floodwaters.</p>	
For assessable development		
<p>PO2 The development is compatible with the level of risk associated with the natural hazard.</p>	<p>AO2 The following uses are not located in land inundated by the Defined Flood Event (DFE) / Storm tide:</p> <ul style="list-style-type: none"> (a) Retirement facility; (b) Community care facility; (c) Child care centre. 	compliant
<p>PO3 Development siting and layout responds to flooding potential and maintains personal safety</p>	<p>For Material change of use</p> <p>AO3.1 New buildings are:</p> <ul style="list-style-type: none"> (a) not located within the overlay area; (b) located on the highest part of the site to minimise entrance of flood waters; (c) provided with clear and direct pedestrian and vehicle evacuation routes off the site. <p>or</p>	compliant



Performance outcomes	Acceptable outcomes	Applicant response
	<p>AO3.2 The development incorporates an area on site that is at least 300mm above the highest known flood inundation level with sufficient space to accommodate the likely population of the development safely for a relatively short time until flash flooding subsides or people can be evacuated.</p> <p>or</p> <p>AO3.3 Where involving an extension to an existing dwelling house that is situated below DFE /Storm tide, the maximum size of the extension does not exceed 70m² gross floor area.</p> <p>Note – If part of the site is outside the Hazard Overlay area, this is the preferred location of all buildings.</p> <p>For Reconfiguring a lot</p> <p>AO3.4 Additional lots: (a) are not located in the hazard overlay area; or (b) are demonstrated to be above the flood level identified for the site.</p> <p>Note - If part of the site is outside the Hazard Overlay area, this is the preferred location for all lots (excluding park or other open space and recreation lots).</p> <p>Note – Buildings subsequently developed on the lots will need to comply with the relevant building assessment provisions under the <i>Building Act 1975</i>.</p>	



Performance outcomes	Acceptable outcomes	Applicant response
	<p>AO3.5 Road and/or pathway layout ensures residents are not physically isolated from adjacent flood free urban areas and provides a safe and clear evacuation route path:</p> <ul style="list-style-type: none"> (a) by locating entry points into the reconfiguration above the flood level and avoiding culs-de-sac or other non-permeable layouts; and (b) by direct and simple routes to main carriageways. <p>AO3.6 Signage is provided on site (regardless of whether the land is in public or private ownership) indicating the position and path of all safe evacuation routes off the site and if the site contains, or is within 100m of a floodable waterway, hazard warning signage and depth indicators are also provided at key hazard points, such as at floodway crossings or entrances to low-lying reserves.</p> <p>or</p> <p>AO3.7 There is no intensification of residential uses within the flood affected areas on land situated below the DFE/Storm tide.</p>	



Performance outcomes	Acceptable outcomes	Applicant response
	<p>For Material change of use (Residential uses) AO3.8 The design and layout of buildings used for residential purposes minimise risk from flooding by providing: (a) parking and other low intensive, non-habitable uses at ground level;</p> <p>Note - The high-set 'Queenslander' style house is a resilient low-density housing solution in floodplain areas. Higher density residential development should ensure only non-habitable rooms (e.g. garages, laundries) are located on the ground floor.</p>	
<p>PO4 Development is resilient to flood events by ensuring design and built form account for the potential risks of flooding.</p>	<p>For Material change of use (Non-residential uses) AO4.2 Non residential buildings and structures allow for the flow through of flood waters on the ground floor. Note - Businesses should ensure that they have the necessary contingency plans in place to account for the potential need to relocate property prior to a flood event (e.g. allow enough time to transfer stock to the upstairs level of a building or off site). Note - The relevant building assessment provisions under the <i>Building Act 1975</i> apply to all building work within the Hazard Area and need to take into account the flood potential within the area. AO4.3 Materials are stored on-site: (a) are those that are readily able to be moved in a flood event; (b) where capable of creating a safety hazard by being shifted by flood waters, are contained in order to minimise movement in times of flood.</p> <p>Notes - (a) Businesses should ensure that they have the necessary contingency plans in place to account for the potential need to relocate property prior to a flood event (e.g. allow enough time to transfer stock to the upstairs level of a building or off site).</p>	<p>Compliant New Driveway is to fall water towards the front discharge points and will also be upgrading the following</p> <ul style="list-style-type: none"> • Upgrading downpipes and stormwater to larger size & more of. • Raising slab heights • More storm water pits • Re contouring landscaping and lawns to fall water away



Performance outcomes	Acceptable outcomes	Applicant response
	(b) Queensland Government Fact Sheet 'Repairing your House after a Flood' provides information about water resilient products and building techniques.	
<p>P05 Development directly, indirectly and cumulatively avoids any increase in water flow velocity or flood level and does not increase the potential flood damage either on site or on other properties.</p> <p>Note – Berms and mounds are considered to be an undesirable built form outcome and are not supported.</p>	<p>For Operational works</p> <p>A05.1 Works in urban areas associated with the proposed development do not involve:</p> <ul style="list-style-type: none"> (a) any physical alteration to a watercourse or floodway including vegetation clearing; or (b) a net increase in filling (including berms and mounds). <p>A05.2 Works (including buildings and earthworks) in non urban areas either:</p> <ul style="list-style-type: none"> (a) do not involve a net increase in filling greater than 50m³; or (b) do not result in any reductions of on-site flood storage capacity and contain within the subject site any changes to depth/duration/velocity of flood waters; <p>or</p> <ul style="list-style-type: none"> (c) do not change flood characteristics outside the subject site in ways that result in: <ul style="list-style-type: none"> (i) loss of flood storage; (ii) loss of/changes to flow paths; (iii) acceleration or retardation of flows or any reduction in flood warning times elsewhere on the flood plain. 	<p>compliant</p>



Performance outcomes	Acceptable outcomes	Applicant response
	<p>For Material change of use</p> <p>AO5.3 Where development is located in an area affected by DFE/Storm tide, a hydraulic and hydrology report, prepared by a suitably qualified professional, demonstrates that the development maintains the flood storage capacity on the subject site; and</p> <p>(a) does not increase the volume, velocity, concentration of flow path alignment of stormwater flow across sites upstream, downstream or in the general vicinity of the subject site; and</p> <p>(b) does not increase ponding on sites upstream, downstream or in the general vicinity of the subject site.</p> <p>For Material change of use and Reconfiguring a lot</p> <p>AO5.4 In non urban areas, buildings and infrastructure are set back 50 metres from natural riparian corridors to maintain their natural function of reducing velocity of floodwaters.</p> <p>Note – Fences and irrigation infrastructure (e.g. irrigation tape) in rural areas should be managed to minimise adverse the impacts that they may have on downstream properties in the event of a flood.</p>	
<p>PO6 Development avoids the release of hazardous materials into floodwaters.</p>	<p>For Material change of use</p> <p>AO6.1 Materials manufactured or stored on site are not hazardous or noxious, or comprise materials that may cause a detrimental effect on the environment if discharged in a flood event;</p>	compliant



Performance outcomes	Acceptable outcomes	Applicant response
	<p>or</p> <p>AO6.2 If a DFE level is adopted, structures used for the manufacture or storage of hazardous materials are: (a) located above the DFE level; or (b) designed to prevent the intrusion of floodwaters.</p> <p>AO6.3 Infrastructure is designed and constructed to resist hydrostatic and hydrodynamic forces as a result of inundation by the DFE.</p> <p>AO6.4 If a flood level is not adopted, hazardous materials and their manufacturing equipment are located on the highest part of the site to enhance flood immunity and designed to prevent the intrusion of floodwaters.</p> <p>Note – Refer to <i>Work Health and Safety Act 2011</i> and associated Regulation and Guidelines, the <i>Environmental Protection Act 1994</i> and the relevant building assessment provisions under the <i>Building Act 1975</i> for requirements related to the manufacture and storage of hazardous materials.</p>	
<p>P07 The development supports, and does not unduly burden, disaster management response or recovery capacity and capabilities.</p>	<p>A07 Development does not: (a) increase the number of people calculated to be at risk of flooding; (b) increase the number of people likely to need evacuation; (c) shorten flood warning times; and</p>	compliant



Performance outcomes	Acceptable outcomes	Applicant response
	(d) impact on the ability of traffic to use evacuation routes, or unreasonably increase traffic volumes on evacuation routes.	
PO8 Development involving community infrastructure: (a) remains functional to serve community need during and immediately after a flood event; is designed, sited and operated to avoid adverse impacts on the community or environment due to impacts of flooding on infrastructure, facilities or access and egress routes; retains essential site access during a flood event; is able to remain functional even when other infrastructure or services may be compromised in a flood event.	AO8.1 The following uses are not located on land inundated during a DFE/Storm tide: (a) community residence; and (b) emergency services; and (c) residential care facility; and (d) utility installations involving water and sewerage treatment plants; and (e) storage of valuable records or items of historic or cultural significance (e.g. archives, museums, galleries, libraries). or AO8.2 The following uses are not located on land inundated during a 1% AEP flood event: (a) community and cultural facilities, including facilities where an education and care service under the Education and care Services National law (Queensland) is operated or child care service under the <i>Child Care Act 2002</i> is conducted, (b) community centres; (c) meeting halls; (d) galleries; (e) libraries.	compliant



Performance outcomes	Acceptable outcomes	Applicant response
	<p>The following uses are not located on land inundated during a 0.5% AEP flood event.</p> <ul style="list-style-type: none"> (a) emergency shelters; (b) police facilities; (c) sub stations; (d) water treatment plant <p>The following uses are not located on land inundated during a 0.2% AEP flood event:</p> <ul style="list-style-type: none"> (a) correctional facilities; (b) emergency services; (c) power stations; (d) major switch yards. <p>and/or</p> <p>AO8.3 The following uses have direct access to low hazard evacuation routes as defined in</p> <p>Table 8.2.4.3.c :</p> <ul style="list-style-type: none"> (a) community residence; and (b) emergency services; and (c) hospitals; and (d) residential care facility; and (e) sub stations; and (f) utility installations involving water and sewerage treatment plants. <p>AO8.4 Any components of infrastructure that are likely to fail to function or may result in contamination when inundated by flood, such as electrical switch gear and</p>	



Performance outcomes	Acceptable outcomes	Applicant response
	<p>motors, telecommunications connections, or water supply pipeline air valves are:</p> <p>(a) located above DFE/Storm tide or the highest known flood level for the site;</p> <p>(b) designed and constructed to exclude floodwater intrusion / infiltration.</p> <p>AO8.5 Infrastructure is designed and constructed to resist hydrostatic and hydrodynamic forces as a result of inundation by a flood.</p>	

Table 8.2.4.3.b - Minimum immunity (floor levels) for development

Minimum immunity to be achieved (floor levels)	Uses and elements of activities acceptable in the event
20% AEP level	<ul style="list-style-type: none"> • Parks and open space.
5% AEP level	<ul style="list-style-type: none"> • Car parking facilities (including car parking associated with use of land).
1% AEP level	<ul style="list-style-type: none"> • All development (where not otherwise requiring an alternative level of minimum immunity).
0.5% AEP level	<ul style="list-style-type: none"> • Emergency services (if for a police station); • Industry activities (if including components which store, treat or use hazardous materials); • Substation; • Utility installation.
0.2% AEP level	<ul style="list-style-type: none"> • Emergency services; • Hospital; • Major electricity infrastructure; • Special industry.

Table 8.2.4.3.c - Degree of flood

Criteria	Low	Medium	High	Extreme
Wading ability	If necessary children and the elderly could wade. (Generally, safe wading velocity depth product is less than 0.25)	Fit adults can wade. (Generally, safe wading velocity depth product is less than 0.4)	Fit adults would have difficulty wading. (Generally, safe wading velocity depth product is less than 0.6)	Wading is not an option.
Evacuation distances	< 200 metres	200-400 metres	400-600 metres	600 metres
Maximum flood depths	< 0.3 metre	< 0.6 metre	< 1.2 metres	1.2 metres
Maximum flood velocity	< 0.4 metres per second	< 0.8 metres per second	< 1.5 metres per second	1.5 metres per second
Typical means of egress	Sedan	Sedan early, but 4WD or trucks later	4WD or trucks only in early stages, boats or helicopters	Large trucks, boats or helicopters
Timing Note: This category cannot be implemented until evacuation times have been established in the Counter Disaster Plan (Flooding)	Ample flood forecasting. Warning and evacuation routes remain passable for twice as long as evacuation time.	Evacuation routes remain trafficable for 1.5 times as long as the evacuation.	Evacuation routes remain trafficable for only up to minimum evacuation time.	There is insufficient evacuation time.

Note: The evacuation times for various facilities or areas would (but not necessarily) be included in the Counter Disaster Plan.
Generally safe wading conditions assume even walking surfaces and no obstructions, steps, soft underfoot etc.

6.2.7 Low-medium density residential zone code

6.2.7.1 Application

- (1) This code applies to assessing development in the Low-medium density residential zone.
- (2) When using this code, reference should be made to Part 5.

6.2.7.2 Purpose

- (1) The purpose of the Low-medium density residential zone code is to provide for a range and mix of dwelling types including dwelling houses and multiple dwellings supported by community uses and small-scale services and facilities that cater for local residents.
- (2) The local government purpose of the code is to:
 - (a) implement the policy direction set in the Strategic Framework, in particular:
 - (i) Theme 1 : Settlement pattern, Element 3.4.2 – Urban settlement, Element 3.4.5 Residential areas and activities, Element 3.4.7 – Mitigation of hazards.
 - (ii) Theme 4 : Strong community and identity, Element 3.7.3 – Active communities, Element 3.7.4 – Sense of place, community and identity, Element 3.7.5 – Housing choice and affordability.
 - (iii) Theme 6 : Infrastructure and transport, Element 3.9.2 - Energy, Element 3.9.3 – Water and waste management, Element 3.9.4 Transport, Element 3.9.5 – Information technology.
 - (b) establish a low-medium density residential character consisting predominantly of low-rise 1 and 2 storey dwelling houses, dual occupancies and multiple dwellings.
 - (c) provide for a diversity in housing choice through other housing types to cater for different housing needs and family structures.
 - (d) provide support for compatible small scale non-residential use activities.
 - (e) ensure development occurs on appropriately sized and shaped lots.
- (3) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Development provides a range of residential dwelling choices including multiple dwellings and other forms of permanent-living residential development, including Residential care facilities.
 - (b) Development encourages and facilitates urban consolidation and the efficient use of physical and social infrastructure.
 - (c) Development is designed to provide safe and walkable neighbourhoods.

- (d) Development maintains a high level of residential amenity having regard to traffic, noise, dust, odour, lighting and other locally specific impacts.
- (e) Development is reflective and responsive to the environmental constraints of the land.
- (f) Development provides a high level of amenity and is reflective of the surrounding character of the area.
- (g) Development is supported by necessary community facilities, open space and recreational areas and appropriate infrastructure to support the needs of the local community.

Criteria for assessment

Table 6.2.7.3.a – Low-medium density residential zone code – assessable development

Performance outcomes	Acceptable outcomes	Applicant response
For self-assessable and assessable development		
PO1 The height of all buildings and structures must be in keeping with the residential character of the area	AO1 Buildings and structures are not more than 8.5 metres and two storeys in height. Note – Height is inclusive of roof height.	Compliant
Setbacks (other than for a dwelling house)		
PO2 Buildings are setback to: <ul style="list-style-type: none"> (a) maintain the character of residential neighbourhoods; (b) achieve separation from neighbouring buildings and from road frontages; (c) maintain a cohesive streetscape; (d) provide daylight access, privacy and appropriate landscaping. 	AO2 Buildings are setback: <ul style="list-style-type: none"> (a) a minimum of 6 metres from the main street frontage; (b) a minimum of 4 metres from any secondary street frontage; (c) 4.5 metres from a rear boundary; (d) 2 metres from a side or an average of half of the height of the building at the side setback, whichever is the greater. 	Compliant
Site coverage		
PO3 The site coverage of all buildings does not result in a built form that is bulky or visually obtrusive.	AO3 The site coverage of any building is limited to 50%.	Compliant



Performance outcomes	Acceptable outcomes	Applicant response
For assessable development		
P04 The establishment of uses is consistent with the outcomes sought for the Low-medium density residential zone and protects the zone from the intrusion of inconsistent uses.	A04 Uses identified in Table 6.2.7.3.b are not established in the Low-medium density residential zone.	Compliant
P05 Development is located, designed, operated and managed to respond to the natural characteristics, features and constraints of the site and surrounds. Note – Planning scheme policy – Site assessments provides guidance on identifying the characteristics and features and constraints of a site and its surrounds.	A05 No acceptable outcomes are prescribed.	Compliant
P06 Development does not adversely affect the residential character and amenity of the area in terms of traffic, noise, dust, odour, lighting or other physical or environmental impacts.	A06 No acceptable outcomes are prescribed.	Compliant
P07 New lots contain a minimum area of 450m ² .	A07 No acceptable outcomes are prescribed.	n/a
P08 New lots have a minimum road frontage of 15 metres.	A08 No acceptable outcomes are prescribed.	n/a
P09 New lots contain a 20 metre x 15 metre rectangle.	A09 No acceptable outcomes are prescribed.	n/a



Table 6.2.7.3.b - Inconsistent uses within the Low-medium density residential zone

Inconsistent uses		
<ul style="list-style-type: none"> • Adult store • Agricultural supplies store • Air services • Animal husbandry • Aquaculture • Bar • Brothel • Bulk landscape supplies • Car wash • Club • Crematorium • Cropping • Detention facility • Emergency services • Extractive industry • Food and drink outlet • Function facility • Funeral parlour • Garden centre • Hardware and trade supplies • High impact industry 	<ul style="list-style-type: none"> • Hospital • Hotel • Indoor sport and recreation • Intensive animal industry • Intensive horticulture • Landing • Low impact industry • Major electricity infrastructure • Major sport, recreation and entertainment facility • Marine industry • Medium impact industry • Motor sport facility • Nature based tourism • Nightclub entertainment facility • Non-resident workforce accommodation • Office • Outdoor sales • Outstation 	<ul style="list-style-type: none"> • Parking station • Permanent plantation • Port services • Renewable energy facility • Research and technology industry • Resort complex • Roadside stall • Rooming accommodation • Rural industry • Rural workers accommodation • Service industry • Shop • Shopping Centre • Showroom • Special industry • Theatre • Tourist attraction • Transport depot • Veterinary services • Warehouse • Wholesale nursery • Winery

Note – This table does not imply that all other uses not listed in the table are automatically consistent uses within the zone. Assessable development must still demonstrate consistency through the assessment process.

7.2.4 Port Douglas/Craigie local plan code

7.2.4.1 Application

- (1) This code applies to assessing development within the Port Douglas/Craigie local plan area as identified on the Port Douglas/Craigie local plan maps contained in Schedule 2.
- (2) When using this code, reference should be made to Part 5.

6.2.5.2 Context and setting

Editor's note - This section is extrinsic material under section 15 of the *Statutory Instruments Act 1992* and is intended to assist in the interpretation of the Port Douglas/Craigie local plan code.

The Port Douglas/Craigie local plan encompasses the traditional Port Douglas town centre and surrounding tourist and residential areas, including Four Mile Beach and Craigie.

Port Douglas was officially named in 1877. It was initially settled as the port of entry and supply for the Hodgkinson goldfield on the Hann Tableland which was proclaimed in 1876. It was the dominant port in Far North Queensland until a decision was made to establish Cairns as the terminus for a new railway in 1884. This ended the town's dominance, and it gradually became a small centre for local residents and fishing activities. During the 1970s and 1980s, a renewed interest in Far North Queensland as a holiday destination led to a boom in large scale tourism and residential development with Port Douglas re-emerging as a premium destination.

The Captain Cook Highway runs north-south to the west of Port Douglas through Craigie (Four Mile). Craigie caters for the permanent resident population associated with Port Douglas, as well as providing for service industries to support business in the town. The majority of urban development is confined to the eastern side of the highway. The main entrance to Port Douglas at the intersection of Port Douglas Road is accentuated by mature oil palms lining both sides of the street for almost the entire length of the corridor into the heart of Port Douglas.

Flagstaff Hill is a prominent headland on the northern side of the Port Douglas town centre providing a green tropical backdrop to the town. Island Point Road runs to the top of Flagstaff Hill and provides access to the iconic lookout overlooking the sweep of Four Mile Beach.

Macrossan Street is the main shopping area in Port Douglas running in a general east-west direction at the base of Flagstaff Hill connecting Four Mile Beach to Dickson Inlet. Tourist and commercial development is concentrated towards the western side of Macrossan Street, with marine orientated activity focussed around the inlet. The western side of the inlet provides unspoiled views across mangroves to the distinctive formations and features of the coastal range.

The street pattern in the town centre is based on the original grid pattern survey of 1878. While the town has lost many of its original buildings to cyclones and redevelopment, a number of important built features remain including the Central Hotel, the Court House Hotel, a number of relocated buildings such as St Mary's Church, the former Clink Theatre and the Court House Museum and scattered memorials such as the Carstens memorial in Macrossan Street

and the Port Douglas War memorial in Wharf Street. The Sugar Wharf on Dickson Inlet was the original terminus of the tramline to Mossman. The tramline now terminates adjacent to the Port Douglas marina and operates as the Balley Hooley passenger service on four kilometres of track between the Port Douglas Marina and St Crispins Station.

A particular characteristic of the local plan area is its high quality, lush landscaping complementing the tropical resort town atmosphere. This theme will be carried throughout the local plan area with gateways, nodes and corridor planting emphasising the role of the town as a tropical tourist destination.

7.2.4.3 Purpose

- (1) The purpose of the Port Douglas/Craigie local plan code is to facilitate development outcomes consistent with community values, the local tropical built-form and protection of the natural environment within the Port Douglas/Craigie local plan area, while providing a platform for investment and prosperity.
 - (a) In addition, the purpose of the code is supported by the Port Douglas Waterfront Master Plan which provides a clear strategic direction for the incremental transformation of the Port Douglas Waterfront, including the following objectives:
 - (b) To set out a vision for revitalisation of the waterfront;
 - (c) To protect and enhance the environmental attributes; and
- (2) To provide a flexible framework, expressed through several key strategies that will assist the Council and community in managing change.
- (3) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Port Douglas will continue to develop as the premium destination for international and domestic tourists in the Far North Queensland Region, while also acting for permanent residents attracted to the associated lifestyle.
 - (b) Major tourist, retail, dining and entertainment facilities will consolidate in the Town Centre and the Waterfront North sub-precincts, with improved pedestrian connections between the town centre and the waterfront.
 - (c) Craigie will develop as an integrated residential community with some low scale tourism development opportunities in appropriate locations. Craigie will also function as small scale commercial and light industry node, providing employment opportunities for the Shire's permanent resident population.
 - (d) All forms of development will complement the tropical image of the town through distinctive tropical vernacular, urban design and landscaping.
 - (e) Character will be enhanced through the identification of gateway sites, landmarks, main approach routes and pedestrian thoroughfares and view corridors;
 - (f) The Flagstaff Hill, Dickson Inlet, Four Mile Beach and other areas of scenic and environmental significance will be protected from development. Vegetation cover will dominate over built form.

- (g) Vegetation, iconic to the character of Port Douglas, including the avenues of Oil Palms, is retained and where appropriate supplemented.
 - (h) Development will be indistinguishable from view from Four Mile Beach. In addition, any development on Flagstaff Hill will be indistinguishable when viewed from vantage points in Port Douglas.
 - (i) Residential areas are designed as pleasant, functional and distinctive, in visually well-defined areas.
- (4) The purpose of the code will be further achieved through the following overall outcomes:
- (a) Precinct 1 – Port Douglas precinct
 - (i) Sub-precinct 1a – Town Centre sub-precinct
 - (ii) Sub-precinct 1b – Waterfront North sub-precinct
 - (iii) Sub-precinct 1c – Waterfront South sub-precinct
 - (iv) Sub-precinct 1d – Limited Development sub-precinct
 - (v) Sub-precinct 1e – Community and recreation sub-precinct
 - (vi) Sub-precinct 1f – Flagstaff Hill sub-precinct
 - (b) Precinct 2 – Integrated Resort precinct
 - (c) Precinct 3 – Craiglie Commercial and Light Industry precinct
 - (d) Precinct 4 – Old Port Road / Mitre Street precinct
 - (e) Precinct 5 – Very Low Density Residential/ Low Scale Recreation/Low Scale Educational/Low Scale Entertainment Uses precinct

Precinct 1 – Port Douglas precinct

- (5) In addition to the overall outcomes, the outcomes sought for the precinct are to ensure that:
- (a) development will contribute to the incremental transformation of the township, preserving and enhancing maritime activities and environmental areas, delivering tropical open spaces and a high quality public realm, and allowing for tourism opportunities and investment.
 - (b) development contributes to the enhancement of the Port Douglas precinct through the following development outcomes:
 - (i) access and connectivity throughout the township is enhanced through a series of improvements to circulation and mobility, including:
 - (A) access to, and connectivity along, the waterfront and foreshore areas is maintained and, where appropriate, enhanced;
 - (B) reducing reliance on the waterfront as a car parking resource.
 - (ii) the use of land in the Port Douglas precinct improves the cohesive layout of the township through:
 - (A) the establishment of distinct sub-precincts that reinforce the character and built form of the Port Douglas local plan area including:

- Port Douglas centre sub-precinct 1a – Town Centre sub-precinct;
 - Port Douglas centre sub-precinct 1b – Waterfront North sub-precinct;
 - Port Douglas centre sub-precinct 1c – Waterfront South sub-precinct;
 - Port Douglas centre sub-precinct 1d – Limited development sub-precinct;
 - Port Douglas centre sub-precinct 1e – Community and recreation precinct;
 - Port Douglas centre sub-precinct 1f – Flagstaff Hill sub-precinct;
- (B) facilitating marina facilities and supporting marine industry uses as a key part of the local economy;
- (C) reducing conflict between industry, community and commercial activities in the waterfront, without diminishing the marine industry capacity in the Port Douglas precinct;
- (iii) environment and sustainability is integrated into the township through:
- (A) preservation and enhancement of the qualities and characteristics of environmental areas of the township;
- (B) water sensitive urban design is considered as a means of water quality improvement and management of overland flow to ensure hard infrastructure solutions in Warner Street can be mitigated;
- (C) design of buildings and access way improvements prioritises walking and cycling modes of transport.
- (iv) the tropical character of the Port Douglas precinct is enhanced by ensuring development:
- (A) maintains and enhances the built form, local character, streetscapes and natural elements of the township;
- (B) is compatible with the desired character and amenity of local places and neighbourhoods;
- (C) does not exceed the height of buildings designations which contribute to the desired form of the township which contains three storey development heights in sub-precinct 1a – Town Centre sub-precinct and part of sub-precinct 1b – Waterfront North sub-precinct;
- (D) implements high quality landscaped environments around buildings and on streets;
- (E) protects the recognisable character and locally significance sites throughout the precinct.
- (v) public spaces and the streetscape are enhanced through:
- (A) an increase in the quantity and quality of public land and places throughout the precinct;
- (B) consolidating community recreation and sporting uses to create a precinct of community focussed activity between Mudlo Street and Wharf Street;
- (C) improved connections between the town centre and the waterfront marina, including an investigation of a plaza on the waterfront;

- (D) improved streetscapes with high quality landscaping, surface treatments and shaded pedestrian environments;
 - (E) the creation of a sense of place through aesthetic streetscapes and built-form character;
 - (F) managing vegetation to ensure succession of planting and the ongoing presence of significant trees.
- (vi) advertising signage is small scale, low-key and complements the tropical character of the town.

Sub-precinct 1a – Town Centre sub-precinct

- (6) In addition to other overall development outcomes, development in the Town Centre sub-precinct facilitates the following development outcomes:
- (a) tourist, retail, dining and entertainment activities are facilitated at an appropriate pedestrian scale;
 - (b) drive-through developments, bulky goods showrooms, outdoor sales, saleyards and other big-box retailing or entertainment facilities are not established;
 - (c) development contributes to a high quality public realm;
 - (d) parking (and associated infrastructure) does not undermine the relationship between buildings and street or pedestrian circulation patterns;
 - (e) consolidation of community and cultural land use activities along Mowbray Street between Wharf Street and Mudlo Street;
 - (f) active street frontages are established along Macrossan and Wharf Streets and other nearby streets as shown on the Port Douglas Centre Active Frontages and Pedestrian and Cycle Network Plan;
 - (g) Live entertainment activities are concentrated within the Live Entertainment Precinct and are subject to the recommendations of a suitably qualified acoustic engineer.

Sub-precinct 1b - Waterfront North sub-precinct

- (7) In addition to other overall development outcomes, development in the Waterfront North sub-precinct facilitates the following development outcomes:
- (a) the precinct evolves as a revitalised open space and waterside development precinct;
 - (b) development within the precinct is designed to be sympathetic to the environmentally sensitive Dickson Inlet and mitigates any adverse impacts;
 - (c) the establishment of mixed-use development is facilitated to promote activity and vitality;
 - (d) public pedestrian access is maximised along the extent of the edge of the waterfront, consisting of a boardwalk or similar structure available for 24-hour use;
 - (e) development contributes to a high quality public realm;
 - (f) built form provides an attractive point of arrival from both land and sea;
 - (g) pedestrian connectivity is safe, efficient and provides for the needs of all users of the Port Douglas waterfront;

- (h) parking (and associated infrastructure) does not undermine the relationship between buildings and street or pedestrian circulation patterns;
- (i) the importance of existing marine-based industries to the area is recognised, not diminished and protected from incompatible uses. Relocation of marine based industries to an alternative precinct does not occur until such time that agreement has been reached among all relevant stakeholders such that development does not diminish the viability of marine based industrial uses that directly serve the Port Douglas tourist and fishing operators and private boat owners;
- (j) marine infrastructure is established to service the tourism, fishing and private boating community;
- (k) Live entertainment activities are concentrated within the Live Entertainment Precinct and are subject to the recommendations of a suitably qualified acoustic engineer;

T (l) the functionality of the Balley Hooley tourist rail is retained.

Sub-precinct 1c – Waterfront South sub-precinct

- (8) In addition to all other overall development outcomes, development in the Waterfront South sub-precinct facilitates the following development outcomes:
- (a) any use of land in the precinct does not affect the environmental, habitat, conservation or scenic values of Dickson Inlet and surrounding land;
 - (b) marine-based industries are established on appropriate land having regard to site suitability, accessibility, surrounding land uses, and location of utilities and services;
 - (c) marine-based industry achieves appropriate environmental standards;
 - (d) industrial buildings have a high standard of layout and building design;
 - (e) landscaping provides an attractive streetscape and screens utility, storage and car parking from the street and other public areas;
 - (f) the precinct is protected from encroachment of incompatible land use activities.

Sub-precinct 1d – Limited Development sub-precinct

- (9) In addition to all other overall development outcomes, development in the Limited Development sub-precinct facilitates the following development outcomes:
- (a) any use of land in the precinct does not affect the environmental, habitat, conservation or scenic values of Dickson Inlet and surrounding land;
 - (b) the open nature and character of the precinct is retained maintaining view lines across the inlet;
 - (c) community and recreation land use activities are established that promote public access to the foreshore.

Sub-precinct 1e – Community and recreation sub-precinct

- (10) In addition to all other overall development outcomes, development in the Community and recreation sub-precinct facilitates the following development outcomes:
- (a) development for community uses, including sport and recreation is facilitated.
 - (b) sport and recreation activities predominantly involve outdoor activities;
 - (c) areas of natural vegetation are protected from further development;
 - (d) shade trees are increased, in appropriate locations, surrounding the sports fields.

Sub-precinct 1f – Flagstaff Hill sub-precinct

- (11) In addition to all other overall development outcomes, development in the Flagstaff Hill sub-precinct facilitates the following development outcomes:
- (a) development is not established where it results in detriment to the vegetated and scenic qualities of Flagstaff Hill;
 - (b) development minimises excavation and filling;
 - (c) buildings and other works are unobtrusive when viewed from vantage points in Port Douglas and are designed and constructed of colours and materials which complement the hill's vegetated state;
 - (d) views from public viewing points within the precinct are protected.

Precinct 2 – Integrated Resort precinct

- (12) In addition to the overall outcomes, development in the Integrated Resort precinct facilitates development in accordance with the *Integrated Development Resort Act, 1987*.

Editor's note – The development of land within this precinct is subject to the Integrated Development Resort Act 1987 (IDRA). Where a conflict exists between this planning scheme and the IDRA, the IDRA prevails.

Precinct 3 – Craiglie Commercial and Light Industry precinct

- (13) In addition to the overall outcomes, development in the Craiglie Commercial and Light Industry precinct facilitates the following overall outcomes:
- (a) development supports the tourism and marine industries in Port Douglas, along with the small-scale commercial and light industry land uses that support the local economy that would otherwise be better suited to a location outside the Port Douglas Centre Precinct unless they pose a safety issue;
 - (b) development adjacent to the Captain Cook Highway presents an attractive appearance to the highway. The rain-trees, melaleucas and eucalypt trees along the Captain Cook Highway are retained where possible, taking into account the Department of Transport and main Road's requirements;

- (c) retailing activities are generally restricted to those which are ancillary and necessarily associated with the primary service and light industry nature of the area;
- (d) adjacent residential areas are protected from industry nuisances;
- (e) lots fronting Downing Street, between Dickson Street and Beor Street, are provided with an appropriate standard of road access and infrastructure, prior to development occurring.

Precinct 4 – Old Port Road / Mitre Street precinct

- (14) In addition to the overall outcomes, development in the Old Port Road / Mitre Street precinct facilitates the following overall outcomes:
- (a) the precinct is intended to be used for outdoor recreational land use activity, primarily as a golf course;
 - (b) areas of significant vegetation are protected from development and retained;
 - (c) other forms of development will only be considered if substantial areas of open space are retained adjacent to existing residential areas to maintain the existing residential amenity of open views across open space.

Precinct 5 – Very Low Density Residential/Low Scale Recreation/Low Scale Educational/Low Scale Entertainment Uses precinct

- (15) In addition to the overall outcomes, development in the Very Low Residential Density/Low Scale Recreation/Low Scale Educational/Low Scale Entertainment Uses precinct facilitates the following overall outcomes:
- (a) residential accommodation does not exceed a maximum of 8.5 metres in building height;
 - (b) minimum lot sizes exceed 2 hectares;
 - (c) very low scale and intensity recreation/ very low scale and intensity educational/ and very low scale entertainment uses may be appropriate in areas of the precinct subject to erosion and other flooding constraints.

Note - Undeveloped lots in this precinct are located on very low-lying land. Council may consider a consolidation of existing land titles via lot reconfiguration to lot sizes less than 2 hectares, where the reconfigured lots are consolidated onto the highest terrain, to avoid a pattern of development consisting of dwelling houses located on isolated islands of raised building pads.

Criteria for assessment**Table 7.2.4.4.a –Port Douglas / Craiglie local plan – assessable development**

Performance outcomes	Acceptable outcomes	Applicant response
For self-assessable and assessable development		
Development in the Port Douglas / Craiglie local plan area generally		
P01 Pedestrians, cyclists, motorists and public transport users can easily move into and through the precinct along planned connectivity routes, identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.	AO1 A pedestrian and cycle movement network is integrated and delivered through development.	compliant
P02 Development retains and enhances key landscape elements including character trees and areas of significant vegetation contributing to the character and quality of the local plan area and significant views and vistas and other landmarks important to the context of Port Douglas / Craiglie (as identified on the Port Douglas/ Craiglie Townscape Plan map contained in Schedule 2).	AO2.1 Development provides for the retention and enhancement of existing mature trees and character vegetation that contribute to the lush tropical character of the town, including: <ul style="list-style-type: none"> (a) the tree covered backdrop of Flagstaff Hill; (b) natural vegetation along watercourses, in particular the Mowbray River, Beor Creek and Dickson Inlet; (c) the tidal vegetation along the foreshore; (d) beachfront vegetation along Four Mile Beach, including the fringe of Coconut Palms; (e) the oil palm avenues along the major roads; (f) the lush landscaping within major roundabouts at key nodes; (g) Macrossan Street and Warner Street; (h) Port Douglas waterfront. 	compliant



Performance outcomes	Acceptable outcomes	Applicant response
	<p>AO2.2 Development protects and does not intrude into important views and vistas as identified on the Port Douglas Townscape Plan map contained in Schedule 2, in particular:</p> <ul style="list-style-type: none"> (a) Flagstaff Hill; (b) Four Mile Beach; (c) Across to the ranges over Dickson Inlet; (d) Mowbray Valley. <p>AO2.3 Important landmarks, memorials and monuments are retained.</p>	
<p>PO3 Development contributes to the protection, reinforcement and where necessary enhancement of gateways and key intersections identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2.</p>	<p>AO3 Development adjacent to the gateways and nodes as identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 incorporates architectural features and landscaping treatments and design elements that enhance the sense of arrival and way finding within the town.</p>	compliant
<p>PO4 Landscaping of development sites complements the existing tropical character of Port Douglas and Craiglie.</p>	<p>AO4 Landscaping incorporates the requirements of Planning scheme policy SC6.7 – Landscaping, in particular landscaping should be capable of achieving a 60% screening of development within 5 years and predominantly consists of endemic vegetation.</p>	compliant
<p>PO5 Development does not compromise the safety and efficiency of the State-controlled road network.</p>	<p>AO5 Direct access is not provided to a State-controlled road where legal and practical access from another road is available.</p>	compliant



Performance outcomes	Acceptable outcomes	Applicant response
For assessable development		
Additional requirements in Precinct 1 – Port Douglas precinct		
PO6 The views and vistas identified on the Port Douglas / Craiglie local plan maps contained in Schedule 2 are maintained.	AO6.1 Development does not impede continued views to scenic vistas and key streetscapes within the local plan area. AO6.2 Unless otherwise specified within this Local Plan, buildings are set back not less than 6 metres from the primary street frontage.	compliant
PO7 Vehicle access, parking and service areas: (a) do not undermine the relationship between buildings and street or dominate the streetscape; (b) are designed to minimise pedestrian vehicle conflict; (c) are clearly identified and maintain ease of access at all times.	AO7.1 For all buildings, parking is: (a) to the side of buildings and recessed behind the main building line; or (b) behind buildings; or (c) wrapped by the building façade, and not visible from the street. AO7.2 Ground level parking incorporates clearly defined pedestrian routes. AO7.3 Any porte-cocheres, disabled and pedestrian accesses are accommodated within the boundary of new or refurbished development. AO7.4 Where the development is an integrated mixed-use development incorporating short term accommodation or multiple dwellings and either food and drink outlet or hotel or shop or shopping centre or office, on-site parking spaces are provided as per the number prescribed in the Parking and access code with a relaxation of 30% of spaces required for the non-residential uses.	compliant



Performance outcomes	Acceptable outcomes	Applicant response
	<p>A07.5 On-site car parking available for public use is clearly signed at the site frontage.</p> <p>A07.6 Boom gates, pay machines or other regulatory devices to control access to a publicly available car parking area are not constructed or installed.</p>	
<p>PO8 Precinct 1 – Port Douglas precinct is not characterised by a proliferation of advertising signs.</p>	<p>A08 No acceptable outcomes are prescribed.</p>	compliant
Additional requirements for Sub-precinct 1a – Town Centre sub-precinct		
<p>PO9 Building heights:</p> <ul style="list-style-type: none"> (a) do not overwhelm or dominate the town centre; (b) respect the desired streetscape; (c) ensure a high quality appearance when viewed from both within the town centre sub-precinct and external to the town centre sub-precinct; (d) remain subservient to the natural environment and the backdrop of Flagstaff Hill. (e) do not exceed 3 storeys. 	<p>A09 Buildings and structures are not more than 3 storeys and 13.5 metres in height, with a roof height of not less than 3 metres.</p> <p>Note – Height is inclusive of the roof height.</p>	compliant
<p>PO10 Building design, the streetscape, pedestrian paths and street front spaces promote integration with the surrounding area and the rest of Precinct 1 – Port Douglas Precinct.</p>	<p>A010 No acceptable outcomes are prescribed.</p>	compliant



Performance outcomes	Acceptable outcomes	Applicant response
PO11 Buildings: (a) address street frontages; (b) ensure main entrances front the street or public spaces; (c) do not focus principally on internal spaces or parking areas.	AO11 No acceptable outcomes are prescribed.	compliant
PO12 Setbacks at ground level provide for: (a) connection between pedestrian paths and public places; (b) areas for convenient movement of pedestrians; (c) changes in gradient of the street.	AO12 Setbacks at ground level: (a) are clear of columns and other obstructions; (b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths.	compliant
AO13 Buildings do not result in a reduction of views and vistas from public places to: (a) Flagstaff Hill; (b) Dickson Inlet; (c) public open space; (d) places of significance.	AO13 No acceptable outcomes are prescribed.	compliant
PO14 Development enhances the distinctive tropical resort town and identity of Port Douglas and encourages pedestrian activity at street level including shade protection across the footpath for the length of the building.	AO14 Development is built up to the street frontage/s at the street level and incorporates a light frame awning, a minimum of 3 metres in width for the length of the street frontage/s; or If a development includes an outdoor dining area at ground/footpath level, the dining area has a maximum setback of 3 metres and the required awning is still maintained along the length of the street frontage/s. Note – PO24 provides more detail on awning design.	compliant



Performance outcomes	Acceptable outcomes	Applicant response
PO15 Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street-level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.	AO15.1 Centre activities establish: at street level on active street frontages; a maximum of one level above street level. AO15.2 Any residential development activities or short term accommodation is located above street level of the active frontage, but not on or up to the street frontage in any development, including mixed use development.	n/a
PO16 Detailed building design: (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements; (c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the lush, vegetated character of the Town Centre sub-precinct is maintained.	AO16 No acceptable outcomes are prescribed.	compliant
PO17 Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through: (a) surface decoration; (b) wall recesses and projections; (c) a variation in wall finishes; windows, balconies, awnings and other visible structural elements.	AO17 No acceptable outcomes are prescribed.	compliant



Performance outcomes	Acceptable outcomes	Applicant response
(d) differentiating between the lower, middle and upper parts of the building by varying the façade and/or the shape of the built form, where comprised of more than two storeys.		
P018 Roofs are not characterised by a cluttered display of plant and equipment, in particular: <ul style="list-style-type: none"> (a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Town Centre sub-precinct; (b) service structures, lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view; (c) rooftops are not used for advertising. 	AO18 No acceptable outcomes are prescribed.	compliant
P019 Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to: <ul style="list-style-type: none"> (a) shade windows; (b) reduce glare; (c) assist in maintaining comfortable indoor temperatures; (d) minimising heat loads; (e) enrich the North Queensland tropical character of the Town Centre sub-precinct; (f) provide architectural interest to building façades. 	AO19 No acceptable outcomes are prescribed.	compliant



Performance outcomes	Acceptable outcomes	Applicant response
PO20 Buildings are finished with high quality materials, selected for: <ul style="list-style-type: none"> (a) their ability to contribute the character of Town Centre sub-precinct; (b) easy maintenance, durability and an ability not to readily stain, discolour or deteriorate. 	AO20 No acceptable outcomes are prescribed	compliant
PO21 Buildings do not incorporate any type of glass or other materials that are likely to reflect the sun's rays in a manner that may create a nuisance, discomfort or a hazard.	AO21 No acceptable outcomes are prescribed.	compliant
PO22 Façades and elevations do not include large blank walls. Openings and setbacks are used to articulate vertical building surfaces.	AO22.1 Development has a maximum length of unbroken building facade of 20 metres and a maximum extent of overall development in the same style/design along the street frontage/s of 40 metres. AO22.2 Any break in the building façade varies the alignment by a 1 metre minimum deviation. AO22.3 A minimum of three of the following building design features and architectural elements detailed below are incorporated to break the extended facade of a development: <ul style="list-style-type: none"> (a) a change in roof profile; (b) a change in parapet coping; (c) a change in awning design; (d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development. 	compliant



Performance outcomes	Acceptable outcomes	Applicant response
PO23 Building facades that face public spaces at ground level: <ul style="list-style-type: none"> (a) complement the appearance of the development and surrounding streetscape; (b) enhance the visual amenity of the public place; (c) include a variety of human scale architectural elements and details; (d) provide an opportunity for the casual and convenient surveillance of public space from within the development. 	AO23 Building facades at the ground floor of development that face public space are designed to ensure: <ul style="list-style-type: none"> (a) a minimum of 70% of the façade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development; (b) a visually prominent main entrance that faces the principal public place; (c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the façade. 	compliant
PO24 Awnings for pedestrian shelter are consistent with the character setting of the Town Centre sub-precinct and: <ul style="list-style-type: none"> (a) extend and cover the footpath to provide protection from the sun and rain; (b) include lighting under the awning; (c) are continuous across the frontage of the site; (d) align to provide continuity with existing or future awnings on adjoining sites; (e) are a minimum of 3.0 metres in width and generally not more than 3.5 metres above pavement height; (f) do not extend past a vertical plane, 1.2 metres inside the kerb-line to enable street trees to be planted and grow; (g) are cantilevered from the main building with any posts within the footpath being non load-bearing. 	AO24 No acceptable outcomes are prescribed.	compliant



Performance outcomes	Acceptable outcomes	Applicant response
PO25 Development integrates with the streetscape and landscaping improvements for Port Douglas.	AO25 Development fronting Davidson Street, Macrossan Street, Wharf Street, Mowbray Street and Warner Street is designed to integrate with the on-street landscaping and design improvements as outlined within the Port Douglas landscape master plan contained within Planning scheme policy SC6.7 – Landscaping. Note - Planning scheme policy SC6.7 - Landscaping provides guidance on meeting the Performance Outcome.	compliant
Additional requirements for Sub-precinct 1b – Waterfront North sub-precinct		
PO26 The establishment of uses is consistent with the outcomes sought for sub-precinct 1b – Waterfront North.	AO26 Uses identified as inconsistent uses in Table 7.2.4.b – Inconsistent uses in sub-precinct 1b Waterfront North sub precinct are not established in sub-precinct 1b - Waterfront North.	n/a
PO27 The bulk and scale of buildings is consistent with surrounding development and steps down to complement the open space areas in the adjoining limited development sub-precinct.	AO27 Buildings and structures are not more than: (a) 3 storeys and 13.5 metres in height , with a roof height of not less than 3 metres, in those parts of the precinct south of Inlet Street; (b) 2 storeys and 8.5 metres in height, with a roof height of not less than 3 metres, in those parts of the precinct north of Inlet Street. Note – Height is inclusive of roof height.	n/a
PO28 Building design, streetscape, pedestrian paths and street front spaces promote integration with the surrounding area and the rest of Precinct 1 – Port Douglas Precinct	AO28 No acceptable outcomes are prescribed.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
PO29 Public pedestrian access along the water's edge is maximised.	AO29.1 Public pedestrian access is provided along the frontage of the water's edge consisting of a boardwalk of a minimum width of 4 metres that is available of 24-hour use. AO29.2 A public plaza is incorporated into the design generally reflecting the requirements of the Port Douglas Waterfront Master Plan, focussing in the vicinity of the 'Duck Pond'. AO29.3 Built envelopes are setback a minimum of 3.0 metres from the board walk, with a shelter/shade zone between the building envelopes and the boardwalk consisting of shade structure, canopies, verandahs and the like.	n/a
PO30 Buildings: (a) address street frontages; (b) ensure main entrances front the street or public spaces.	AO30 No acceptable outcomes are prescribed.	n/a
PO31 Setbacks at ground level provide for: (a) connection between pedestrian paths and public places; (b) areas for convenient movement of pedestrians; (c) changes in gradient.	AO31 Setbacks at ground level: (a) are clear of columns and other obstructions; (b) have pavement matching the gradient of adjoining footpaths and connecting pedestrian areas on adjoining sites; (c) connect without any lip or step to adjoining footpaths.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
PO32 Buildings do not result in a reduction of views and vistas from public places to: (a) Dickson Inlet; (b) public open space; (c) places of significance.	AO32 No acceptable outcomes are prescribed.	n/a
PO33 Development enhances the distinctive tropical resort town and identity of Port Douglas and encourages pedestrian activity at ground level including shade protection across the footpath and open space areas.	AO33 No acceptable outcomes are prescribed.	n/a
PO34 Development is predominantly commercial in nature with any tourist accommodation having a secondary focus and not located on the street-level frontage where active frontages are encouraged as identified the Port Douglas local plan maps contained in Schedule 2.	AO34.1 Centre activities establish: (a) at street level on active street frontages; (b) a maximum of one level above street level. AO34.2 Residential development activities or short term accommodation is located above street /ground floor level of the active frontage, but not on or up to the street / public frontage in any development, including mixed use development.	n/a
PO35 Detailed building design: (a) enhances the visual amenity of the streetscape; (b) has a legible and attractive built form that is visually enhanced by architectural elements; (c) contributes to a distinctive tropical north Queensland, seaside tourist town character; (d) integrates major landscaping elements to maximise their aesthetic value to ensure that the lush, vegetated character of the Waterfront North sub-precinct is maintained.	AO35 No acceptable outcomes are prescribed.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
PO36 Buildings exhibit variations to their external appearance and the shape of the built form to provide visual interest through: <ul style="list-style-type: none"> (a) surface decoration; (b) wall recesses and projections; (c) a variation in wall finishes; windows, balconies, awnings and other visible structural elements. (d) differentiating between the lower, middle and upper parts of the building by varying the façade and/or the shape of the built form, where comprised of more than two storeys. 	AO36 No acceptable outcomes are prescribed.	n/a
PO37 Roofs are not characterised by a cluttered display of plant and equipment, in particular: <ul style="list-style-type: none"> (a) building caps and rooftops contribute to the architectural distinction of the building and create a coherent roofscape for the Waterfront North sub-precinct; (b) service structures, lift motor rooms and mechanical plant and equipment are designed as an architectural feature of the building or are screened from public view; (c) rooftops are not used for advertising. 	AO37 No acceptable outcomes are prescribed.	n/a
PO38 Windows and sun/rain control devices are used in the building form, in particular, sun shading devices are provided to: <ul style="list-style-type: none"> (a) shade windows; (b) reduce glare; (c) assist in maintaining comfortable indoor temperatures; (d) minimising heat loads; 	AO38 No acceptable outcomes are prescribed.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
(e) enriching the North Queensland tropical character of the Waterfront North sub-precinct; (f) architectural interest to building façades.		
PO39 Buildings are finished with high quality materials, selected for: (a) their ability to contribute the character of Waterfront North sub-precinct; (b) easy maintenance, durability and an ability not to readily stain, discolour or deteriorate.	AO39 No acceptable outcomes are prescribed.	n/a
PO40 Buildings do not incorporate any type of glass or other materials that are likely to reflect the sun's rays in a manner that may create a nuisance, discomfort or a hazard.	AO40 No acceptable outcomes are prescribed.	n/a
PO41 Façades and elevations do not include large blank walls and openings and setbacks are used to articulate vertical building surfaces.	AO41.1 Development has a maximum length of unbroken building facade of 20 metres and a maximum extent of overall development in the same style/design along the street frontage/s of 40 metres. AO41.2 Any break in the building façade varies the alignment by a 1 metre minimum deviation. AO41.3 A minimum of three of the following building design features and architectural elements detailed below are incorporated to break the extended facade of a development: (a) a change in roof profile; (b) a change in parapet coping; (c) a change in awning design;	n/a

Performance outcomes	Acceptable outcomes	Applicant response
	(d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development	
PO42 Building facades that face public spaces at ground level: (a) complement the appearance of the development and surrounding streetscape; (b) enhance the visual amenity of the public place; (c) include a variety of human scale architectural elements and details; (d) provide an opportunity for the casual and convenient surveillance of public space from within the development.	AO42 Building facades at the ground floor of development that face public space are designed to ensure: (a) a minimum of 70% of the façade area is comprised of windows, wall openings or shop fronts that permit the casual surveillance of the public space from the development; (b) a visually prominent main entrance that faces the principal public place; (c) vertical architectural elements and features are incorporated at 3 metre or less intervals along the length of the façade.	n/a
PO43 Awnings for pedestrian shelter are consistent with the character setting of the Waterfront North sub-precinct and: (a) extend and cover the footpath to provide protection from the sun and rain; (b) include lighting under the awning; (c) are continuous across pedestrian circulation areas; (d) align to provide continuity with existing or future awnings on adjoining sites; (e) are a minimum of 3 metres in width and generally not more than 3.5 metres above pavement height; (f) do not extend past a vertical plane, 1.2 metres inside the street kerb-line to enable street trees to be planted and grow;	AO43 No acceptable outcomes are prescribed.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
(g) are cantilevered from the main building with any posts within the footpath being non load-bearing.		
PO44 The Balley Hooley rail line and turn-table is retained and incorporated into development and maintains its functionality.	AO44.1 Bally Hooley rail line and turn-table is retained and incorporated into development to maintain its functionality. AO44.2 Where development provides floor area for the Bally Hooley rail station, the gross floor area of the rail line and station does not generate a requirement for additional vehicle parking.	n/a
PO45 Development recognises the importance of and relationship between the marina, commercial and residential development in the Waterfront North sub-precinct, and includes measures to mitigate the impact of: (a) noise; (b) odour; (c) hazardous materials; (d) waste and recyclable material storage.	AO45 No acceptable outcomes are prescribed.	n/a
PO46 Formalised public spaces and pedestrian paths/areas on freehold land are made accessible to the public.	AO46 No acceptable outcomes are prescribed.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
PO47 Buildings, civic spaces, roads and pedestrian links are enhanced by: <ul style="list-style-type: none"> (a) appropriate landscape design and planting; (b) themed planting that defines entry points, and creates strong 'entry corridors' into the waterfront; (c) lighting and well-considered discrete signage that complements building and landscape design; (d) public artwork and other similar features that reflect the heritage and character of the Port Douglas Waterfront. 	AO47 No acceptable outcomes are prescribed.	n/a
PO48 Buildings are designed and sited to provide vistas along shared pedestrian/open space and movement areas in suitable locations.	AO48 No acceptable outcomes are prescribed.	n/a
PO49 Development does not diminish the viability of marine-based industrial uses that directly serve the Port Douglas tourist and fishing operators and private boat owners, particularly with respect to the slipway operation.	AO49 No acceptable outcomes are prescribed.	n/a
PO50 Marine infrastructure to service the tourism, fishing and private boating community is provided.	AO50 No acceptable outcomes are prescribed.	n/a
PO51 Changes to the Port Douglas Waterfront quay-line do not cause adverse impacts to the environmentally sensitive Dickson Inlet.	AO51 Development that results in changes to the Port Douglas Waterfront quay-line is only established where an Ecological assessment report provides support to the changes. Note - Planning scheme policy SC6.8 – Natural environment provides guidance on preparing an ecological assessment report.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
Additional requirements for Sub-precinct 1c – Waterfront South sub-precinct		
PO52 The establishment of uses is consistent with the outcomes sought for Precinct 1c – Waterfront South.	AO52 Uses identified as inconsistent uses in Table 7.2.4.4.c are not established in Precinct 1c – Waterfront South.	n/a
PO53 Development does not adversely impact on the natural environment, natural vegetation or watercourses.	AO53.1 An Ecological assessment report is prepared identifying the environmental qualities of the surrounding natural and built features which are to be managed. Note - Planning scheme policy SC6.8 – Natural environment provides guidance on preparing an ecological assessment report. AO53.2 An Environmental Management Plan is prepared to manage potential impacts of the operation of the development on surrounding natural areas. Note - Planning scheme policy SC6.4 – Environmental management plans contains information to demonstrate compliance and guidance on preparing an Environmental Management Plan.	n/a
PO54 Development of land at the end of Port Street adjacent to Dickson Inlet incorporates a slipway, or an alternative functioning facility, with capacity to service the Port Douglas marine and tourism industry.	AO54 A master plan for the development is provided and implemented to demonstrate the integration of the slipway, or an alternative functioning facility, with other supporting service industry activities that service the marine and tourism industry of Port Douglas.	n/a
PO55 Buildings and structures are of a height, and are set back from side boundaries and other sensitive areas to ensure the scenic amenity and environmental qualities of the adjacent area are not adversely affected.	AO55.1 Development has a height of not more than 10 metres. AO55.2 Development is setback from all property boundaries not less than 3 metres.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
PO56 The site coverage of all buildings and structures ensures development: <ul style="list-style-type: none"> (a) is sited in an existing cleared area or in an area approved for clearing; (b) has sufficient area for the provision of services; (c) development does not have an adverse effect on the environmental, habitat, conservation or landscape values of the on-site and surrounding sensitive areas. 	AO56 No acceptable outcomes are prescribed.	n/a
PO57 Premises include adequate provision for service vehicles, to cater for generated demand. Loading areas for service vehicles are designed to: <ul style="list-style-type: none"> (a) be accommodated on-site; (b) maximise safety and efficiency of loading; (c) protect the visual and acoustic amenity of sensitive land use activities; (d) minimise adverse impacts on natural characteristics of adjacent areas. 	AO57.1 Sufficient manoeuvring area is provided on-site to allow a Medium Rigid Vehicle to enter and leave the site in a forward gear. AO57.2 Development is designed to ensure all service vehicles are contained within the site when being loaded/unloaded. AO57.3 Driveways, parking and manoeuvring areas are constructed and maintained to: <ul style="list-style-type: none"> (a) minimise erosion from storm water runoff; (b) retain all existing vegetation. 	n/a
PO58 Development ensures adverse impacts from service vehicles on the road network, external to the site, are minimised.	AO58 No acceptable outcomes are prescribed.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
PO59 Entry to the site is landscaped to enhance the amenity of the area and provide a pleasant working environment.	AO59 Areas used for loading and unloading, storage, utilities and car parking are screened from public view: (a) by a combination of landscaping and screen fencing; (b) dense planting along any road frontage is a minimum width of 3 metres.	n/a
PO60 Landscaping is informal in character and complementary to the existing natural environment, provides screening and enhances the visual appearance of the development.	AO60 For any development landscaping is in accordance with the Plant species schedule in Planning scheme policy SC6.7– Landscaping.	n/a
Additional requirements for Sub-precinct 1d – Limited Development sub-precinct		
PO61 The height of buildings and structures contributes to the desired form and outcomes for the sub-precinct and are limited to a single storey.	AO61 Buildings and structures are not more than one storey and 4 metres in height. Note - Height is inclusive of the roof height.	n/a
Additional requirements for Sub-precinct 1e – Community and recreation sub-precinct		
PO62 The precinct is developed for organised sporting activities and other community uses.	AO62 No acceptable outcomes are prescribed.	n/a
Additional requirements for Sub-precinct 1f – Flagstaff Hill sub-precinct		
PO63 Flagstaff Hill is protected from inappropriate development to protect the hill as an important natural landmark feature of Port Douglas and as a vegetated backdrop to the Town centre.	AO63 No acceptable outcomes are prescribed	n/a



Performance outcomes	Acceptable outcomes	Applicant response
PO64 All development on Flagstaff Hill is designed to minimise the visibility of the development and to ensure development is subservient to the natural landscape and topography of the site, including through: <ul style="list-style-type: none"> (a) building design which minimises excavation and filling; (b) buildings being designed to step down the site and incorporate foundations and footings on piers or poles; (c) buildings being visually unobtrusive and incorporating exterior finishes and muted colours which are non-reflective and complement the colours of the surrounding vegetation and view-shed; (d) protection of the views from public viewing points in the Port Douglas precinct. 	AO64 No acceptable outcomes are prescribed.	n/a
Additional requirements for Precinct 3 – Craiglie Commercial and Light Industry precinct		
PO65 Development supports the tourism and marine industries in Port Douglas, along with the small-scale commercial and light industry land uses that support the local economy that would otherwise be better suited to a location outside the Port Douglas Town Centre Precinct.	AO65 Development consists of service and light industries and associated small scale commercial activities.	n/a
PO66 Development on lots adjacent to the Captain Cook Highway is sited, designed and landscaped to provide an attractive visual approach to Port Douglas with all buildings, structures and car parking areas setback a sufficient distance from the frontage to enable landscaping to soften or screen the appearance of the development.	AO66.1 Buildings and structures are setback 8 metres from the Captain Cook Highway frontage, or no closer to the Captain Cook Highway frontage than buildings and structures on adjoining sites (averaged), whichever is the greater.	n/a



Performance outcomes	Acceptable outcomes	Applicant response
	<p>AO66.2 The setback area to the Captain Cook Highway frontage is landscaped with advanced dense planting including tree species (100 litre bag stock), which will, at maturity, exceed the height of the building(s) on the site.</p> <p>AO66.3 Advertising signs are discreet in appearance with no large advertising signs, including tenancy signs, located on or near the Captain Cook Highway frontage, or within any landscaped setback area.</p> <p>AO66.4 Car parking areas, loading and other service areas are designed to be screened from the Captain Cook Highway and are located so as to not be visually prominent from the Captain Cook Highway.</p>	
Additional requirements for Precinct 6 – Very Low Residential Density / Low Scale Recreation / Low Scale Educational / Low Scale Entertainment Uses precinct		
<p>PO67 No additional lots are created within the precinct.</p>	<p>AO67 No acceptable outcomes are prescribed.</p>	n/a
<p>PO68 Reconfigured lots have a minimum lot size of 2 hectares, unless the lot reconfiguration transfers lots to the higher parts of the land, to avoid the need to fill existing lots to accommodate dwelling houses.</p>	<p>AO68 No acceptable outcomes are prescribed.</p>	n/a

Table 7.2.4.4.b – Inconsistent uses in sub-precinct 1b – Waterfront North sub-precinct

Inconsistent uses		
<ul style="list-style-type: none">• Agricultural supplies store• Air services• Animal husbandry• Animal keeping• Aquaculture• Brothel• Bulk landscape supplies• Car wash• Cemetery• Crematorium• Cropping• Detention facility• Dual occupancy• Dwelling house	<ul style="list-style-type: none">• Extractive industry• Funeral parlour• High impact industry• Intensive animal industry• Intensive horticulture• Major electricity infrastructure• Major sport, recreation and entertainment facility• Medium impact industry• Motor sport facility,• Outstation• Permanent plantation	<ul style="list-style-type: none">• Relocatable home park• Roadside stall• Rural industry• Rural workers accommodation• Service station• Showroom• Special industry• Tourist park• Transport depot• Veterinary services• Warehouse• Wholesale nursery• Winery

Table 7.2.4.4.c – Inconsistent uses in sub-precinct 1c – Waterfront South sub-precinct

Inconsistent uses		
<ul style="list-style-type: none"> • Adult store • Agricultural supplies store • Air services • Animal husbandry • Animal keeping • Brothel • Bulk landscape supplies • Car wash • Cemetery • Child care centre • Community care centre • Community residence • Community use • Crematorium • Cropping • Detention facility • Dual occupancy • Dwelling house • Dwelling unit • Extractive industry • Function facility • Funeral parlour • Garden centre 	<ul style="list-style-type: none"> • Hardware and trade supplies • Health care services • Home based business • Hospital • Hotel • Indoor sport and recreation • Intensive animal industry • Intensive horticulture • Major electricity infrastructure • Major sport, recreation and entertainment facility • Market • Motor sport facility • Multiple dwelling • Nature-based tourism • Nightclub entertainment facility • Outdoor sales • Outdoor sport and recreation • Outstation 	<ul style="list-style-type: none"> • Permanent plantation • Place of worship • Relocatable home park • Residential care facility • Resort complex • Retirement facility • Roadside stall • Rooming accommodation • Rural industry • Rural workers accommodation • Sales office • Shopping centre • Short-term accommodation • Showroom • Special industry • Theatre • Tourist attraction • Tourist park • Transport depot • Veterinary services • Warehouse • Wholesale nursery • Winery

Note –

Table 7.2.4.4.b – Inconsistent uses in sub-precinct 1b – Waterfront North sub-precinct or

Table 7.2.4.4.c – Inconsistent uses in sub-precinct 1c – Waterfront South sub-precinct do not imply that all other uses not listed in the table are automatically consistent uses within the zone. Assessable development must still demonstrate consistency through the assessment process.

8.2.10 Transport network overlay code

8.2.10.1 Application

- (1) This code applies to assessing a material change of use, reconfiguring a lot, operational work or building work within the Transport network overlay; if:
 - (a) self-assessable or assessable development where the code is identified as being applicable in the Assessment criteria for the Overlay Codes contained in the Levels of Assessment Tables in section 5.6;
 - (b) impact assessable development.
- (2) Land within the Transport network overlay is identified on the Transport network (Road Hierarchy) overlay map and the Transport network (Pedestrian and Cycle) overlay map in Schedule 2 and includes the following sub-categories:
 - (a) Transport network (Road Hierarchy) overlay sub-categories:
 - (i) State controlled road sub-category;
 - (ii) Sub-arterial road sub-category;
 - (iii) Collector road sub-category;
 - (iv) Access road sub-category;
 - (v) Industrial road sub-category;
 - (vi) Major rural road sub-category;
 - (vii) Minor rural road sub-category;
 - (viii) Unformed road sub-category;
 - (ix) Major transport corridor buffer area sub-category.
 - (b) Transport network (Pedestrian and Cycle) overlay sub-categories:
 - (i) Principal route;
 - (ii) Future principal route;
 - (iii) District route;
 - (iv) Neighbourhood route;
 - (v) Strategic investigation route.

8.2.10.2 Purpose

- (1) The purpose of the Transport network overlay code is to:
 - (a) implement the policy direction of the Strategic Framework, in particular:
 - (i) Theme 1: Settlement pattern Element 3.4.2 Urban settlement, Element 3.4.3 Activity centres;
 - (ii) Theme 6: Infrastructure and transport Element 3.9.4 Transport;
 - (b) enable an assessment of whether development is suitable on land within the Transport network overlay.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) development provides for transport infrastructure (including active transport infrastructure);
 - (b) development contributes to a safe and efficient transport network;
 - (c) development supports the existing and future role and function of the transport network;
 - (d) development does not compromise the safety and efficiency of major transport infrastructure and facilities.

Criteria for assessment**Table 8.2.10.3 a – Transport network overlay code – assessable development**

Performance outcomes	Acceptable outcomes	Applicant response
For assessable development		
PO1 Development supports the road hierarchy for the region. Note -A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.	AO1.1 Development is compatible with the intended role and function of the transport network as identified on the Transport network overlay maps contained in Schedule 2. AO1.2 Development does not compromise the safety and efficiency of the transport network.	Compliant



Performance outcomes	Acceptable outcomes	Applicant response
	AO1.3 Development is designed to provide access via the lowest order road, where legal and practicable access can be provided to that road.	Compliant
PO2 Transport infrastructure is provided in an integrated and timely manner. Note - A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.	AO2 Development provides infrastructure (including improvements to existing infrastructure) in accordance with: (a) the Transport network overlay maps contained in Schedule 2; (b) any relevant Local Plan. Note – The Translink Public Transport Infrastructure Manual provides guidance on the design of public transport facilities.	Compliant
PO3 Development involving sensitive land uses within a major transport corridor buffer area is located, designed and maintained to avoid or mitigate adverse impacts on amenity for the sensitive land use.	AO3 No acceptable outcomes are prescribed. Note – Part 4.4 of the Queensland Development Code provides requirements for residential building design in a designated transport noise corridor.	Compliant
PO4 Development does not compromise the intended role and function or safety and efficiency of major transport corridors. Note - A Traffic impact assessment report prepared in accordance with Planning scheme policy SC6.10 - Parking and access is one way to demonstrate achievement of the Performance Outcomes.	AO4.1 Development is compatible with the role and function (including the future role and function) of major transport corridors. AO4.2 Direct access is not provided to a major transport corridor where legal and practical access from another road is available.	Compliant



Performance outcomes	Acceptable outcomes	Applicant response
	<p>AO4.3 Intersection and access points associated with major transport corridors are located in accordance with: (a) the Transport network overlay maps contained in Schedule 2; and (b) any relevant Local Plan.</p> <p>AO4.4 The layout of development and the design of the associated access is compatible with existing and future boundaries of the major transport corridor or major transport facility.</p>	
<p>PO5 Development retains and enhances existing vegetation between a development and a major transport corridor, so as to provide screening to potential noise, dust, odour and visual impacts emanating from the corridor.</p>	<p>AO5 No acceptable outcomes are prescribed.</p>	Compliant
Pedestrian and cycle network		
<p>PO6 Lot reconfiguration assists in the implementation of the pedestrian and cycle movement network to achieve safe, attractive and efficient pedestrian and cycle networks</p>	<p>AO6.1 Where a lot is subject to, or adjacent to an element of the pedestrian and cycle Movement network (identified on the Transport network overlay maps contained in Schedule 2) the specific location of this element of the pedestrian and cycle network is incorporated in the design of the lot layout.</p> <p>AO6.2 The element of the pedestrian and cycle network is constructed in accordance with the Design Guidelines set out in Sections D4 and D5 of the Planning scheme policy SC6.5 – FNQROC Regional Development Manual.</p>	N/A