

SARA reference: 2604-51886 SRA
 Council reference: CA 2025_5880/1
 Applicant reference: 2025-09-82 - Seymour Group

26 May 2026

Chief Executive Officer
 Douglas Shire Council
 PO Box 723
 Mossman QLD 4873
 enquiries@douglas.qld.gov.au

Attention: Neil Beck

Dear Sir/Madam

SARA referral agency response – 71-85 Port Douglas Road, Port Douglas

(Referral agency response given under section 56 of the *Planning Act 2016*)

The development application described below was confirmed as properly referred by the State Assessment and Referral Agency (SARA) on 21 April 2026.

Response

Outcome:	Referral agency response – with conditions	
Date of response:	26 May 2026	
Conditions:	The conditions in Attachment 1 must be attached to any development approval	
Advice:	Advice to the applicant is in Attachment 2	
Reasons:	The reasons for the referral agency response are in Attachment 3	

Development details

Description:	Development permit	Reconfiguring a lot (1 lot into 40 lots and common property)
	Preliminary approval	Material change of use for use rights consistent with the low-medium density residential zone and to vary the effect of the planning scheme by amending the table of assessment for short term

accommodation

SARA role:	Referral agency
SARA trigger:	Schedule 10, Part 9, Division 4, Subdivision 2, Table 1, Item 1 (Planning Regulation 2017) – Reconfiguring a lot near a State transport corridor Schedule 10, Part 9, Division 4, Subdivision 2, Table 4, Item 1 (Planning Regulation 2017) – Material change of use of premises near a State transport corridor
SARA reference:	2604-51886 SRA
Assessment manager:	Douglas Shire Council
Street address:	71-85 Port Douglas Road, Port Douglas
Real property description:	Lot 1 on SP150468
Applicant name:	Seymour Land Pty Ltd
Applicant contact details:	C/- Aspire Town Planning and Project Services PO Box 1040 Mossman QLD 4873 admin@aspireqld.com
State-controlled road access permit:	This referral included an application for a road access location, under section 62A(2) of <i>Transport Infrastructure Act 1994</i> . Below are the details of the decision: <ul style="list-style-type: none"> • Approved • Reference: TMR26-049812 • Date: 25 May 2026 <p>If you are seeking further information on the road access permit, please contact the Department of Transport and Main Roads (DTMR) at Far.North.Queensland.IDAS@tmr.qld.gov.au.</p>
<i>Human Rights Act 2019</i> considerations:	A consideration of the 23 fundamental human rights protected under the <i>Human Rights Act 2019</i> has been undertaken as part of this decision. It has been determined that this decision does not limit human rights.

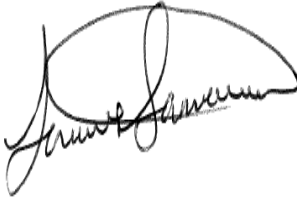
Representations

An applicant may make representations to a concurrence agency, at any time before the application is decided, about changing a matter in the referral agency response (s.30 Development Assessment Rules). Copies of the relevant provisions are in **Attachment 4**.

A copy of this response has been sent to the applicant for their information.

For further information please contact Charlton Best, Senior Planning Officer, on 07 4037 3200 or via email CairnsSARA@dasilgp.qld.gov.au who will be pleased to assist.

Yours sincerely

A handwritten signature in black ink, appearing to read 'Javier Samanes', written in a cursive style.

Javier Samanes
Manager

cc Seymour Land Pty Ltd c/- Aspire Town Planning and Project Services, admin@aspireqld.com

enc Attachment 1 - Referral agency conditions
Attachment 2 - Advice to the applicant
Attachment 3 - Reasons for referral agency response
Attachment 4 - Representations about a referral agency response provisions
Attachment 5 - Documents referenced in conditions

Attachment 1—Referral agency conditions

(Under section 56(1)(b)(i) of the *Planning Act 2016* the following conditions must be attached to any development approval relating to this application) (Copies of the documents referenced below are found at Attachment 5)

No.	Conditions	Condition timing
<p>Schedule 10, Part 9, Division 4, Subdivision 2, Table 1, Item 1 – Reconfiguring a lot near a State transport corridor and Schedule 10, Part 9, Division 4, Subdivision 2, Table 4, Item 1 – Material change of use of premises near a State transport corridor – The chief executive administering the <i>Planning Act 2016</i> nominates the Director-General of the Department of Transport and Main Roads to be the enforcement authority for the development to which this development approval relates for the administration and enforcement of any matter relating to the following conditions:</p>		
1.	<p>Stormwater infrastructure works must be undertaken generally in accordance with the following plans:</p> <p>(a) Preliminary Stormwater Drainage Catchment Plan prepared by Bourn Engineers, dated 18.12.2025, Drawing DA400, Revision 1.</p> <p>(b) Preliminary Detention Basin Layout Plan prepared by Bourn Engineers, dated 18.12.2025, Drawing DA401, Revision 1.</p>	<p>Prior to submitting the Plan of Survey to the local government for approval.</p>
2.	<p>(a) Construct noise barriers, 1.8m and 2.0m in height including returns, at the locations shown in Figure 7-1 Recommended Noise Barriers (Map 7) of the Road Traffic Noise Intrusion Assessment Report prepared by SLR, dated 19 December 2025, Reference 620.042901.00001 and Revision 1.0.</p> <p>(b) Design the noise barrier generally in accordance with:</p> <ul style="list-style-type: none"> • Section 7.2 Noise Barriers and Section 8.0 Conclusion of the Road Traffic Noise Intrusion Assessment Report prepared by SLR, dated 19 December 2025, Reference 620.042901.00001 and Revision 1.0 and the following standards: <ul style="list-style-type: none"> o the Department of Transport and Main Roads' Road Traffic Noise Management Code of Practice, Volume 1, Chapter 7 and technical Specification MRTS15; o the Department of Transport and Main Roads' technical Specifications MRTS15 Noise Fences (as amended by QR-CTS-Part 41); and o the Department of Transport and Main Roads' Standard Drawings. <p>(c) RPEQ certification must be provided to Cairns Corridor Management Unit (Far.North.Queensland.IDAS@tmr.qld.gov.au) within the Department of Transport and Main Roads, confirming that the development has been constructed in accordance with parts (a) and (b) of this condition.</p>	<p>(a) – (c): Prior to submitting the Plan of Survey to the local government for approval and to be maintained at all times.</p>

3.	<p>(a) Road accesses are located generally in accordance with Preliminary Roadworks and Drainage Layout Plan prepared by Bourn Engineers, dated 18.12.25, Reference DA200 and Revision 1, as amended in red by SARA.</p> <p>(b) Provide road access works comprising of a sealed two-way road access driveway with an appropriate width and sealed access crossover (service access) at the road access locations, referred to in part (a) of this condition generally in accordance TMR Layout Plan (6504 – 2.50km), prepared by Queensland Government Transport and Main Roads, dated 14/05/2026, Reference TMR25-049812, Issue C.</p> <p>(c) Design and construct the road access driveway works including the service access, referred to in part (b) of this condition, generally in accordance with:</p> <ul style="list-style-type: none"> • Volume 3 – Guide to Road Design of the Department of Transport and Main Roads’ Road Planning and Design Manual, 2nd Edition, September 2025; • Far North Queensland Regional Organisation of Councils (FNQROC) Standard Drawing S1005 – Typical Road Cross Sections Type 1, Type 2 & Type 3 dated 27/08/20, Revision E; and • Far North Queensland Regional Organisation of Councils (FNQROC) Standard Drawing S1015 – Access Crossovers, dated 05/12/23, Revision F. 	<p>(a) At all times.</p> <p>(b) and (c): Prior to submitting the Plan of Survey to the local government for approval.</p>
4.	Direct access is not permitted between Port Douglas Road and the subject site.	At all times.
5.	<p>(a) Close and remove the existing vehicular property access located between Lot 1 on SP150468 and Port Douglas Road (Davidson Street) as shown on TMR Layout Plan (6504 – 2.50km), prepared by Queensland Government Transport and Main Roads, dated 14/05/2026, Reference TMR25-049812, Issue C.</p> <p>(b) The road works to close and remove access, referred to in part (a) of this condition, must be in accordance with the Department of Transport and Main Roads’ Road Planning and Design Manual, 2nd Edition. The road works must:</p> <ul style="list-style-type: none"> (i) remove any driveway crossover and any drainage structure/s beneath the driveway; (ii) revegetate between the edge of the guardrail and the property; and (iii) establish a pedestrian pathway. 	(a) and (b): Prior to submitting the Plan of Survey to the local government for approval.

Attachment 2—Advice to the applicant

General advice	
1.	<p>Terms and phrases used in this document are defined in the <i>Planning Act 2016</i>, its regulation or the State Development Assessment Provisions (SDAP) (version 3.5). If a word remains undefined it has its ordinary meaning.</p>
Further development permits required	
2.	<p>Road Works Approval</p> <p>The proposed development will be required to extend and widen the existing service road (driveway) to the proposed access location within the Port Douglas Road corridor, a state-controlled road.</p> <p>Under section 33 of the <i>Transport Infrastructure Act 1994</i>, written approval is required from the Department of Transport and Main Roads to carry out road works on a state-controlled road.</p> <p>Please contact the Cairns district office of the Department of Transport and Main Roads by email at Far.North.Queensland.IDAS@tmr.qld.gov.au to make an application for road works approval.</p> <p>This approval must be obtained prior to commencing any works on the state-controlled road reserve. The approval process may require the approval of engineering designs of the proposed works, certified by a Registered Professional Engineer of Queensland (RPEQ).</p> <p>Please contact the Department of Transport and Main Roads as soon as possible to ensure that gaining approval does not delay construction.</p>
3.	<p>Road Corridor Permit – Landscaping</p> <p>The development is proposing to undertake landscaping / planting works including an entry statement feature within the Port Douglas Road corridor, a state-controlled road.</p> <p>An application for a Road Corridor Permit is required for any ancillary works and encroachments on the state-controlled road under section 50(2) and Schedule 6 of the <i>Transport Infrastructure Act 1994</i> and Part 5 and Schedule 1 of the Transport Infrastructure (State-Controlled Roads) Regulation 2006.</p> <p>Please contact the Cairns district office of the Department of Transport and Main Roads by email at Far.North.Queensland.IDAS@tmr.qld.gov.au to make an application and obtain a road corridor permit.</p> <p>Ancillary works and encroachments include but are not limited to advertising signs or other advertising devices, paths or bikeways, buildings/shelters/structures, vegetation clearing, landscaping and planting.</p>

Attachment 3—Reasons for referral agency response

(Given under section 56(7) of the *Planning Act 2016*)

The reasons for the SARA's decision are:

SARA assessed the development against the following code of the State Development Assessment Provisions (SDAP), version 3.5:

- State code 1: Development in a state-controlled road environment (State code 1)

The development can be conditioned to comply with the assessment benchmarks of State code 1 of SDAP in that the development:

- does not increase the likelihood or frequency of accidents, fatalities or serious injury for users of a state-controlled road
- does not adversely impact the function and efficiency of state-controlled roads or future state-controlled roads
- does not adversely impact the state's ability to plan, construct, maintain, upgrade or operate state-controlled roads
- does not significantly increase the cost to the state to plan, construct, upgrade or maintain state-controlled roads.

Material used in the assessment of the application:

- the development application material and submitted plans
- *Planning Act 2016*
- Planning Regulation 2017
- the State Development Assessment Provisions (version 3.5)
- the Development Assessment Rules
- SARA DA Mapping system
- *Human Rights Act 2019*.

Attachment 4—Representations about a referral agency response provisions

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Attachment 5—Documents referenced in conditions

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Development Assessment Rules—Representations about a referral agency response

The following provisions are those set out in sections 28 and 30 of the Development Assessment Rules¹ regarding **representations about a referral agency response**

Part 6: Changes to the application and referral agency responses

28 Concurrence agency changes its response or gives a late response

- 28.1. Despite part 2, a concurrence agency may, after its referral agency assessment period and any further period agreed ends, change its referral agency response or give a late referral agency response before the application is decided, subject to section 28.2 and 28.3.
- 28.2. A concurrence agency may change its referral agency response at any time before the application is decided if—
- (a) the change is in response to a change which the assessment manager is satisfied is a change under section 26.1; or
 - (b) the Minister has given the concurrence agency a direction under section 99 of the Act; or
 - (c) the applicant has given written agreement to the change to the referral agency response.²
- 28.3. A concurrence agency may give a late referral agency response before the application is decided, if the applicant has given written agreement to the late referral agency response.
- 28.4. If a concurrence agency proposes to change its referral agency response under section 28.2(a), the concurrence agency must—
- (a) give notice of its intention to change its referral agency response to the assessment manager and a copy to the applicant within 5 days of receiving notice of the change under section 25.1; and
 - (b) the concurrence agency has 10 days from the day of giving notice under paragraph (a), or a further period agreed between the applicant and the concurrence agency, to give an amended referral agency response to the assessment manager and a copy to the applicant.

¹ Pursuant to Section 68 of the *Planning Act 2016*

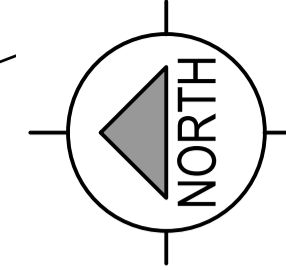
² In the instance an applicant has made representations to the concurrence agency under section 30, and the concurrence agency agrees to make the change included in the representations, section 28.2(c) is taken to have been satisfied.

Part 7: Miscellaneous

30 Representations about a referral agency response

30.1. An applicant may make representations to a concurrence agency at any time before the application is decided, about changing a matter in the referral agency response.³

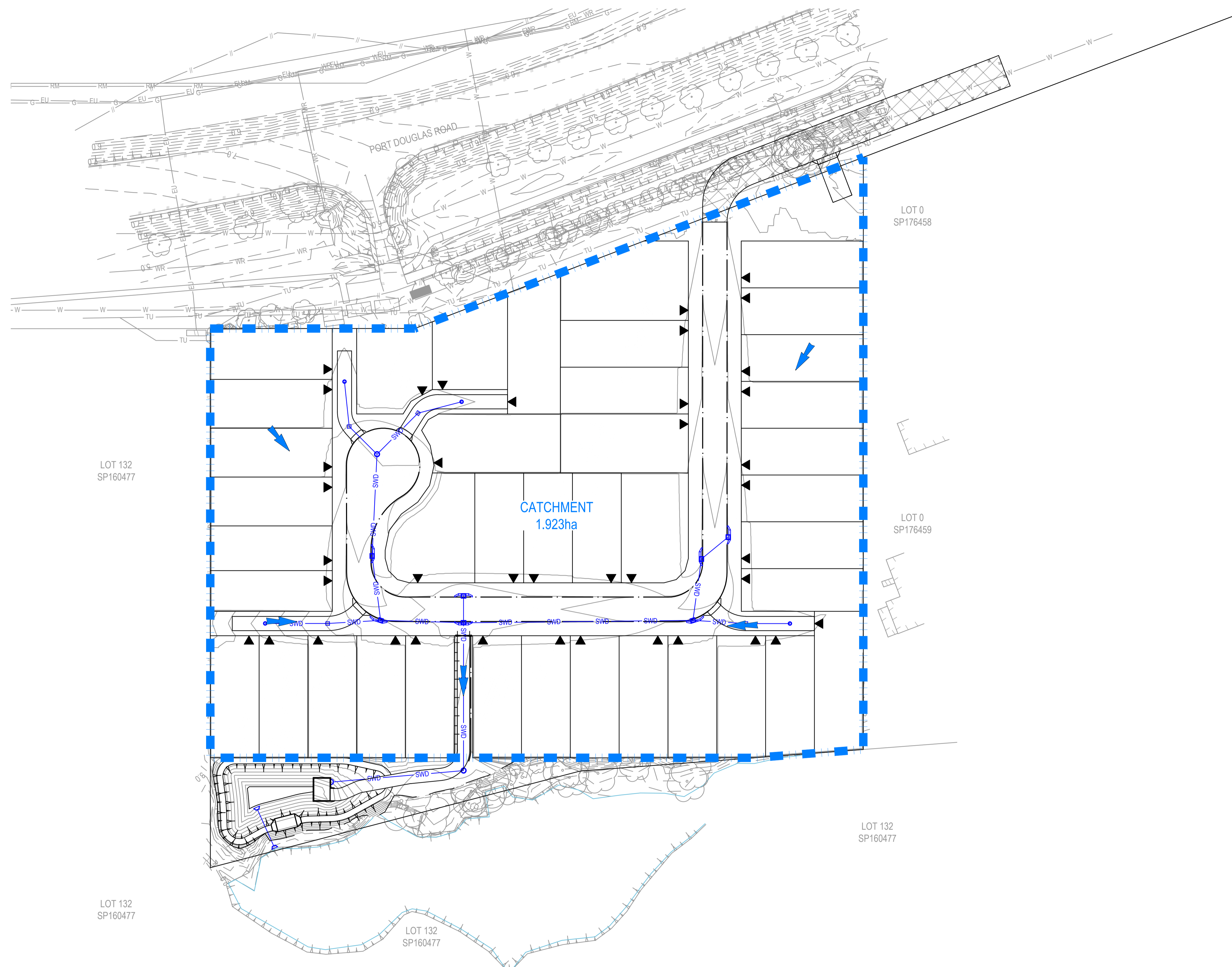
³ An applicant may elect, under section 32, to stop the assessment manager's decision period in which to take this action. If a concurrence agency wishes to amend their response in relation to representations made under this section, they must do so in accordance with section 28.



PLANS AND DOCUMENTS
referred to in the REFERRAL
AGENCY RESPONSE

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Date: 26 May 2026

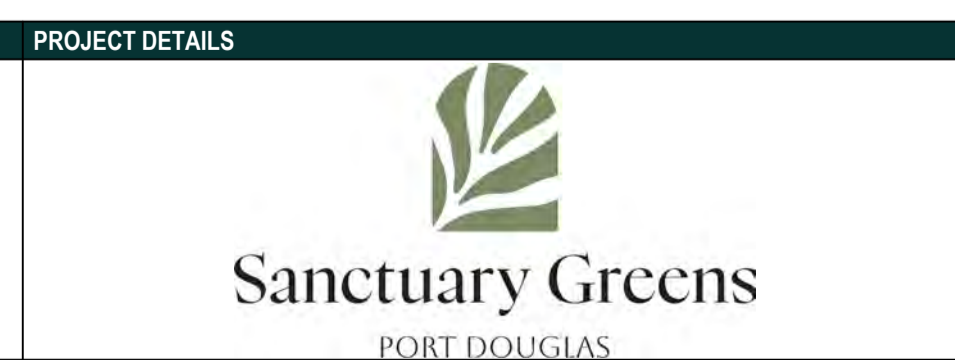
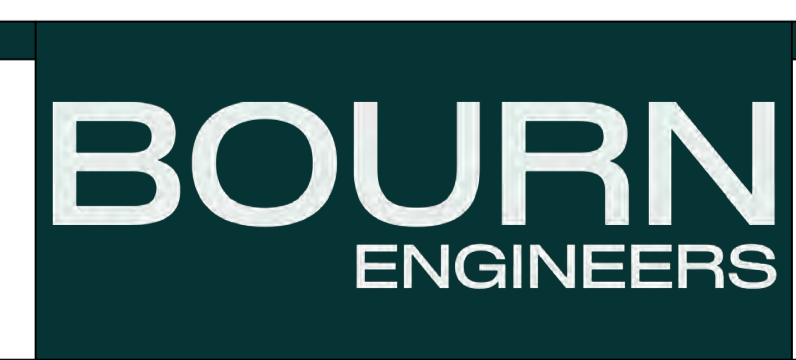
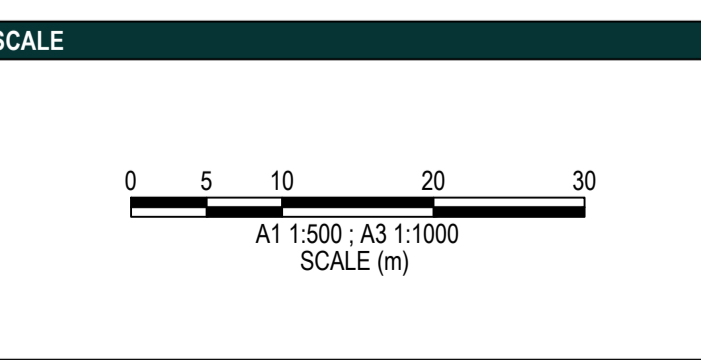


NOTE:
REFER DRAWING BE25041-001-DA200 FOR LEGEND

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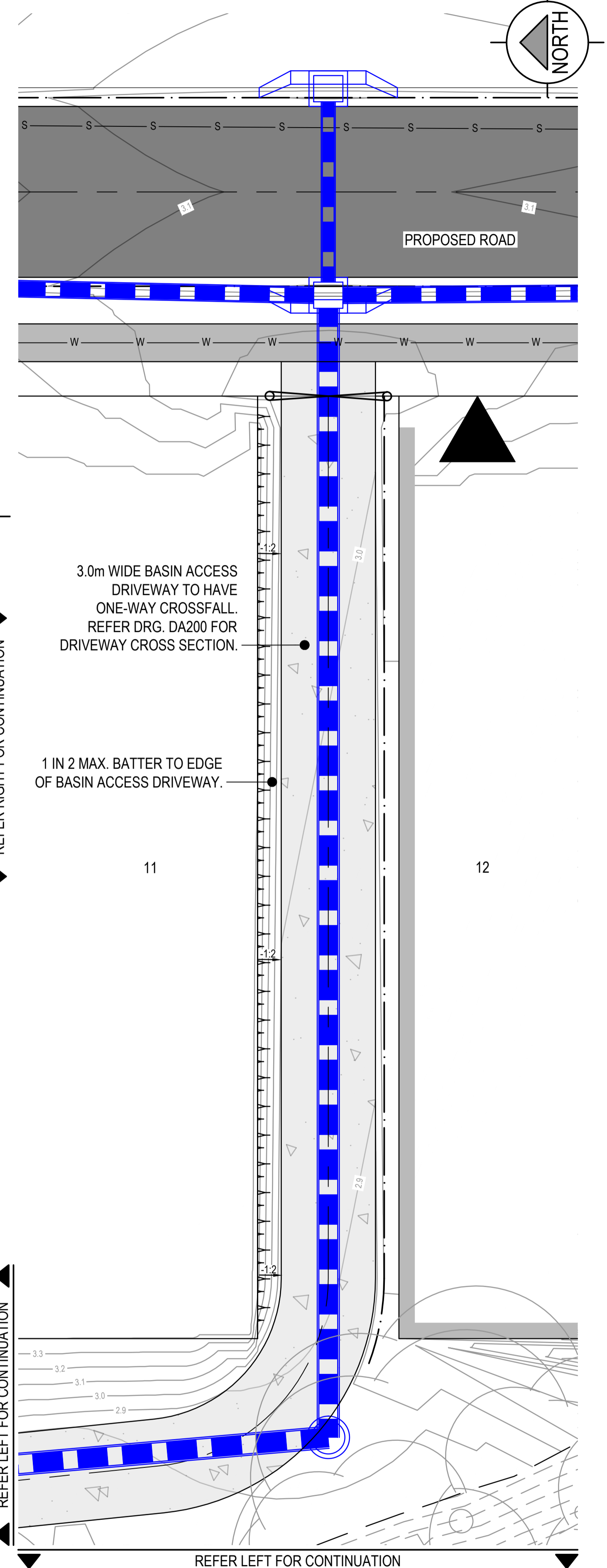
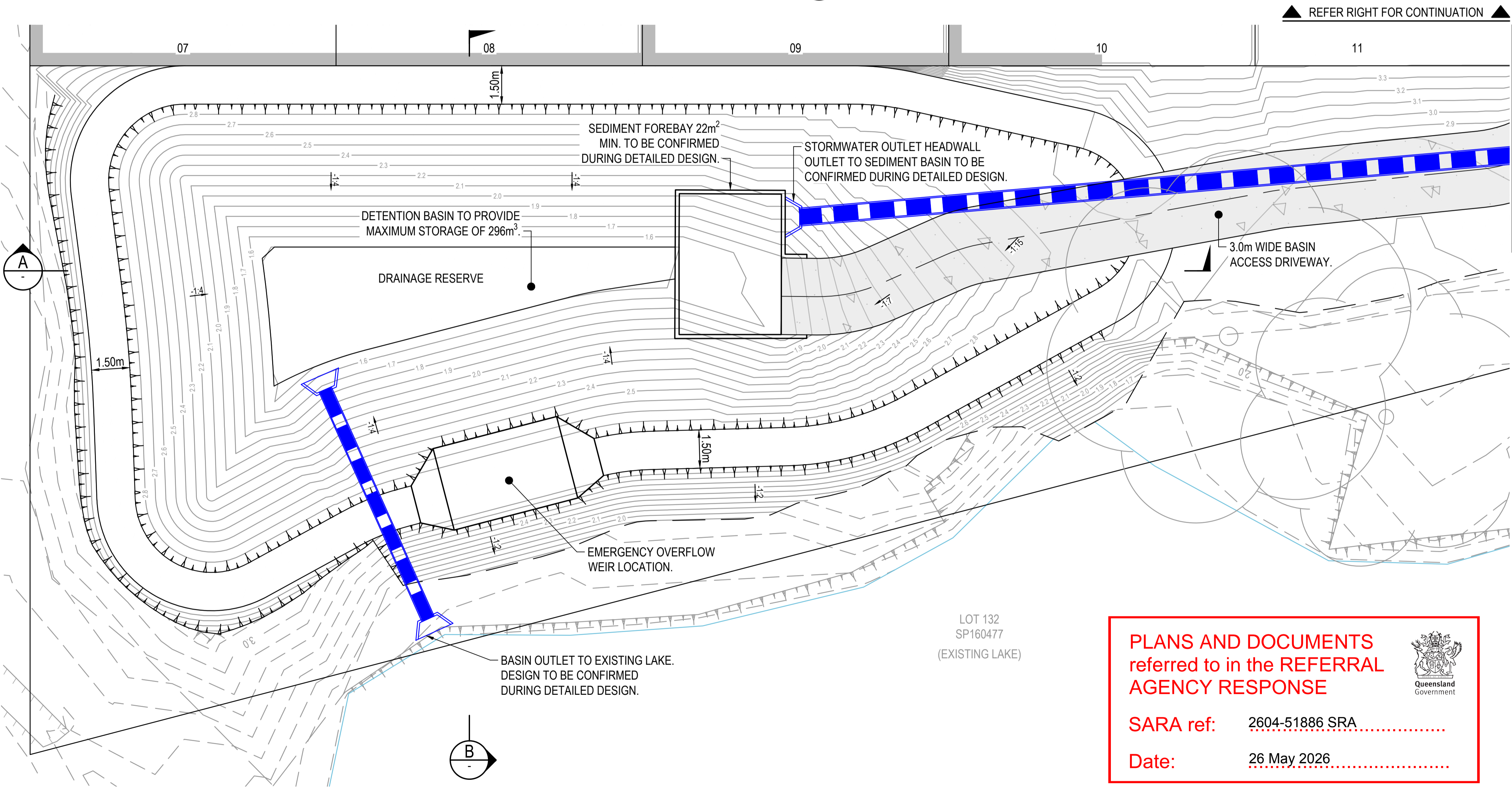
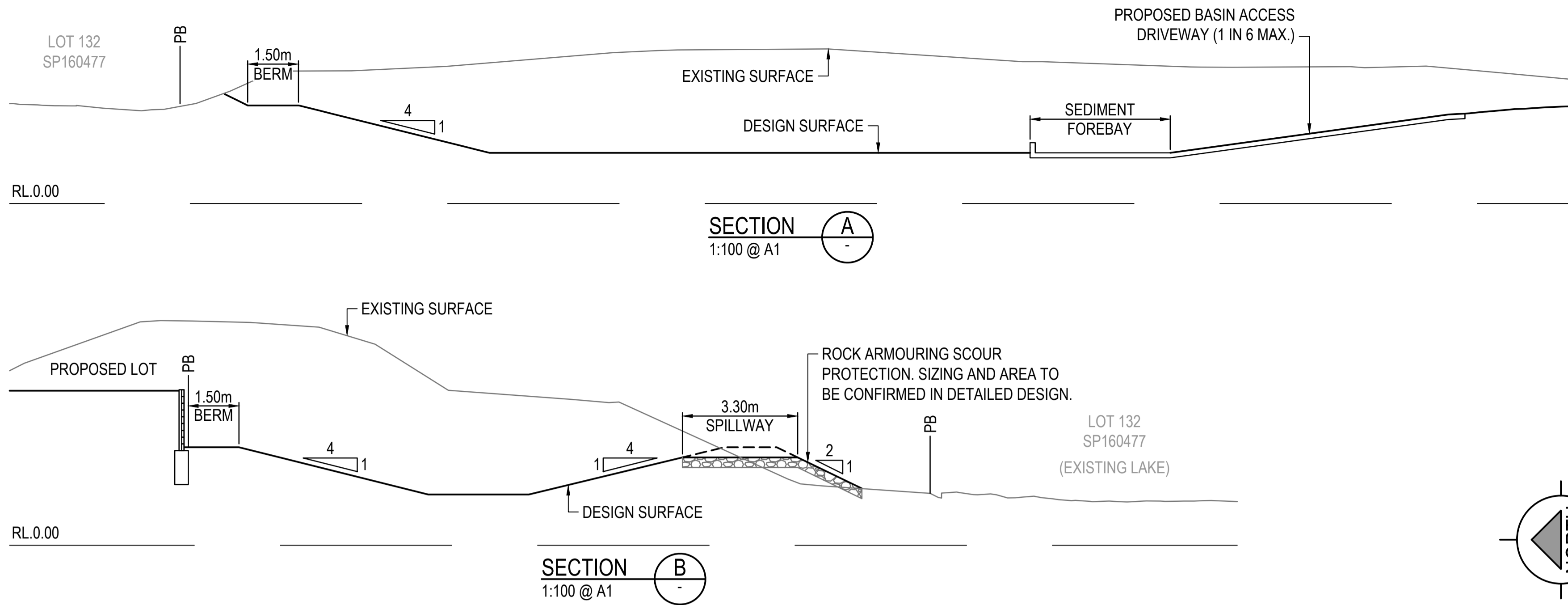
REV	DATE	DRW	DES	CHK	APP	REVISION DESCRIPTION
1	18.12.25	MG	BF	AG	AG	ISSUED FOR APPROVAL

STATUS	SCALE
FOR APPROVAL	
APPROVED	14278
A. GILBOY	18.12.25



DRAWING TITLE		
PRELIMINARY STORMWATER DRAINAGE CATCHMENT PLAN		
PROJECT NUMBER	DRAWING NUMBER	REVISION
BE25041-001	DA400	1

NOTE:
REFER DRAWING BE25041-001-DA200 FOR LEGEND.



PLANS AND DOCUMENTS referred to in the REFERRAL AGENCY RESPONSE

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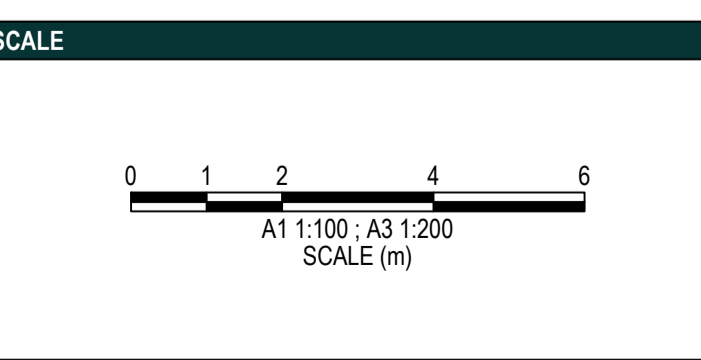
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REV	DATE	DRW	DES	CHK	APP	REVISION DESCRIPTION
1	18.12.25	MG	BF	AG	AG	ISSUED FOR APPROVAL

FOR APPROVAL

APPROVED

A. GILBOY



BOURN ENGINEERS

CLIENT

SEYMOUR

PROJECT DETAILS

Sanctuary Greens
PORT DOUGLAS

DRAWING TITLE

PRELIMINARY DETENTION BASIN LAYOUT PLAN

PROJECT NUMBER: **BE25041-001**

DRAWING NUMBER: **DA401**

REVISION: **1**

**PLANS AND DOCUMENTS
referred to in the REFERRAL
AGENCY RESPONSE**



SARA ref: 2604-51886 SRA

Date: 26 May 2026



Subdivision of Land Parcel SP150468 at 71-85 Port Douglas Road, Port Douglas

Road Traffic Noise Intrusion Assessment

Seymour Group Pty Ltd

PO Box 2454 Chermside Centre
QLD 4032

Prepared by:

SLR Consulting Australia Pty Ltd

SLR Project No.: 620.042901.00001

19 December 2025

Revision: 1.0

Revision Record


Revision	Date	Prepared By	Checked By	Authorised By
1.0	19 December 2025	T Anderson	Rodrigo Olavarria	

Basis of Report

This report has been prepared by SLR Consulting Australia Pty Ltd (SLR) with all reasonable skill, care and diligence, and taking account of the timescale and resources allocated to it by agreement with Seymour Group Pty Ltd (the Client). Information reported herein is based on the interpretation of data collected, which has been accepted in good faith as being accurate and valid.

This report is for the exclusive use of the Client. No warranties or guarantees are expressed or should be inferred by any third parties. This report may not be relied upon by other parties without written consent from SLR.

SLR disclaims any responsibility to the Client and others in respect of any matters outside the agreed scope of the work.

<p>PLANS AND DOCUMENTS referred to in the REFERRAL AGENCY RESPONSE</p> <p>SARA ref: 2604-51886 SRA.....</p> <p>Date: 26 May 2026.....</p>	
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7.0 Recommendations

7.1 SDAP State Code 1 Requirements

PLANS AND DOCUMENTS
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As indicated in **Section 4.1**, the proposed development will be located within 25m from a state-controlled road. Therefore, the applicable requirements from the SDAP State Code 1 are to be implemented. These are as follows:

- The development provides a noise barrier, earth mound or alternative noise attenuation measures where these strategies cannot be practically applied, to achieve a maximum free field noise level of ≤ 60 dBA $L_{A10,18hr}$ within the development.
- Building facade elements meet the minimum QDC MP4.4 Weighted Sound Reduction Index (R_w) (refer to **Section 7.2**).
- Balconies, podiums and roof decks, where above ground floor level, must include:
 - Continuous solid gap-free structure or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia).
 - Highly acoustically absorbent material treatment for the total area of the soffit above balconies, podiums and roof decks.

A Noise Reduction Coefficient (NRC) of 0.7 or greater is recommended for the balcony soffits. This is readily achievable with perforated fibre-cement systems with insulation inside the cavity. An acoustically tested system is to be selected to ensure the acoustic performance is achieved.

The SDAP State Code 1 specification for solid balustrade and sound absorptive soffit does not state a noise criterion for its implementation. Thus, to address the SDAP requirement, it is recommended that the treatments be implemented to all balconies of habitable spaces with a predicted QDC MP4.4 Noise Category 1 or greater, as minimum.

7.2 Noise Barriers


For the external noise levels to meet the SDAP State Code 1 PO38 and PO39 external noise criteria and the predicted QDC MP4.4 noise categories detailed in **Section 6.0**, noise barriers are recommended to be built as follows:

- The location of the modelled 1.8 m and 2.0 m high noise barriers is shown in **Figure 7-1**.

The noise barriers can be made of an earth mound, acoustic fence or a combination thereof. **Table 6-1** shows the approximate ground elevation at the base of the barrier at mid span of the adjacent allotment and the approximate elevation of the closest road lane.
- The noise barriers modelled elevation is detailed in **Table 6-1**, and final barrier heights as specified in **Figure 7-1** can incorporate retaining walls, and are relative to the adjacent allotment pad heights in **Appendix B**.
- The barriers must generally be installed without gaps between panels and posts. Small gaps between the bottom of the panels are permissible if required for drainage. However, these must be minimised.
- The noise barriers must have a minimum surface density of 12.5 kg/m² (excluding structural components):



- Appropriate materials can include overlapped timber, fibre cement, masonry, or a combination thereof.
- Other construction materials are also suitable where the panels (structural components excluded) meet the minimum surface density and the barrier is built following the above guidance.
- Noise barriers are required to be designed and constructed in accordance with Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019.

PLANS AND DOCUMENTS referred to in the REFERRAL AGENCY RESPONSE	
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Date: 26 May 2026



LOT 132
SP160477

LOT 132
SP160477



**Subdivision of Land Parcel SP150468 at 71-85
Port Douglas Road, Port Douglas**

Prepared for Seymour Group Pty Ltd
By SLR Consulting Australia Pty Ltd

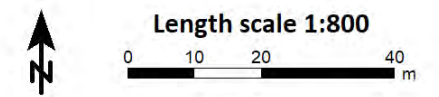
Map
7

1.8 m and 2.0 m Noise Barrier Locations

Project engineer: TA
Created: 19/12/2025
Processed with SoundPLAN 8.2
Project No.: 620.042901.00001

Signs and symbols
— 2.0 m Noise Barrier
— 1.8 m Noise Barrier

Figure 7-1: Recommended Noise Barriers



8.0 Conclusion

SLR Consulting Pty Ltd (SLR) have completed a road traffic noise assessment of the proposed subdivision of land parcel SP150468 at 71-85 Port Douglas Road, Port Douglas, known as 'Sanctuary Greens'. This report addresses the road traffic noise intrusion from the state-controlled Port Douglas Road transport noise corridor onto residential allotments within the Project.

The assessment was conducted following guidance from the Queensland Department of Transport and Main Roads (DTMR) – Transport Noise Management: Code of Practice Volume 1 - Road Traffic Noise, dated November 2013 (CoP Vol 1).

A computational noise model was used to predict future road traffic noise levels from Port Douglas Road, considering a 10-year horizon after Project completion (assumed 2038).

For the external noise levels to meet the SDAP State Code 1 PO38 and PO39 external noise criteria and the predicted noise categories detailed in this report, noise barriers are recommended to be built as follows:

- The location of the modelled 1.8 m and 2.0 m high noise barriers is shown in **Figure 7-1**.
The noise barriers can be made of an earth mound, acoustic fence or a combination thereof. **Table 6-1** shows the approximate ground elevation at the base of the barrier at mid span of the adjacent allotment and the approximate elevation of the closest road lane.
- The noise barriers modelled elevation is detailed in **Table 6-1**, and final barrier heights as specified in **Figure 7-1** can incorporate retaining walls, and are relative to the adjacent allotment pad heights in **Appendix B**.
- The barriers must generally be installed without gaps between panels and posts. Small gaps between the bottom of the panels are permissible if required for drainage. However, these must be minimised.
- The noise barriers must have a minimum surface density of 12.5 kg/m² (excluding structural components):
 - Appropriate materials can include overlapped timber, fibre cement, masonry, or a combination thereof.
 - Other construction materials are also suitable where the panels (structural components excluded) meet the minimum surface density and the barrier is built following the above guidance.
 - Noise barriers are required to be designed and constructed in accordance with Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019.

The residual noise levels after the implementation of 1.8 m and 2.0 m high noise barriers were assessed against noise criteria derived from the Queensland Development Code Mandatory Part 4.4 (QDC MP4.4).

QDC MP4.4 Categories applicable to all the Project allotments are presented in **Table 6-1**.

The R_w rating applicable to the dwelling facade elements with reference to the QDC MP4.4 Categories are presented in **Table 4-6**. Acceptable forms of construction are reproduced from Schedule 2 of QDC MP4.4 reproduced in **Table 4-7**, noting that other forms of construction are acceptable where they meet the required R_w rating.



The predicted QDC MP4.4 Noise Categories presented in this report represent the highest Noise Category for any part of the allotment (rather than on the building envelope). A lower Noise Category may be applicable depending on the position and layout of the building on the allotment. The constructed dwellings and other allotment fences will also screen road noise. For these reasons, the Noise Categories are considered a conservative assessment of transport noise.

A lower Noise Category should be acceptable at specific facades of the future dwellings depending on the layout of these within the allotment, pending demonstration of the road traffic noise levels onto specific habitable spaces within a dwelling via an allotment specific noise assessment based on architectural drawings, presented by the allotment owner.

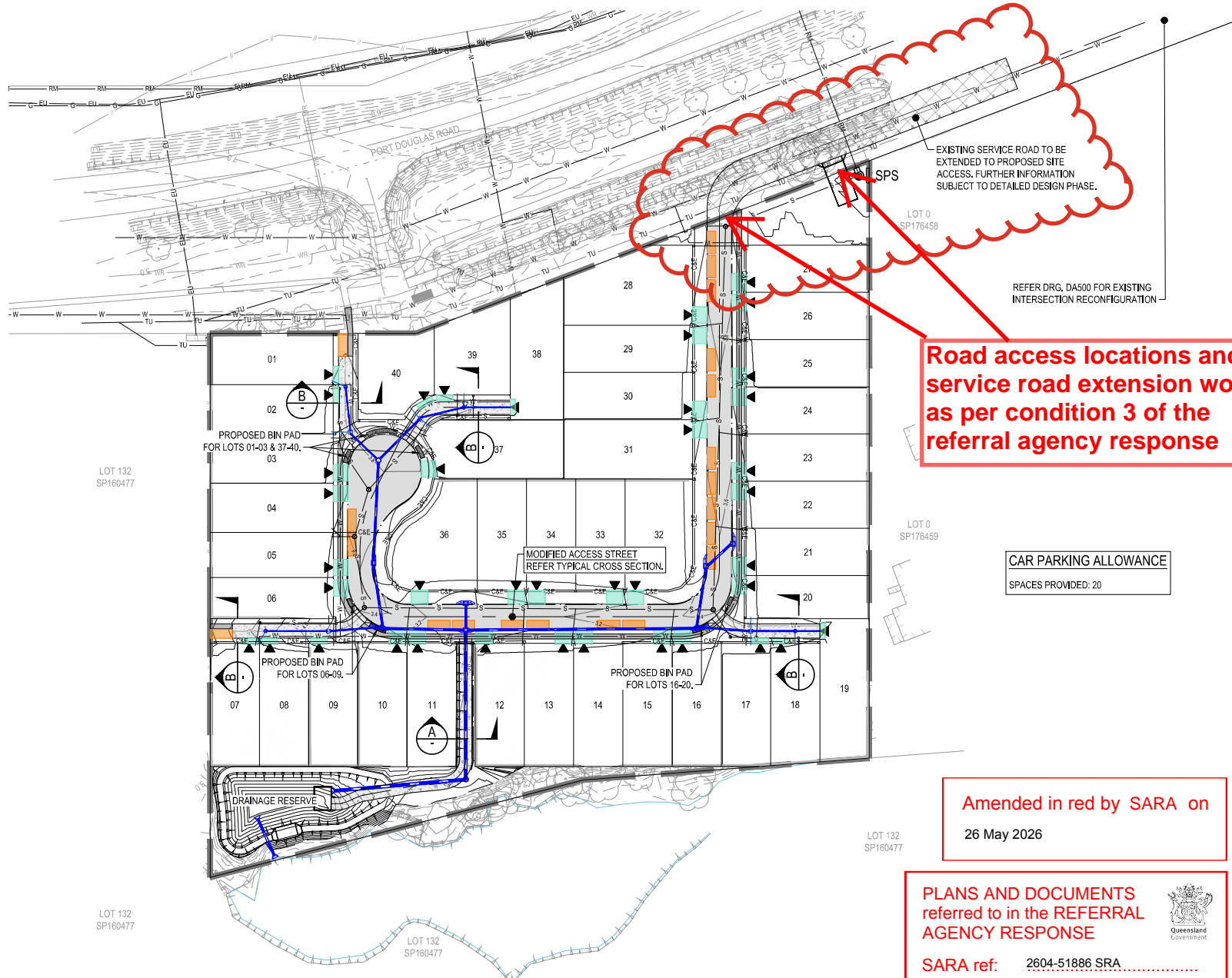
**PLANS AND DOCUMENTS
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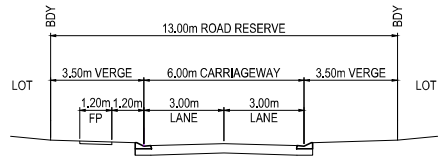




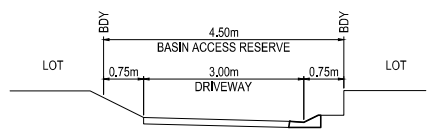
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---	PROPERTY BOUNDARY	---
---	CONTOUR	---
---	ROAD CONTROL	---
---	KERB TYPE 'LAYBACK K&C'	---
---	EDGE OF BITUMEN	---
---	INDICATIVE DRIVEWAY LOCATION	---
---	ON-STREET PARALLEL PARKING	---
---	STORMWATER DRAINAGE	---
---	STORMWATER GULLY	---
---	STORMWATER HEADWALL	---
---	RETAINING WALL	---
---	ROAD PAVEMENT	---
---	CONCRETE DRIVEWAY	---
---	CONCRETE FOOTPATH	---
---	SERVICE ROAD EXTENSION	---
---	LOCK RAIL GATE	---
---	TREE	---
S	SEWER MAIN	S
RM	SEWER RISING MAIN	RM
W	WATER MAIN	W
WR	RECYCLED WATER MAIN	WR
---	COMBINED COMMS. & ELEC.	C&E
---	ELECTRICAL UNDERGROUND	---
---	GAS PIPE	---
---	TELECOM UNDERGROUND	---
---	FENCE	---
---	BUILDING	---

Road access locations and service road extension works as per condition 3 of the referral agency response

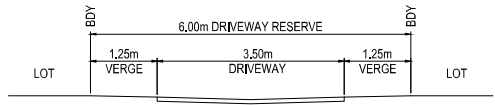
CAR PARKING ALLOWANCE
SPACES PROVIDED: 20



MODIFIED ACCESS STREET CROSS SECTION (13.00m ROAD RESERVE)
SCALE - 1:100 (A1)



SECTION A
1:50 (A1)



SECTION B
1:50 (A1)

Amended in red by SARA on
26 May 2026

PLANS AND DOCUMENTS referred to in the REFERRAL AGENCY RESPONSE

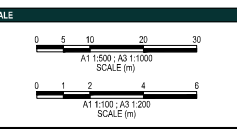
SARA ref: 2604-51886 SRA.....

Date: 26 May 2026.....



C:\land\Bentley\p04\Bourne Engineers\Drawings\DA\Project\BE2504_1\45_Plan_Douglas\001_Stage_1\DA\BE2504-01-DA-0201.dwg, 16.12.25, 16:22

REV	DATE	DRW	DES	CHK	APP	REVISION DESCRIPTION	STATUS	SCALE
1	16.12.25	MG	BF	AS	AG	ISSUED FOR APPROVAL	A. GILBOY	16.12.25



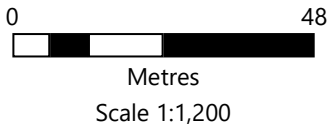
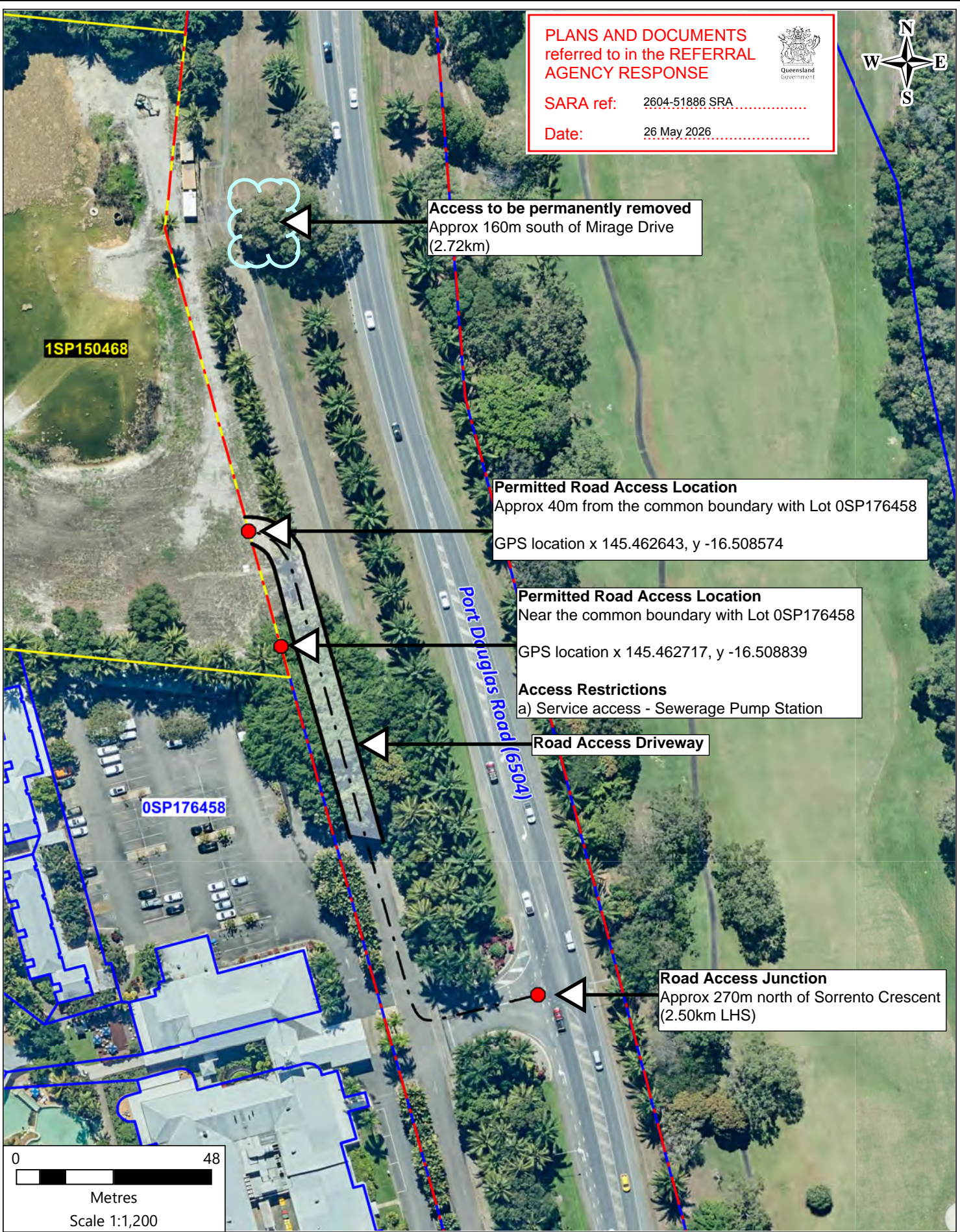
DRAWING TITLE		
PRELIMINARY ROADWORKS AND DRAINAGE LAYOUT PLAN		
PROJECT NUMBER	DRAWING NUMBER	REVISION
BE25041-001	DA200	1

PLANS AND DOCUMENTS referred to in the REFERRAL AGENCY RESPONSE



SARA ref: 2604-51886 SRA

Date: 26 May 2026



Branch/Unit : **Corridor Management / Far North District**

Projection/Datum : Geocentric Datum of Australia (GDA) 2020

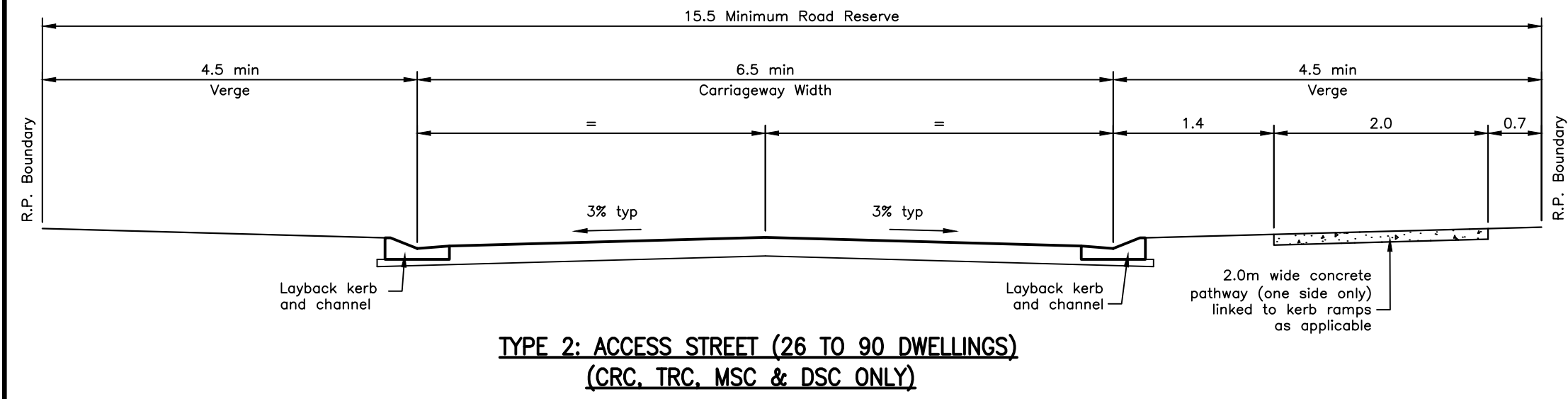
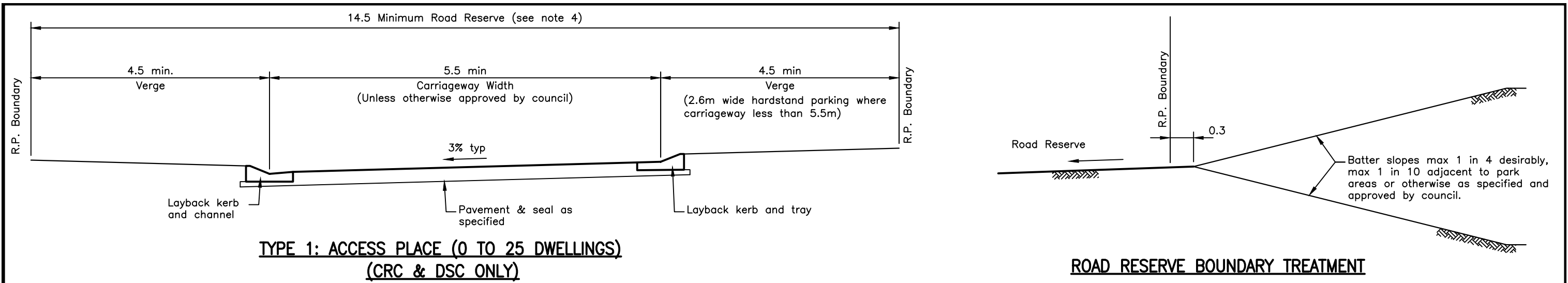
- Land parcel
- Subject land
- State-controlled road corridor

TMR Layout Plan (6504 - 2.50km)

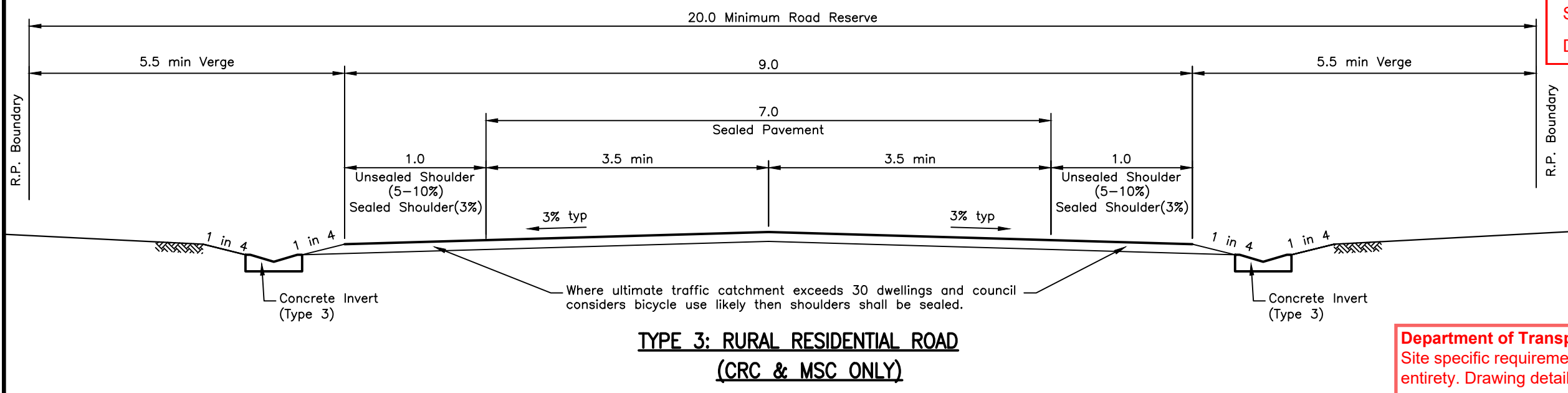


Queensland Government
Transport and Main Roads

Plan: 1 / 1	Issue: C	Date: 14/05/2026
Drawn by: RPK	File ref: TMR26-049812	



- NOTES**
1. This drawing is provided to assist in the interpretation of Table D1.1 of the FNQROC Regional Development Manual. This drawing should be read in conjunction with the above table and the remainder of the manual. Where conflicts occur Table D1.1 shall take precedence.
 2. All dimensions are in metres U.N.O.
 3. Refer to Project Drawings for pavement & seal details & subsoil drainage requirements.
 4. The minimum reserve width overrides the sum of other minimum dimensions.
 5. Driveway cuts or fills must not encroach on the road reserve.



PLANS AND DOCUMENTS referred to in the REFERRAL AGENCY RESPONSE

SARA ref: 2604-51886 SRA.....

Date: 26 May 2026.....

Department of Transport and Main Roads note:
Site specific requirements may not reflect this example in its entirety. Drawing details must reflect site specific conditions for Road Works / Road Access Works.

REVISIONS	DATE
E TYPE 3 RENAMED	27/08/20
D AMENDMENTS TO ROAD TYPE INFO	23/10/17
C 0.7m DISTANCE AMENDMENT	26/11/14
B PAVEMENT UNDER KERB ADDED	OCT 11
A ORIGINAL ISSUE	FEB 09

DISCLAIMER

The authors and sponsoring organisations shall have no liability or responsibility to the user or any other person or entity with respect to any liability, loss or damage caused or alleged to be caused, directly or indirectly, by the adoption and use of these Standard Drawings including, but not limited to, any interruption of service, loss of business or anticipatory profits, or consequential damages resulting from the use of these Standard Drawings. Persons must not rely on these Standard Drawings as the equivalent of, or a substitute for, project-specific design and assessment by an appropriately qualified professional.

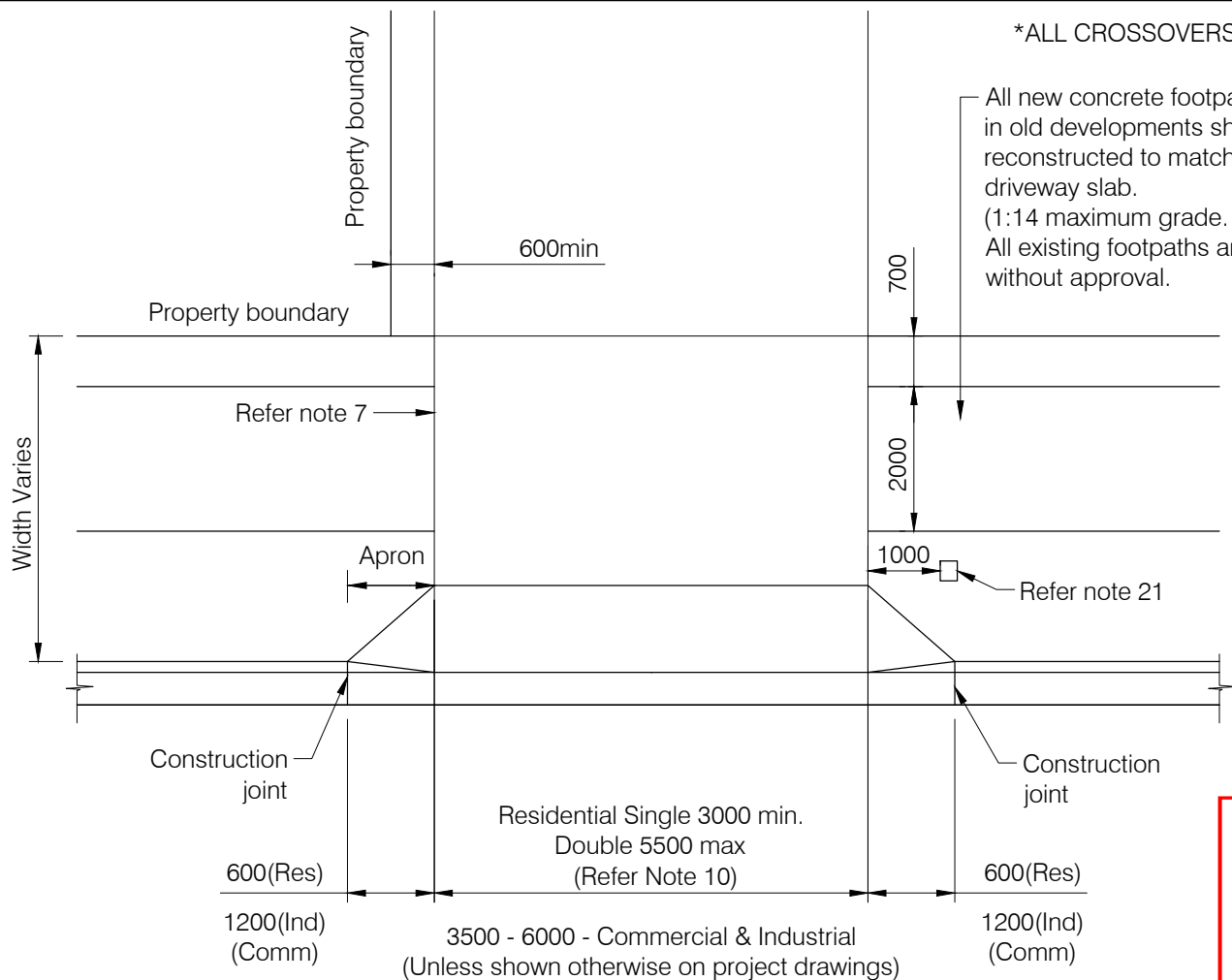


TYPICAL ROAD CROSS SECTIONS
TYPE 1, TYPE 2 & TYPE 3

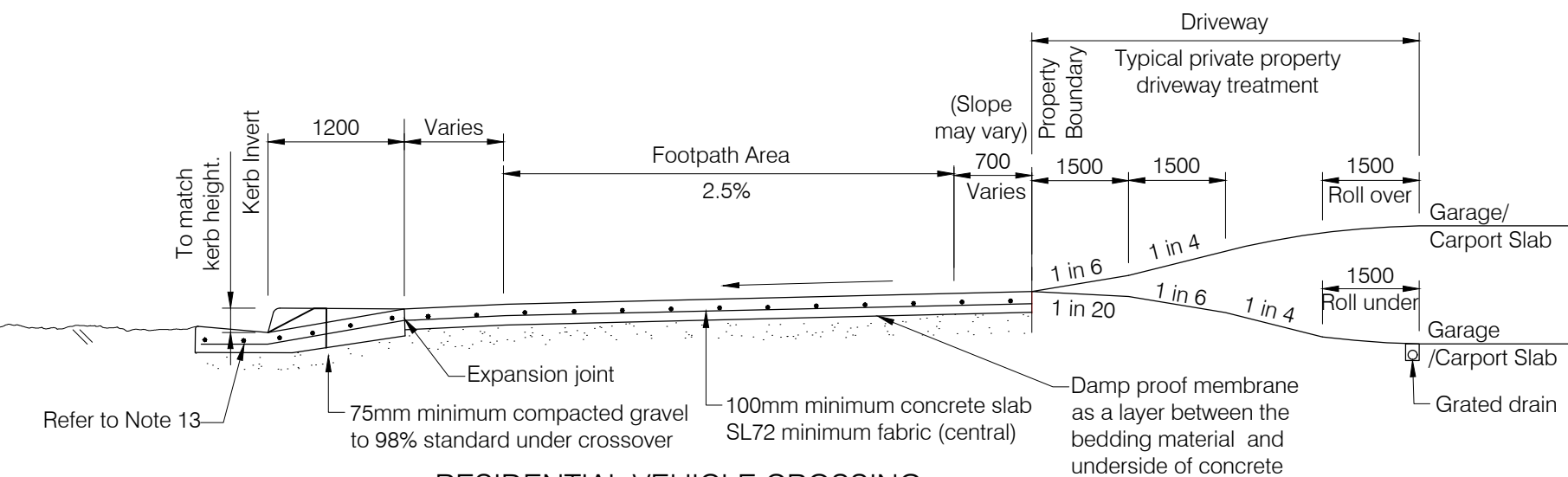
Standard Drawing
S1005

A	B	C	D	E
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*ALL CROSSOVERS NOT COMPLYING WITH THIS DRAWING REQUIRE APPROVAL



PLAN


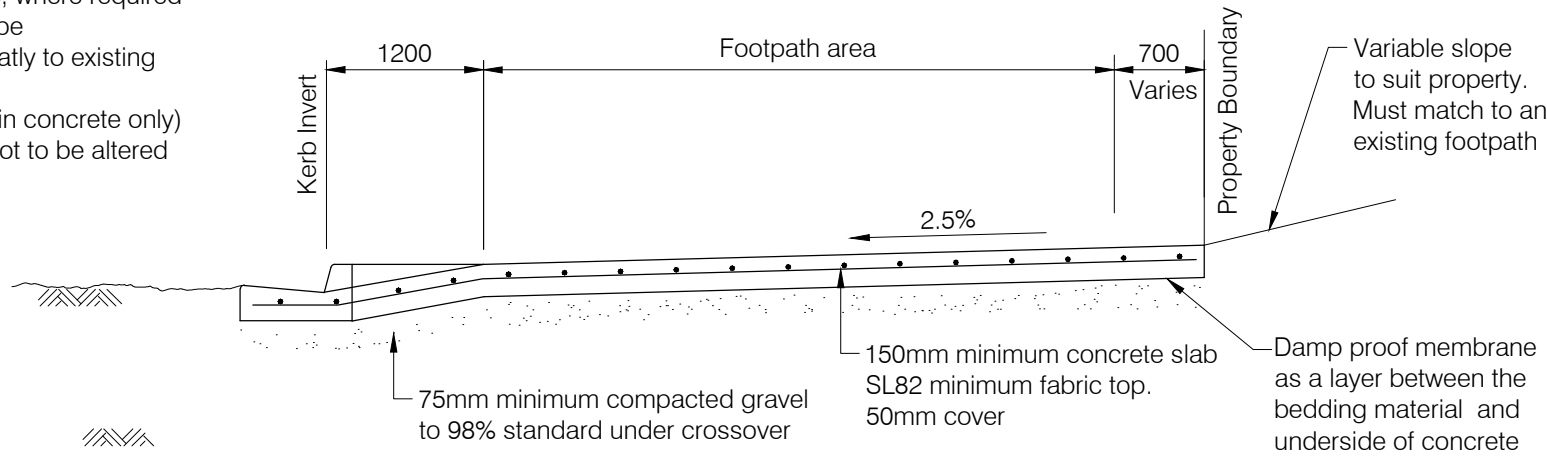


RESIDENTIAL VEHICLE CROSSING
(OPTIONAL KERB TYPE)

PLANS AND DOCUMENTS referred to in the REFERRAL AGENCY RESPONSE

SARA ref: 2604-51886 SRA

Date: 26 May 2026

COMMERCIAL & INDUSTRIAL VEHICLE CROSSING

NOTES

- All joints to existing kerbs shall be sawcut prior to breaking out concrete for removal. Entire section of kerb to be removed.
- Concrete is to be N32 min residential, N32 min. commercial/industrial in accordance with AS1379 and AS3600.
- All concrete to be broom finished.
- Where a concrete footpath abuts a crossing an expansion joint shall be installed and the footpath levels must not be changed.
- Expansion joints to be 10mm thick, closed cell cross linked polyethylene foam (85-150kg/m), 12mm round galvanised dowels @ 600 Ctrs
- Depths of concrete and reinforcing steel shown are minimum requirements for good foundations and average traffic loadings. Where this does not apply, depths of concrete and reinforcing steel shall be increased to suit specific conditions.
- Where an existing footpath is saw cut and a new footpath is installed abutting the existing concrete, an expansion joint shall be formed in accordance with note 5. Dowels may be fixed into existing concrete by drilling and fixing using a chemical anchoring solution.
- Subgrade to be compacted to 95% standard.
- All dimensions are in millimetres.
- 'Residential' refers to single dwelling or duplex. All other crossings as per commercial/industrial details.
- Where new sections of footpath are required, these shall be 2000mm wide and constructed in accordance with standard drawing s1035.
- For Cook Shire Council, fibre can be used in lieu of reinforcement fabric.
- For layback kerb residential crossing, the undamaged tray may be left in situ and 12mm galvanised dowels @ 600mm Ctrs installed.
- Relocating or removal of a street tree requires an approval.
- Stormwater downpipe outlets to be located clear of crossover and aprons.
- Refer to FNQROC Development Manual Section D9 for street tree clearances.
- Driveway to be 500mm clear of electrical pillars.
- Driveway edge to be 1m clear of light and power poles.
- All new downpipe connectors to kerb + channel are to use kerb adapter to match kerb profile.
- Refer to S2005 where hydrants are located in driveways.
- Minimum 1m clearance from edge of driveways to utilities, including adjacent power poles, light poles and junction boxes, etc.
- Driveway edge to be 600mm clear of any stormwater kerb inlet pits
- Additional options for modified treatments for constrained applications for access crossovers are given on S1015-Sheet 2. Design will require Council approval.

Department of Transport and Main Roads note:
Site specific requirements may not reflect this example in its entirety. Drawing details must reflect site specific conditions for Road Works / Road Access Works.

REVISIONS	DATE	
F	NOTES AND FOOTPATH AREA AMENDED	05/12/23
E	NEW DETAIL AND NOTES ADDED	27/08/20

DISCLAIMER

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ACCESS CROSSOVERS
Sheet 1 of 2

Standard Drawing
S1015

E	F		
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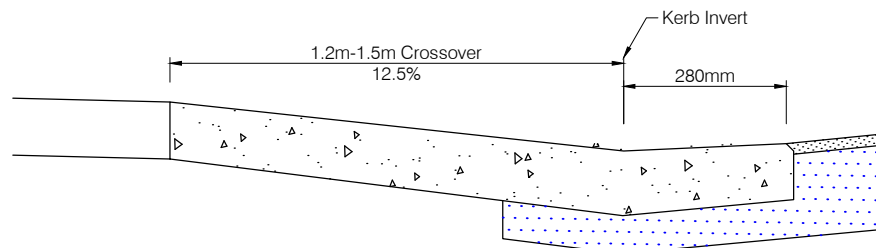


PLANS AND DOCUMENTS referred to in the REFERRAL AGENCY RESPONSE

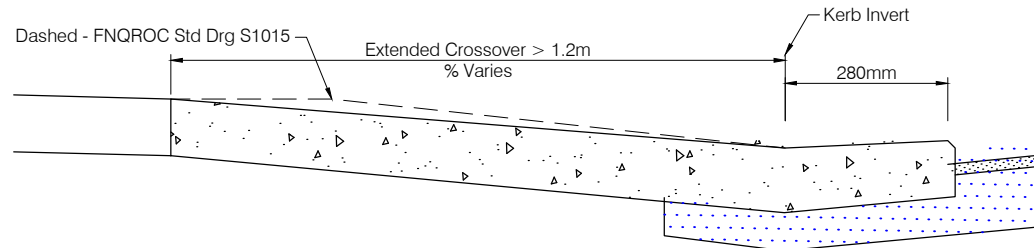
SARA ref: 2604-51886 SRA

Date: 26 May 2026

*OPTIONS 1 TO 6 REQUIRE SPECIFIC COUNCIL APPROVAL



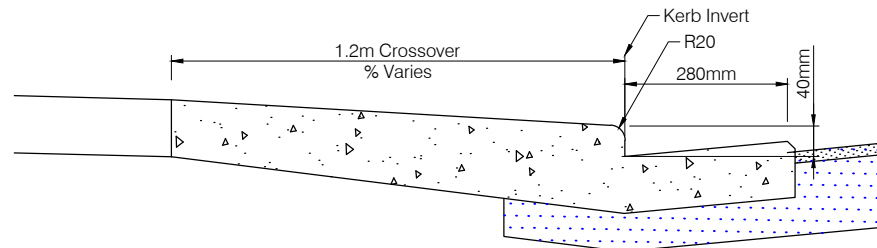
Option 1: 12.5% Ramp
NTS



Design Considerations

- Positives:**
- To improve vehicle clearances (helpful with steep shoulder crossfalls)
- Negatives:**
- Proximity of pedestrian footpaths may restrict crossover lengths.

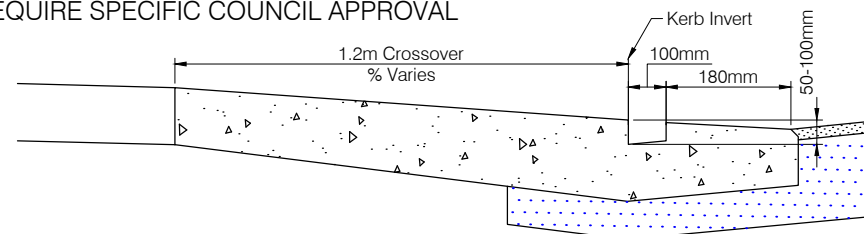
Option 2: Extended access crossover similar to FNQROC standard drawing S1015
NTS



Design Considerations

- Positives:**
- To improve vehicle clearances (helpful with steeper verges)
- Negatives:**
- Less smooth transition for vehicles.
 - Minor noise increase.

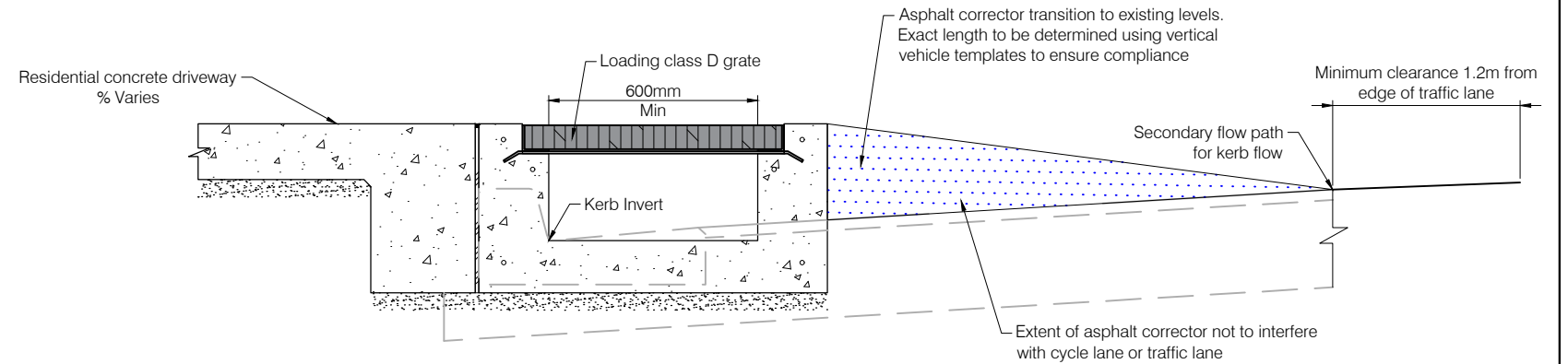
Option 3: 40mm lip at kerb invert
NTS



Design Considerations

- Positives:**
- To improve vehicle clearances (helpful with steeper verges)
- Negatives:**
- Less smooth transition for vehicles.
 - Minor noise increase
 - Minor blockage risk - not suitable where there is excessive vegetation matter present
 - Slight reduction in road shoulder, stormwater, flow capacity
 - Potential obstruction/hazard to non-vehicle users

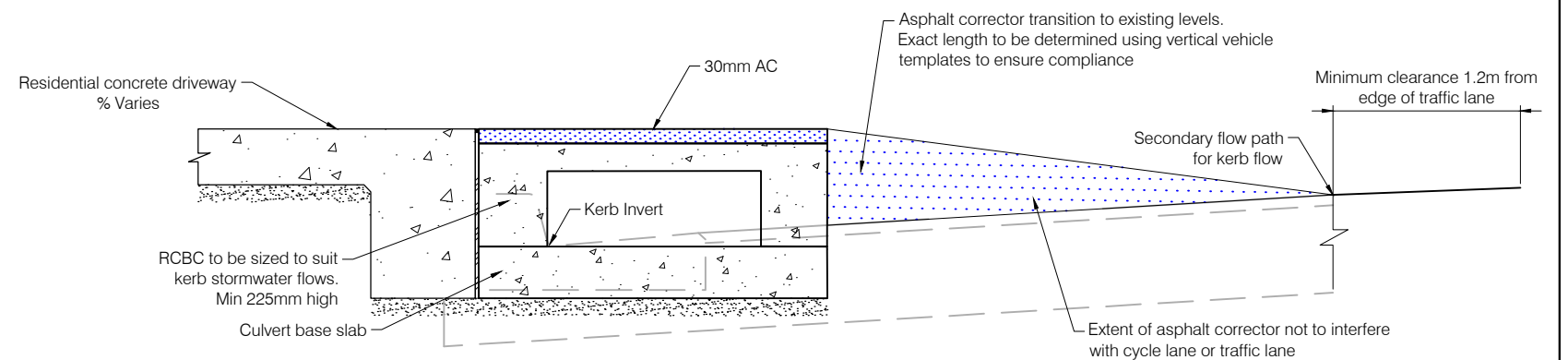
Option 4: 100mm channel at kerb invert
NTS



Design Considerations

- Positives:**
- Improve vertical vehicle clearance where there is a significant difference in height between road shoulder and property levels.
 - Grates make for easier clearance of debris
- Negatives:**
- Less smooth transition for vehicles.
 - Minor noise increase
 - Minor blockage risk
 - Significant reduction in road shoulder, stormwater, flow capacity (potentially only suitable for extremely wide shoulders)
 - Medium blockage risk

Option 5: Grated vehicle crossing
NTS



Design Considerations

- Positives:**
- Improve vertical vehicle clearance where there is a significant difference in height between road shoulder and property levels.
- Negatives:**
- Less smooth transition for vehicles.
 - Minor blockage risk
 - Significant reduction in road shoulder, stormwater, flow capacity (potentially only suitable for extreme)
 - Medium blockage risk

Option 6: RCBC with 30mm asphalt overlay over culvert
NTS

Department of Transport and Main Roads note:
Site specific requirements may not reflect this example in its entirety. Drawing details must reflect site specific conditions for Road Works / Road Access Works.

NOTES

- All reinforcing for options 1-4 to be as per sheet 1 for relevant crossing type.
- Option 5 & 6 - refer project specific drawings for for structural details.
- All modified treatment options to have vertical vehicle template checks to comply with AS2890.1 (Vehicle B85 or B99 as applicable)
- Consideration of impacts on stormwater flooded widths by selected option to be assessed by project engineer for compliance with minor & major stormwater events
- Modified treatments for constrained applications on this drawing require an application to council and a specific site approval by council.
- Options 1 to 6 generally provide increasing complexity and interference/loss of amenity to road users, therefore higher level options are considered less desirable by council and will not be accepted without reasonable consideration and assessment of the lower level options.
- The development application to council proposing the selected access crossover treatment design shall include:
 - Documented design drawings
 - Justification in writing for the selected higher level treatment options
 - Reasons why simpler lower level treatments and the standard treatment not be adopted
- Justification of the selected higher level treatment option shall include design considerations of the following where relevant:
 - Location, function, road geometry, road hierarchy and posted speed limit
 - Obstruction and hazard presented to all road users including vehicles, cyclists and pedestrians
 - Obstruction to stormwater drainage including risk of flooding of traffic lanes
 - Potential for debris to block stormwater flow
 - Roadway and stormwater channel maintenance and cleaning
 - Safety of roadway access and egress for crossover users
 - Adequate and safe vertical vehicle clearance

DISCLAIMER

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ACCESS CROSSOVERS
Sheet 2 of 2
Modified Treatments For Constrained Applications

Standard Drawing

S1015

A	ORIGINAL ISSUE	05/12/23
	REVISIONS	DATE

A

Our ref TMR26-049812
Your ref
Enquiries Ronald Kaden



Department of
Transport and Main Roads

25 May 2026

Decision Notice – Permitted Road Access Location (s62(1) *Transport Infrastructure Act 1994*)

This is not an authorisation to commence work on a state-controlled road¹

Development application reference number CA 2025_5880/1, lodged with Douglas Shire Council involves constructing or changing a vehicular access between Lot 1SP150468, the land the subject of the application, and Port Douglas Road (a state-controlled road).

In accordance with section 62A(2) of the *Transport Infrastructure Act 1994* (TIA), this development application is also taken to be an application for a decision under section 62(1) of TIA.

Applicant Details

Name and address Seymour Land Pty Ltd
C/- Aspire Town Planning and Project Services
PO Box 1040
Mossman QLD 4873

Application Details

Address of Property 71 Port Douglas Road, Port Douglas QLD 4877
Real Property Description 1SP150468
Aspect/s of Development Preliminary Approval for Material Change of Use for use rights associated with the Low-Medium Density Residential Zone
Development Permit for Reconfiguring a Lot (1 Lot into 40 Lots and Common Property)

Decision (given under section 67 of TIA)

It has been decided to approve the application, subject to the following conditions:

No.	Conditions of Approval	Condition Timing
Road Access Location		
A. General		
1	The Permitted Road Access Locations are: (a) Approximately 40 metres from the common boundary with Lot 0SP176458, (b) Near the common boundary with Lot 0SP176458,	At all times.

¹ Please refer to the further approvals required under the heading 'Further approvals'

No.	Conditions of Approval	Condition Timing
	in accordance with: (i) TMR Layout Plan (6504 - 2.50km) Issue C 14/05/2026 TMR26-049812 Attachment D	
2	Direct access is prohibited between Port Douglas Road and Lot 1SP150468 at any location other than the Permitted Road Access Location described in Condition 1 and shown on: (i) TMR Layout Plan (6504 - 2.50km) Issue C 14/05/2026	At all times.
3	The use of the permitted road access location described in Condition 1(b) is to be restricted to: a) Service access for Sewerage Pump Station	At all times.
4	Road works comprising: a) Lengthening, widening and other modifications to the existing one way access between the Road Access Junction and the subject land. b) Widening and sealing of the road access driveway, c) Line marking must be provided generally in accordance with: <ul style="list-style-type: none"> • Road Planning and Design Manual, 2nd edition, Department of Transport and Main Roads, September 2025 and • Far North Queensland Regional Organisation of Councils (FNQROC) Standard Drawings. 	Prior to submitting the Plan of Survey to the local government for approval, and prior to the commencement of the use of the Road Access Works, and to be maintained at all times.
5	The existing Road Access Works situated: (a) between Port Douglas Road and Lot 1SP150468, and (b) approximately 50 metres from the northern property boundary must be removed and the table drain and revegetation reinstated between the pavement edge and the property boundary in accordance with: <ol style="list-style-type: none"> i. TMR Road Planning and Design Manual 2nd Edition, ii. TMR Road Drainage Manual, iii. TMR Road Pavement Manual, iv. TMR's Technical Specifications & Standards, v. TMR Standard Drawings Roads, and vi. FNQROC drawings requirements 	Prior to completion of Road Access Works for new permitted road access location

Reasons for the decision

The reasons for this decision are as follows:

- a) The subject land (Lot 1 on SP150468) has road frontage and vehicle access via Port Douglas Road, a state-controlled road.
- b) The proposed development is intending to construct a new access via the existing access to Oaks Resort at 87 Port Douglas Road.
- c) Therefore, a decision under section 62 of the Transport Infrastructure Act is required as:

- (i) the application is deemed to be a changed access,
- (ii) the proposed development is increasing traffic generation,
- (iii) no decision for road access location has been given for this location,
- (iv) the previous decision for road access location given by the department on 31 March 2021 is not valid.

Please refer to **Attachment A** for the findings on material questions of fact and the evidence or other material on which those findings were based.

Information about the Decision required to be given under section 67(2) of TIA

1. There is no guarantee of the continuation of road access arrangements, as this depends on future traffic safety and efficiency circumstances.
2. This decision has been based on the current land use and the historic nature of the access subject to this decision. Be advised that if the land is further developed and/or intensified, the department will reassess the access requirements in accordance with the department's policies at that time to ensure that the road safety and transport efficiency outcomes for the state-controlled road network are maximised. This may or may not require all future access to be provided via the local road network.
3. In accordance with section 70 of the TIA, the applicant for the planning application is bound by this decision. A copy of section 70 is attached as **Attachment B**, as required, for information.

Further information about the decision

1. In accordance with section 67(7) of TIA, this decision notice:
 - a) starts to have effect when the development approval has effect; and
 - b) stops having effect if the development approval lapses or is cancelled; and
 - c) replaces any earlier decision made under section 62(1) in relation to the land.
2. In accordance with section 485 of the TIA and section 31 of the *Transport Planning and Coordination Act 1994* (TPCA), a person whose interests are affected by this decision may apply for a review of this decision only within 28 days after notice of the decision was given under the TIA. A copy of the review provisions under TIA and TPCA are attached in **Attachment C** for information.
3. In accordance with section 485B of the TIA and section 35 of TPCA a person may appeal against a reviewed decision. The person must have applied to have the decision reviewed before an appeal about the decision can be lodged in the Planning and Environment Court. A copy of the Appeal Provisions under TIA and TPCA is attached in **Attachment C** for information.

Further approvals

The Department of Transport and Main Roads also provides the following information in relation to this approval:

1. Road Access Works Approval Required – Written approval is required from the department to carry out road works that are road access works (including driveways) on a state-controlled road

in accordance with section 33 of the TIA. This approval must be obtained prior to commencing any works on the state-controlled road. The approval process may require the approval of engineering designs of the proposed works, certified by a Registered Professional Engineer of Queensland (RPEQ). Please contact the department to make an application.

If further information about this approval or any other related query is required, Ronald Kaden, Technical Officer (Development Control) should be contacted by email at cairns.office@tmr.qld.gov.au or on (07) 4045 7151.

Yours sincerely



Liliya Yates
Principal Engineer (Civil)

Attachments: Attachment A – Decision evidence and findings
Attachment B - Section 70 of TIA
Attachment C - Appeal Provisions
Attachment D - Permitted Road Access Location Plan

Attachment A

Decision Evidence and Findings

Findings on material questions of fact:

1. A development application (Reference MCUI2020_3711/1) over Lot 1SP150468 was refused by Douglas Shire Council on 05 October 2021.
2. The Planning and Environment Court (Reference 2827 of 2021) upheld the refusal on 14 November 2023.
3. Therefore the decision under section 62 of the *Transport Infrastructure Act 1997* made by the department (Reference TMR21-032012) dated 31 March 2021 is invalid.
4. Douglas Shire Council has planned cycle network upgrades along the frontage of Lot 3RP729991, 0SP176458 (Oaks Resort), and 1SP150468. These works will have an affect on the proposed developer works.

Evidence or other material on which findings were based:

Title of Evidence / Material	Prepared by	Date	Reference no.	Version / Issue
TMR Layout Plan (6504 - 2.50km)	Queensland Government Transport and Main Roads	14/05/2026	TMR26-049812	C
Decision Notice Refusal 71-85 Port Douglas Road Lot 1 SP150468	Douglas Shire Council	05 October 2021	MCUI 2020_3711/1 (1039662)	-
Planning Appeal Refusal	Planning and Environment Court of Queensland	14 November 2023	2827 of 2021	
Traffic Impact Assessment	SLR Consulting Australia Pty Ltd	19 December 2025	620.042901.0000 2	1.1
External Intersection Reconfiguration Layout Plan	Bourn Engineers	18.12.25	DA500	1
Engineering Services Report	Bourn Engineers	16 December 2025	BE25041	A
Proposed Landscape Plan Proposed Subdivision	LA3	December 2025	-	B
Geotechnical Assessment	GEO Design	18 November 2025	25050AA-D-L01	1
Vehicle Access to state-controlled roads policy	Queensland Government Transport and Main Roads	2023	-	-

Attachment B

Section 70 of TIA

Transport Infrastructure Act 1994

Chapter 6 Road transport infrastructure

Part 5 Management of State-controlled roads

70 Offences about road access locations and road access works, relating to decisions under s 62(1)

- (1) This section applies to a person who has been given notice under section 67 or 68 of a decision under section 62(1) about access between a State-controlled road and adjacent land.
- (2) A person to whom this section applies must not—
 - (a) obtain access between the land and the State-controlled road other than at a location at which access is permitted under the decision; or
 - (b) obtain access using road access works to which the decision applies, if the works do not comply with the decision and the noncompliance was within the person's control; or
 - (c) obtain any other access between the land and the road contrary to the decision; or
 - (d) use a road access location or road access works contrary to the decision; or
 - (e) contravene a condition stated in the decision; or
 - (f) permit another person to do a thing mentioned in paragraphs (a) to (e); or
 - (g) fail to remove road access works in accordance with the decision.

Maximum penalty—200 penalty units.

- (3) However, subsection (2)(g) does not apply to a person who is bound by the decision because of section 68.

Attachment C
Appeal Provisions

Transport Infrastructure Act 1994
Chapter 16 General provisions

485 Internal review of decisions

- (1) A person whose interests are affected by a decision described in schedule 3 (the **original decision**) may ask the chief executive to review the decision.
- (2) The person is entitled to receive a statement of reasons for the original decision whether or not the provision under which the decision is made requires that the person be given a statement of reasons for the decision.
- (3) The *Transport Planning and Coordination Act 1994*, part 5, division 2—
 - (a) applies to the review; and
 - (b) provides—
 - (i) for the procedure for applying for the review and the way it is to be carried out; and
 - (ii) that the person may apply to QCAT to have the original decision stayed.

485B Appeals against decisions

- (1) This section applies in relation to an original decision if a court (the appeal court) is stated in schedule 3 for the decision.
- (2) If the reviewed decision is not the decision sought by the applicant for the review, the applicant may appeal against the reviewed decision to the appeal court.
- (3) The *Transport Planning and Coordination Act 1994*, part 5, division 3—
 - (a) applies to the appeal; and
 - (b) provides—
 - (i) for the procedure for the appeal and the way it is to be disposed of; and
 - (ii) that the person may apply to the appeal court to have the original decision stayed.
- (4) Subsection (5) applies if—
 - (a) a person appeals to the Planning and Environment Court against a decision under section 62(1) on a planning application that is taken, under section 62A(2), to also be an application for a decision under section 62(1); and

(b) a person appeals to the Planning and Environment Court against a decision under the Planning Act on the planning application.

(5) The court may order—

(a) the appeals to be heard together or 1 immediately after the other; or

(b) 1 appeal to be stayed until the other is decided.

(6) Subsection (5) applies even if all or any of the parties to the appeals are not the same.

(7) In this section—

original decision means a decision described in schedule 3.

reviewed decision means the chief executive's decision on a review under section 485.

31 Applying for review

- (1) A person may apply for a review of an original decision only within 28 days after notice of the original decision was given to the person under the transport Act.
- (2) However, if—
 - (a) the notice did not state the reasons for the original decision; and
 - (b) the person asked for a statement of the reasons within the 28 days mentioned in subsection (1)the person may apply within 28 days after the person is given the statement of the reasons.
- (3) In addition, the chief executive may extend the period for applying.
- (4) An application must be written and state in detail the grounds on which the person wants the original decision to be reviewed.

32 Stay of operation of original decision

- (1) If a person applies for review of an original decision, the person may immediately apply for a stay of the decision to the relevant entity.
- (2) The relevant entity may stay the original decision to secure the effectiveness of the review and any later appeal to or review by the relevant entity.
- (3) In setting the time for hearing the application, the relevant entity must allow at least 3 business days between the day the application is filed with it and the hearing day.
- (4) The chief executive is a party to the application.
- (5) The person must serve a copy of the application showing the time and place of the hearing and any document filed in the relevant entity with it on the chief executive at least 2 business days before the hearing.
- (6) The stay—
 - (a) may be given on conditions the relevant entity considers appropriate; and
 - (b) operates for the period specified by the relevant entity; and
 - (c) may be revoked or amended by the relevant entity.
- (7) The period of a stay under this section must not extend past the time when the chief executive reviews the original decision and any later period the relevant entity allows the applicant to enable the applicant to appeal against the decision or apply for a review of the decision as provided under the QCAT Act.

(8) The making of an application does not affect the original decision, or the carrying out of the original decision, unless it is stayed.

(9) In this section—

relevant entity means—

(a) if the reviewed decision may be reviewed by QCAT—QCAT; or

(b) if the reviewed decision may be appealed to the appeal court—the appeal court.

35 Time for making appeals

(1) A person may appeal against a reviewed decision only within—

(a) if a decision notice is given to the person—28 days after the notice was given to the person; or

(b) if the chief executive is taken to have confirmed the decision under section 34(5)—56 days after the application was made.

(2) However, if—

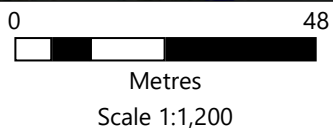
(a) the decision notice did not state the reasons for the decision; and

(b) the person asked for a statement of the reasons within the 28 days mentioned in subsection (1)(a);

the person may apply within 28 days after the person is given a statement of the reasons.

(3) Also, the appeal court may extend the period for appealing.

TMR26-049812 Attachment D



Branch/Unit : **Corridor Management / Far North District**

Projection/Datum : Geocentric Datum of Australia (GDA) 2020

- Land parcel
- Subject land
- State-controlled road corridor

**TMR Layout Plan
(6504 - 2.50km)**



Queensland Government
Transport and Main Roads

Plan: 1 / 1	Issue: C	Date: 14/05/2026
Drawn by: RPK	File ref: TMR26-049812	

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